



CITY OF KEENE LAND USE CODE AD HOC COMMITTEE

Friday, July 12, 2019
4:00 pm
2nd Floor Conference Room
Keene City Hall
3 Washington St, 03431

AGENDA

1. Call to Order
2. Minutes of June 21, 2019
3. Review of Form Based Zoning Draft Subdistrict Dimensional/Form Standards
4. Review of Draft Land Development Code Chapters
 - a. Site Plan Development Standards
5. Next Meeting – July 26, 2019
6. Adjourn

City of Keene
New Hampshire

AD HOC LAND USE CODE STEERING COMMITTEE
MEETING MINUTES

Friday, June 21, 2019

4:30 PM

**2nd Floor Conference Room,
City Hall**

Members Present:

Mayor Kendall Lane
Councilor David Richards
Councilor George Hansel
Councilor Maggie Rice
Douglas Barrett
Jim Phippard
Gary Spykman (Arrived Late)

Staff Present:

Rhett Lamb, Community Development
Director/Assistant City Manager
Tara Kessler, Senior Planner
Mari Brunner, Planner Technician

Members Not Present:

Katie Cassidy Sutherland

1) Call to Order

Mayor Lane called the meeting to order 4:01 PM. The project consultant from Camiros, Chris Jennette, joined the meeting by phone.

2) Approve Minutes of June 7, 2019

Councilor Richards moved to approve the minutes of June 7, 2019, which Councilor Hansel seconded and the Ad Hoc Land Use Code Steering Committee carried unanimously.

3) Review of Form Based Zoning Draft Subdistrict Dimensional/Form Standards

Ms. Kessler displayed a map of the seven subdistricts, which the Committee heard a technical report on in May 2019. The consultant, Chris Jennette, shared high-level details about the subdistricts, for which information was included in the meeting packet.

Mr. Jennette reminded the Committee about the purpose and intent of form-based zoning, how it differs from Keene's zoning currently, and he shared dimensions that were missing from the last discussion on the topic. A form-based zoning approach is fundamentally different from Keene's traditional code and prioritizes "form" (size), placement, and design of the building as opposed to the use in the building. Traditional zoning codes like Keene's were first concerned with controlling use of land, followed by physical compatibility and design. Form-based zoning is good for downtown Keene, where it is important to maintain and enhance the distinct sense of place. This form-based code will not disregard uses entirely, but those uses will be second to physical compatibility of the buildings downtown.

The benefit of form-based zoning for downtown Keene is less subjective interpretation of standards, which will make them more predictable for the City and developers. This thoughtful approach will create desirable patterns of development or change patterns where that is the goal. This form will also make the zoning code more readable with photos and graphics more than text, and thus will reduce confusion about what is required downtown.

With a form-based code, new buildings in each subdistrict will be reviewed against a set of regulations to ensure new construction compliments the existing character of the area. Form based zoning will ensure buildings interface well with the street and each other. Draft regulations for each subdistrict were listed in table in the meeting packet. The standards were organized into categories of key considerations: setbacks, building height, parking location, current zoning districts included, and proposed mix of uses permitted. Graphics will be included ultimately to bridge the gap between current development and the vision for future development. The table also includes a brief purpose statement for each subdistrict, which were pulled from the technical review report and recommendations from the Joint Committee.

Mr. Jennette continued describing the proposed format for building codes and development standards beginning on page 27 of 48 in the meeting packet. This is the basic format of how each subdistrict will look. The Downtown Core (DC) subdistrict is the heart of downtown Keene and meant to accommodate the highest development in a horizontally and vertically mixed-use environment.

He discussed the proposed form standards for DC as an example and reiterated that this was a high-level discussion:

- Dimensions & Siting
 - No minimum lot area or width requirement
 - Maximum building length 250'. Breaks in the building to create variety are ideal.
 - The concept of a build-to line/zone, which requires that buildings are located a certain distance from the front line of a property.
 - Established primary and secondary street setbacks, which gets at the concept of treating development along streets differently in areas of the downtown. Main Street is proposed as a primary street, where buildings should be closer to the street to reinforce that development pattern. Winter Street is an example that could be a secondary street where there may be greater flexibility with building placement.

Councilor Hansel asked what determines primary and secondary streets and Mr. Jennette said this Committee makes that decision. Mr. Jennette referred to page 14 of 48 in the meeting packet with a note with anticipated primary streets: Main Street, Court Street, Washington Street, Roxbury Street, Emerald Street, Gilbo Avenue, and School Street; all others in the Downtown Core would be considered secondary. Mr. Lamb said it is up to this Committee to determine the criteria for primary and secondary streets. Mr. Barrett recalled a criteria that if a building was on the corner of a primary and secondary street that the primary entrance must be oriented toward the primary street. He said the list was

complicated and made him consider West Street as primary, which poses problems in areas like Central Square. He questioned the new primary entrance of the library, which Mr. Lamb said is West Street. Mr. Lamb recalled this is a discussion of the overall building patterns and there will be subtlety like Mr. Barrett explained, but the overall goal is to illustrate a distinction between the two street types. Ms. Kessler used an example to illustrate and described the build-to percentage, or the percentage of building that needs to be along the build-to line. Mr. Jennette noted this is a percentage of the building, not the lot; this is a minimum and could be 100%.

Mayor Lane noted that lot lines on Main Street are not straight and some sidewalks are owned privately by the property owner. In those instances, 80% against the lot line puts some buildings in the middle of the sidewalks, which does not fit the current downtown and needs to be considered as unique to Keene. Mr. Lamb agreed that Mr. Jennette should consider that for some building facing Main Street. Mr. Jennette said there is a way to address this by building in language for another element to determine that setback, like the back of the sidewalk. Councilor Hansel also said it will be important to measure from the back of the curb because curb space might be modified in the future. Ms. Kessler confirmed that on a primary street 80% of the building must be at the build-to line, whereas it is only 60% on secondary streets that could accommodate greater sidewalk width variation in the future.

- Building Height
 - The proposed minimum building height is 18' and the maximum building height is 85' and 7 stories. Unique new elements include:
 - Minimum height stepback – buildings taller than 65' must have a height stepback of 15' from the front façade line, somewhere between the ground and fifth stories. This ensures no buildings are an entire seven-stories directly against the sidewalk, for example, and minimizes visual impact and congestion.
 - Corner tower element – allows for a portion of a corner building's frontage in both width and depth that can violate the stepback and create a punctuated corner.
 - Permitted height exception – allows a height increase of 25% of the maximum square feet on the top floor up to 8' high, not to create an additional story but to vary the roofline and create visual interest.

Councilor Rice asked if the purpose of corner tower elements is to designate certain blocks. Mr. Jennette said the objective is to punctuate corners with unique geometry, similar to buildings with unique corner features at acute angles on Central Square, or the architectural variety of a corner like Gilbo and Main with an exception to the stepback requirement. Ms. Kessler clarified this is an option, not a requirement.

- Activation
 - These standards relate to the pedestrian interaction with buildings. Controlling the minimum ground floor height encourages a ground floor tall enough so pedestrians feel a part of an active sidewalk mixed-use

environment. A taller building can permit a greater variety of uses of the ground floor spaces.

- Regulating maximum blank wall area at 20'. A blank wall is defined as a section of wall without architectural detailing, windows, or murals. Blank walls can create an uncomfortable pedestrian experience.
- Standards for building entry spacing: maximum of 30' between building entrances along primary streets and 60' on secondary streets. This is to reinforce the idea of permeable spaces for pedestrians to move on and off the sidewalk.
- Maximum vertical distance between the building entry and sidewalk is a unique feature of Keene in the older commercial or mixed-use buildings, with entrances elevated off the sidewalk. Notwithstanding ADA requirements, this standard builds in addition flexibility if someone wanted to reinforce this pattern on new construction.
- Minimum transparency requirements for what pedestrians can see through, i.e., windows and doors. On the ground floor, 50% of the total façade area or between 2' - 12' from the sidewalk must be transparent. Minimum upper floor transparency is required at 15% of either the total façade or as a percentage of each story.

Mr. Barrett referred to the maximum building entry space requirement at every 30' and said that seems close, which can be good, but he asked if 30' is common on a block like Main Street that is divided with multiple businesses. Mr. Jennette said the 30' is a walkable standard that applies to the length of a single building on a single lot. The Committee discussed the merits and potential challenges of the 30' standard. Ultimately, Mr. Lamb said this is a matter of attractiveness at street-level. While it seems rigid, more precise regulations will create a usable checklist for developers and staff. Mr. Jennette clarified this refers only to accessible public entryways, but agreed there could be clearer language defining the entryways.

Councilor Hansel posed the situation of a major redevelopment that wants to maintain the façade of the building; for example, if an entire building were demolished on Main Street but the façade was maintained in front of the new construction. Mr. Jennette said the goal of these standards is not to disrupt historic facades so he imagines there could be exceptions for the situation Councilor Hansel presented.

- Parking
 - Parking dictates a lot about development so the goal is to control parking location and access through these standards.
 - In the DC, parking is permitted only to the rear of a building and the minimum primary street parking setback would not be applicable because parking is not permitted on primary streets.
 - Regulates things like the minimum setback for parking areas on secondary streets and from interior, side, and rear lot lines.

- Parking access is prohibited from primary streets and access from a secondary street must be 12' wide for a one-way entrance and 24' wide for a two-way entrance.
- Solid masonry wall, if constructed, must be a minimum of 3' in height, to a maximum of 4.5' in height. Such wall must be constructed at the required setback for the length of the entire parking lot.

Ms. Kessler recalled that the Committee started the discussion on standards using the DC as an example; however, there are detailed tables of standards for all seven districts included in the meeting packet. Mr. Jennette broadly defined those standards using DC as an example. Staff is working on the table internally and will bring this forward again at a future meeting in hopefully a more succinct and graphical way as the Committee becomes more familiar. Councilor Hansel noted that staff had already reduced 50 pages of narrative dimensions to these 12 pages of tables.

Ms. Kessler shifted focus to the Neighborhood Redevelopment (NR) subdistrict because the City has never had a predeveloped goal for this area before, which makes drafting dimensional standards challenging. It is unclear if the goal is to preserve the development pattern today or promote a design pattern in these transition corridors. By the next meeting, staff needed confirmation to either move forward with the NR as presented and continue with community outreach in each subdistrict, or make modifications before the community outreach phase. The Committee needs to be confident in anything presented to the public. The Committee discussed the NR subdistrict, looking at the downtown map of subdistricts. Mr. Lamb recalled recommended uses in the NR subdistrict listed in the technical report.

This discussion is to confirm the subdistricts are laid out as the committee wants. Originally staff proposed six districts closer to the downtown core but the Joint Committee process changed the defined area of downtown and what will be included. Some primarily residential areas around Blake, Wilson, and Davis Streets, as well as areas along Dunbar, Water, Willow Streets were included in the Form Based Zoning District. Initially, there was discussion about allowing for mixed use of smaller retail and office in these areas, in addition to residential uses. Mayor Lane said it was hard to distinguish between Residential Transition (RT) and NR; he was not convinced they should be two separate districts. He understood minor setback differences but ultimately the proposed uses are very similar. Mr. Lamb said staff originally conceived RT as a replacement to the Office District. Mr. Lamb thought the difference between RT and NR was largely based on form. Neighborhoods to the north are traditional office district neighborhoods with larger buildings and lots, for example the School Street neighborhood. The neighborhoods to the south are denser on smaller lots. Strict dimensional standards had to reflect to some degree what is already there unless imposing substantial changes to any development there. Mr. Jennette said that in addition to the form, this is an area where it is beneficial to think about distinctions and uses between RT and NR. He said that RT exhibits the larger form with larger setbacks on bigger lots, but they tend to be a mixture of residential and low intensity office/commercial uses. He said that NR is essentially residential areas of transition between the downtown core and fully residential neighborhoods now included downtown. He asked how to regulate these

neighborhoods as a part of downtown. Ms. Kessler said building height is important to that question.

Councilor Hansel referred to the proposed mix of uses permitted, where under NR it says mixed-use; he asked what incentivizes mixed-use so that it is only applied in this district versus the others. Mr. Jennette said this was a typo he would correct.

Mr. Spykman said it seemed that including the Commercial Edge subdistrict areas trapped the residential neighborhoods in NR within the borders of the expanded downtown. He said ultimately there are no active changes to these residential areas but this process recognizes them within the expanded downtown borders. Mr. Jennette asked then if there is an existing residential zoning district that would better accommodate these than anything created in this process. Mr. Phippard said one place he sees a difference between the RT and NR is in the section on downtown design standards; Mr. Lamb noted that this section is still under review and staff was not prepared to answer many questions. Mr. Barrett said there appear to be real differences but he said those might apply well to NR.

Councilor Rice noted that parking location is in the rear in RT but rear and side in NR. She asked about a single-family unit with a driveway in the front. Mr. Lamb said the driveway can be in the front but the parking areas (where you leave the car overnight) should be to the rear or side. He said where there is more land, staff is asking for more parking at the back of the building. You can park a car in a driveway though they technically have to be behind the front line of the building, which is the standard today, but there are areas where this standard does not apply because there is insufficient space. He said if the vision is not to change the standards dramatically but they are now looped into the form-based zone area, then there are two solutions: 1) take it back to normal zoning and out of the form based, and 2) set up a new district that does not allow new substantial uses like retail. Mr. Jennette said form-based zoning means the residential areas would be more intentional in how siting and size of any redevelopment in those areas as opposed to keeping it in the current zone. Mr. Spykman said the form could be developed to match current standards, which Mayor Lane agreed with. Councilor Hansel said those areas are ripe for serious housing redevelopment so the code should accommodate unique new housing in residential areas to allow creativity in future redevelopment. The Committee continued discussing challenges and opportunities for the RT and NR subdistricts. Staff will work on making standards reflect the variety of these residential areas in their current form for discussion at a future meeting.

4) Review of Draft Land Development Code Chapters
a. Historic District Regulations

Ms. Kessler said that as staff became more confident in form-based standards and regulations, opportunities arose to do even more with the Historic District. Staff's concern is the dual processes of the Historic District Commission (HDC) and the Planning Board today. Ms. Brunner reviewed proposed opportunities to streamline these processes through the form based zoning process. Any proposed changes align with the Building Better Together project goals:

- Simple - the updated HDC regulations will be more concise and will include tables and graphics to make it easier for the user to understand and navigate the document.
- Efficient - the proposed changes would increase efficiency by removing the requirement for newer buildings to go through the HDC process and thus increasing the number of projects that can be approved administratively by staff, and placing all standards related to the HDC in one chapter.
- Thoughtful - in response to increased interest within the community to incorporate arts into the downtown, standards for artwork attached to historic structures have been developed and added to the regulations to make the standards and process for art installations transparent and clear.

Councilor Hansel asked if there are credits available for redeveloping a site and implementing traditional architectural features. He said there may be features to incentivize salvaging for an existing building being renovated or a new construction that maintains a similar design. Mr. Spykman said he thinks the core of form-based code is what Councilor Hansel described. Ms. Kessler said that depending on the demolition plans, the developer could come before the HDC for review and approval of demolition proposals in the downtown Historic District. Regarding an incentive to use a historical pattern in new construction, Mr. Lamb said staff will look into what incentives exist. Councilor Hansel said there will be necessary changes over time and the goal is to preserve those details, while allowing for change. Mr. Lamb said that HDC review seeks to maintain the character of a building being altered in a substantial way but the form-based code would apply to any new buildings. The Committee returned to Councilor Hansel's previous example about maintaining a historical façade, which Ms. Kessler referred to as a façade easement and an example includes the Colony House on West Street.

5) Next Meeting – July 12, 2019

6) Adjournment

Hearing no further business, Mayor Lane adjourned the meeting at 5:19 PM.

Respectfully submitted by,
Katrinya Kibler, Minute Taker

Reviewed and edited by Tara Kessler, Senior Planner

DC Downtown Core Subdistrict

Purpose: The DC Subdistrict is the heart of downtown Keene, accommodating the highest intensity of development. The Subdistrict is intended to accommodate a rich mix of commercial, residential, civic, cultural and open space uses in a highly walkable, vertically and horizontally mixed-use environment.

Standard	Requirement
Lot and Building Dimensions	
Minimum Lot Area	None
Minimum Lot Width	None
Maximum Building Length	250'
Building Siting	
Primary Street Setback	0' BTL
Primary Street Build-to Percentage	80% Min.
Note: Primary streets will be named/mapped as necessary. Anticipated primary streets include: Main, Court, Washington, Roxbury, Emerald, Gilbo, and School. All others considered secondary.	
Secondary Street Setback	0' BTL when 150' or less from intersection with Primary. Otherwise 0-10' BTZ
Secondary Street Build-to Percentage	60% Min.
Interior Side Setback	0' unless abutting residential district or RT subdistrict, then 15' Min.
Rear Setback	0' unless abutting residential district or RT subdistrict, then 15' Min.
Building Height	
Minimum Building Height	18'
Maximum Building Height	85' and 7 stories
Required Building Height Stepback	15' minimum setback from building line for buildings taller than 65'. Stepback must occur above the ground story and no higher than the 5 th story.
Corner Tower Element	Limited in width and depth to a maximum of 25% of the building frontage. May exceed height at which stepback is required, but may not exceed the overall building height
Permitted Height Exception	Buildings may exceed maximum permitted height by up to 8' for a maximum of 25% of the square footage of the top floor. Such additional height may not be used to create an additional story.
Building Activation on Primary and Secondary Streets	
Minimum Ground Floor Height (FTF)	14'
Maximum Blank Wall Area	20'
Building Entry Spacing	30' max. between entries on Primary Streets, 50' max. between entries on Secondary Streets
Vertical Distance Between Bld. Entry and Sidewalk	3' max.
Ground Floor Transparency	50% min.
Upper Floor Transparency	15% min.
Parking Location & Access	
Permitted Parking Location	Parking Permitted to the rear of the building only
Minimum Primary Street Parking Setback	N/A (Not permitted along Primary Street)
Minimum Secondary Street Parking Setback	15' Min. May be reduced to 7' behind the front building line, with the construction of a solid masonry wall, 3' min. to 4' max. in height at the setback for the length of the parking lot, excluding access ways.
Minimum Interior Side Parking Setback	None, unless abutting residential district or RT subdistrict, then 5'. This will need to align with buffer standards.
Minimum Rear Parking Setback	None, unless abutting residential district or RT subdistrict, then 10'. This will need to align with buffer standards.
Minimum Parking Access Width	Primary Street: Not permitted Secondary Street: 12' for one-way, 24' for two-way

DG Downtown Growth Subdistrict

Purpose: The DG Subdistrict accommodates the reuse of existing structures within downtown Keene, as well as new construction of significant size. It is intended to provide the flexibility needed to create a mixed-use environment in areas of downtown where growth is desired, with standards for new construction and infill development that complement the walkable urban form of Keene’s downtown.

Standard	Requirement
Lot and Building Dimensions	
Minimum Lot Area	None
Minimum Lot Width	None
Maximum Building Length	350’, unless a passage is provided per Section __, then 500’ including width of passage. Note: Standards for building passage design will be included (width, height, transparency)
Building Siting	
Primary Street Setback	0-5’ BTZ
Primary Street Build-to Percentage	80% Min.
Secondary Street Setback	5-15’ BTZ
Secondary Street Build-to Percentage	60% Min.
Interior Side Setback	0’ unless abutting residential district or RT subdistrict, then 15’ Min.
Rear Setback	0’ unless abutting residential district or RT subdistrict, then 25’ Min.
Building Height	
Minimum Building Height	24’
Maximum Building Height	85’ and 7 stories
Required Building Height Stepback	15’ minimum setback from building line for buildings taller than 65’. Stepback must occur above the ground story and no higher than the 5th story.
Corner Tower Element	Limited in width and depth to a maximum of 25% of the building frontage. May exceed height at which stepback is required, but may not exceed the overall building height
Permitted Height Exception	Buildings may exceed maximum permitted height by up to 8’ for a maximum of 25% of the square footage of the top floor. Such additional height may not be used to create an additional story.
Building Activation on Primary and Secondary Streets	
Minimum Ground Floor Height (FTF)	14’
Maximum Blank Wall Area	30’
Building Entry Spacing	50’ max. between entries
Vertical Distance Between Bld. Entry and Sidewalk	3’ max.
Ground Floor Transparency	50% min. on Primary Street, 40% min. on Secondary Street
Upper Floor Transparency	15% min.
Parking Location & Access	
Permitted Parking Location	Parking Permitted to the rear of the building only
Minimum Primary Street Parking Setback	N/A (Not permitted along Primary Street)
Minimum Secondary Street Parking Setback	15’ Min. May be reduced to 7’ behind the front building line, with the construction of a solid masonry wall, 3’ min. to 4’ max. in height at the setback for the length of the parking lot, excluding access ways.
Minimum Interior Side Parking Setback	None, unless abutting residential district or RT subdistrict, then 5’. This will need to align with buffer standards.
Minimum Rear Parking Setback	None, unless abutting residential district or RT subdistrict, then 10’. This will need to align with buffer standards.
Parking Access	12’ for one-way, 24’ for two-way

CE Commercial Edge Subdistrict

Purpose: The Commercial Edge Subdistrict accommodates a variety of moderately intense commercial development at the edges of downtown Keene, providing a transition into lower intensity commercial or residential development outside of the delineated downtown area. Such development may be a mixture of walkable development transitioning into a more auto-oriented development pattern.

Standard	Requirement
Lot and Building Dimensions	
Minimum Lot Area	10,000sf
Minimum Lot Width	50'
Maximum Building Length	None
Building Siting	
Front Setback	0-20' BTZ
Front Build-to Percentage	60% Min.
Corner Side Setback	0-20' BTZ
Corner Side Build-to Percentage	40% Min.
Interior Side Setback	0' unless abutting residential district or RT subdistrict, then 20' Min.
Rear Setback	0' unless abutting residential district or RT subdistrict, then 25' Min.
Building Height	
Minimum Building Height	None
Maximum Building Height	40' and 3 stories
Permitted Height Exception	May exceed maximum height by up to 5' for a total of 25% of the square footage of the top floor. Such additional height may not be used to create an additional story.
Building Activation	
Minimum Ground Floor Height (FTF)	12'
Maximum Blank Wall Area	30'
Vertical Distance Between Bld. Entry and Sidewalk	18" max.
Ground Floor Transparency	30% min.
Upper Floor Transparency	15% min.
Parking Location	
Permitted Parking Location	Parking permitted to the rear of the building only
Permitted parking location based upon feedback from Steering Committee during initial review of subdistricts and technical report. We recommend revising to allow parking to the rear and side of structures. Limiting parking to the rear makes more sense in denser areas such as the DC and DG, but may be too limiting for lots in the CE areas, which are intended to accommodate a mix of pedestrian and automobile traffic.	
Minimum Front Parking Setback	N/A (Not permitted)
We recommend allowing parking to the side of structures, through a provision that would require a front parking setback of 5' behind the front building line. This provides flexibility while ensuring no parking may be located in front of structures.	
Minimum Corner Side Parking Setback	15', or 5' behind the corner side building line, whichever is less
Minimum Interior Side Parking Setback	None, unless abutting residential district or RT subdistrict, then 10'. This will need to align with buffer standards.
Minimum Rear Parking Setback	None, unless abutting residential district or RT subdistrict, then 15'. This will need to align with buffer standards.
Parking Access	12' for one-way, 24' for two-way

RT Residential Transition Subdistrict

Purpose: The Residential Transition Subdistrict is intended to accommodate a variety of residential, open space, and low intensity commercial uses in a mixed-use environment of attached and detached structures. Development within the RT Subdistrict is intended to complement and transition into existing residential neighborhoods adjacent to downtown Keene.

Standard	Requirement
Lot and Building Dimensions	
Minimum Lot Area	8,000sf
Minimum Lot Width	60'
Maximum Building Coverage	40%
Maximum Impervious Surface Coverage	60%
Building Siting	
Front Setback	15' Min.
Corner Side Setback	10' Min.
Interior Side Setback	10' Min.
Rear Setback	15' Min.
Building Height	
Minimum Building Height	N/A
Maximum Building Height	40' and 3 stories
Building Activation	
Vertical Distance Between Bld. Entry and Sidewalk	5' max.
Ground Floor Transparency	Multi-family and Nonresidential uses: 25% Min.
Upper Floor Transparency	Multi-family and Nonresidential uses: 10% Min.
Parking Location	
Permitted Parking Location	Parking permitted to the rear and side of the building
See previous note on parking location. Note change in permitted parking location from Technical report. We think allowing parking to the rear and side is more appropriate for the RT area, considering current development patterns.	
Minimum Front Parking Setback	5' behind front building line
Minimum Corner Side Parking Setback	5' behind corner side building line
Minimum Interior Side Parking Setback	5'
Minimum Rear Parking Setback	5'
Parking Access	12' for one-way, 22' for two-way

DL Downtown Limited Subdistrict

Purpose: The DL Subdistrict is intended to accommodate the unique development pattern exhibited in downtown Keene to the north of Central Square. The DL Subdistrict accommodates a low-intensity, limited mixture of uses in structures of a lower height, located close to the street.

Standard	Requirement
Lot and Building Dimensions	
Minimum Lot Area	None
Minimum Lot Width	None
Maximum Building Coverage	65%
Maximum Impervious Surface Coverage	80%
Building Siting	
Front Setback	0-15' BTZ
Corner Side Setback	0-25' BTZ
Interior Side Setback	0' unless abutting residential district or RT subdistrict, then 10' Min.
Rear Setback	0' unless abutting residential district or RT subdistrict, then 15' Min.
Building Height	
Minimum Building Height	None
Maximum Building Height	35' and 2.5 stories
Building Activation	
Maximum Blank Wall Area	20'
Vertical Distance Between Bld. Entry and Sidewalk	4' max.
Ground Floor Transparency	30% min.
Upper Floor Transparency	15% min.
Parking Location	
Permitted Parking Location	Parking permitted to the rear and side of the building
Minimum Front Parking Setback	10', or equivalent to front building line, whichever is greater
Minimum Corner Side Parking Setback	10'
Minimum Interior Side Parking Setback	None, unless abutting residential district or RT subdistrict, then 5'. This will need to align with buffer standards.
Minimum Rear Parking Setback	None, unless abutting residential district or RT subdistrict, then 5'. This will need to align with buffer standards.
Parking Access	12' for one-way, 22' for two-way

NR Neighborhood Redevelopment Subdistrict

Purpose: The Neighborhood Redevelopment Subdistrict is intended to facilitate the development of a mixture of uses and building forms aligned with the City’s vision for neighborhoods that are largely bordered by or encompassed within more intensely developed areas within downtown. The subdistrict accommodates a variety of development forms and encourages the creation of higher-density residential development.

Standard	Requirement
Lot and Building Dimensions	
Minimum Lot Area	6,000sf
Minimum Lot Width	50'
Maximum Building Coverage	40%
Maximum Impervious Surface Coverage	50%
Building Siting	
Front Setback	15' Min.
Corner Side Setback	10' Min.
Interior Side Setback	10' Min.
Rear Setback	25' Min.
Building Height	
Minimum Building Height	None
Maximum Building Height	50' and 4 stories
Permitted Height Exception	May exceed maximum height by up to 5' for a total of 25% of the square footage of the top floor.. Such additional height may not be used to create an additional story.
Building Activation	
Maximum Blank Wall Area	30'
Prominent Entry Spacing	Multi-family and Nonresidential uses: 50' max. between entries
Vertical Distance Between Bld. Entry and Sidewalk	4' max.
Ground Floor Transparency	Multi-family and nonresidential: 35% min.
Upper Floor Transparency	Multi-family and nonresidential: 15% min.
Parking Location	
Permitted Parking Location	Parking permitted to the rear and side of the building
Minimum Front Parking Setback	5' behind front building line
Minimum Corner Side Parking Setback	15'
Minimum Interior Side Parking Setback	5'
Minimum Rear Parking Setback	5'
Parking Access	11' for one-way, 22' for two-way

IC Institutional Campus Subdistrict

Purpose: The Institutional Campus Subdistrict accommodates the unique form of Keene State College as it interfaces with the mixed-use, walkable fabric of downtown Keene.

Standard	Requirement
Lot and Building Dimensions	
Minimum Lot Area	None
Minimum Lot Width	None
Maximum Building Length	350', unless a passage is provided per Section __, then 500' including width of passage
Maximum Building Coverage	50%
Maximum Impervious Surface Coverage	75%
Building Siting	
Front Setback	30' Min.
Corner Side Setback	15' Min.
Interior Side Setback	0' unless abutting residential district or RT subdistrict, then 15'
Rear Setback	0' unless abutting residential district or RT subdistrict, then 25'
Building Height	
Minimum Building Height	None
Maximum Building Height	50' at required setbacks; structures may increase height over 50' by setting back an additional 2' for every 1' of additional height from all required setbacks
Parking Location	
Minimum Front Parking Setback	15'
Minimum Corner Side Parking Setback	15'
Minimum Interior Side Parking Setback	None, unless abutting residential district or RT subdistrict, then 15'. This will need to align with buffer standards.
Minimum Rear Parking Setback	None, unless abutting residential district or RT subdistrict, then 15'. This will need to align with buffer standards.
Parking Access	12' for one-way, 24' for two-way

Downtown Design Standards

The following table establishes the design standards for **multifamily and nonresidential** uses in the Downtown Subdistricts. An “X” in the cell indicates that such standard is applicable in the subdistrict. Where a cell is blank, such standard does not apply.

These standards have been created through an analysis of Downtown Keene, as well as best practices in design standards from similar communities. Select standards from the HDC regulations, as well as the Planning Board Development Standards have been incorporated as well.

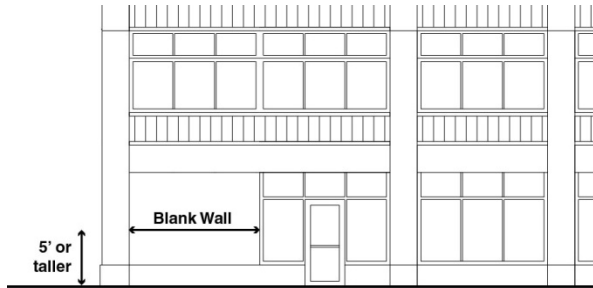
Standard	Subdistricts						
	DC	DG	DL	CE	RT	NR	IC
Building Orientation and Entrances							
All buildings must be oriented toward a public street. When located on lots that front on both a Primary and Secondary Street, buildings must be oriented toward the Primary Street.	X	X	X	X	X	X	
All buildings must contain at least one Primary entrance.	X	X	X	X	X	X	X
Primary entrances to buildings must be located on the street to which the building is oriented. Where a building has frontage along both a Primary Street and a Secondary Street, the primary entrance must be oriented toward the Primary Street, or at the corner.	X	X	X	X	X	X	X
Primary building entrances must be at sidewalk level or within the maximum vertical distance between building entry and sidewalk. “Garden level,” or below-grade primary entrances are prohibited.	X	X	X	X	X	X	X
All primary entrances must include direct pedestrian connections between street facing doors to adjacent sidewalks, and may not be set back more than ten feet from the building line.	X	X	X	X	X	X	X
Primary entrances must incorporate at least two of the following design elements to ensure clear visibility and architectural prominence: <ul style="list-style-type: none"> a. Details including but not limited to arches, friezes, columns, pilasters, transom or sidelight windows mosaic or tile work, murals, or other public art as an integral feature. b. Integral landscape or seating elements such as planters and benches or seat walls. c. Three-dimensional elements such as corner towers, porticos, arcades or colonnades. 	X	X	X	X		X	X
Secondary access points must be provided from rear parking lots. Facades that abut parking areas and contain a public entrance must include pedestrian walkways.	X	X	X	X		X	X
Building Form and Façade Composition							
For buildings of 150’ in length or more, facades must be divided into multiple “modules,” expressed through significant architectural changes such as a change in materials, a change in pattern elements such as fenestration, columns and pilasters, or a change in building setback through recesses or projections. Such modules may be no wider than 50 feet.	X	X	X	X		X	
Commercial storefronts must include traditional pedestrian-oriented elements such as display windows, bulkheads, transoms and cornices.	X	X	X	X		X	
Buildings must be designed with consistent building materials and treatments that wrap around all façades visible from a public street. Where material or color treatments change, there should be a significant change in surface plane of a minimum of six inches in difference. Differing materials are encouraged to terminate at inside corners.	X	X	X	X	X	X	
Mechanical equipment such as compressor units must be screened to minimize visibility. If located on a roof, such equipment must be set back to ensure minimal visibility from the sidewalk. If located at ground level, mechanical equipment must be located to the rear of the building and landscaped or screened to minimize visual impact.	X	X	X			X	X

Facades that abut rear parking areas and contain a public entrance must maintain a minimum transparency of 20%.	X	X	X			X	
Facades must express a traditional visual distinction between the ground floor and upper stories through architectural features or detailing, change in materials, or a change in pattern elements such as fenestration.	X	X	X			X	
Buildings must maintain a predominantly traditional residential appearance through the use of architectural elements including but not limited to pitched roof forms, porches, balconies, bay windows, and stoops. (RT only)					X		
Roof Design	DC	DG	DL	CE	RT	NR	IC
Rooflines of 150' in length or more must be varied through the use of architectural design elements such as dormers, gables, or projected wall features. Such elements of variation may be no wider than 75'.	X	X	X	X		X	
Reflective roof surfaces that produce glare are prohibited, except for solar panels or cool roofs intended to radiate absorbed or non-reflected solar energy and reduce heat transfer to the building.	X	X	X	X	X	X	X
Where parapet walls are used, they must feature three-dimensional cornice treatments or other shadow-creating details along their tops.	X	X	X	X	X	X	X
Site Design	DC	DG	DL	CE	RT	NR	IC
Sites must be designed to ensure safe pedestrian access from the public right-of-way, and safe pedestrian circulation within the development.	X	X	X	X	X	X	X
A cohesive visual character must be maintained through the use of coordinated hardscape (paving materials, lighting, street furniture, etc.) and landscape treatments within the development.	X	X	X			X	X
Off-street parking and traffic flow must not interfere with the flow of pedestrian travel or otherwise detract from the aesthetic character of a development or redevelopment. All off-street parking must be screened or aligned so that light from vehicle headlights do not adversely impact abutting properties.	X	X	X	X	X	X	X

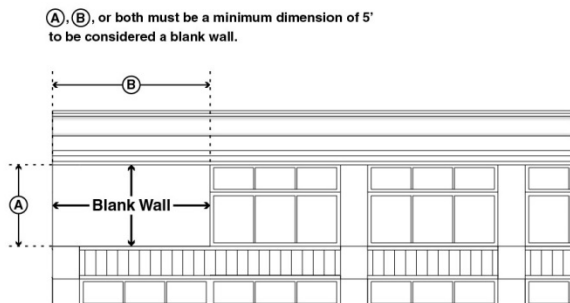
Definitions and Illustrations

A series of additional definitions and illustrations will be needed to explain some of the new terms used within the Form-Based Zoning. An initial set of these definitions is included below for reference.

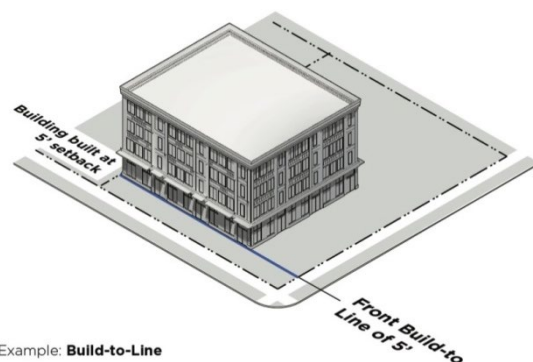
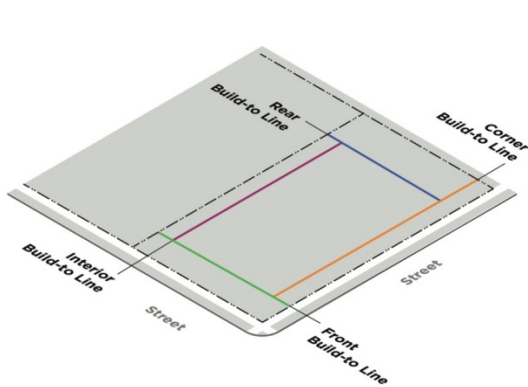
Blank Wall, Ground Floor. The horizontal linear dimension of contiguous building wall that does not contain fenestration, doors, or decorative elements such as banding, medallions, artwork such as murals and mosaics, change in wall plane of at least three inches, or other architectural or material embellishment. Any wall less than five feet in height is not considered to be a blank wall.



Blank Wall, Upper Floor. The horizontal or vertical linear dimension of contiguous building wall that does not contain fenestration, doors, or decorative elements such as banding, medallions, artwork such as murals and mosaics, change in wall plane of at least three inches, or other architectural or material embellishment. Any wall with a minimum dimension of less than five feet (height or width) is not considered to be a blank wall.

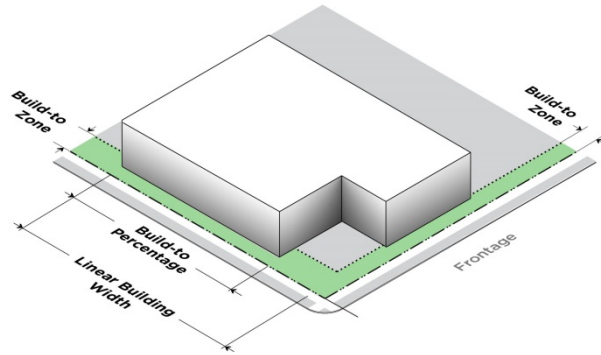


Build-To Line (BTL). A build-to line (BTL) is a set line on a lot, measured perpendicularly from the applicable lot line, where a structure must be located. The building line of a structure must be located on the build-to line. Facade articulation, such as window or wall recesses and projections are not counted as the building line, which begins at the applicable façade wall.

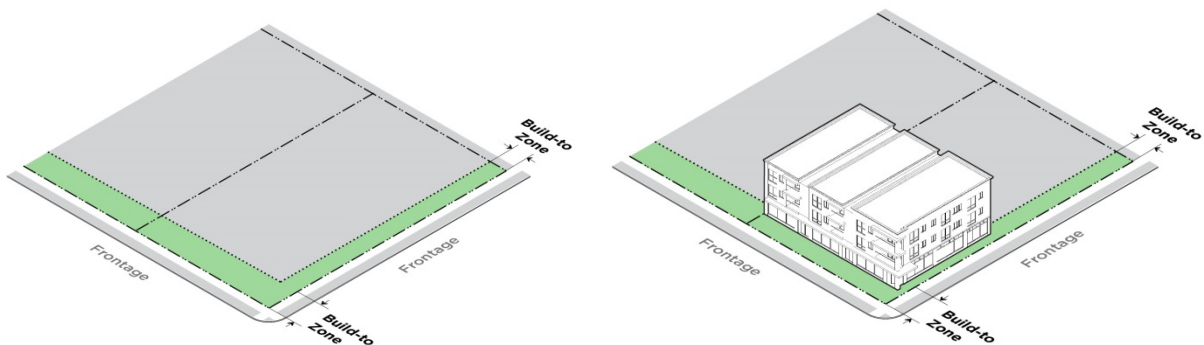


Example: Build-to-Line

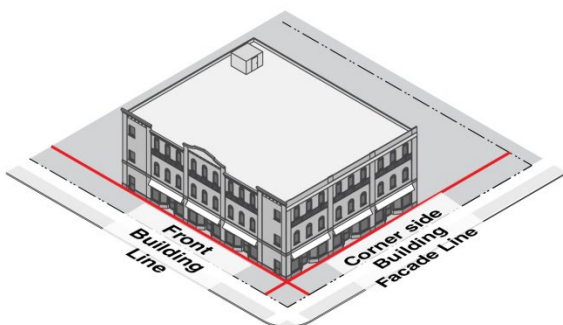
Build-To Percentage. A build-to percentage specifies the percentage of the building facade that must be located within the build-to zone or at the build-to line. Facade articulation, such as window or wall recesses and projections, do not count against the required build-to percentage. Plazas, outdoor dining, and other public open space features that are also bounded by a building facade parallel to the frontage are counted as meeting the build-to percentage. Build-to percentage is calculated by building facade, not lot width.



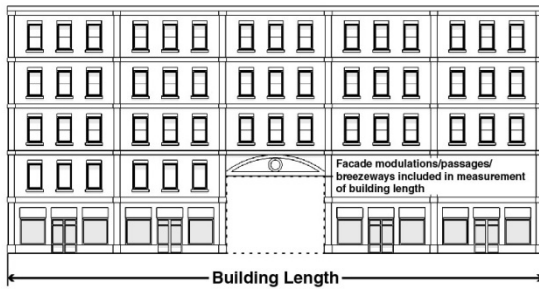
Build-To Zone (BTZ). A build-to zone (BTZ) is the area on a lot, measured perpendicularly from the lot line, within which a structure must locate. A build-to zone sets a minimum and maximum dimension within which the building facade line must be located (e.g. 0-5'). Facade articulation, such as window or wall recesses and projections are not counted as the building facade line, which begins at the applicable facade wall.



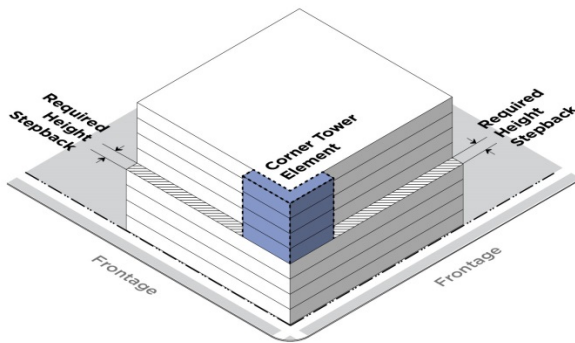
Building Line. The vertical plane along a lot where the building's facade is located. Upper story building lines relate to that part of the facade that requires a setback.



Building Length. Building length is measured as the length of the facade abutting a frontage type. Passageways, breezeways, ground floor passages, and similar building connections are included in the calculation of total building length.



Corner Tower Element. A corner tower element is an accentuated vertical element located on a building corner at a street intersection that can be located within a required stepback. In no case may the corner tower element exceed the building's overall height.







Stepback. A stepback is the required additional distance that upper stories of a structure must be recessed from the facade of the stories below. A required stepback must continue through all upper stories once established, though it may be increased from any required minimums on any stories.

Transparency. Transparency is the required amount of window area as a percentage of the specified facade area. Doors are included in ground floor transparency when such doors are designed with glass or other transparent materials. To qualify as transparent for the calculation, the glazing must meet the following standards:

- 1) Ground floor transparency must have a minimum 60% Visible Light Transmittance (VLT) and no more than 15% Visible Light Reflectance (VLR).
- 2) For upper floor glazing, the glazing must have a minimum of 40% VLT and no more than 15% VLR.
- 3) Colored tinting is prohibited.
- 4) The following do not meet the ground floor or upper floor transparency requirements and do not count in meeting the standard:
 - a) Windows with interior shadowboxes
 - b) Glass block
 - c) Printed window film, regardless of whether it allows views into or out of the building.

2020 Building Better Together (UDO)

 <p>BUILDING BETTER TOGETHER KEENE NEW HAMPSHIRE</p>	 <p>SIMPLE New regulations will be easy to navigate & will include graphics to outline a clear process, from start to finish.</p>	 <p>EFFICIENT The updated structure will provide a set of clear procedures for development queries, & will create a more streamlined application process – eliminating the need to navigate multiple points of contact.</p>	 <p>THOUGHTFUL Although our regulations have worked in the past, they can be outdated & confusing. This update will help guide us into the future, while protecting the crucial elements that make this a great place to live, work, & play.</p>	<p>Site Plan Development Standards</p> <p>Draft as of July 8, 2019</p>
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BACKGROUND:

The City's Development Standards are used by the Planning Boards as the foundation for their review of Site Plan applications for commercial and multifamily development/redevelopment proposals. Currently, there are 19 Development Standards that include: Drainage; Sedimentation and Erosion Control; Hillsides; Flooding; Snow Storage; Screening; Landscaping; Lighting; Air Quality; Stump Dumps; Sewer and Water; Noise; Hazardous Materials; Surface Waters; Wetlands; Comprehensive Access Management; Traffic; Excavation and Filling; and, Architecture and Visual Appearance. These standards were first adopted in 1994 and have since been amended multiple times.

SUMMARY OF MAJOR CHANGES & REASONS:

- Staff removed language from each of the standards that was not directly related to the Planning Board's review of applications or that was advisory in nature.
- Staff removed standards that were solely references to other local ordinances such as Noise, Hillsides, and Flooding. Staff will incorporate reference to these ordinances as part of an application form to ensure that applicants are aware of/adhere to these regulations when developing a site plan application. Staff also removed the standard for Stump Dump, as this could also be an application form item.
- Staff removed standards that solely reference state or federal regulations such as Air Quality and Hazardous Materials.
- Staff consolidated the standards for Surface Water and Wetlands into one standard. Staff also consolidated Comprehensive Access Management and Traffic into one standard.
- Staff have relocated landscaping standards related to parking lots to the parking chapter of the LDC, which will be part of the Zoning Ordinance. Staff incorporated new standards related to landscaping plant selection and added a section called "Modifications."
- Staff modified the lighting levels and thresholds included in the lighting section. Of note, would be the increase in the uniformity ratio for commercial parking areas from 4:1 to 5:1, and the change in maximum illuminance for canopies/gas station islands from 5.5 fc to 12.5 fc, except for those that are adjacent or within residential zoning districts or associated with a preexisting nonconforming use.
- Staff modified the screening standard to include general standards related to screening and to address screening of mechanical equipment.

REMAINING ISSUES/QUESTIONS

- The standard for Architecture and Visual Appearance has not yet been updated. Staff are waiting to identify how design standards will be incorporated into the Form Based Zoning District before finalizing edits to this standard.
- Staff are currently evaluating the thresholds for major and minor project review, with the intent of increasing the opportunity for projects to be reviewed administratively.

ALIGNMENT WITH PROJECT GOALS:

Simple: The text of each standard has been streamlined and modified to make the regulations easier to read and navigate. Language that was advisory or not specific to the Board's review authority has been removed.

Efficient: The number of standards has been reduced from 19 to 11, while maintaining those standards that are most relevant to the Board's review of applications.

Thoughtful: Staff have proposed changes to some of the standards in response to feedback received from the development community and in an effort to keep the standards up to date with current technology and development practices.

ATTACHMENTS:

- A. Draft Development Standards Dated July 8, 2019.

DRAFT SITE PLAN DEVELOPMENT STANDARDS

1. DRAINAGE AND STORMWATER MANAGEMENT

Runoff:

- a) The development of a site shall not result in increased volume or velocity of runoff onto adjacent properties or surface water bodies unless specifically approved by the City and agreed to in a formal, legally binding manner by the affected property owner(s).
- b) The applicant must provide sufficient data to demonstrate compliance with this requirement.

Low Impact Design:

- a) Site design and drainage systems constructed to comply with this standard shall incorporate Low Impact Development (LID). Examples include but are not limited to bio-retention, porous pavement, vegetated swales, infiltration trenches, rooftop disconnection, cisterns, green roofs, vegetated filter strips, reduced impervious surfaces, stormwater disconnection, preservation and enhancement of native vegetation.
 - i. Where site conditions exist that make on-site infiltration impractical, the Applicant shall demonstrate that the use of LID measures is not possible before proposing to use traditional, structural stormwater management measures including but not limited to stormwater retention and detention ponds and underground storage systems.

Quality of Stormwater Discharge:

- a) Treatment systems (e.g. wet detention basins, constructed wetlands, and/or LID measures) combined with contamination prevention practices (e.g. frequent sweeping of parking lots, the use of oil/gas traps in catch basins) shall be utilized to assure that upon discharge to surface waters, wetlands, floodplain compensatory storage basins or wellfield protection areas, drainage water meets Federal Clean Water Act standards.
- b) Drainage systems which divert runoff into surface waters, wetlands, wellfield protection areas, or floodplain compensatory storage areas shall utilize LID measures or wet or dry sediment basins or similar devices in combination with erosion prevention and sediment control practices to avoid sediment loading of the receiving area(s).

Bike Friendly:

New street or trail drainage grates shall be of a design that allows a bicyclist to ride over it safely.

2. SEDIMENTATION AND EROSION CONTROL

Pre-Construction:

- a) Each project shall be designed to prevent sedimentation and erosion during and subsequent to construction in the following ways:
 - i. Minimize disturbance of natural soil cover and vegetation,
 - ii. Minimize, in area and duration, exposed soil and unstable soil conditions,
 - iii. Protect receiving water bodies, wetlands and storm sewer inlets,
 - iv. Minimize off-site sediment transport on vehicles and equipment,
 - v. Minimize work in and adjacent to water bodies and wetlands,
 - vi. Maintain stable slopes,
 - vii. Minimize disturbance to and protect surrounding soils, root systems and trunks of trees adjacent to site activity that are intended to be left standing,
 - viii. Minimize the compaction of all site soils,
 - ix. Pre-treat runoff to remove solids before discharging to infiltration areas,
 - x. Install LID measures as appropriate.
- b) Design and placement of all measures shall comply with the standards and practices contained in the most recent version of the New Hampshire Department of Environmental Services NH Stormwater Manual, unless specifically approved by the City Engineer.

Post-Construction:

- a) To assure that erosion and sediment control measures work properly, and to assure that revegetation and slope stabilization takes place in a timely manner and is properly maintained, the City may require a security deposit for up to 12 months after the completion of construction.
- b) For critical or large areas of disturbance on steep slopes or adjacent to surface waters, the City may require inspections by a qualified firm or individual.

3. SNOW STORAGE AND REMOVAL

- a) Snow shall be stored and or removed so as to:
 - i. allow the continued safe passage of vehicles into, out of, and through all travel lanes and parking areas;
 - ii. prevent accumulation on adjacent properties (unless specific approval for such storage has been obtained); and
 - iii. prevent flooding of adjacent properties, including City streets.
- b) Snow may not be pushed, piled or otherwise moved directly into surface waters.
- c) Snowmelt discharge and associated runoff should be stored and its drainage routed so that it does not cause erosion.

4. LANDSCAPING

Plant Selection:

- a) No plant material may be installed that is listed by the NH Department of Agriculture, Markets & Food as an invasive species per NH RSA 430:54 and NH Administrative Rules AGR 3800.
- b) Plant materials shall be hardy to regional climate conditions per the U.S. Department of Agriculture's Plant Hardiness Zone Map.
- c) Landscape alternatives to turfgrass lawn, such as native trees, shrubs, and perennial groundcovers shall be utilized whenever feasible.

Installation:

- a) Trees shall be planted utilizing the best available practices to develop essential root structure, to grow to their full stature, and to perform environmental services at the highest possible levels.
- b) Plant materials shall be installed in soil of sufficient volume, composition, and nutrient balance to sustain healthy growth.
- c) For trees plantings in areas of non-native or compacted soil, the existing soil shall be excavated to enable the placement of 300 cubic feet of native, permeable soil in an area no less than 6 feet wide and 3 feet deep.
- d) Protective devices, such as temporary fencing shall be installed prior to the start of site work to protect the root masses of existing vegetation and areas intended for infiltration to the satisfaction of a City Planner or Engineer. Such fencing shall be located to the outside dripline of shade and ornamental trees and/or to a diameter distance that matches the height of all shrubs and/or perennial plants.

Location:

- a) The majority of green space required by zoning and the majority of site landscaping shall be located on the front portions of the lot, visible from public streets.
- b) All landscaping shall be located without impeding visibility or safety.
- c) Trees shall be located to avoid above-ground and below-ground utilities.

Maintenance:

- a) Any plant material that is significantly damaged, missing, disease-ridden, or dead shall be abated by the property owner within one year or the next planting season, whichever occurs first.
- b) All plant materials shall be maintained in a healthy, growing condition
- c) All landscape elements shall be considered as elements of the project in the same manner as parking, building materials and other site details.

Modifications:

Minor revisions to planting plans may be approved by the Community Development Director or their designee if there is no reduction in the quantity, or changes to the size (at maturity and planting) or location of plant materials and if the proposed plants are of the same general category (e.g. shade, ornamental, evergreen).

5. SCREENING

General Standards:

- a. Screening in the form of landscaping or other treatment (e.g. berms, walls, fences, etc.) shall be used to:
 - i. screen loading areas, trash storage and transfer areas, heating and cooling equipment, electrical equipment and other areas likely to generate noise, dust or other disruptive conditions;
 - ii. form a buffer between non-residential and residential uses;
 - iii. form a buffer between single-family and multi-family projects, especially where multi-family projects are different in height, form or material than the adjacent single family units; and
 - iv. screen parking lots from adjacent properties.
- b. Screening shall be of a material, color, size, and pattern compatible with the existing and proposed building(s) or structure(s) on the site
- c. Chain link fencing with slats is prohibited for screening.
- d. No screening shall be so constructed or installed as to constitute a hazard to traffic or safety.
- e. The Planning Board may, at its discretion, approve an alternative screening design that accomplished the screening objectives set forth in this section.

Specific Standards:

- a. Waste Storage Containers - Waste storage containers (e.g. dumpsters) shall be fully screened by a solid fence of wood, masonry, vinyl or other material deemed acceptable by the Planning Board and shall comply with the minimum standards for property in Chapter 18 of the City Code of Ordinances.
 - i. Fencing around waste storage containers shall be at least 6 feet in height or height equal to the height of the container if the height is greater than 6 ft.
- b. Mechanical Equipment - Heating and cooling equipment, venting, electrical or other mechanical equipment, and associated conduit shall not be visible from adjacent buildings and public rights-of-way whenever possible. If any portion of this equipment will be visible from adjacent buildings or public rights-of-way, it shall be screened from view.
- c. Drive-Through Businesses - Drive-through businesses shall erect screening between their business and any adjacent residential areas. At a minimum, said screening shall consist of a living hedge not less than 4 feet in height or, as an alternative, may consist of a solid fence of wood or masonry at least 6 feet high.

6. LIGHTING

Applicability:

- a) This standard shall only apply to outdoor lighting fixtures.
- b) When 50% or more of the light fixtures or poles of an existing outdoor lighting installation are being modified, extended, expanded, or added to, the entire outdoor lighting installation shall be subject to the requirements of this Section.
- c) This standard does not apply to sign illumination, which is addressed in Section X of this LDC.

Prohibited:

- a) Floodlighting is prohibited, unless:
 - i. The Community Development Director or their designee determines that there will be no negative impact upon motorists and neighboring properties; and
 - ii. The lights are directed toward the rear of a lot away from the road and neighboring properties, and are placed on heat or motion sensors.
- b) Uplighting is prohibited.

General Standards:

- a) Shielding: All outdoor lights, including freestanding and wall mounted, shall be fully-shielded and/or dark skies compliant (International Dark Sky Association Seal of Approval or equivalent) fixtures with no portion of the bulb visible.
- b) Glare: Lighting shall be installed and directed in such a manner as to prevent glare at any location, on or off the property.
- c) Light Trespass: The maximum light level of any light fixture cannot exceed 0.1 footcandles measured at the property line and cannot exceed 1 footcandle measured at the right-of-way line of a street.
- d) Illumination: All illumination shall be of a white light and shall have a color rendering index (CRI) greater than seventy (70). The color-temperature or correlated color temperature (CCT) of lighting shall not exceed 3,500 Kelvins.
- e) Height: The mounting height of fixtures, as measured from the finished grade to the top of the fixture or pole (inclusive of fixture) shall not exceed the following heights:

Zoning District	Pole Height
<ul style="list-style-type: none"> ▪ All residential zoning districts ▪ Agriculture District ▪ Conservation District 	15 feet
<ul style="list-style-type: none"> ▪ Central Business ▪ Central Business Limited ▪ Neighborhood Business ▪ Office 	20 feet
<ul style="list-style-type: none"> ▪ Commerce ▪ Commerce Limited ▪ Industrial ▪ Industrial Park ▪ Corporate Park ▪ Health Care ▪ Business Growth and Reuse 	30 feet

- f) Hours of Operation: Outdoor lighting shall not be illuminated between the hours of 10:00 p.m. and 6:00 a.m. with the following exceptions:
 - i. Security lighting, provided the average illumination on the ground or on any vertical surface does not exceed 1 foot candle.
 - ii. If the use is being operated, normal illumination shall be allowed during the activity and for not more than one hour before or after the activity occurs.
 - iii. For 24-hour businesses, lighting levels shall be reduced by a minimum of 50% between the hours of 10:00 p.m. and 6:00 a.m.
 - iv. The Planning Board may stipulate a specific time when lighting other than that used for security purposes should be turned off and this determination shall be noted on the final lighting plan submitted for signature.
- g) Wiring: Wiring for outdoor lighting shall be placed underground.

Use Specific Standards:

In addition to the Outdoor Lighting Standards in Section II.E above, the following standards apply to the categories of uses listed below.

- a) Commercial Parking Areas -Outdoor lighting of commercial parking and related circulation areas, including the unenclosed areas of parking structures, shall comply with the following standards.
 - i. Average illumination levels of parking lot lighting shall not exceed 3.5 footcandles.

- ii. The ratio of the average to the minimum illumination level (also known as the uniformity ratio) must not exceed 5:1 in footcandles.
- b) Canopies and Gas Station Islands - Canopy lighting, including lighting on gasoline station/convenience store aprons, shall comply with the following standards:
- i. Areas around pump islands and under canopies shall be illuminated so that the average illuminance at grade level does not exceed 12.5 footcandles.
 - ii. For canopies located in or directly adjacent to residential zoning districts and/or where they are associated with a pre-existing non-conforming use, the average illumination at grade level shall not exceed 5.5 footcandles.
 - iii. Light fixtures mounted under a canopy shall be recessed so that the lens cover is recessed into, or flush with, the underside of the canopy.
 - iv. No light fixtures shall be attached to the sides or top of the canopy, nor shall the sides or top of the canopy be illuminated.
 - v. Areas away from fueling pump islands, as defined by the extent of the canopy, shall be subject to parking area lighting regulations as set forth in Section X.
- c) Walkways - Lighting of outdoor walkways, alleys, and pedestrian paths shall comply with the following standards:
- i. The average illumination level on a walkway or pathway surface shall not be less than 0.5 footcandles. Maximum lighting levels shall not exceed 5 footcandles.
 - ii. The area over which the average illumination level is computed shall only include the walkway surface plus an area on each side not more than 5 feet in width.
 - iii. Lighting fixtures other than full cut-off fixtures may be used but shall be designed to minimize glare, direct illumination downward, and shall have an initial output of no more than 1,200 initial lumens.

7. SEWER AND WATER

- a) All sewer and water utilities shall comply with the City's Utility Standards.
- b) The City may require technical studies, at the applicant's expense to assure that existing sewer and water services will not be adversely affected by the proposed development and that there in fact is adequate sewer and water capacity for the proposed development.
- c) All new development shall comply with the City's industrial pre-treatment program.

8. TRAFFIC & ACCESS MANAGEMENT

Traffic:

- a) Any commercial, office or industrial project involving 100 or more vehicle trips per day, or residential projects involving 15 or more units, as determined by the most recent published version of the Trip Generation Manual, shall demonstrate that their project will not diminish the capacity or safety of existing City streets, bridges and/or intersections, prior to the issuance of a building permit.
- b) If improvements to roadways, bridges, signals, or intersections are required for a proposed development to avoid diminishing the existing capacity or safety of these public systems, those improvements shall be made as part of the development, at the developer's expense.
- c) Any development along West Street from School Street to the Bypass, and along Winchester Street from Island Street to the Bypass shall be reviewed by the State Department of Transportation for traffic impact.

Driveways and Curb Cuts:

- a) Entrances and exits onto public streets shall be designed to provide safe and convenient vehicular passage into and out of the site.
- b) Wherever possible, the number of curb cuts or driveways on public streets shall be limited to one per lot.
- c) The use of common driveways and service roads is encouraged, and in some instances may be required.
- d) All driveways must comply with Sections X of this LDC.

- e) A State driveway permit is required for any new driveway on a State Road.

Access Management:

- a) Interior circulation and parking shall be designed to assure safe passage of all vehicles and pedestrians into, out of, and throughout the site.
- b) On-site queuing areas shall be provided to prevent congestion on City streets.
- c) New development shall provide safe and efficient access from roads and streets to all users, regardless of their mode of transportation.
- d) Where appropriate, connections shall be made for the continuation of sidewalks, walkways and bicycle lanes within the property, between adjoining properties, and site amenities shall be installed such as bicycle racks, benches, shade trees, and bus stop shelters. These connections shall apply:
 - i. Grade changes, textures, colors or other methods of distinguishing sidewalks, walkways and crosswalks from vehicular travel; and,
 - ii. Appropriate lighting, signage, crosswalks, and other safety devices.
- e) For Commercial and Multi-Family residential developments, bicycle parking shall be provided in racks or other similar facility. Bicycle parking shall not be located within parking areas for motorized vehicles.

Accessibility:

- a) Pedestrian facilities shall be designed to accommodate persons with disabilities in accordance with the access standards required by the Americans with Disabilities Act (ADA).
- b) Sidewalks, shared use paths, street crossings and other infrastructure shall be constructed so that all pedestrians, including people with disabilities, can travel independently.

9. FILLING AND EXCAVATION

- a) All development shall comply with the City's Fill and Excavation regulations.
- b) Any project, which involves the placement of fill within the floodplain, shall comply with the City's Floodplain Ordinance.
- c) If the placement of fill or excavation impacts wetlands, the applicant shall comply with Federal and State wetlands regulations and procedures.
- d) The City will give particular attention to excavation or fill proposed within or adjacent to wellfield protection areas, surface waters, and hillsides.
- e) Any project, which will result in 50 or more trucks of earth or gravel entering or leaving a site, shall submit a plan to the Community Development Department as to the proposed truck route(s). The Community Development Department shall consult with the Police Department and City Engineer, and as appropriate, the State Highway Department in reviewing the proposed haul routes.

10. SURFACE WATERS & WETLANDS

All development shall comply with all federal and state wetland and surface water regulations, the City's Surface Water Protection Ordinance, the City's policy of no net loss of wetland values or functions, as well as any other applicable City regulations.

11. ARCHITECTURE AND VISUAL APPEARANCE

Intent:

The City of Keene is treasured for its natural beauty, cultural activity, and rich heritage. The geographic and social growth of the City, and its distinctive architectural identity, are heavily influenced by its agricultural, industrial, milling, and railroading history.

It is the Planning Boards intent that future development reflect design influences of this historic architecture and settlement pattern in such a way that new and redeveloped buildings blend aesthetically with the City's historic character.

The purpose of the architecture and visual appearance standards set forth in this Section is to preserve the unique character of the City, to visually improve our gateways and commercial corridors, to preserve our viewsheds and view corridors, and to preserve property values by encouraging development and redevelopment that is consistent with the prevailing scale, orientation, and design of the City.

General Standards for Aesthetic Character:

The following standards shall govern the aesthetic character of development and redevelopment:

1. Front facades and exterior walls shall be articulated to reduce the massive scale and the uniform, impersonal appearances often found in contemporary commercial development, and to provide visual interest consistent with the City's distinctive architectural identity, unique character, and prevailing human scale.
2. The height of any proposed structure or improvement shall not overwhelm the prevailing architectural scale of the City, impede upon any viewshed identified in the Viewshed Overlay District set forth in the Telecommunications Tower and Antennas Ordinance (Chapter 102, Article VII of the Code of Ordinances for the City of Keene) or view corridors (such as Central Square), or detract from valued architectural resources.
3. Structures shall have architectural features (such as dominant gable ends, cornices, granite sills, arched openings, and/or large windows framed with architecturally consistent trim) and patterns that provide visual interest at the scale of the pedestrian, reduce massive aesthetic effects, and harmonize with the City's distinctive architectural identity, unique character, and prevailing scale.
 - i. Architectural features shall not serve primarily as an advertisement, commercial display, or identifying characteristics corresponding to corporate identity.
 - ii. Architectural features shall conform to accepted architectural principles of design and construction.
1. Exterior materials, textures, and colors shall minimize visual aggressiveness (for example, light colors; glossy finishes, reflective glass or dark tinted exteriors; or natural aluminum, stainless steel, or metal exterior finishes) and shall harmonize with the City's distinctive architectural identity and unique character.
2. Any subsequent substitutions, alterations, or changes to materials, textures and colors previously approved by the Planning Board shall require additional Planning Board review and approval.
5. The presence of any existing development in the surrounding area that does not conform to these standards for aesthetic character shall not exempt the applicant from complying with the standards.

Site Design and Relationship to Surrounding Community:

The following standards govern site design and its relationship to the surrounding area and structures:

1. Orientation of structures on a site shall conform to a parallel or orthogonal pattern in relation to the City street pattern.
2. Off-street parking and traffic flow shall not interfere with the flow of pedestrian travel or otherwise detract from the aesthetic character of a development or redevelopment. All required off-street parking shall be to the side and/or rear of buildings on the proposed site, and such parking shall be screened or aligned so that headlights do not adversely impact abutting properties.
3. The placement, orientation, or design of a development, and the structures thereon, shall not interfere with viewsheds identified in the Viewshed Overlay District set forth in the Telecommunications Tower and Antennas Ordinance (Chapter 102, Article VII of the Code of Ordinances for the City of Keene) or view corridors (such as Central Square).