



CITY OF KEENE LAND USE CODE AD HOC COMMITTEE

Friday, July 26, 2019
4:00 pm
2nd Floor Conference Room
Keene City Hall
3 Washington St, 03431

AGENDA

1. Call to Order
2. Minutes of July 12, 2019
3. Continued Discussion on Form Based Zoning Draft Subdistrict Dimensional/Form Standards
4. Review of Draft Site Plan Development Standards
5. Next Meeting – August 2, 2019
6. Adjourn

City of Keene
New Hampshire

AD HOC LAND USE CODE UPDATE STEERING COMMITTEE
MEETING MINUTES

Friday, July 12, 2019

4:00 PM

2nd Floor Conference Room,

City
Hall

Members Present:

Mayor Kendall Lane
Councilor George Hansel
Maggie Rice
Jim Phippard
Doug Barrett
Katie Sutherland

Staff Present:

Med Kopczynski, Economic Development Director
Tara Kessler, Senior Planner
Mari Brunner, Planner Technician

Members Not Present:

Councilor David Richards
Gary Spykman

1) Call to Order

Mayor Lane called the meeting to order at 4:03 PM.

2) Minutes of June 21, 2019

Councilor Hansel moved to approve the minutes of June 21, 2019, which Mr. Phippard seconded and the Ad Hoc Land Use Code Steering Committee carried unanimously.

3) Review of Form Based Zoning Draft Subdistrict Dimensional/Form Standards

Ms. Kessler led a discussion of the draft subdistrict dimensional and form standards presented by the consultants at Camiros and included in the meeting packet. She sought Committee feedback on moving forward with the Camiros products. She recalled the project goal to move toward form based zoning and ensure greater predictability with more objective standards; predictability will provide a more efficient review process. Clearer standards will enhance the ability for administrative review by providing a checklist of standards to work through with applicants. Mr. Kopczynski said this might be the most crucial discussion in the Land Use Code update process. The intent is to streamline a process for administrative permitting to a certain degree from the front desk of the Community Development Department. Administrative review will still be too complicated for the 5-10% of projects that will require Planning Board (PB) approval. The discussion that would begin at this meeting is on what projects will still rise to needing PB approval, recognizing that projects not reaching that level will be handled administratively using a checklist format. The goal is to develop predictability that is flexible enough to accommodate good architecture, which is challenging; and it must be done in a manner that Council and the PB have confidence in what happens administratively.

Ms. Kessler referred to the table beginning on page 16 of the meeting packet. The Committee discussed the draft building orientation and entrance standards, for which the goal is pedestrian access and safe circulation through sites. Camiros drew largely from the 19 PB standards and the HDC standards. Ms. Kessler recalled that through this process, review of new development in the Historic District is now the purview of the PB and not the HDC. The Committee provided the following feedback for the draft building orientation and entrance standards:

- More clarity is needed from Camiros about the purpose and importance of the first three design standards, and on what building orientation means more specifically from their perspective.
- If the goal of these standards is transparency, perhaps a simpler standard could require a percentage of open area to prevent long expanses of blank wall. Staff noted that is included within the dimensional standards proposed by Camiros.
- The Committee needs clarification on the location of primary entrances. What would happen if a business has its street address on a secondary street and the main entrance is located on the secondary street? Does it still need a primary entrance on the primary street?
 - Committee members agreed that currently many businesses in town have entrances on the Primary Street associated with their address, yet they direct patrons to secondary entrances off of parking lots. An example is the Vision Center on West Street. It is not ideal to require entrances that serve no purpose. Is the primary entrance requirement for access or aesthetics?
 - Requiring a primary entrance on the Primary Street might eliminate flexibility for corner lots with multistory office buildings.
- Where parking is required downtown, it must be at the side or rear. If the primary entrance must be at the front, pedestrians will not be required to walk around the building to the primary entrance; though as written, this last standard implies pedestrians must walk around. The intent, however, is to have a secondary side or rear entrance where there is parking and a walkway would also be required from the parking lot to the primary and secondary entrances.
 - A requirement for secondary entrances from parking lots felt too prescriptive for some Committee members. In situations like the new MOCO building, for example, a rear entrance at the parking lot would require additional staff for security, which is impractical. From another perspective, pedestrians/cyclists who experience downtown appreciate a requirement for entrances on primary streets. There are many things to balance.
- The fifth standard addresses setbacks and build-to percentages intended to provide flexibility for front elements like courtyards/gathering spaces off the sidewalk; examples today include the Colonial Theater and MoCo Arts. The Committee wants to provide flexibility for such elements but the draft standards do not address this possibility clearly.
- The Committee felt the sixth draft standard was too restrictive (e.g., mosaics and historic detailing) and would inhibit modern and contemporary architectural features. It was unclear how this standard would relate to provisions for building spacing. Staff clarified that this standard would only be applicable/required for one entrance on the building. Committee members said developers should not be required to add unnecessary and obscure architectural elements to fulfill a checklist and get approval; this is opposite of the project intent and will result in poor design.
- The need for/focus of primary entrances is unclear.
 - Are primary entrances just for architectural detail or to provide functional pedestrian access?

The goal of this project is a predictable, prescriptive code that does not limit creativity; when that is not possible the PB process will be triggered. Councilor Hansel said building form and façade composition are more important to him than orientation and entrances. Historically, the PB has provided developers valuable feedback about massing. Committee members agreed it is important for the draft standards to clearly articulate the importance of breaking up massing. Committee members thought this goal could be accomplished with a checklist in most instances. Materials used in development downtown are very important, which the PB does well determining. With a thoughtful checklist process first, however, the PB will only need to focus on three or four important standards instead of the 19 the must consider currently. Staff could also develop a list of materials exempt from PB review (i.e., high quality materials desired downtown) to expedite the process as well.

The Committee continued providing feedback about the draft standards in general:

- Ms. Sutherland said standards need to be more form-based; they are too detailed currently. The word *traditional* appears too frequently and could be eliminated entirely because the concept is already reinforced in many ways.
- Mr. Barrett asked, if the important point is really form and façade composition, if it really matters if a door or window breaks-up massing.
 - Perhaps aesthetics and functionality need to be addressed separately.
 - Perhaps *primary* does not need to be defined architecturally because it is already addressed under building form and composition. If the goal is pedestrian access, perhaps the standard can just require an entrance from a primary street. If security is a challenge for buildings like the Courthouse, the PB can grant a Variance.
 - Perhaps entrances can focus on pedestrian access from parking lots and Main Street; and form and composition can focus on aesthetics.
- Ms. Rice agreed with Mr. Barrett and said her vision for downtown in 30 years is more people walking and biking, with less car traffic. The Committee should keep this goal of a walkable downtown in mind, which will require appealing pedestrian entrances. Ms. Sutherland agreed. Mr. Kopczynski said the idea is to make the buildings address the street.
- Mr. Phippard noted the draft standards are fairly clear for new construction. However, he said these draft standards are more challenging for renovations and asked when these standards will be imposed on redevelopment. Ms. Kessler said Camiros drafted these standards primarily for new development. She said the Committee can set those parameters.
- Mayor Lane posed a question about the Colony Mill as it shifts from commercial to residential and when/if it will have to comply with these standards; there are no (or minimal) external changes, but the interior is entirely new. Staff suggested there could be a threshold for changes to the exterior or use. Councilor Hansel recalled the goal to promote renovating existing structures when possible, rather than always building new. Process changes could present an opportunity to incentivize reuse.
- The Committee discussed examples of buildings downtown today (e.g., the old Post Office building on West Street), which are excellent aesthetically but would not comply with these draft standards. In this regard, Committee members questioned if the consultants really looked at Keene's existing downtown form and pattern of development. Staff reported that the consultants closely studied the form of buildings in the downtown and that they walked the downtown multiple times with staff.

Ms. Kessler addressed authority the HDC and PB will have when this new land use code is implemented. The HDC will still review demolition of existing buildings downtown and modifications to existing buildings; all new development will go to the PB. The Committee has not yet talked about the thresholds for site plan review, and for what will require full PB review and approval.

Staff acknowledged Committee concern about massing and materials; Mr. Kopczynski noted some projects will go to the PB based on size alone. The Committee will also need to consider how such projects are considerate of abutters. Mr. Phippard agreed that abutters can be very concerned about a new use effecting them and often have complaints, but they might not have an opportunity to voice those concerns if developers just fulfill a checklist. He asked if abutters will be notified when permits are granted. Ms. Kessler said that staff have been discussing the question of abutter notification as part of discussions around review thresholds. There is an option that staff are considering to have a technical review committee review and make decisions on minor projects. This process could require abutter notification. Mr. Barrett expressed concern about unclear public notice for abutters, particularly for residential abutters; clarifying an approach in the administrative review process would ease Mr. Barrett's concerns. Councilor Hansel agreed there are times when abutters have substantive input that the PB should consider; he thinks it is logical to include abutters when a Variance is considered by staff as well.

Staff will continue considering the importance of abutter notice and opportunity for comment in the administrative review process. Mr. Phippard said he intentionally approaches abutters to try to mitigate any concerns, which is usually appreciated. He asked if there will be a letter to notify property owners who are rezoned. Ms. Kessler said that because this a significant change to downtown, there will be a certified mailing when an Ordinance is presented to Council. While developing an approach for abutters, Mr. Phippard suggested staff consider something efficient, knowing people will not realize or go out of their way to understand changes. Ms. Sutherland agreed and added that as a design professional representing clients, she would be uncomfortable with staff inviting abutters to meetings to discuss concerns that the developer is not invited to. She said sometimes abutters have serious concerns and other times they are unnecessary. Ms. Kessler said any such interaction will likely fall under the rules of procedure for a regular public meeting; these details are in progress to streamline the process and keep public transparency and decision making. This goes back to the question of thresholds for PB review. Ms. Sutherland asked if abutters are currently notified of minor projects and Ms. Kessler said no; however, every six months staff update the PB on minor project approvals.

Mr. Kopczynski cautioned getting to a point where no one can do anything, ever. He recalled that right now, the PB looks at 19 standards and a whole project subjectively. He and Councilor Hansel agreed the focus of most PB meeting are massing, materials, impact, and exemptions. Councilor Hansel suggested there could be checklist standards for massing and premium materials that do not need PB input; Mr. Phippard and Mr. Barrett cautioned it is not always that simple with new materials. Mr. Kopczynski and Mayor Lane agreed that massing is about how a building itself fits on a lot, how it contributes to a diverse roofline, and minimizing long blank walls.

Staff will continue working on the checklist of standards or another system the Committee identifies as best. Ms. Kessler summarized staff's next steps based on Committee feedback:

- Staff will continue working on an approach to build predictability and streamline where possible with massing, material type, and abutters in mind. Staff heard Committee concerns about a clear point at which PB review is triggered, with an ultimate goal to foster creativity.
- Staff will minimize standards that refer to architectural features and options for visual distinction; Camiros provided this draft knowing the Committee will help scale it back.
- Staff will ensure the draft includes a design builder's perspective because not all projects have an architect looking at the project holistically.

Mr. Phippard noted he receives questions about whether the form based zoning will be expanded into other zones. Ms. Kessler recalled that in phase one, this Committee and consultants tested different approaches with Council and landed on a hybrid approach to form based zoning only in the downtown, to see how it works there first before considering expansion. Mr. Kopczynski envisions challenges expanding form based zoning outside the downtown. Most of the City is not downtown and he is unsure these standards would have a role in those environments. Ms. Kessler agreed staff has hesitated to talk about form based in traditional residential zones.

4) Review of Draft Land Development Code Chapters
a. **Site Plan Development Standards**

This conversation will continue at the next meeting but Ms. Kessler noted how the draft site plan development standards align with the overall project goals:

- **Simple:** The text of each standard has been streamlined and modified to make the regulations easier to read and navigate. Language that was advisory or not specific to the Board's review authority has been removed.
- **Efficient:** The number of standards have been reduced from 19 to 11, while maintaining those standards that are most relevant to the Board's review of applications.

- Thoughtful: Staff have proposed changes to some of the standards in response to feedback received from the development community and in an effort to keep the standards up to date with current technology and development practices.

5) **Next Meeting July 26, 2019**

6) **Adjournment**

Hearing no further business, Mayor Lane adjourned the meeting at 5:31 PM.

Respectfully submitted by,
Katie Kibler, Minute Taker
July 19, 2019

Reviewed and edited by Tara Kessler, Senior Planner

DC Downtown Core Subdistrict

Purpose: The DC Subdistrict is the heart of downtown Keene, accommodating the highest intensity of development. The Subdistrict is intended to accommodate a rich mix of commercial, residential, civic, cultural and open space uses in a highly walkable, vertically and horizontally mixed-use environment.

Standard	Requirement
Lot and Building Dimensions	
Minimum Lot Area	None
Minimum Lot Width	None
Maximum Building Length	250'
Building Siting	
Primary Street Setback	0' BTL
Primary Street Build-to Percentage	80% Min.
Note: Primary streets will be named/mapped as necessary. Anticipated primary streets include: Main, Court, Washington, Roxbury, Emerald, Gilbo, and School. All others considered secondary.	
Secondary Street Setback	0' BTL when 150' or less from intersection with Primary. Otherwise 0-10' BTZ
Secondary Street Build-to Percentage	60% Min.
Interior Side Setback	0' unless abutting residential district or RT subdistrict, then 15' Min.
Rear Setback	0' unless abutting residential district or RT subdistrict, then 15' Min.
Building Height	
Minimum Building Height	18'
Maximum Building Height	85' and 7 stories
Required Building Height Stepback	15' minimum setback from building line for buildings taller than 65'. Stepback must occur above the ground story and no higher than the 5 th story.
Corner Tower Element	Limited in width and depth to a maximum of 25% of the building frontage. May exceed height at which stepback is required, but may not exceed the overall building height
Permitted Height Exception	Buildings may exceed maximum permitted height by up to 8' for a maximum of 25% of the square footage of the top floor. Such additional height may not be used to create an additional story.
Building Activation on Primary and Secondary Streets	
Minimum Ground Floor Height (FTF)	14'
Maximum Blank Wall Area	20'
Building Entry Spacing	30' max. between entries on Primary Streets, 50' max. between entries on Secondary Streets
Vertical Distance Between Bld. Entry and Sidewalk	3' max.
Ground Floor Transparency	50% min.
Upper Floor Transparency	15% min.
Parking Location & Access	
Permitted Parking Location	Parking Permitted to the rear of the building only
Minimum Primary Street Parking Setback	N/A (Not permitted along Primary Street)
Minimum Secondary Street Parking Setback	15' Min. May be reduced to 7' behind the front building line, with the construction of a solid masonry wall, 3' min. to 4' max. in height at the setback for the length of the parking lot, excluding access ways.
Minimum Interior Side Parking Setback	None, unless abutting residential district or RT subdistrict, then 5'. This will need to align with buffer standards.
Minimum Rear Parking Setback	None, unless abutting residential district or RT subdistrict, then 10'. This will need to align with buffer standards.
Minimum Parking Access Width	Primary Street: Not permitted Secondary Street: 12' for one-way, 24' for two-way

DG Downtown Growth Subdistrict

Purpose: The DG Subdistrict accommodates the reuse of existing structures within downtown Keene, as well as new construction of significant size. It is intended to provide the flexibility needed to create a mixed-use environment in areas of downtown where growth is desired, with standards for new construction and infill development that complement the walkable urban form of Keene’s downtown.

Standard	Requirement
Lot and Building Dimensions	
Minimum Lot Area	None
Minimum Lot Width	None
Maximum Building Length	350', unless a passage is provided per Section __, then 500' including width of passage. Note: Standards for building passage design will be included (width, height, transparency)
Building Siting	
Primary Street Setback	0-5' BTZ
Primary Street Build-to Percentage	80% Min.
Secondary Street Setback	5-15' BTZ
Secondary Street Build-to Percentage	60% Min.
Interior Side Setback	0' unless abutting residential district or RT subdistrict, then 15' Min.
Rear Setback	0' unless abutting residential district or RT subdistrict, then 25' Min.
Building Height	
Minimum Building Height	24'
Maximum Building Height	85' and 7 stories
Required Building Height Stepback	15' minimum setback from building line for buildings taller than 65'. Stepback must occur above the ground story and no higher than the 5th story.
Corner Tower Element	Limited in width and depth to a maximum of 25% of the building frontage. May exceed height at which stepback is required, but may not exceed the overall building height
Permitted Height Exception	Buildings may exceed maximum permitted height by up to 8' for a maximum of 25% of the square footage of the top floor. Such additional height may not be used to create an additional story.
Building Activation on Primary and Secondary Streets	
Minimum Ground Floor Height (FTF)	14'
Maximum Blank Wall Area	30'
Building Entry Spacing	50' max. between entries
Vertical Distance Between Bld. Entry and Sidewalk	3' max.
Ground Floor Transparency	50% min. on Primary Street, 40% min. on Secondary Street
Upper Floor Transparency	15% min.
Parking Location & Access	
Permitted Parking Location	Parking Permitted to the rear of the building only
Minimum Primary Street Parking Setback	N/A (Not permitted along Primary Street)
Minimum Secondary Street Parking Setback	15' Min. May be reduced to 7' behind the front building line, with the construction of a solid masonry wall, 3' min. to 4' max. in height at the setback for the length of the parking lot, excluding access ways.
Minimum Interior Side Parking Setback	None, unless abutting residential district or RT subdistrict, then 5'. This will need to align with buffer standards.
Minimum Rear Parking Setback	None, unless abutting residential district or RT subdistrict, then 10'. This will need to align with buffer standards.
Parking Access	12' for one-way, 24' for two-way

CE Commercial Edge Subdistrict

Purpose: The Commercial Edge Subdistrict accommodates a variety of moderately intense commercial development at the edges of downtown Keene, providing a transition into lower intensity commercial or residential development outside of the delineated downtown area. Such development may be a mixture of walkable development transitioning into a more auto-oriented development pattern.

Standard	Requirement
Lot and Building Dimensions	
Minimum Lot Area	10,000sf
Minimum Lot Width	50'
Maximum Building Length	None
Building Siting	
Front Setback	0-20' BTZ
Front Build-to Percentage	60% Min.
Corner Side Setback	0-20' BTZ
Corner Side Build-to Percentage	40% Min.
Interior Side Setback	0' unless abutting residential district or RT subdistrict, then 20' Min.
Rear Setback	0' unless abutting residential district or RT subdistrict, then 25' Min.
Building Height	
Minimum Building Height	None
Maximum Building Height	40' and 3 stories
Permitted Height Exception	May exceed maximum height by up to 5' for a total of 25% of the square footage of the top floor. Such additional height may not be used to create an additional story.
Building Activation	
Minimum Ground Floor Height (FTF)	12'
Maximum Blank Wall Area	30'
Vertical Distance Between Bld. Entry and Sidewalk	18" max.
Ground Floor Transparency	30% min.
Upper Floor Transparency	15% min.
Parking Location	
Permitted Parking Location	Parking permitted to the rear of the building only
Permitted parking location based upon feedback from Steering Committee during initial review of subdistricts and technical report. We recommend revising to allow parking to the rear and side of structures. Limiting parking to the rear makes more sense in denser areas such as the DC and DG, but may be too limiting for lots in the CE areas, which are intended to accommodate a mix of pedestrian and automobile traffic.	
Minimum Front Parking Setback	N/A (Not permitted)
We recommend allowing parking to the side of structures, through a provision that would require a front parking setback of 5' behind the front building line. This provides flexibility while ensuring no parking may be located in front of structures.	
Minimum Corner Side Parking Setback	15', or 5' behind the corner side building line, whichever is less
Minimum Interior Side Parking Setback	None, unless abutting residential district or RT subdistrict, then 10'. This will need to align with buffer standards.
Minimum Rear Parking Setback	None, unless abutting residential district or RT subdistrict, then 15'. This will need to align with buffer standards.
Parking Access	12' for one-way, 24' for two-way

RT Residential Transition Subdistrict

Purpose: The Residential Transition Subdistrict is intended to accommodate a variety of residential, open space, and low intensity commercial uses in a mixed-use environment of attached and detached structures. Development within the RT Subdistrict is intended to complement and transition into existing residential neighborhoods adjacent to downtown Keene.

Standard	Requirement
Lot and Building Dimensions	
Minimum Lot Area	8,000sf
Minimum Lot Width	60'
Maximum Building Coverage	40%
Maximum Impervious Surface Coverage	60%
Building Siting	
Front Setback	15' Min.
Corner Side Setback	10' Min.
Interior Side Setback	10' Min.
Rear Setback	15' Min.
Building Height	
Minimum Building Height	N/A
Maximum Building Height	40' and 3 stories
Building Activation	
Vertical Distance Between Bld. Entry and Sidewalk	5' max.
Ground Floor Transparency	Multi-family and Nonresidential uses: 25% Min.
Upper Floor Transparency	Multi-family and Nonresidential uses: 10% Min.
Parking Location	
Permitted Parking Location	Parking permitted to the rear and side of the building
See previous note on parking location. Note change in permitted parking location from Technical report. We think allowing parking to the rear and side is more appropriate for the RT area, considering current development patterns.	
Minimum Front Parking Setback	5' behind front building line
Minimum Corner Side Parking Setback	5' behind corner side building line
Minimum Interior Side Parking Setback	5'
Minimum Rear Parking Setback	5'
Parking Access	12' for one-way, 22' for two-way

DL Downtown Limited Subdistrict

Purpose: The DL Subdistrict is intended to accommodate the unique development pattern exhibited in downtown Keene to the north of Central Square. The DL Subdistrict accommodates a low-intensity, limited mixture of uses in structures of a lower height, located close to the street.

Standard	Requirement
Lot and Building Dimensions	
Minimum Lot Area	None
Minimum Lot Width	None
Maximum Building Coverage	65%
Maximum Impervious Surface Coverage	80%
Building Siting	
Front Setback	0-15' BTZ
Corner Side Setback	0-25' BTZ
Interior Side Setback	0' unless abutting residential district or RT subdistrict, then 10' Min.
Rear Setback	0' unless abutting residential district or RT subdistrict, then 15' Min.
Building Height	
Minimum Building Height	None
Maximum Building Height	35' and 2.5 stories
Building Activation	
Maximum Blank Wall Area	20'
Vertical Distance Between Bld. Entry and Sidewalk	4' max.
Ground Floor Transparency	30% min.
Upper Floor Transparency	15% min.
Parking Location	
Permitted Parking Location	Parking permitted to the rear and side of the building
Minimum Front Parking Setback	10', or equivalent to front building line, whichever is greater
Minimum Corner Side Parking Setback	10'
Minimum Interior Side Parking Setback	None, unless abutting residential district or RT subdistrict, then 5'. This will need to align with buffer standards.
Minimum Rear Parking Setback	None, unless abutting residential district or RT subdistrict, then 5'. This will need to align with buffer standards.
Parking Access	12' for one-way, 22' for two-way

NR Neighborhood Redevelopment Subdistrict

Purpose: The Neighborhood Redevelopment Subdistrict is intended to facilitate the development of a mixture of uses and building forms aligned with the City’s vision for neighborhoods that are largely bordered by or encompassed within more intensely developed areas within downtown. The subdistrict accommodates a variety of development forms and encourages the creation of higher-density residential development.

Standard	Requirement
Lot and Building Dimensions	
Minimum Lot Area	6,000sf
Minimum Lot Width	50'
Maximum Building Coverage	40%
Maximum Impervious Surface Coverage	50%
Building Siting	
Front Setback	15' Min.
Corner Side Setback	10' Min.
Interior Side Setback	10' Min.
Rear Setback	25' Min.
Building Height	
Minimum Building Height	None
Maximum Building Height	50' and 4 stories
Permitted Height Exception	May exceed maximum height by up to 5' for a total of 25% of the square footage of the top floor.. Such additional height may not be used to create an additional story.
Building Activation	
Maximum Blank Wall Area	30'
Prominent Entry Spacing	Multi-family and Nonresidential uses: 50' max. between entries
Vertical Distance Between Bld. Entry and Sidewalk	4' max.
Ground Floor Transparency	Multi-family and nonresidential: 35% min.
Upper Floor Transparency	Multi-family and nonresidential: 15% min.
Parking Location	
Permitted Parking Location	Parking permitted to the rear and side of the building
Minimum Front Parking Setback	5' behind front building line
Minimum Corner Side Parking Setback	15'
Minimum Interior Side Parking Setback	5'
Minimum Rear Parking Setback	5'
Parking Access	11' for one-way, 22' for two-way

IC Institutional Campus Subdistrict

Purpose: The Institutional Campus Subdistrict accommodates the unique form of Keene State College as it interfaces with the mixed-use, walkable fabric of downtown Keene.

Standard	Requirement
Lot and Building Dimensions	
Minimum Lot Area	None
Minimum Lot Width	None
Maximum Building Length	350', unless a passage is provided per Section __, then 500' including width of passage
Maximum Building Coverage	50%
Maximum Impervious Surface Coverage	75%
Building Siting	
Front Setback	30' Min.
Corner Side Setback	15' Min.
Interior Side Setback	0' unless abutting residential district or RT subdistrict, then 15'
Rear Setback	0' unless abutting residential district or RT subdistrict, then 25'
Building Height	
Minimum Building Height	None
Maximum Building Height	50' at required setbacks; structures may increase height over 50' by setting back an additional 2' for every 1' of additional height from all required setbacks
Parking Location	
Minimum Front Parking Setback	15'
Minimum Corner Side Parking Setback	15'
Minimum Interior Side Parking Setback	None, unless abutting residential district or RT subdistrict, then 15'. This will need to align with buffer standards.
Minimum Rear Parking Setback	None, unless abutting residential district or RT subdistrict, then 15'. This will need to align with buffer standards.
Parking Access	12' for one-way, 24' for two-way

Downtown Design Standards

The following table establishes the design standards for *multifamily and nonresidential* uses in the Downtown Subdistricts. An “X” in the cell indicates that such standard is applicable in the subdistrict. Where a cell is blank, such standard does not apply.

These standards have been created through an analysis of Downtown Keene, as well as best practices in design standards from similar communities. Select standards from the HDC regulations, as well as the Planning Board Development Standards have been incorporated as well.

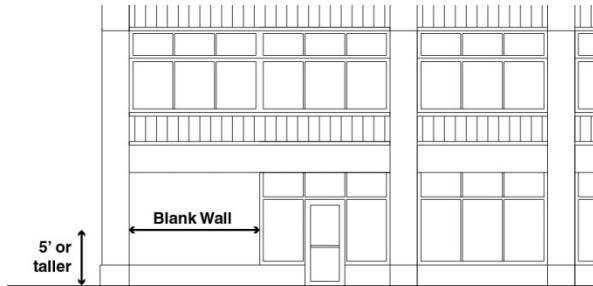
Standard	Subdistricts						
	DC	DG	DL	CE	RT	NR	IC
Building Orientation and Entrances							
All buildings must be oriented toward a public street. When located on lots that front on both a Primary and Secondary Street, buildings must be oriented toward the Primary Street.	X	X	X	X	X	X	
All buildings must contain at least one Primary entrance.	X	X	X	X	X	X	X
Primary entrances to buildings must be located on the street to which the building is oriented. Where a building has frontage along both a Primary Street and a Secondary Street, the primary entrance must be oriented toward the Primary Street, or at the corner.	X	X	X	X	X	X	X
Primary building entrances must be at sidewalk level or within the maximum vertical distance between building entry and sidewalk. “Garden level,” or below-grade primary entrances are prohibited.	X	X	X	X	X	X	X
All primary entrances must include direct pedestrian connections between street facing doors to adjacent sidewalks, and may not be set back more than ten feet from the building line.	X	X	X	X	X	X	X
Primary entrances must incorporate at least two of the following design elements to ensure clear visibility and architectural prominence: <ul style="list-style-type: none"> a. Details including but not limited to arches, friezes, columns, pilasters, transom or sidelight windows mosaic or tile work, murals, or other public art as an integral feature. b. Integral landscape or seating elements such as planters and benches or seat walls. c. Three-dimensional elements such as corner towers, porticos, arcades or colonnades. 	X	X	X	X		X	X
Secondary access points must be provided from rear parking lots. Facades that abut parking areas and contain a public entrance must include pedestrian walkways.	X	X	X	X		X	X
Building Form and Façade Composition							
For buildings of 150’ in length or more, facades must be divided into multiple “modules,” expressed through significant architectural changes such as a change in materials, a change in pattern elements such as fenestration, columns and pilasters, or a change in building setback through recesses or projections. Such modules may be no wider than 50 feet.	X	X	X	X		X	
Commercial storefronts must include traditional pedestrian-oriented elements such as display windows, bulkheads, transoms and cornices.	X	X	X	X		X	
Buildings must be designed with consistent building materials and treatments that wrap around all façades visible from a public street. Where material or color treatments change, there should be a significant change in surface plane of a minimum of six inches in difference. Differing materials are encouraged to terminate at inside corners.	X	X	X	X	X	X	
Mechanical equipment such as compressor units must be screened to minimize visibility. If located on a roof, such equipment must be set back to ensure minimal visibility from the sidewalk. If located at ground level, mechanical equipment must be located to the rear of the building and landscaped or screened to minimize visual impact.	X	X	X			X	X

Facades that abut rear parking areas and contain a public entrance must maintain a minimum transparency of 20%.	X	X	X			X	
Facades must express a traditional visual distinction between the ground floor and upper stories through architectural features or detailing, change in materials, or a change in pattern elements such as fenestration.	X	X	X			X	
Buildings must maintain a predominantly traditional residential appearance through the use of architectural elements including but not limited to pitched roof forms, porches, balconies, bay windows, and stoops. (RT only)					X		
Roof Design	DC	DG	DL	CE	RT	NR	IC
Rooflines of 150' in length or more must be varied through the use of architectural design elements such as dormers, gables, or projected wall features. Such elements of variation may be no wider than 75'.	X	X	X	X		X	
Reflective roof surfaces that produce glare are prohibited, except for solar panels or cool roofs intended to radiate absorbed or non-reflected solar energy and reduce heat transfer to the building.	X	X	X	X	X	X	X
Where parapet walls are used, they must feature three-dimensional cornice treatments or other shadow-creating details along their tops.	X	X	X	X	X	X	X
Site Design	DC	DG	DL	CE	RT	NR	IC
Sites must be designed to ensure safe pedestrian access from the public right-of-way, and safe pedestrian circulation within the development.	X	X	X	X	X	X	X
A cohesive visual character must be maintained through the use of coordinated hardscape (paving materials, lighting, street furniture, etc.) and landscape treatments within the development.	X	X	X			X	X
Off-street parking and traffic flow must not interfere with the flow of pedestrian travel or otherwise detract from the aesthetic character of a development or redevelopment. All off-street parking must be screened or aligned so that light from vehicle headlights do not adversely impact abutting properties.	X	X	X	X	X	X	X

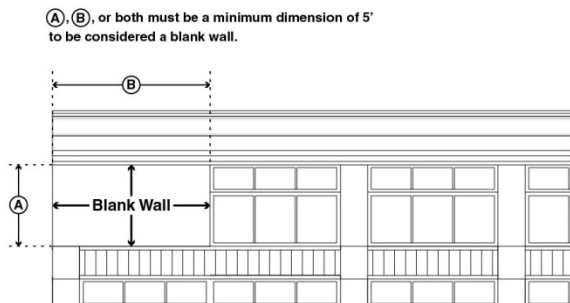
Definitions and Illustrations

A series of additional definitions and illustrations will be needed to explain some of the new terms used within the Form-Based Zoning. An initial set of these definitions is included below for reference.

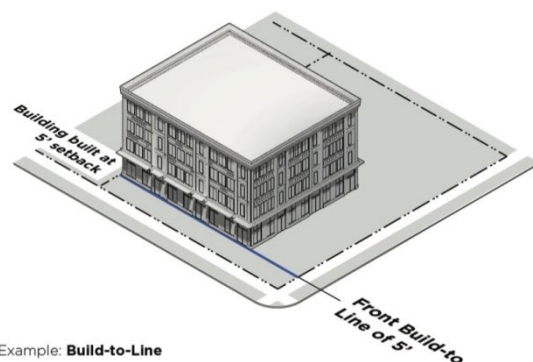
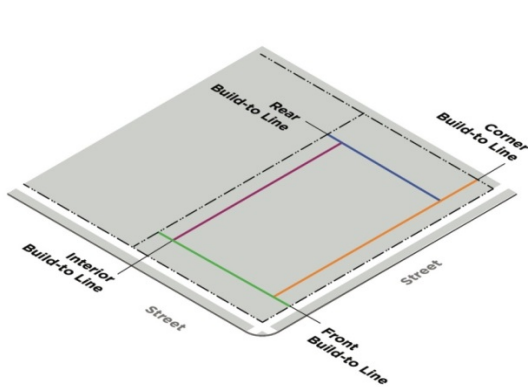
Blank Wall, Ground Floor. The horizontal linear dimension of contiguous building wall that does not contain fenestration, doors, or decorative elements such as banding, medallions, artwork such as murals and mosaics, change in wall plane of at least three inches, or other architectural or material embellishment. Any wall less than five feet in height is not considered to be a blank wall.



Blank Wall, Upper Floor. The horizontal or vertical linear dimension of contiguous building wall that does not contain fenestration, doors, or decorative elements such as banding, medallions, artwork such as murals and mosaics, change in wall plane of at least three inches, or other architectural or material embellishment. Any wall with a minimum dimension of less than five feet (height or width) is not considered to be a blank wall.

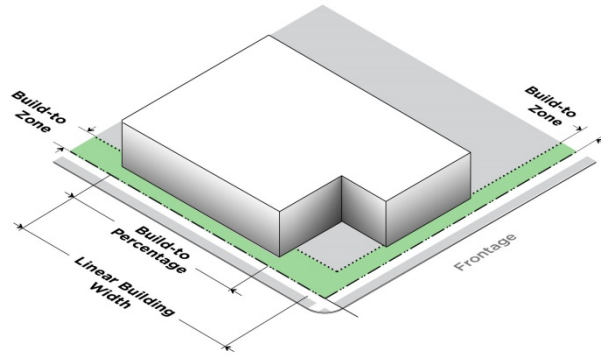


Build-To Line (BTL). A build-to line (BTL) is a set line on a lot, measured perpendicularly from the applicable lot line, where a structure must be located. The building line of a structure must be located on the build-to line. Facade articulation, such as window or wall recesses and projections are not counted as the building line, which begins at the applicable façade wall.

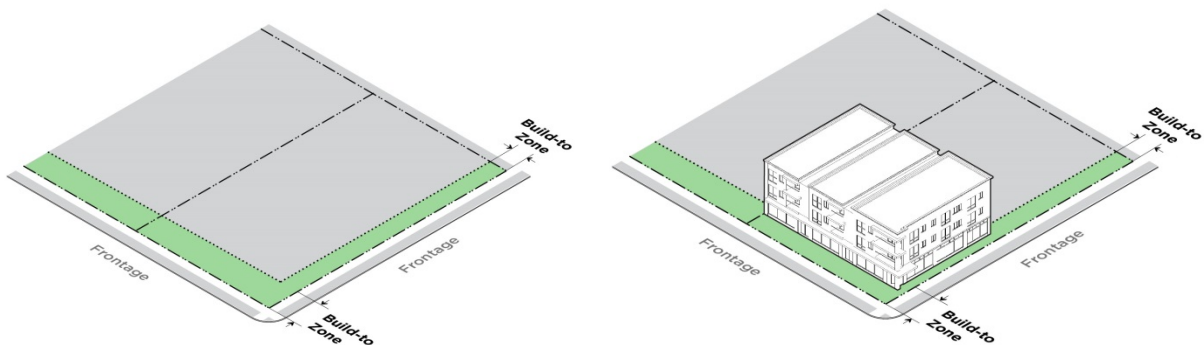


Example: Build-to-Line

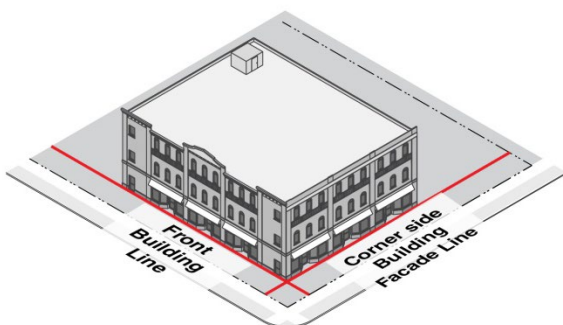
Build-To Percentage. A build-to percentage specifies the percentage of the building facade that must be located within the build-to zone or at the build-to line. Facade articulation, such as window or wall recesses and projections, do not count against the required build-to percentage. Plazas, outdoor dining, and other public open space features that are also bounded by a building facade parallel to the frontage are counted as meeting the build-to percentage. Build-to percentage is calculated by building facade, not lot width.



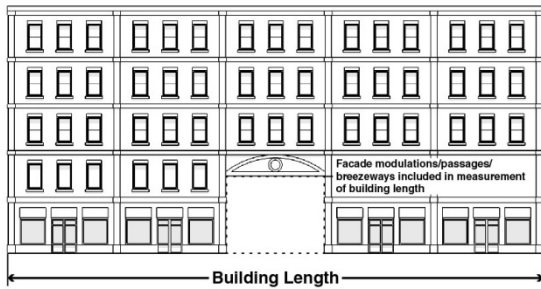
Build-To Zone (BTZ). A build-to zone (BTZ) is the area on a lot, measured perpendicularly from the lot line, within which a structure must locate. A build-to zone sets a minimum and maximum dimension within which the building facade line must be located (e.g. 0-5'). Facade articulation, such as window or wall recesses and projections are not counted as the building facade line, which begins at the applicable facade wall.



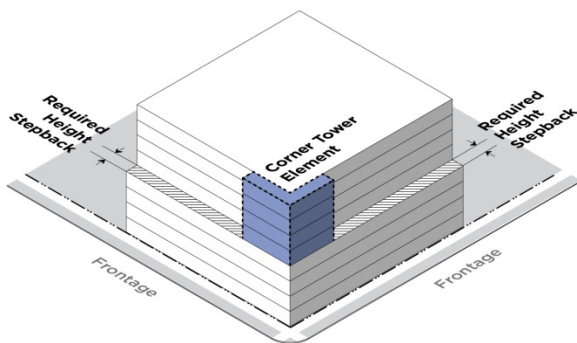
Building Line. The vertical plane along a lot where the building's facade is located. Upper story building lines relate to that part of the facade that requires a setback.



Building Length. Building length is measured as the length of the facade abutting a frontage type. Passageways, breezeways, ground floor passages, and similar building connections are included in the calculation of total building length.



Corner Tower Element. A corner tower element is an accentuated vertical element located on a building corner at a street intersection that can be located within a required stepback. In no case may the corner tower element exceed the building's overall height.







Stepback. A stepback is the required additional distance that upper stories of a structure must be recessed from the facade of the stories below. A required stepback must continue through all upper stories once established, though it may be increased from any required minimums on any stories.

Transparency. Transparency is the required amount of window area as a percentage of the specified facade area. Doors are included in ground floor transparency when such doors are designed with glass or other transparent materials. To qualify as transparent for the calculation, the glazing must meet the following standards:

- 1) Ground floor transparency must have a minimum 60% Visible Light Transmittance (VLT) and no more than 15% Visible Light Reflectance (VLR).
- 2) For upper floor glazing, the glazing must have a minimum of 40% VLT and no more than 15% VLR.
- 3) Colored tinting is prohibited.
- 4) The following do not meet the ground floor or upper floor transparency requirements and do not count in meeting the standard:
 - a) Windows with interior shadowboxes
 - b) Glass block
 - c) Printed window film, regardless of whether it allows views into or out of the building.

2020 Building Better Together (UDO)

 <p>BUILDING BETTER TOGETHER KEENE NEW HAMPSHIRE</p>	 <p>SIMPLE New regulations will be easy to navigate & will include graphics to outline a clear process, from start to finish.</p>	 <p>EFFICIENT The updated structure will provide a set of clear procedures for development queries, & will create a more streamlined application process – eliminating the need to navigate multiple points of contact.</p>	 <p>THOUGHTFUL Although our regulations have worked in the past, they can be outdated & confusing. This update will help guide us into the future, while protecting the crucial elements that make this a great place to live, work, & play.</p>	<p>Site Plan Development Standards</p> <p>Draft as of July 8, 2019</p>
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BACKGROUND:

The City's Development Standards are used by the Planning Boards as the foundation for their review of Site Plan applications for commercial and multifamily development/redevelopment proposals. Currently, there are 19 Development Standards that include: Drainage; Sedimentation and Erosion Control; Hillsides; Flooding; Snow Storage; Screening; Landscaping; Lighting; Air Quality; Stump Dumps; Sewer and Water; Noise; Hazardous Materials; Surface Waters; Wetlands; Comprehensive Access Management; Traffic; Excavation and Filling; and, Architecture and Visual Appearance. These standards were first adopted in 1994 and have since been amended multiple times.

SUMMARY OF MAJOR CHANGES & REASONS:

- Staff removed language from each of the standards that was not directly related to the Planning Board's review of applications or that was advisory in nature.
- Staff removed standards that were solely references to other local ordinances such as Noise, Hillsides, and Flooding. Staff will incorporate reference to these ordinances as part of an application form to ensure that applicants are aware of/adhere to these regulations when developing a site plan application. Staff also removed the standard for Stump Dump, as this could also be an application form item.
- Staff removed standards that solely reference state or federal regulations such as Air Quality and Hazardous Materials.
- Staff consolidated the standards for Surface Water and Wetlands into one standard. Staff also consolidated Comprehensive Access Management and Traffic into one standard.
- Staff have relocated landscaping standards related to parking lots to the parking chapter of the LDC, which will be part of the Zoning Ordinance. Staff incorporated new standards related to landscaping plant selection and added a section called "Modifications."
- Staff modified the lighting levels and thresholds included in the lighting section. Of note, would be the increase in the uniformity ratio for commercial parking areas from 4:1 to 5:1, and the change in maximum illuminance for canopies/gas station islands from 5.5 fc to 12.5 fc, except for those that are adjacent or within residential zoning districts or associated with a preexisting nonconforming use.
- Staff modified the screening standard to include general standards related to screening and to address screening of mechanical equipment.

REMAINING ISSUES/QUESTIONS

- The standard for Architecture and Visual Appearance has not yet been updated. Staff are waiting to identify how design standards will be incorporated into the Form Based Zoning District before finalizing edits to this standard.
- Staff are currently evaluating the thresholds for major and minor project review, with the intent of increasing the opportunity for projects to be reviewed administratively.

ALIGNMENT WITH PROJECT GOALS:

Simple: The text of each standard has been streamlined and modified to make the regulations easier to read and navigate. Language that was advisory or not specific to the Board's review authority has been removed.

Efficient: The number of standards has been reduced from 19 to 11, while maintaining those standards that are most relevant to the Board's review of applications.

Thoughtful: Staff have proposed changes to some of the standards in response to feedback received from the development community and in an effort to keep the standards up to date with current technology and development practices.

ATTACHMENTS:

- A. Draft Development Standards Dated July 8, 2019.

DRAFT SITE PLAN DEVELOPMENT STANDARDS

1. DRAINAGE AND STORMWATER MANAGEMENT

Runoff:

- a) The development of a site shall not result in increased volume or velocity of runoff onto adjacent properties or surface water bodies unless specifically approved by the City and agreed to in a formal, legally binding manner by the affected property owner(s).
- b) The applicant must provide sufficient data to demonstrate compliance with this requirement.

Low Impact Design:

- a) Site design and drainage systems constructed to comply with this standard shall incorporate Low Impact Development (LID). Examples include but are not limited to bio-retention, porous pavement, vegetated swales, infiltration trenches, rooftop disconnection, cisterns, green roofs, vegetated filter strips, reduced impervious surfaces, stormwater disconnection, preservation and enhancement of native vegetation.
 - i. Where site conditions exist that make on-site infiltration impractical, the Applicant shall demonstrate that the use of LID measures is not possible before proposing to use traditional, structural stormwater management measures including but not limited to stormwater retention and detention ponds and underground storage systems.

Quality of Stormwater Discharge:

- a) Treatment systems (e.g. wet detention basins, constructed wetlands, and/or LID measures) combined with contamination prevention practices (e.g. frequent sweeping of parking lots, the use of oil/gas traps in catch basins) shall be utilized to assure that upon discharge to surface waters, wetlands, floodplain compensatory storage basins or wellfield protection areas, drainage water meets Federal Clean Water Act standards.
- b) Drainage systems which divert runoff into surface waters, wetlands, wellfield protection areas, or floodplain compensatory storage areas shall utilize LID measures or wet or dry sediment basins or similar devices in combination with erosion prevention and sediment control practices to avoid sediment loading of the receiving area(s).

Bike Friendly:

New street or trail drainage grates shall be of a design that allows a bicyclist to ride over it safely.

2. SEDIMENTATION AND EROSION CONTROL

Pre-Construction:

- a) Each project shall be designed to prevent sedimentation and erosion during and subsequent to construction in the following ways:
 - i. Minimize disturbance of natural soil cover and vegetation,
 - ii. Minimize, in area and duration, exposed soil and unstable soil conditions,
 - iii. Protect receiving water bodies, wetlands and storm sewer inlets,
 - iv. Minimize off-site sediment transport on vehicles and equipment,
 - v. Minimize work in and adjacent to water bodies and wetlands,
 - vi. Maintain stable slopes,
 - vii. Minimize disturbance to and protect surrounding soils, root systems and trunks of trees adjacent to site activity that are intended to be left standing,
 - viii. Minimize the compaction of all site soils,
 - ix. Pre-treat runoff to remove solids before discharging to infiltration areas,
 - x. Install LID measures as appropriate.
- b) Design and placement of all measures shall comply with the standards and practices contained in the most recent version of the New Hampshire Department of Environmental Services NH Stormwater Manual, unless specifically approved by the City Engineer.

Post-Construction:

- a) To assure that erosion and sediment control measures work properly, and to assure that revegetation and slope stabilization takes place in a timely manner and is properly maintained, the City may require a security deposit for up to 12 months after the completion of construction.
- b) For critical or large areas of disturbance on steep slopes or adjacent to surface waters, the City may require inspections by a qualified firm or individual.

3. SNOW STORAGE AND REMOVAL

- a) Snow shall be stored and or removed so as to:
 - i. allow the continued safe passage of vehicles into, out of, and through all travel lanes and parking areas;
 - ii. prevent accumulation on adjacent properties (unless specific approval for such storage has been obtained); and
 - iii. prevent flooding of adjacent properties, including City streets.
- b) Snow may not be pushed, piled or otherwise moved directly into surface waters.
- c) Snowmelt discharge and associated runoff should be stored and its drainage routed so that it does not cause erosion.

4. LANDSCAPING

Plant Selection:

- a) No plant material may be installed that is listed by the NH Department of Agriculture, Markets & Food as an invasive species per NH RSA 430:54 and NH Administrative Rules AGR 3800.
- b) Plant materials shall be hardy to regional climate conditions per the U.S. Department of Agriculture's Plant Hardiness Zone Map.
- c) Landscape alternatives to turfgrass lawn, such as native trees, shrubs, and perennial groundcovers shall be utilized whenever feasible.

Installation:

- a) Trees shall be planted utilizing the best available practices to develop essential root structure, to grow to their full stature, and to perform environmental services at the highest possible levels.
- b) Plant materials shall be installed in soil of sufficient volume, composition, and nutrient balance to sustain healthy growth.
- c) For trees plantings in areas of non-native or compacted soil, the existing soil shall be excavated to enable the placement of 300 cubic feet of native, permeable soil in an area no less than 6 feet wide and 3 feet deep.
- d) Protective devices, such as temporary fencing shall be installed prior to the start of site work to protect the root masses of existing vegetation and areas intended for infiltration to the satisfaction of a City Planner or Engineer. Such fencing shall be located to the outside dripline of shade and ornamental trees and/or to a diameter distance that matches the height of all shrubs and/or perennial plants.

Location:

- a) The majority of green space required by zoning and the majority of site landscaping shall be located on the front portions of the lot, visible from public streets.
- b) All landscaping shall be located without impeding visibility or safety.
- c) Trees shall be located to avoid above-ground and below-ground utilities.

Maintenance:

- a) Any plant material that is significantly damaged, missing, disease-ridden, or dead shall be abated by the property owner within one year or the next planting season, whichever occurs first.
- b) All plant materials shall be maintained in a healthy, growing condition
- c) All landscape elements shall be considered as elements of the project in the same manner as parking, building materials and other site details.

Modifications:

Minor revisions to planting plans may be approved by the Community Development Director or their designee if there is no reduction in the quantity, or changes to the size (at maturity and planting) or location of plant materials and if the proposed plants are of the same general category (e.g. shade, ornamental, evergreen).

5. SCREENING

General Standards:

- a. Screening in the form of landscaping or other treatment (e.g. berms, walls, fences, etc.) shall be used to:
 - i. screen loading areas, trash storage and transfer areas, heating and cooling equipment, electrical equipment and other areas likely to generate noise, dust or other disruptive conditions;
 - ii. form a buffer between non-residential and residential uses;
 - iii. form a buffer between single-family and multi-family projects, especially where multi-family projects are different in height, form or material than the adjacent single family units; and
 - iv. screen parking lots from adjacent properties.
- b. Screening shall be of a material, color, size, and pattern compatible with the existing and proposed building(s) or structure(s) on the site
- c. Chain link fencing with slats is prohibited for screening.
- d. No screening shall be so constructed or installed as to constitute a hazard to traffic or safety.
- e. The Planning Board may, at its discretion, approve an alternative screening design that accomplished the screening objectives set forth in this section.

Specific Standards:

- a. Waste Storage Containers - Waste storage containers (e.g. dumpsters) shall be fully screened by a solid fence of wood, masonry, vinyl or other material deemed acceptable by the Planning Board and shall comply with the minimum standards for property in Chapter 18 of the City Code of Ordinances.
 - i. Fencing around waste storage containers shall be at least 6 feet in height or height equal to the height of the container if the height is greater than 6 ft.
- b. Mechanical Equipment - Heating and cooling equipment, venting, electrical or other mechanical equipment, and associated conduit shall not be visible from adjacent buildings and public rights-of-way whenever possible. If any portion of this equipment will be visible from adjacent buildings or public rights-of-way, it shall be screened from view.
- c. Drive-Through Businesses - Drive-through businesses shall erect screening between their business and any adjacent residential areas. At a minimum, said screening shall consist of a living hedge not less than 4 feet in height or, as an alternative, may consist of a solid fence of wood or masonry at least 6 feet high.

6. LIGHTING

Applicability:

- a) This standard shall only apply to outdoor lighting fixtures.
- b) When 50% or more of the light fixtures or poles of an existing outdoor lighting installation are being modified, extended, expanded, or added to, the entire outdoor lighting installation shall be subject to the requirements of this Section.
- c) This standard does not apply to sign illumination, which is addressed in Section X of this LDC.

Prohibited:

- a) Floodlighting is prohibited, unless:
 - i. The Community Development Director or their designee determines that there will be no negative impact upon motorists and neighboring properties; and
 - ii. The lights are directed toward the rear of a lot away from the road and neighboring properties, and are placed on heat or motion sensors.
- b) Uplighting is prohibited.

General Standards:

- a) Shielding: All outdoor lights, including freestanding and wall mounted, shall be fully-shielded and/or dark skies compliant (International Dark Sky Association Seal of Approval or equivalent) fixtures with no portion of the bulb visible.
- b) Glare: Lighting shall be installed and directed in such a manner as to prevent glare at any location, on or off the property.
- c) Light Trespass: The maximum light level of any light fixture cannot exceed 0.1 footcandles measured at the property line and cannot exceed 1 footcandle measured at the right-of-way line of a street.
- d) Illumination: All illumination shall be of a white light and shall have a color rendering index (CRI) greater than seventy (70). The color-temperature or correlated color temperature (CCT) of lighting shall not exceed 3,500 Kelvins.
- e) Height: The mounting height of fixtures, as measured from the finished grade to the top of the fixture or pole (inclusive of fixture) shall not exceed the following heights:

Zoning District	Pole Height
<ul style="list-style-type: none"> ▪ All residential zoning districts ▪ Agriculture District ▪ Conservation District 	15 feet
<ul style="list-style-type: none"> ▪ Central Business ▪ Central Business Limited ▪ Neighborhood Business ▪ Office 	20 feet
<ul style="list-style-type: none"> ▪ Commerce ▪ Commerce Limited ▪ Industrial ▪ Industrial Park ▪ Corporate Park ▪ Health Care ▪ Business Growth and Reuse 	30 feet

- f) Hours of Operation: Outdoor lighting shall not be illuminated between the hours of 10:00 p.m. and 6:00 a.m. with the following exceptions:
 - i. Security lighting, provided the average illumination on the ground or on any vertical surface does not exceed 1 foot candle.
 - ii. If the use is being operated, normal illumination shall be allowed during the activity and for not more than one hour before or after the activity occurs.
 - iii. For 24-hour businesses, lighting levels shall be reduced by a minimum of 50% between the hours of 10:00 p.m. and 6:00 a.m.
 - iv. The Planning Board may stipulate a specific time when lighting other than that used for security purposes should be turned off and this determination shall be noted on the final lighting plan submitted for signature.
- g) Wiring: Wiring for outdoor lighting shall be placed underground.

Use Specific Standards:

In addition to the Outdoor Lighting Standards in Section II.E above, the following standards apply to the categories of uses listed below.

- a) Commercial Parking Areas -Outdoor lighting of commercial parking and related circulation areas, including the unenclosed areas of parking structures, shall comply with the following standards.
 - i. Average illumination levels of parking lot lighting shall not exceed 3.5 footcandles.

- ii. The ratio of the average to the minimum illumination level (also known as the uniformity ratio) must not exceed 5:1 in footcandles.
- b) Canopies and Gas Station Islands - Canopy lighting, including lighting on gasoline station/convenience store aprons, shall comply with the following standards:
- i. Areas around pump islands and under canopies shall be illuminated so that the average illuminance at grade level does not exceed 12.5 footcandles.
 - ii. For canopies located in or directly adjacent to residential zoning districts and/or where they are associated with a pre-existing non-conforming use, the average illumination at grade level shall not exceed 5.5 footcandles.
 - iii. Light fixtures mounted under a canopy shall be recessed so that the lens cover is recessed into, or flush with, the underside of the canopy.
 - iv. No light fixtures shall be attached to the sides or top of the canopy, nor shall the sides or top of the canopy be illuminated.
 - v. Areas away from fueling pump islands, as defined by the extent of the canopy, shall be subject to parking area lighting regulations as set forth in Section X.
- c) Walkways - Lighting of outdoor walkways, alleys, and pedestrian paths shall comply with the following standards:
- i. The average illumination level on a walkway or pathway surface shall not be less than 0.5 footcandles. Maximum lighting levels shall not exceed 5 footcandles.
 - ii. The area over which the average illumination level is computed shall only include the walkway surface plus an area on each side not more than 5 feet in width.
 - iii. Lighting fixtures other than full cut-off fixtures may be used but shall be designed to minimize glare, direct illumination downward, and shall have an initial output of no more than 1,200 initial lumens.

7. SEWER AND WATER

- a) All sewer and water utilities shall comply with the City's Utility Standards.
- b) The City may require technical studies, at the applicant's expense to assure that existing sewer and water services will not be adversely affected by the proposed development and that there in fact is adequate sewer and water capacity for the proposed development.
- c) All new development shall comply with the City's industrial pre-treatment program.

8. TRAFFIC & ACCESS MANAGEMENT

Traffic:

- a) Any commercial, office or industrial project involving 100 or more vehicle trips per day, or residential projects involving 15 or more units, as determined by the most recent published version of the Trip Generation Manual, shall demonstrate that their project will not diminish the capacity or safety of existing City streets, bridges and/or intersections, prior to the issuance of a building permit.
- b) If improvements to roadways, bridges, signals, or intersections are required for a proposed development to avoid diminishing the existing capacity or safety of these public systems, those improvements shall be made as part of the development, at the developer's expense.
- c) Any development along West Street from School Street to the Bypass, and along Winchester Street from Island Street to the Bypass shall be reviewed by the State Department of Transportation for traffic impact.

Driveways and Curb Cuts:

- a) Entrances and exits onto public streets shall be designed to provide safe and convenient vehicular passage into and out of the site.
- b) Wherever possible, the number of curb cuts or driveways on public streets shall be limited to one per lot.
- c) The use of common driveways and service roads is encouraged, and in some instances may be required.
- d) All driveways must comply with Sections X of this LDC.

- e) A State driveway permit is required for any new driveway on a State Road.

Access Management:

- a) Interior circulation and parking shall be designed to assure safe passage of all vehicles and pedestrians into, out of, and throughout the site.
- b) On-site queuing areas shall be provided to prevent congestion on City streets.
- c) New development shall provide safe and efficient access from roads and streets to all users, regardless of their mode of transportation.
- d) Where appropriate, connections shall be made for the continuation of sidewalks, walkways and bicycle lanes within the property, between adjoining properties, and site amenities shall be installed such as bicycle racks, benches, shade trees, and bus stop shelters. These connections shall apply:
 - i. Grade changes, textures, colors or other methods of distinguishing sidewalks, walkways and crosswalks from vehicular travel; and,
 - ii. Appropriate lighting, signage, crosswalks, and other safety devices.
- e) For Commercial and Multi-Family residential developments, bicycle parking shall be provided in racks or other similar facility. Bicycle parking shall not be located within parking areas for motorized vehicles.

Accessibility:

- a) Pedestrian facilities shall be designed to accommodate persons with disabilities in accordance with the access standards required by the Americans with Disabilities Act (ADA).
- b) Sidewalks, shared use paths, street crossings and other infrastructure shall be constructed so that all pedestrians, including people with disabilities, can travel independently.

9. FILLING AND EXCAVATION

- a) All development shall comply with the City's Fill and Excavation regulations.
- b) Any project, which involves the placement of fill within the floodplain, shall comply with the City's Floodplain Ordinance.
- c) If the placement of fill or excavation impacts wetlands, the applicant shall comply with Federal and State wetlands regulations and procedures.
- d) The City will give particular attention to excavation or fill proposed within or adjacent to wellfield protection areas, surface waters, and hillsides.
- e) Any project, which will result in 50 or more trucks of earth or gravel entering or leaving a site, shall submit a plan to the Community Development Department as to the proposed truck route(s). The Community Development Department shall consult with the Police Department and City Engineer, and as appropriate, the State Highway Department in reviewing the proposed haul routes.

10. SURFACE WATERS & WETLANDS

All development shall comply with all federal and state wetland and surface water regulations, the City's Surface Water Protection Ordinance, the City's policy of no net loss of wetland values or functions, as well as any other applicable City regulations.

11. ARCHITECTURE AND VISUAL APPEARANCE

Intent:

The City of Keene is treasured for its natural beauty, cultural activity, and rich heritage. The geographic and social growth of the City, and its distinctive architectural identity, are heavily influenced by its agricultural, industrial, milling, and railroading history.

It is the Planning Boards intent that future development reflect design influences of this historic architecture and settlement pattern in such a way that new and redeveloped buildings blend aesthetically with the City's historic character.

The purpose of the architecture and visual appearance standards set forth in this Section is to preserve the unique character of the City, to visually improve our gateways and commercial corridors, to preserve our viewsheds and view corridors, and to preserve property values by encouraging development and redevelopment that is consistent with the prevailing scale, orientation, and design of the City.

General Standards for Aesthetic Character:

The following standards shall govern the aesthetic character of development and redevelopment:

1. Front facades and exterior walls shall be articulated to reduce the massive scale and the uniform, impersonal appearances often found in contemporary commercial development, and to provide visual interest consistent with the City's distinctive architectural identity, unique character, and prevailing human scale.
2. The height of any proposed structure or improvement shall not overwhelm the prevailing architectural scale of the City, impede upon any viewshed identified in the Viewshed Overlay District set forth in the Telecommunications Tower and Antennas Ordinance (Chapter 102, Article VII of the Code of Ordinances for the City of Keene) or view corridors (such as Central Square), or detract from valued architectural resources.
3. Structures shall have architectural features (such as dominant gable ends, cornices, granite sills, arched openings, and/or large windows framed with architecturally consistent trim) and patterns that provide visual interest at the scale of the pedestrian, reduce massive aesthetic effects, and harmonize with the City's distinctive architectural identity, unique character, and prevailing scale.
 - i. Architectural features shall not serve primarily as an advertisement, commercial display, or identifying characteristics corresponding to corporate identity.
 - ii. Architectural features shall conform to accepted architectural principles of design and construction.
1. Exterior materials, textures, and colors shall minimize visual aggressiveness (for example, light colors; glossy finishes, reflective glass or dark tinted exteriors; or natural aluminum, stainless steel, or metal exterior finishes) and shall harmonize with the City's distinctive architectural identity and unique character.
2. Any subsequent substitutions, alterations, or changes to materials, textures and colors previously approved by the Planning Board shall require additional Planning Board review and approval.
5. The presence of any existing development in the surrounding area that does not conform to these standards for aesthetic character shall not exempt the applicant from complying with the standards.

Site Design and Relationship to Surrounding Community:

The following standards govern site design and its relationship to the surrounding area and structures:

1. Orientation of structures on a site shall conform to a parallel or orthogonal pattern in relation to the City street pattern.
2. Off-street parking and traffic flow shall not interfere with the flow of pedestrian travel or otherwise detract from the aesthetic character of a development or redevelopment. All required off-street parking shall be to the side and/or rear of buildings on the proposed site, and such parking shall be screened or aligned so that headlights do not adversely impact abutting properties.
3. The placement, orientation, or design of a development, and the structures thereon, shall not interfere with viewsheds identified in the Viewshed Overlay District set forth in the Telecommunications Tower and Antennas Ordinance (Chapter 102, Article VII of the Code of Ordinances for the City of Keene) or view corridors (such as Central Square).

