



Planning Board – Monday, August 24, 2020, 6:30PM

AGENDA

NOTE: This meeting will be conducted using the online meeting platform, Zoom.* The public may view the meeting online by visiting www.zoom.us/join and enter the **Meeting ID: 857 8338 6440**. If you are unable to attend the meeting online, you may call the toll-free # (888) 475-4499 and enter the Meeting ID to listen to the meeting. More info on how to access this meeting is available at ci.keene.nh.us/planning-board. If you encounter any issues accessing this meeting, please call 603-757-0676 during the scheduled meeting time.

- I. **Call to Order** – Roll Call
- II. **Minutes of Previous Meeting** – July 27, 2020 Meeting
- III. **Driveway Application:**

Symonds School Driveway, 79 Park Ave – Applicant and owner, Union School District, is requesting to create an additional driveway at Symonds School located at 79 Park Ave (TMP# 575-015-000) by opening an existing gate that is located at the end of Wheelock Street and using it as a bus entrance and exit. The property is 7.2 acres in size and is located in the Low Density District.

- IV. **Public Hearings:**

SPR-972, Mod. 6, Site Plan – CONTINUED – Colony Mill Retail Building, 210-222 West Street – Applicant and owner Brady Sullivan Properties proposes to construct a 4,875-sf retail and office building with two drive-throughs on the properties located at 210-222 West St (TMP# 576-009-000), 0 Gilbo Ave (TMP# 576-008-000) and 0 Island St (TMP#s 583-011-000, 583-012-000, 583-014-000, & 583-015-000). A waiver is requested from Development Standard 10 – Lighting. These properties are 4.6-ac, 0.94-ac, 0.3-ac, 0.21-ac, 0.1-ac, and 0.34-ac in size and are located in the Commerce District.

SPR-01-13, Mod. 2, Site Plan – Indoor Shooting Range, 19 Ferry Brook Road – Applicant and owner Cheshire County Shooting Sports Education Foundation proposes to install a 3,300-sf indoor shooting range on the property located at 19 Ferry Brook Road (TMP# 214-021-000). A waiver is requested from Development Standard #6 – Landscaping. This property is located in both the Town of Sullivan and the City of Keene. The portion of the property in Keene is 55-acres and is located in the Rural District.

- IV. **Community Development Director Report**

- V. **New Business**

- VI. **Upcoming Dates of Interest – September 2020**

- Joint PB/PLD Committee – September 14, 6:30 PM
- Planning Board Steering Committee – September 15, 11:00 AM
- Planning Board Site Visits – September 23, 8:00 AM – **To Be Confirmed**
- Planning Board Meeting – September 28, 6:30 PM

**In Emergency Order #12, issued by the Governor pursuant to Executive Order #2020-04, which declared a COVID-19 State of Emergency, the requirement that a quorum of a public body be physically present at the meeting location under RSA 91-A:2, III(b), and the requirement that each part of a meeting of a public body be audible or otherwise discernible to the public at the meeting location under RSA 91-A:2, III(c), have been waived. Public participation may be provided through telephonic and other electronic means.*

**CITY OF KEENE
NEW HAMPSHIRE
PLANNING BOARD
MEETING MINUTES**

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Monday July 27, 2020 6:30 PM Council Chambers

Members Present
Douglas Barrett, Chairman
Chris Cusack, Vice-Chair
Michael Burke
Councilor George Hansel
Andrew Weglinski
Mayor George Hansel
Gail Sommers
David Orgaz
Pamela Russell Slack
Councilor Michael Remy
Emily Lavigne Bernier, Alternate

Staff:
Rhett Lamb, Asst. City Manager/Community
Development Director
Tara Kessler, Senior Planner

I. Call to order – Roll Call

Chair Barrett called the meeting to order at 6:30 PM and read a prepared statement explaining how Emergency Order #12, pursuant to Executive Order #2020-04 issued by the Governor of the State of New Hampshire, gives authority for public meetings to be held remotely and shared information about how members of the public can listen and share comments. Roll call was conducted. All members reported their location and whether anyone else was present in the room with them.

II. Minutes of previous meeting – June 22, 2020 Meeting

A motion was made by Mayor George Hansel to accept the June 22, 2020 minutes as presented. The motion was seconded by Councilor Michael Remy and was unanimously approved by roll call vote.

III. Boundary Line Adjustment: SPR-972 Mod. 6, Site Plan – Colony Mill New Retail Building, 210-222 West Street – Applicant and owner Brady Sullivan Properties proposes to construct a 4,875-sf retail and office building with two drive-throughs on the properties located at 210-222 West St (TMP# 576-009-000), 0 Gilbo Ave (TMP# 576-008-000) and 0 Island St (TMP#s 583-011-000, 583-012-000, 583-014-000, & 583-015-000). A waiver is requested from Development Standard 10 – Lighting. These properties are 4.6-ac, 0.94-ac, 0.3-ac, 0.21-ac, 0.1-ac, and 0.34-ac in size and are located in the Commerce District.

30 David Orgaz asked to be recused from this application. Chair Barrett asked Emily
31 Lavigne Bernier to take David Orgaz's place for the public hearing.

32

33 A. Board Determination of Completeness.

34 Senior Planner Tara Kessler recommended to the Board that Application SPR-972 Mod.
35 #6 was complete. She noted the Applicant has submitted a voluntary merger application
36 to merge the six parcels impacted by this proposal into one parcel, which has been
37 reviewed by City staff and found to be in compliance with zoning. This merger would
38 need to be recorded with the Registry of Deeds if the application is approved.

39

40 A motion was made by Mayor George Hansel that the Board accept this application as
41 complete. The motion was seconded by Councilor Michael Remy and was unanimously
42 approved by roll call vote.

43

44 B. Public Hearing

45 Mr. Rob Pearson introduced himself as the Project Manager for Brady Sullivan and
46 addressed the Board. He said that he is joined tonight by Charles Panasis and Ben Kelley,
47 owners of Brady Sullivan, as well as Kelly Dowd, legal representation, Amy Sanders, PE
48 from Fuss and O'Neil, and Shaun Kelly from Vanasse and Associates.

49

50 Mr. Pearson referred to an aerial image of the site and said this project is going to be
51 located in the bottom left (southwest) corner of the property off Island Street. He referred
52 to the proposed demolition plan, which shows the landscape island and pavement that
53 will be cut out. He added that these areas will be replaced with some changes, which will
54 be addressed later in the presentation.

55

56 Mr. Pearson referred to the proposed site plan and explained that the new building will be
57 65 feet by 75 feet with drive through businesses on the north and south sides. The
58 building will have a brick veneer and white stucco to match existing buildings in the area.
59 The site plan shows traffic flow and parking within the property to accommodate the two
60 drive-through businesses. Mr. Pearson also referred to the dumpster layout and bike racks
61 being proposed for the project. He noted the plan is to have a crosswalk connecting the
62 two islands where the bike racks will be located and another crosswalk going from the
63 drive-through for easy transition from the bike rack to the building. Mr. Pearson stated
64 their proposal is also to provide public access from Island Street to the property.

65

66 Mr. Pearson then referred to the drainage plan completed by their consultant. Three tests
67 pits were done to make sure runoff was sustainable. With respect to utilities, the bulk of
68 the utilities will be coming in from Island Street (underground).

69

70 Mr. Pearson continued, stating that the existing lighting will be used and two new lights
71 are being proposed and they will be installed with no light trespass onto adjoining
72 properties. Mr. Pearson added if the lights need to be re-scaled, the lighting consultant is
73 present today and it can be discussed. He further stated the applicant did submit a request for
74 a lighting waiver but this new proposal will eliminate the need for this waiver.

75

76 Silt fencing will be used to control sedimentation and erosion during construction, and
77 fencing will also be placed around the existing trees and as many as possible will be
78 preserved. They are taking down five trees but are adding 14 crab apple trees and maybe
79 four more as well in the landscape island closest to the casino.
80

81 Shawn Kelly, Traffic Engineer, addressed the Board next. Mr. Kelly stated for this study
82 they focused on three time periods; weekday morning peak traffic, weekday evening peak
83 traffic, and Saturday mid-day peak traffic. The intersections that were included in the
84 study were the intersection at West Street and Island Street, Gilbo Avenue, side access
85 driveway onto Island Street and Gilbo Avenue and the Intersection of Pearl Street and
86 Island Street. The traffic expected to frequent this coffee shop and bank is mostly
87 “bypass” traffic. Based on ITE data for the bank it would 30% - 40% and for the coffee
88 shop, it would be considerably higher at about 80%. Based on this calculation, this new
89 use could generate about 50 cars to this site in the morning and evening, and about 76
90 trips on a Saturday morning. The traffic is projected to come from different directions,
91 but mostly from Winchester Street. The increase being projected at the highest level
92 would be about 10-15 cars per hour (1.5% increase or less), which Mr. Kelly noted did
93 not result in notable impact for traffic operations.
94

95 Mr. Kelly went on to say their focus mostly is on site access and layout, to make sure
96 there was safe access onto Gilbo Avenue and Island Street. There is a recommendation to
97 locate the bay queues on the southern end of the side where queuing will be lower than
98 the northern end. Morning peak queuing could be higher but this will still be contained
99 within the site and not impact off site operations. He noted the team has been looking at
100 some potential improvements; connectivity from rail trail and bicycle racks. He felt the
101 off-site impact because of this proposal would be insignificant.
102

103 Mr. Kelly stated staff had raised concern as to when these traffic counts were done. The
104 morning and evening counts were done prior to the State of Emergency and felt the State
105 of Emergency could have had an impact on the count. He stated they had looked at traffic
106 counts that were done when the gas station across the street was developed. He felt the
107 volumes were not far off. He noted even if the high level from the gas station was taken,
108 it would still result in level of service B or C – Mr. Kelly felt there could be some delay
109 but did not feel this development could push it into failing movements. This concluded
110 Mr. Kelly’s presentation.
111

112 Attorney Kelly Dowd addressed the Board and asked Mr. Kelly whether he noticed any
113 decline in level of service at any of the intersections. Mr. Kelly noted if the current wait
114 today at a signal is 26 second, with this project in place it could increase to 28 seconds
115 but it is not a significant impact.
116

117 Attorney Dowd went on to address concerns raised in the staff report. With respect to
118 snow storage, this site has a contract for snow removal and this project would be part of
119 that contract.
120

121 In regards to the traffic study, Attorney Dowd went on to say this is a commercial
122 building constructed on specifications, there is no leaseholder, and it is undetermined
123 what commercial entity is going to occupy this space. As a result, the applicant has taken
124 a conservative approach to traffic study by taking into consideration two intensive uses –
125 Dunkin Donuts and a bank. The attorney stated there will be no change to the level of
126 service at the intersections.

127
128 He referred to the staff recommendation for 260 feet of sidewalk to be constructed.
129 Attorney Dowd noted this would be close to 50% of the entire construction cost to the
130 project. He also noted it has been suggested that the applicant pay for a traffic light at the
131 intersection of Island Street and the Rail Trail, which would be at a cost of about
132 \$18,000. Attorney Dowd pointed out there is already an existing sidewalk on the west
133 side of Island Street. There are also three crosswalks; on West Street, at the Rail Trail,
134 and one south of that location. He felt pedestrians as a result would not have an issue
135 accessing this site via a sidewalk. Hence, the applicant does not see any rationale for
136 adding more sidewalks to this site.

137
138 Next he referred to the intersection at the Rail Trail, which also has a sidewalk. Attorney
139 Dowd stated it is unclear to the applicant how adding a bank and a Dunkin Donuts at this
140 site would change bike and pedestrian traffic to the extent that a lighted crossing system
141 would need to be added at the expense of the applicant. He also stated the applicant is
142 also concerned about the request to extend the Rail Trail through their property.

143
144 Attorney Dowd went on to say this is the third project Brady Sullivan has brought before
145 this Board unsuccessfully. This project has no zoning issues and is consistent with what
146 exists in the area. He went on to say the applicant is concerned with the city's request,
147 which would cost half as much as it would cost to develop the entire building.

148
149 Community Development Director Rhett Lamb stated that the recommendations to add a
150 beacon at the bike path, the sidewalk extension, and the pedestrian connection into the
151 site came from the applicant's traffic consultant. He felt it was inappropriate for the
152 applicant to say staff came up with suggestions in order for this project to be approved,
153 which he indicated was not the case. Attorney Dowd responded by saying there has been
154 concern raised about pedestrian access on site and noted the applicant will be locating
155 pedestrian connections through the islands.

156
157 Chair Barrett stated he was surprised at the cost of constructing 160 feet of sidewalk and
158 asked what this estimate was based on. Attorney Dowd stated they do not know who the
159 final tenant is going to be. Mr. Pearson added the sidewalk length is 265 feet. Mr. Charles
160 Panasis of Brady Sullivan stated the cost of sidewalk is approximately \$85 per foot to
161 construct. With reference to the issue with the sidewalk, he noted items 9 – 11 (which
162 included the sidewalk) came from the city, but it were inadvertently added in with the
163 Traffic Report.

164
165 Chair Barrett questioned the price of the building construction. Mr. Penasas stated it
166 would be close to \$500,000. It was calculated the sidewalk would be at a cost of nearly

167 \$200,000. Chair Barrett reiterated based on his calculation it was close to \$90,000.
168 Attorney Dowd stated even if the cost of the sidewalk was \$60,000, based on the
169 commercial establishments that surround this area, he did not feel it was appropriate to
170 place this burden on the applicant.

171
172 Ms. Russell Slack asked to view the area for this sidewalk. Mr. Pearson stated it would be
173 north of where this project is going in.

174
175 Staff comments were next. Ms. Kessler addressed the Board, and went through the
176 Planning Board's Development Standards that are relevant to this application.

177
178 Drainage – The applicant is proposing two bio-retention systems on site and their
179 drainage report indicates they are meeting city standards and indicates there will be no
180 increase of volume or velocity leaving this site due to this proposal.

181
182 Snow Storage – Applicant's proposal is to remove snow from the site.

183
184 Landscaping – The applicant has indicated some trees will be removed on site, but
185 overall, there will be an increase in the number of trees on site. There is a landscape
186 island, which was previously landscaped, and these trees were removed to install a storm
187 drain and the applicant has agreed to re-install these trees. Should the Board be inclined
188 to approve this application, staff would suggest the applicant provide a revised plan sheet
189 showing the installation of those four trees that were removed.

190
191 Screening – The applicant proposes to install a dumpster in the southeast corner of the
192 site with a chain link fence. Staff recommend that the submittal of a revised dumpster
193 screening detail to show a wooden fence to match the proposed building, which meets
194 Board standards, be a condition of approval.

195
196 Utilities – Ms. Kessler said that staff would need the size of the water and sewer
197 connection prior to signature of the plan. The Applicant has agreed to submit this
198 information.

199
200 Lighting – Staff has received a photometric plan, but this plan does indicate minor light
201 trespass at the property line. The Applicant is working on bringing these light levels to
202 meet the standard.

203
204 Architecture and Visual Appearance – This would be a two-story 4,875 square foot multi-
205 tenant retail and office building. The plan is for three tenants to occupy the building, each
206 with its own entrance. The exterior would be brick façade with EIFS board and metal
207 system.

208
209 Comprehensive Access Management – Ms. Kessler stated she was first going to focus on
210 vehicle traffic on site. She noted this is a proposal that is a first for the city; one building
211 with two drive-through businesses, The drive-through on the northern side is being
212 speculated to be a coffee shop. The drive-through to the south is being speculated to be a

213 bank. Ms. Kessler noted there is insufficient space on site for vehicles to queue between
214 the two drive-thrus. She noted there is only space for one vehicle to be able to queue and
215 wait. Any additional queuing would cause conflict with parking adjacent to it on the left
216 side.

217
218 The other concern staff has with queuing of vehicles is the conflict they might have with
219 the exiting northern drive-through lane. She noted staff had discussed safety concerns for
220 pedestrians navigating through the site and staff has not seen a revised plan to address
221 this issue.

222
223 With respect to offsite traffic accessing the site, one of the recommendations in the traffic
224 report submitted by the Applicant was the installation of a flashing beacon at the Rail
225 Trail and Island Street intersection. This flashing beacon is currently shown on the
226 proposed site plan. However, in a recent conversation with the applicant they indicated
227 that they are not interested in making this improvement. Should the applicant not be
228 interested in this installation, this item would need to be removed from the site plan prior
229 to signature. Ms. Kessler further stated staff will not be pressing on the issue of the
230 installation of a sidewalk, but felt the Board may wish to consider this as a topic of
231 discussion. The other item included in the traffic report was the installation of hi-
232 visibility bicycle crossing warning signs at this intersection. With that, Ms. Kessler
233 turned the presentation over to Mr. Lamb

234
235 Mr. Lamb stated in talking with the Brady Sullivan team, staff had questions regarding
236 how counts were collected with respect to the numbers as the pandemic took hold in the
237 area. He indicated staff raised this question because the level of service at the intersection
238 and along West Street were numbers he was not familiar with and felt the numbers were
239 being under represented. Mr. Lamb stated the Brady Sullivan team was advised of this
240 and they understood the concern raised. The report today breaks down the traffic impact
241 based on this development and felt this was the right thing to do. Regardless of whether
242 the level of service is currently B or C probably does not have a significant effect. He
243 noted he wanted to make sure the data was adequate as a starting point, which is critical
244 in deciding whether the project will have any impact that needs to be mitigated.

245
246 Vice-Chair Cusack referred to the circle shown on the northern portion of the drive-
247 through, which also has a passing lane where both cars will face Island Street. The Vice-
248 Chair asked which way these vehicles will turn. He also noted on the top of the page
249 there are two competing one-way lanes. The Vice-Chair stated he was concerned about
250 the internal traffic pattern. Ms. Kessler noted there would be a one-way sign installed at
251 the end of the northern drive-thru as well as at the southern drive-thru.

252
253 Mayor Hansel raised a concern about the number of cars in the queue and questioned if
254 there were industry standards for this type of use. He said that if too many cars are added,
255 then eventually there is going to be an issue. Mr. Shawn Kelly explained they have
256 modeled the southern drive-through as a bank and not for restaurant use. He indicated
257 they plan on having a window at this location just for teller activity and another window
258 for ATM use to move vehicles along. He indicated they do not expect the southern

259 portion to extend into the parking area. With respect to the northern portion, a coffee
260 shop is busiest in the morning, the average max queue is about nine and the second lane
261 is a bypass lane but agreed to turning the bypass lane into a right turn only to eliminate
262 the conflict of two vehicles turning the same way.

263

264 Mr. Lamb stated this discussion with respect to traffic should also take into consideration
265 where any pedestrian crosswalks would go. He went on to say the Board has two
266 standards that relate to traffic; one has to do with impact to neighboring streets and
267 intersections – he referred to the Dunkin Donut site on Avon Street where traffic
268 constantly extends into the right of way. The Avon Street site plan was approved by the
269 Planning Board based on a traffic report from their traffic consultant and
270 recommendations from the owner that the queue will never get that far. He indicated
271 there is not much concern with the public right of way in this case as there is sufficient
272 internal capacity. However, when it comes to the Comprehensive Management Standard,
273 access for vehicles need to be addressed as well pedestrian movement.

274

275 Ms. Kessler referred the Board to the language from the Comprehensive Management
276 Standard as follows:

277 *“Driveway locations, sidewalks, crosswalks, bicycle/pedestrian paths and accessible*
278 *paths of travel shall be considered as an integral design feature of every development*
279 *proposal. Where appropriate, connections shall be made for the continuation of*
280 *sidewalks, walkways and bicycle lanes within the property, between adjoining properties,*
281 *and site amenities shall be installed such as bicycle racks, benches and bus stop*
282 *shelters.”* Ms. Kessler noted the Board standards are very clear about adhering to safe
283 passage for pedestrian travel throughout a site.

284

285 Mr. Pearson stated their plan was to work with the city to add a crosswalk and bike rack
286 to the main drive to the island and back to the building.

287

288 The Chairman asked for public comment next.

289

290 Mr. Anthony Mastronardi of 70 Island Street addressed the Board. Mr. Mastronardi
291 stated he has one request; he would like the construction of a six-foot fence along the
292 southern boundary. He indicated when he constructed his property (when Emile Legere
293 owned the Colony Mill), the Planning Board requested Mr. Mastronardi to locate a fence
294 on his side of the property (south side). He explained a fence would address three things;
295 stationary lights and head lights coming towards his property, as well as the debris that
296 has been at this location for a few years, and lastly one of his tenants treats children with
297 disabilities and he does to want them wandering into a traffic lane.

298

299 With no further comment, the Chairman closed the public hearing.

300

301 Vice-Chair Cusack asked for comments on the request for a fence. Ms. Kessler stated
302 there is a fence requirement under zoning in a residential district. This use, however, is a
303 commercial use in the Commerce District. She noted this item was raised with the
304 applicant who has agreed to install landscaping consisting of a mix of evergreen shrubs

305 and crabapple trees along this edge of the property. She added it would not be outside the
306 Board's purview to request a fence to block vehicular headlight impact on adjacent
307 properties.

308

309 Vice-Chair Cusack went on to say his concern is with Comprehensive Access
310 Management – he noted the Board is not specifically approving a bank and is not certain
311 what use might go into this property. He stated he has concerns about internal movement
312 of traffic.

313

314 Mr. Lamb stated with respect to pedestrian access, it has been stated today that
315 crosswalks and walkways could be added to the plan and asked the Board to look closely
316 at this. He further stated his concern is the Board discussing changes to a plan no-one has
317 seen yet and cautioned the Board moving forward giving staff the authority to approve
318 significant changes to the plan.

319

320 Mayor Hansel said that with respect to queuing, it seems there is no danger this could
321 impact the public right away but felt there could be internal conflicts with tenants and
322 asked whether there was a different way this should be looked at by the Board. Mr. Lamb
323 stated some of the Board standards clearly address public improvement and
324 infrastructure. Many of these are also oriented to what is happening on a site, drainage for
325 instance. When done properly, drainage will not impact off site uses. In the same manner,
326 if traffic is addressed correctly, it will not impact off site uses. Mr. Lamb went on to say,
327 Standard 12, Comprehensive Management is built around the idea of safe access of
328 pedestrians and vehicles and how they get on to a property.

329

330 Chair Barrett referred to the southern tenant and what exactly that use would be; it has
331 been stated if this were to be a coffee shop as the applicant's representative has indicated,
332 they would need to look at this use. With respect to the sidewalk, he indicated he
333 understands the applicant's position that this is not their responsibility but felt it was not
334 helpful to have such an over statement of the cost for construction of this sidewalk. He
335 also noted the flashing light that would need to be removed from the site plan.
336 He went on to commend the landscaping being proposed, he stated he likes the idea this
337 project will bring vitality to the area and would bring a service to the tenants.

338

339 Vice-Chair Cusack asked if this plan was to be approved whether the Board will be
340 approving a plan without showing crosswalks on this plan. Chair Barrett stated he would
341 not be comfortable approving something that is not shown on the plan. Mr. Lamb stated it
342 would be outside the Board's authority to approve the plan without walkways and
343 crosswalks already shown on the plan. Mayor Hansel stated he sees a crosswalk on the
344 western side of the site and questioned whether this was not the applicant's plan. The
345 Chairman stated what is being referred to are additional crosswalks. Mr. Lamb agreed it
346 was more pedestrian connections from Island Street; what is shown on the plan currently
347 is a crosswalk to the middle of the accessible parking spaces.

348

349 Mayor Hansel clarified whether a resident of the Colony Mill was to access these
350 businesses, the preferred route would be to go out to Island Street, down the sidewalk and

351 utilize the crosswalk. If they walk through the parking lot, they would be walking in front
352 of the queue. Mr. Lamb stated this is not something that has been decided, and would be
353 something the applicant would need to decide and come up with a route. Ms. Kessler
354 added if you are a resident of the Colony Mill, the question is how you can safely access
355 the southern portion of the site. Because of the gap in the sidewalk, the safe method
356 would be to cross over at the intersection at Island and West Street, come down Island
357 Street and use that crosswalk on Island Street.

358
359 Chair Barrett asked for clarification about what is being referred to as additional
360 crosswalks within the site. Would this be crosswalk markings located within the parking
361 area instead of constructing the 265 foot sidewalk on the east side of Island Street? Ms.
362 Kessler noted as was stated by Mr. Lamb, staff would be requesting the applicant to
363 demonstrate how this area has a safe passage of travel for visitors to the site. The
364 Chairman asked what the timeframe has been with discussion between staff and the
365 applicant regarding some of these items. Ms. Kessler stated comments with respect to
366 vehicular conflict within the site was raised in a Memo a week and half after the
367 application was submitted. Since that time, staff has been working with the applicant on
368 revised plan sets to address these questions. She noted the timeframe is always pretty
369 tight for the applicant and staff for review and comment of application; Friday before this
370 meeting was when follow-up comments were provided to the applicant on the revised
371 plan. Ms. Kessler went on to say the applicant was made aware of pedestrian safety,
372 conflict with drive-through queuing, and access to the site from the surrounding sidewalk
373 network early on in the process.

374
375 Mayor Hansel stated he would like to see a solution as to how pedestrians can safely
376 move through the northern portion of the site. He noted there is a lot of parking in this
377 area.

378
379 The Chairman felt there were enough concerns raised that would make it difficult for him
380 to approve this application tonight.

381
382 Ms. Sommers asked whether the Board had enough information to provide a conditional
383 approval. Chair Barrett stated with some of these issues, particularly pedestrian access,
384 by approving this application conditionally, the Board would be asking staff to approve
385 something the Board has not seen. He asked if specific conditions were placed, whether
386 staff will then be comfortable. Mr. Lamb stated, as the Board gets clearer about what it
387 wants, it becomes easier for staff to take on the authority administratively. However, not
388 having seen a solution to pedestrian access – it is too unknown to ask staff to take this on
389 administratively.

390
391 Mr. Burke stated he would like to see this property developed but there are way too many
392 questions that need to be clarified before it can be approved. He suggested continuing the
393 application for a month.

394
395 Ms. Russell Slack felt there were too many outstanding issues for her to be able to cast a
396 vote tonight and suggested continuing it for a month.

397

398

C. Board Discussion and Action

399

A motion was made by Pamela Russell Slack that the Planning Board continue this application to the August 24 Planning Board meeting. The motion was seconded by Councilor Remy.

402

403

Vice-Chair Cusack stated he would like to see the crosswalk, the fence along the south side, and new numbers for use of the southern drive-through at the meeting next month.

405

406

Councilor Remy thanked the applicant for coming up with this great plan but agreed there were some outstanding issues that would need to be answered.

407

408

409

The motion made by Pamela Russell Slack carried on a unanimous roll call vote.

410

411

Mr. Orgaz rejoined the session.

412

413

IV. Community Development Director Report

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Mr. Lamb stated he would like Ms. Kessler to address the public outreach meetings for Building Better Together. Ms. Kessler stated the draft Land Development Code document will be reviewed at the Joint Committee session on August 10th and stated she would be sharing some tools with the Board and public to navigate the draft. August 12 (at noon) and August 13 (6:30 pm) would be zoom sessions the public could join in. She noted the public has ways to provide comment and raise questions with staff online or arrange to schedule a one-on-one interview with staff. Staff will also be reaching out to the development committee during that week. Mr. Lamb recognized all the efforts Ms. Kessler has put into this document.

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424

The meeting adjourned at 8:35 PM.

425

426

Respectfully submitted,

427

428

Krishni Pahl

429

Minute Taker

430

431

Reviewed by and edited by Mari Brunner, Planner

432

433

434

FROM: Rob Hitchcock
381-4667

\$50.00 fee required, check payable to the City of Keene

1/5/88

**City of Keene
New Hampshire**

Date: 8/13/2020

Location of Proposed Driveway: NW End of Wheelock St. (attach map showing location)
(Street Address)

Description of Proposed Action: Open existing gate for School Bus (attach diagram
only access in the morning and afternoon. The gate will otherwise be closed showing dimensions
and drainage)

Name of Owner: SAU 29
Address: 193 Maple Avenue, Keene NH
Daytime Phone: (603) 357-9005

For City Use Only:

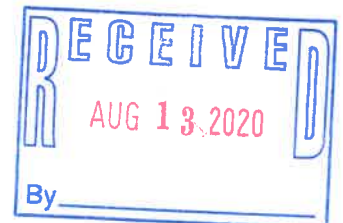
Reviewed by City Engineer: _____ Date: _____ Signed: _____

Denied _____ Approved _____ Approved w/conditions _____

- Culvert required under driveway.
- No drainage is to be allowed from the driveway onto the City of Keene right of way.
- Driveway not to block gutter flow in street.

Other: _____

Planning Board Approval Date



Symonds Elementary School
Keene, NH

Narrative - Driveway Permit

August 13, 2020

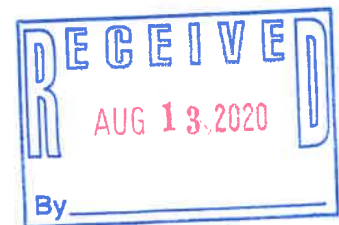
To reduce congestion and improve safety at Symonds Elementary School the School would like to separate the bus traffic from the parent drop off traffic. The proposal is to open the existing gated access at the west end of Wheelock Street for use by buses only, of which there are 4 or 5. The gates would be manually opened and closed by School staff. The gate will be closed at all times other than to allow bus access for morning drop off and afternoon pick up.

All parent drop off traffic will remain on the Wheelock Park side of the School.

Wheelock Street is a legal City street measuring 20' to 22' wide depending where you measure, with a 4' asphalt sidewalk on the south side only. The sidewalk is setback from the Street pavement by a 4'+/- strip of grass. The grade of Wheelock Street is relatively flat, the alignment is straight, and the existing sight distance far exceeds the 200' requirement. There are no changes proposed to Wheelock Street.

The School recently constructed fencing and sidewalk for a designated bus drop off area, entirely on School property. The fencing installed by the School accommodates and separates the children using the sidewalk and the playground from the bus traffic. The turnaround area is large enough to accommodate both full size school buses and the Keene Fire Department ladder truck. The stormwater from the bus turnaround remains on-site.

This proposal meets all the measurable standards required by Section 70-135.- Driveway permit and standards.



SVE Associates

Engineering * Surveying * Landscape Architecture * Planning
PO Box 1818, Brattleboro, VT 05302 Phone: (802) 257-0561 E-mail svek@sveassoc.com

LEGEND

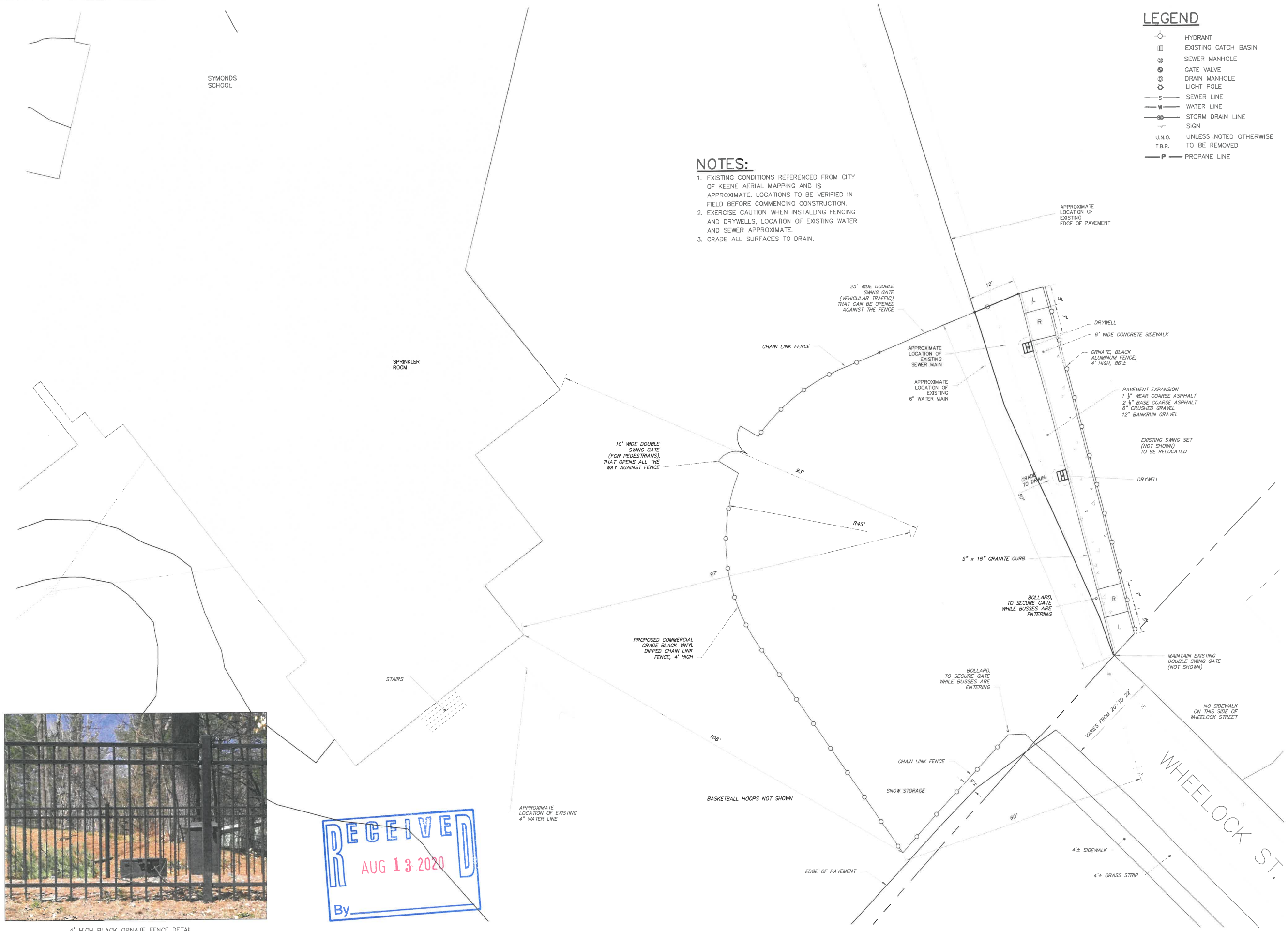
- HYDRANT
- ⊠ EXISTING CATCH BASIN
- ⊙ SEWER MANHOLE
- ⊙ GATE VALVE
- ⊙ DRAIN MANHOLE
- ⊙ LIGHT POLE
- S — SEWER LINE
- W — WATER LINE
- SD — STORM DRAIN LINE
- SIGN
- U.N.O. UNLESS NOTED OTHERWISE
- T.B.R. TO BE REMOVED
- P — PROPANE LINE



0 10' 20'
SCALE: 1"=10'

NOTES:

1. EXISTING CONDITIONS REFERENCED FROM CITY OF KEENE AERIAL MAPPING AND IS APPROXIMATE. LOCATIONS TO BE VERIFIED IN FIELD BEFORE COMMENCING CONSTRUCTION.
2. EXERCISE CAUTION WHEN INSTALLING FENCING AND DRYWELLS, LOCATION OF EXISTING WATER AND SEWER APPROXIMATE.
3. GRADE ALL SURFACES TO DRAIN.



REV.	DESCRIPTION	DATE
1	MISCELLANEOUS	08/11/20

DESIGNED BY: LPS
 CHECKED BY: RH
 DATE PLAN: 08/14/19
 DATE SURVEY: N/A
 PROJECT NO.: K2854
 CAD NO.: K2854 CONCEPT 7

SVE Associates

Engineering
 Surveying
 Landscape Architecture
 Planning
 439 West River Road
 Brattleboro, VT 05302
 Phone (802) 257-0561
 Fax (802) 257-0721
 website: www.sveassoc.com

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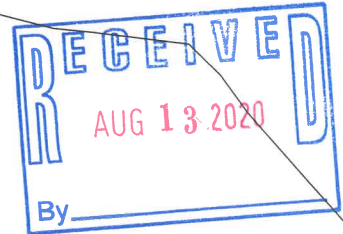
Project:
SYMONDS ELEMENTARY SCHOOL
 79 PARK AVENUE
 KEENE, NEW HAMPSHIRE

BUS ROUTE
 CONCEPT #7

SHEET
C-7



4' HIGH BLACK ORNATE FENCE DETAIL





City of Keene, N.H.
Transmittal Form

October 15, 2019

TO: Mayor and Keene City Council

FROM: Councilors Robert O'Connor & Randy Filiault

THROUGH: Patricia A. Little, City Clerk

ITEM: C.9.

SUBJECT: Councilors O'Connor and Filiault - Proposed New Bus Route through the Wheelock Street, Newman Street and Pine Avenue Neighborhood

COUNCIL ACTION:

In City Council October 17, 2019.

Public meeting is being sponsored by Symonds School and is scheduled for October 23, 2019 at 6:00 PM.

Referred to the November 13, 2019 Municipal Services, Facilities and Infrastructure Committee.

ATTACHMENTS:

Description

Communication - Councilors O'Connor & Filiault

BACKGROUND:

Councilors O'Connor and Filiault are requesting that the City Council review the proposed new bus route through the Wheelock, Newman and Pine Avenue neighborhood. An accompanying letter as well as a petition articulates their specific concerns.

ROBERT J. O'CONNOR

382 Park Avenue Keene, NH 03431

October 07, 2019

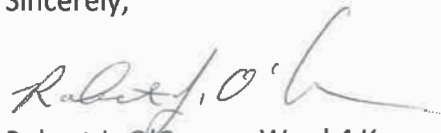
Dear Mayor and City Council,

I am writing this letter to address concerns presented to me by several residents from Wheelock Street, Newman Street and Pine Avenue concerning a proposed new bus route through this neighborhood. These residents would like this new change to be studied by the city as they feel it's a safety concern for the neighborhood with increased bus and vehicular traffic.

I'm requesting that the Municipal Services, Facilities and Infrastructure Committee have the opportunity to review these changes at the next MSFI committee meeting. Please see the attached letter from these concerned citizens.

Thank you for your time and consideration in this matter.

Sincerely,



Robert J. O'Connor-Ward 4 Keene City Councilor



Randy L. Filiault-Keene City Councilor at Large

In City Council October 17, 2019.

Public meeting is being sponsored by Symonds School and is scheduled for October 23, 2019 at 6:00 PM.

Referred to the November 13, 2019 Municipal Services, Facilities and Infrastructure Committee.



Assistant City Clerk

Wheelock St, Newman St. and Pine Ave. Neighbors
Keene, NH 03431

October 7, 2019

Robert Malay
NH SAU 29
193 Maple Ave
Keene, NH 03431

CC: Mayor Kendall W. Lane - mayor@ci.keene.nh.us

CC: Principal Richard Cate - rcate@sau29.org

Dear Superintendent Malay;

I am writing this letter on behalf of a group of concerned residents in the Pine Avenue/Wheelock Street area to request a meeting to further discuss the impending changes to the school bus route. In addition to our concerns about the rapid timeline and limited communication which has marked this process, this group of residents have significant concerns with regards to the safety of both the students and residents. While you and Mr. Cate have received multiple inquiries from concerned neighbors, it seems critical to summarize these issues prior to the desired meeting.

The original letter from Mr. Cate, dated September 26, 2019, arrived less than one week before the scheduled meeting held on October 2, 2019 and was only sent to residents directly along the proposed route. This was insufficient time to allow residents to shift already busy work schedules in order to attend. The letter states *"after several years of studying and exploring alternatives, we have decided to create a bus circle and route our 5 school buses through Pine and Wheelock Streets."* Several of our concerned residents have owned their homes for multiple years. However, regardless of whether school parents were informed of these issues, area residents were not apprised nor was our input sought. Given we are intimately aware of potential safety issues in the neighborhood, it would seem prudent to survey the residents about their observations and concerns prior to deciding on a solution. Furthermore, it has come to the attention of our group that the school has been aware of this impending change for at least a few months. At no point during this process were the pros and cons of various plans discussed with area residents nor was the schematic proposed or presented until the meeting on October 2. The lack of communication alone seems justification for pausing the construction and allowing the concerns of the residents to be thoroughly considered and addressed.

As you are now no doubt aware, our group has several safety concerns we wish to have addressed during the desired meeting and which bear repeating at this juncture:

1. Wheelock Street is too narrow for the congestion which will be caused by the presence of five school buses. Given the location and apparent narrowness of the proposed circle, will buses have to queue down Wheelock Street as they wait for a turn to leave students by the new playground fence? If so, this will make Wheelock Street challenging at best and very likely impassable with the added presence of snowbanks.
2. Additional large vehicle traffic and congestion will make it more difficult for students to walk safely to and from school. Specifically, there are a large amount of school children walking daily from

Newman St. with no sidewalk and are now crossing Wheelock St. to the sidewalk and have been witnessed to almost being hit due to parents dropping and picking up students and other residential traffic. There is no longer even a student crossing guard at this intersection and current traffic speeds down Pine Ave. Between the increased congestion, the narrowness of the street, the height of New Hampshire snowbanks, and now the anticipated 5 school buses, this is a serious safety issue for little pedestrians walking to school.

3. Will buses be allowed to block driveways? Even if this is not allowed, who is liable for accidents caused by difficult sight lines due to large school buses? Should an accident occur, who will be responsible for subsequent increases in car insurance and medical expenses which would not have occurred had a more feasible plan been developed?
4. The flow of traffic will be heavily impacted every morning and afternoon, by several buses looking to exit from Pine St. to Park Ave. As you know buses of that size need a bigger gap to be able to pull out into Park Ave. which will cause extensive delays to residents to get places on time.
5. While Mr. Cate has stated that parents will no longer be allowed to leave their children at the back gate of the school, area residents are concerned that this behavior will continue. Parents have been seen leaving their cars at the back gate while residents attempted to navigate to get into the street so that they could leave for work. Another parent left his/her vehicle at the end of a resident's driveway for 30 minutes while a resident was waiting to leave to attend a doctor's appointment. The parent was unrepentant and uncaring when the issue was addressed. Because of the history of these issues, we are highly doubtful that the parents of your students will follow traffic plans appropriately and as expected. At this time it is also legal to park along Wheelock St. To our knowledge, no one has contacted the city about an ordinance for no parking during school drop off or pick up with a fine. So unless this becomes enforceable it will continue.
6. The corner of Wheelock St. and Pine Ave. already has traffic moving too quickly for pedestrian and vehicle safety. The residents at 17 Wheelock St. have difficulty navigating out of their driveway safely during snowy winters as the snowbanks create a blind corner, the icy puddles creates dangerously slippery surface, and this will now be more dangerous with the presence of passing school buses. Furthermore, due to incorrect road grading, there is often a large puddle or icy area at the end of that driveway for much of the winter and into the spring.
7. Galen Speigler, a young man with a physical disability, currently resides at 16 Wheelock St, located on the corner of Pine and Wheelock. As there are no sidewalks on his side of Pine Ave, he currently has to swing out into the street when he is traveling in his motorized wheelchair. Considering the narrow road, the presence of walkers, and the large buses, this is another serious safety issue.
8. Many of us have older homes which need frequent maintenance and as stated before, our streets and driveways are narrow. If there is any prohibition of parking in the street, where are trucks and large machinery expected to park, if it is not in the street? Are the residents expected to vacate during the day(s)/week(s) while work is being done? Who is expected to pay the additional hotel and/or parking bills so that the school can have access to our entire street(s) at the allotted time? Residents cannot be expected to foot an additional expense so that the city and school can appropriate our street.

9. One of our residents has contacted an area realtor who informed us that such a change may have a negative impact on our property values. Should this occur, shall we expect compensation from the school district? And an abatement of our property taxes? Or was the city planning to push this through without due consideration of the homeowners and residents...hoping that we would be none the wiser and thus continue paying our taxes as currently assessed? I pose that disagreeable (and hopefully incorrect) possibility to make this point: at the very least, given even a remote possibility that our greatest asset—our homes-- may lose value, the school district has/had an obligation to explore every alternative AND to discuss them with area residents in an atmosphere of transparency and collaboration.

While we respect Mr. Cate's statements that all possibilities were explored, we believe that there are indeed alternatives which are safer and more desirable than the current plan. The proposed bus circle appears to be located near an old driveway which seems to have been used to access the old front door of the building. It is unclear why this driveway, located closer to the current bus circle, is not being used as an alternative to the unsafe congestion which will be created by changing the bus patterns. Furthermore, there is a large parking lot available at Wheelock Park. As this is already city property, it is unclear why this is not being considered as a prospective resource which could be utilized.

Given that the new bus route (and the construction of both the bus circle and the fence) are likely to remain a permanent part of the Symonds School transportation arrangement for the foreseeable future, doesn't it make sense to make certain that it is the best possible arrangement for all affected? Given that the school is part of the larger neighborhood community, doesn't it make sense to move forward to repair the mistaken impressions from this lack of effective communication and collaborate on a solution which bears the best interests of all involved?

We hope to hear from you shortly and we look forward to resolving these concerns with administrators.

Respectfully,

The Wheelock St., Newman St. and Pine Ave. residents

Signatures in next page

Wanda Chumari 44 Wheelock St
 Wanda Chumari
 58 Russell St Keene NH 03431
 42 Park Ave Keene NH 03431
 7116r

Name	Address & Email	Signature
Jen English	29 Pine Ave	Jen English
Dave Cannon	21 Wheelock St	Dave Cannon
Marilyn Beardon	26 Wheelock St	Marilyn Beardon
Tim Campbell	7 Newman St	Tim Campbell
Annie Braley	12 Newman St	Annie Braley
James Allmair	14 Newman St	James Allmair
Christie Hand	18 Newman St	Christie Hand
Jeffrey Ellert	19 Newman St	Jeffrey Ellert
Kayleigh Hart	24 Newman St	Kayleigh Hart
Jean Durbin	25 Newman St	Jean Durbin
Russell Durbin	69 Pine Ave	Russell Durbin
Liz Bleck	62 Pine Ave	Liz Blanchard
Eli Rivera	55 Pine Ave	Eli Rivera
Mike Brown	53 Pine Ave	Mike Brown
Kim Drone	54 Pine Ave.	Kim Drone
Andrea Wright	34 Wheelock St.	Andrea Wright
Hilda de Moya	47 Pine Ave	Hilda de Moya
Roland J Bergeron	41 Pine Ave	Roland J Bergeron
ALISON FERRA-MATAR	39 Pine Ave	ALISON FERRA-MATAR
Laura Mack	21 Pine Ave	Laura Mack
Mary Granger	19 Pine Avenue	Mary Granger
Lauren Power	10 Pine Ave	Lauren Power
Ben Pierce	10 Pine Ave	Ben Pierce
Nicole Bondle	18 Fairview St.	Nicole Bondle
Carah Chavira	41 Russell St.	Carah Chavira
Beth Silegy	14 Nelson St	Beth Silegy

On a vote of 4-0, the Municipal Services, Facilities, and Infrastructure Committee granted more time to allow staff time to research the records of the Planning Board and the City Council.

2) Councilors O'Connor and Filiault – Proposed New Bus Route through the Wheelock Street, Newman Street, and Pine Avenue Neighborhood

Councilor O'Connor stated that he was contacted by the neighborhood group around Symonds School about the proposed bus route change and the crosswalk. Regarding the new bus route, neighbors are concerned about an increase in traffic, future snow removal, and the safety of the children walking to and from school. There has been a meeting with Symonds School, open to the public, which went well. The Superintendent decided not to go forward with the new bus route until this meeting happened.

David Calhoun, of 21 Wheelock Street, stated that he wants to give an update on what transpired in the local community. He continued that he would first read a letter he received from Symonds School Principal Richard Cate, on September 26, 2019:

“This letter is to provide notification of an impending change in traffic flow that will occur in early November, impacting the Pine and Wheelock Street area. You are invited to attend a meeting Wednesday, October 2, at 6 PM in the school library, at which time further details will be provided, should you desire them. Symonds School was originally constructed in 1928 and at the time vehicular traffic was almost nonexistent. Over the years we have struggled to create traffic patterns that are safe for our children and provide access to parents by keeping cars, school buses, and pedestrians separate as possible. After several years of studying and exploring alternatives, we have [Mr. Calhoun stated that he assumes ‘we’ is ‘SAU 29’] decided to create a bus circle on our playground and route our five school buses through Pine and Wheelock Streets. The buses will pick up and drop off students in this newly-created bus circle. The new bus circle will only be for school buses. The existing gate at the entrance to Wheelock Street will remain closed except for brief periods in the morning and afternoons to allow buses access to the circle. The circle on the playground will be fenced to keep play areas and bus circles separate. The circle area in the front of the school will become an area for parent drop-off and pick-up, creating a safe environment for our children. If you would like more details and have any questions concerning this, please attend the meeting October 2 from 6 to 6:45 in the school library.”

Mr. Calhoun stated that this was the first notification anyone in the neighborhood had received saying that this was even a possibility. He continued that he was the only attendee at the meeting referenced in the letter. The idea was to hear and discuss what the potential was. Since he was the only one, he figured everyone else in the neighborhood was happy with the plan. He found out the next morning he was the only one given the letter. The other parents and pedestrians in that area were not contacted. They found out later, after that meeting, about the proposal. It was a more than a proposal. Construction started two days after the meeting. They were upset not to be informed. They put together a petition to see if they could halt any further progress.

They were informed the school would finish the project because money was already made available for it, which surprised him. On October 23rd about 30 or 40 people met at the library to discuss concerns. They feel that this is not a cure to the problem, it is not helpful, and it is a safety issue for the children as well. About 30 or 40 children walk the streets in this area each morning to go to school. They are requesting a review of this to see if there is a way around any kind of option at the school. It is a school problem but it has been made a community problem because it will affect the local area.

Mr. Calhoun continued that he is asking the MSFI Committee to see if things could be done differently and to ask if the proper channels have been gone through. It seems like this was done in very little time.

Councilor Manwaring stated that the Councilors present this evening will ask questions and then staff will comment and then the public can speak.

Councilor Hooper asked if it is correct that the bus circle has been completed. Mr. Calhoun replied yes.

The City Attorney, Tom Mullins, stated that people from the Keene School District (KSD) are here, too. He continued that Chair Manwaring might want to call on them.

Chair Manwaring asked if there are sidewalks on Wheelock Street. Mr. Calhoun replied on one side, yes, and on one side of Pine Avenue. He continued that there is none on Newman Street, which is adjacent to Wheelock Street.

Chair Manwaring asked to hear from the Keene School District.

Robert Malay, of 5 Morgan Lane, identified himself as the Superintendent of the KSD (Keene School District) and SAU 29. He stated that Symonds School Principal Richard Cate is here with him, as is Kenneth Dooley, Director of Buildings and Grounds, and they can provide more detail than he can. He continued that he would defer to them for questions.

Richard Cate, of Westmoreland, stated that he has been the principal of Symonds School for 40 years. He continued that the building was built in 1928 and at that point only had walking access. They added a wing in 1952, changing the entranceway. Access was through Wheelock Park. It has continued that way. As the school has grown in size, and particularly with the advent of modern transportation, increasing numbers of students are driven to and from school. Traffic congestion in the park has been a long-standing issue. There were a series of near misses starting in 2015. A student and a staff member were each bumped by a car, for example. The traffic is backed up in the morning and afternoon and is in gridlock.

Mr. Cate continued that he and other KSD staff have been looking at this situation for a long time. The Keene Police Department has sent staff from their Traffic Department out several times. There have been consultations. The bus company complained about the

situation because they could not even get in because of all the cars in the loop. He and KSD staff started to investigate and it took a couple years to settle on this option, with the bus circle out back. Budgets are tough; it took a long time to get the project included in the budget. In the spring, it was rumored that there was money to do this project. They were not sure it was funded until September. At that point, it became clear that the funds would be available. He noted Mr. Dooley sent him an email on about September 24, saying that they had the money and the project would start the next week. At that point, he sent the letter to abutters. He only chose the residents on the actual route of the bus and did not notify the rest of the neighborhood. Only one person showed up at the first meeting. Police have looked at the situation and believe that overall, and given the safety issues they have had at the front of the building, this is the safest alternative for the children. They looked at the issues. The project is taking place.

Kenneth Dooley, resident of Marlow, stated that the project is nearing completion. He continued that they did not find out until the end of the summer that they had funds for this project. The Keene Board of Education (KBE) directed them to complete the Symonds School circle. They had to move fast due to weather and contractor obligations. The circle will not be in use until further discussion.

Chair Manwaring asked if there has been parent or neighborhood input while the investigation of options was happening. Mr. Cate replied that the PTA has given input, but they had not had a neighborhood meeting. He continued that this project was a dream they had and they investigated different alternatives and felt this was the best one to make the situation better. It was the best way to separate buses from parent and pedestrian traffic as much as possible. There is a sidewalk that runs adjacent to Wheelock Street. It has been a dead end and no longer will be, at least for a half hour each in the morning and afternoon.

Chair Manwaring asked how many buses there are. Mr. Cate replied four or five. He continued that it varies; sometimes Special Education students are bused in. Next year there might be fewer buses once the school start time is changed.

Councilor Filiault asked if it is correct that the new bus route has not yet been implemented. Mr. Cate replied that that is correct. He continued that they have been following the same traffic patterns they have had for 20 years, until it is determined that this new situation is safe and everyone is satisfied.

The City Manager stated that the City reviewed this. She continued that the Police Department has been at the school. They thought that this would be an improvement to the safety. All that the City thinks needs to occur is for the KSD to go through the Planning Board to get a curb cut approved. That has not occurred. However, that would be an application that would need to come from the KSD. The City Council cannot make a motion and make the KSD do that. She has talked with the Superintendent and they are willing to do that.

Chair Manwaring asked what a curb cut does.

Mr. Blomquist, Public Works Director and Emergency Management, stated that the City became aware of this project at Symonds School when their contractor dropped construction plans off as informational. He continued that to give a brief history, in 1927 City Council discontinued Wheelock Street for about 500 feet to provide space for construction of the school. Staff did quick research of the 1949 flyovers – it appeared that there was an access point at the end of Wheelock Street but it did not appear to be used; all of the access appeared to be coming from Wheelock Park and that area. They looked at the 1984 flyovers. Those had a better indication of the gate and lock. Staff's opinion is that that has not been an access point for a very long time, if ever. The KSD did not realize that access to public ways requires a driveway permit, issued through RSA 236-13. The State has granted the Department of Transportation (NHDOT) Commissioner and the legislative bodies the authority to regulate access from private properties onto public ways.

Mr. Blomquist continued that in this case the City Council has delegated that authority to the Planning Board. The Planning Board issues all driveway permits through their site plan approval process or a permitting process, for which applicants apply. Some people say, "I've been here a very long time and never got a permit." When the RSA was adopted, the legislation said, "The Department of Transportation Commissioner or Planning Board shall retain continuing jurisdiction over the adequacy and safety of every existing driveway, entrance, exit, and approach to highways, whether or not an access was constructed or installed pursuant to a permit under this section." This means the Planning Board has authority to give a permit for these changes to Symonds School. The KSD would submit an application, and staff would review it and talk with the applicant if needed, and the Planning Board would review it at one of their meetings and issue a decision. Chapter 70 is the design standards that deals with design in Keene. Section 135 deals with driveway standards, mostly safety-oriented around the access point, looking at sight distance and that sort of thing. It is not a City Council process; it is a Planning Board process.

Councilor Lamoureux asked, if the Planning Board authorizes the curb cut, does that mean they can move the gate any time they want, or would that be part of the process? Mr. Blomquist replied that it depends on the conditions on the permit. He continued that the permit may restrict the times the gate can be opened. Today it may be opened and closed. The Planning Board does not look at just today; they also look into the future, because a driveway permit is a permanent right. There may be conditions after today. There might be a condition to say, "If you change the use you have to come back and see the Planning Board."

Councilor Filiault asked the City Attorney for clarification. Outside of the Councilors offering their opinions tonight, what legal structure do they have? Do they only have their personal opinions to offer? The City Attorney replied yes, essentially. He continued that that is why staff recommends the committee accept this as informational. It has to go to the Planning Board. Any discussion the committee has tonight is just

discussion. He encourages people from the neighborhood to attend the Planning Board meeting as well.

Councilor Filiault asked if the committee should make a motion to send this to the Planning Board. The City Attorney replied no, this is a little different. He gave an example of a past incident when it was appropriate for the committee to make a motion like that. He explained that this is different because it is up to the applicant to bring the matter to the Planning Board and file the application for the driveway permit. That is why staff recommends the committee accept it as informational.

Chair Manwaring stated that she heard tonight that people have concerns about children's safety, walking to school in that area. She asked Mr. Blomquist if the Planning Board would address that. Mr. Blomquist replied that he couldn't say what the Planning Board will address. He continued that for all driveway permit applications, they look at the conditions, and the effect on the area. If pedestrians are a concern, the Planning Board will discuss it. Their rules to follow are in Section 135.

Chair Manwaring asked to hear from the public.

Councilor Jones stated that the committee is hearing what has to be done with the KSD and the Planning Board but they are not addressing what the neighbors are asking for. He continued that they do not want the traffic coming through there. He wants to know if staff can suggest another way. Is there some kind of thing they can do in Wheelock Park, like a turnaround? He thinks the neighbors want another option, so that the buses are not using Newman Street. That is the heart of the matter, not whether to get a driveway permit.

Lisa Bowman of Russell Street stated that she has children at Symonds School. She continued that her concern is that when they did the study and gave statistics for walkers, it was grossly understated. It does not address the traffic going in and out of Wheelock Park and causing traffic jams. Children would still have to jump across the Wheelock Park entrance to get in and out of the school. With the new sidewalks going in, she wants to know if there is a plan to move the crossing guard to the entrance to Wheelock Park for the children's safety.

Chair Manwaring asked who does the crossing guards. The City Manager replied the Police Department. She continued that she does not know the answer but she knows they are moving the crossing as part of this new improvement. They are looking to take it a little bit away from that intersection and maybe that will address it. She thinks the crossing guard will be where they put the crosswalk, which is not as far down as Ms. Bowman is recommending.

Councilor O'Connor stated that he appreciates the neighborhood bringing this forward. He continued that he also commends Mr. Cate who has been the principal for 40 years doing his own studies and trying to figure this out. It is dangerous out front. Trying to come up with a plan is important. He went on to suggest an official traffic study to be

done. Mr. Cate did his own and has notes and ideas, but they should have an official one, with a combination of City staff and KSD staff to appease both sides and come up with a compromise. The City Council is hearing a lot about neighborhoods and safety lately. These conversations have led to changes. The MSFI Committee has a decision to make. They should look at the whole situation and what is happening in this neighborhood. Maybe staff can do something like what Councilor Jones mentioned.

Chair Manwaring asked to hear from Mr. Blomquist, asking if he can come up with any other ideas. Mr. Blomquist replied that they have professional staff that can assist, but in many ways, this is like any other private property owner who is doing something to their own property, following the regulations of the City. If the KSD wants to chat with the City, they are open to that, like they are with any property owner.

Councilor Filiault stated that this is frustrating and he is grappling with where the committee can go with this. They are not on the school board and cannot see the whole situation. The City Council listens to neighborhoods. When virtually a whole neighborhood says they do not like a plan, the City Council listens. Clearly, this neighborhood does not like the plan. The City Council is limited. Councilor O'Connor is correct. This seems like trying to fix a problem by creating a bigger problem. Is there anything the City Council can do to prevent this? Why do this plan if so many people are against it? He thinks more talk needs to happen. The neighbors are showing frustration from not having been in the loop long enough. He hopes the KSD can slow this down and talk more. The Planning Board can slow things down if they choose to. The City Council members can call the Planning Board members and encourage that. Sometimes the City Council goes at a snail's pace but sometimes they get it right by going slow. More people need to get involved with this. If this were a City issue, he would say no to this plan. He thinks this should not be pushed through.

Councilor Lamoureux stated that he agrees, but that is what the Planning Board process is for. He continued that the KSD needs to apply to the Planning Board so all of that discussion can happen, to look at safety and all those things. This is the way to go to make the process work. All of that will come up during the Planning Board process. Everyone can have his or her say in the proper forum.

Councilor Hooper stated that he agrees with Councilor Lamoureux and Councilor Filiault. He continued that it obviously needs more discussion. The KSD came forth with their plan and he gives them credit: safety was their concern. The neighbors have more concerns beyond that. The conversation needs to continue. He thinks the Planning Board should discuss all of these concerns.

Mr. Malay stated that on October 23 the KSD slowed the process to a halt. He continued that they have stopped moving forward with any changes in traffic flow. They were notified on October 25 about the need for the driveway permit. He understands that the City Council is limited. The KSD wants to do right, too. They want to make sure students are safe. He will have his staff work with City staff to see if they see something that has not been looked at yet. They have a discussion planned November 21 at

Symonds School, not just for the neighbors, but also for the parents who drop their children off and the parents of children who ride the buses and of the children who walk from the other side of the school. It will be an expanded conversation. They are doing exactly what the City Council is saying. Those processes are in motion.

Councilor Filiault asked if Mr. Malay/the KSD has objections to seeking the permit from the Planning Board. Mr. Malay replied no, they will follow the process that everyone is expected to follow.

Councilor Clark stated that it seems like this whole problem is emanating from the fact that so many people are driving their children to and from school and blocking the entrance. He continued that over the last dozen years or so the City has spent so much money, millions, on Safe Routes to School (SRTS). The KSD should think about communicating with parents about how it is not that dangerous to let children walk to and from school. It needs to be thought about. There currently is a very dangerous situation with traffic. He tries to avoid certain streets when school is beginning or ending. People could solve this themselves without having to spend a lot of money on circles like this. They could ask themselves whether they could do measures in their own homes and families to make this a better situation.

Wanda Schumann stated that she has lived on Wheelock Street for over 40 years. She continued that her only point is the safety of the children. It has always been an issue. There is a gate that separates the school from Wheelock Street. There are children running all over and parents coming through there, and it is awful. She is surprised a child has not been hurt or killed there. When the neighborhood has all of this heaped on them, it is overwhelming to know that the KSD has decided this is the best way to take care of the problem. Wheelock Street is 20 feet wide. Can you imagine the great big buses come down it? That does not make sense. She has a problem with what the KSD thinks needs to be done.

Chair Manwaring replied that the Planning Board would look at the width of the streets.

Sarah Chavira, of 41 Russell Street, stated that she walks children to and from Symonds School. She continued that she echoes Ms. Schumann's concerns about the narrow width of the street, and in the winter, she mostly has to walk with the children in the street because the sidewalks are not plowed well. Neighbors brought up that concern at the meeting at Symonds and the answer was, the KSD assumes that the City takes care of plowing the sidewalks. Mr. Cate spends all of his time at the front of the school dealing with the dangerous situation. He did not know there has been a problem with the clearing of the snow. It is already a treacherous walk without the added traffic. Then you add the people who want to bypass the mess at the front and drop the children off in the back. If this goes through, how would they prevent parents in cars doing that also in the bus area? Are they allowed to say it is for buses only? Neighbors asked about getting a crossing guard for Newman Street, which crosses Wheelock Street, and were told it is not in the budget. Children's safety is the concern.

Councilor Jones stated to the neighbors that the Planning Board has criteria they have to follow, including traffic and safety. He continued that the neighborhood should go to the Planning Board meeting. They need actual numbers and to prove their point; they cannot just say, “We have a lot of traffic.”

Chair Manwaring encouraged neighbors to attend the Planning Board meeting. She continued that the City Council hears their concerns.

Councilor Filiault stated that the committee is limited as to what they can do, so they will accept this as informational. He continued that the City Council will always listen. School issues do not always come to the City Council, but if anyone needs someone to talk to or to ask the City Council to make recommendations to the Planning Board, they can call the City Council members – their numbers are public.

Councilor Filiault made the following motion, which was seconded by Councilor Lamoureux.

On a vote of 4-0, the Municipal Services, Facilities, and Infrastructure Committee accepted the communication as informational.

3) Councilor Philip Jones – Maintenance of the Wilson Pond Dam on Arch Street and Communication from Robert Malay/Keene School District – Maintenance of the Wilson Pond Dam

Councilor Jones stated that he sent this to get it on the agenda and thinks it is very important. He continued that his letter spells out dates that came from the Keene Board of Education (KBE). They want the City to take over the dam. They are willing to build a new one. They want the City to take over maintenance by January 3, 2020. He does not think that will happen. There is a question as to who owns it. Does the KBE have the right to make this proposal, since it is not really their property? He has been finding different things from different people. This was gifted from Phineas Chamberlin, the original owner of Keene Industrial Paper and Kipco Party Outlet. There are permits that need to be done. He has been finding out more from staff and they will talk about the legalese.

Councilor Jones continued that he wants to talk about what happens if the dam goes away. About 15 to 20 years ago, there was an article in the Keene Sentinel about how environmentally sensitive the area is. There were pictures of children catching turtles in the pond. The flora and fauna might be endangered. That pond affects the market value of people who live around it. They need to look upstream and downstream. There is a park kind of behind Shadow Lane that many might not know is there, an area which is environmentally sensitive. Near the parking lot of Pizza Down Under, there are wetlands because of the dam and those would go away if the dam went away. There are sensitive wetlands downstream, too, at the end of Bent Court. The brook meets up with White Brook and it would affect the flow there, too. Arch Street is flood prone. The Arch Street condominiums sometimes have had over two feet of water, and flooding could

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SPR-972 MODIFICATION #6 – SITE PLAN REVIEW – COLONY MILL RETAIL BUILDING, 210-222 WEST STREET

Request:

Applicant and owner Brady Sullivan Properties proposes to construct a 4,875-sf retail and office building with two drive-throughs on the properties located at 210-222 West St (TMP# 576-009-000), 0 Gilbo Ave (TMP# 576-008-000) and 0 Island St (TMP#s 583-011-000, 583-012-000, 583-014-000, & 583-015-000). A waiver is requested from Development Standard 10 – Lighting. These properties are 4.6-ac, 0.94-ac, 0.3-ac, 0.21-ac, 0.1-ac, and 0.34-ac in size and are located in the Commerce District.

Background:

The Public Hearing for this Site Plan Application was opened at the July 27, 2020 Planning Board meeting. This Staff Report provides an analysis of revised plans submitted by the Applicant since the July meeting with regard to the Planning Board’s Development Standards. More information on this proposal, as well as a more detailed staff report, can be found in the July 27, 2020 Planning Board Agenda Packet, which is available for review at ci.keene.nh.us/planning-board.



Image above: The proposed project site at 0 Island St is highlighted in red and the star represents the approximate location of the proposed building.

Application Analysis: The following is a review of the Planning Board development standards relevant to the most recent revisions submitted to this site plan application.

6. **Landscaping:** The Applicant has proposed to install a 4-ft high wood fence along the majority of the southern property line to address screening of vehicular headlights. With the proposed addition of this fence, they have noted that they are no longer proposing to install 7 trees and 18 shrubs along this southern boundary. The change would impact the proposed landscaping as follows:

- 7 existing trees are proposed to be removed in the area of the proposed project. The trees that would be removed include 4 trees along Island Street, 2 trees in the grassy area on the southwest portion of the site, and 1 tree in a parking lot landscape island.

- 7 crab apple trees are proposed to be installed along Island Street.

- A mix of 72 shrubs and 12 grass plants are proposed in planting areas around the proposed building.

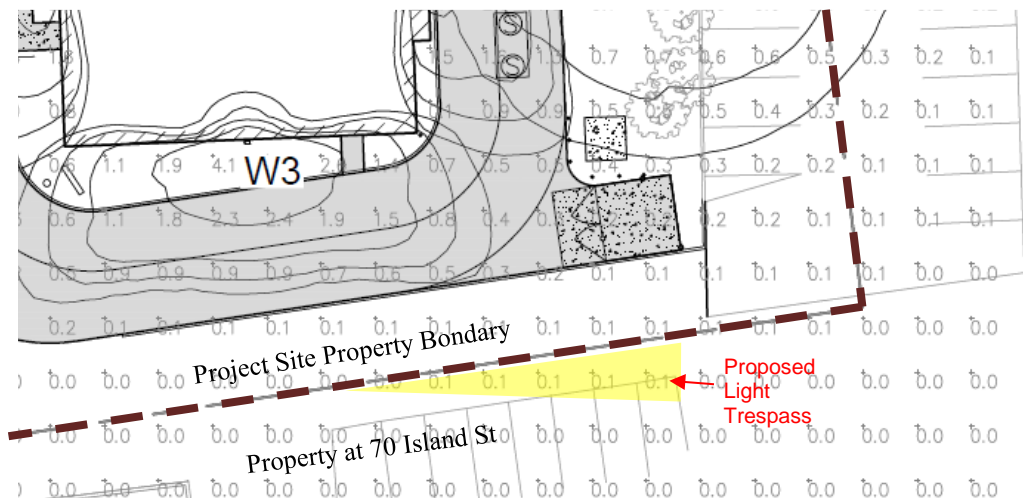
In addition to the landscaping described above, the Applicant has proposed to replace 4 trees, which were previously removed from an interior parking lot island without site plan approval, as well as 2 trees on the east side of the site, which are dead and need to be removed.

8. **Screening:** At the July 27th public hearing, the Planning Board heard from an abutter, who owns the property at 70 Island Street. This abutter expressed interest in seeing a solid fence installed along the southern property line to screen vehicular headlights and to serve as a barrier to individuals seeking to travel between the adjacent properties. Since this meeting, the Applicant has submitted a revised plan that displays the installation of a 4-ft high solid fence along the majority of the length of the southern

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property boundary. The Applicant has proposed to remove the landscaping proposed for this area of the site, which was originally intended to screen the drivethrough lane in this area. See item 6. Landscaping (above) for more information on the proposed changes to landscaping on the site.

10. **Lighting:** The Applicant has submitted a revised lighting plan that reduces the amount of light that would cross onto the neighboring property at 70 Island Street; however, there is a small amount of light (0.1 footcandle) that would shed onto the north side of the property at 70 Island St. The image below highlights in yellow the area where light is proposed to cross onto the adjacent property based on the photometric plan submitted by the Applicant.



The Applicant has submitted a waiver request from the Standard 10.C.2 which states “Lighting shall be designed, located, installed and directed in such a manner as to prevent light levels from crossing property boundary lines...” In determining whether to grant this waiver request, the Board should consider the Planning Board’s waiver criteria, which are listed below.

“X. WAIVER OF REGULATIONS AND DEVELOPMENT STANDARDS

1. *Unless otherwise set forth in these regulations, the Planning Board may waive strict compliance with the Board’s Development Standards or other regulations on a case-by-case basis, when criteria established for the granting of specific standards are found to be met, and in any case, as long as the granting of such waiver meets each of the following criteria:*
 - a) That granting the waiver will not be contrary to the spirit and intent of these regulations;*
 - b) That granting the waiver will not increase the potential for creating adverse impacts to abutters, the community or the environment; and*
 - c) That granting the waiver has not been shown to diminish the property values of abutting properties.*
 - d) Consideration will also be given to whether strict conformity with the regulations or Development Standards would pose an unnecessary hardship to the applicant.”*

STAFF REPORT

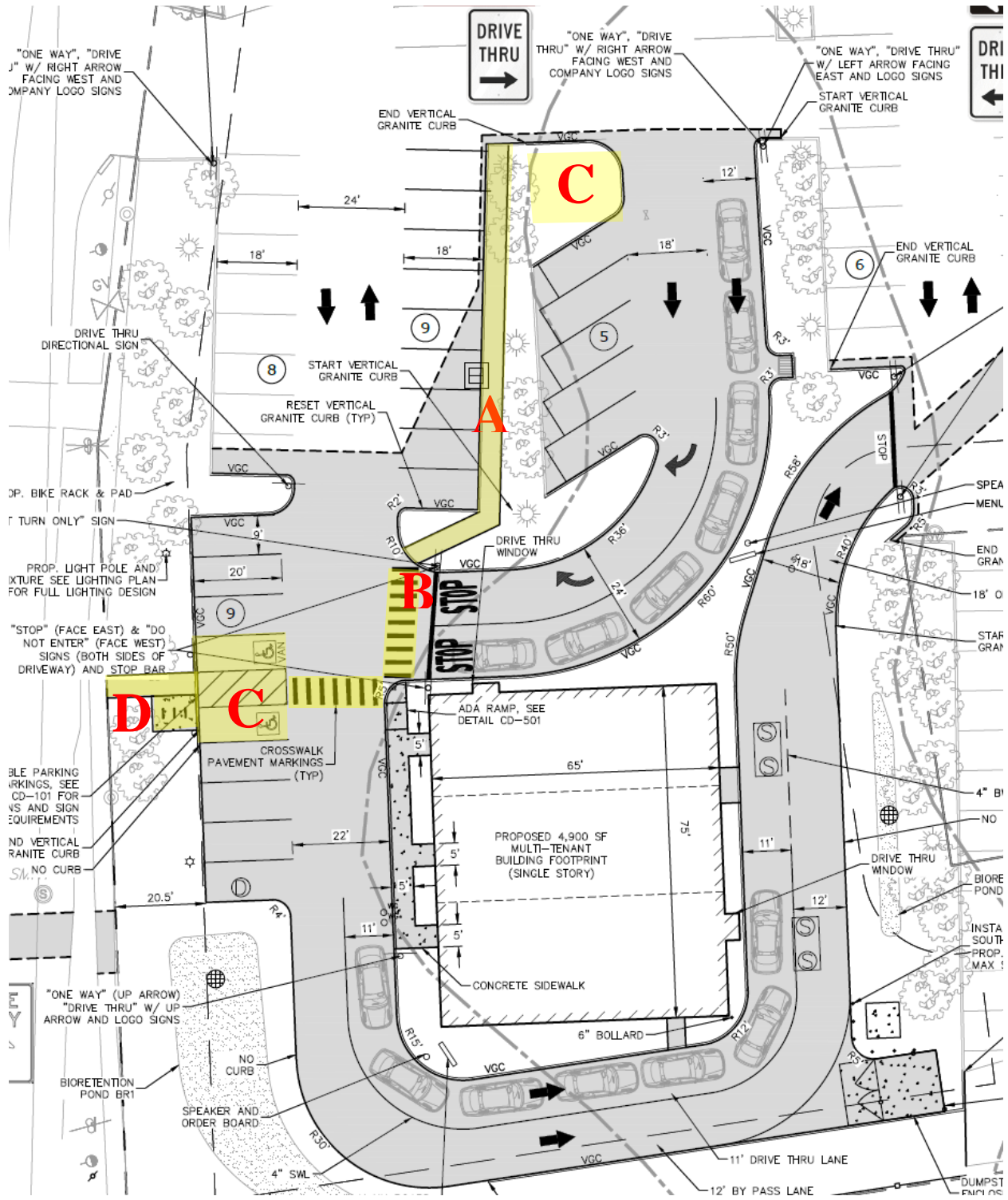
12. Traffic: The Applicant has submitted a revised plan, which removes the Rectangular Rapid Flashing Beacon (RRFB) from the area adjacent to the existing crosswalk on Island St at Cheshire Rail Trail. This Applicant is not proposing to install this device.
13. Comprehensive Access Management: At the Planning Board meeting on July 27, City staff expressed concerns about the safety of pedestrians traveling from parking areas on the site to the proposed building, as well as for the connection between the adjacent sidewalk network and nearby rail trail to the site. This development standard states that “Driveway locations, sidewalks, crosswalks, bicycle/pedestrian paths and accessible paths of travel shall be considered as an integral design feature of every development proposal.” In addition, Sub-section e.1. of this standard states “Where appropriate, connections shall be made for the continuation of sidewalks, walkways and bicycle lanes within the property, between adjoining properties, and site amenities shall be installed such as bicycle racks, benches and bus stop shelters.”

Staff also expressed concern for the potential vehicular conflicts posed by queueing of the proposed drive-throughs, as the entrance for the southern-most drive-through is shared with the exit for the northern-most drive-through. In addition, concern was noted for the potential conflict that queued vehicles in the drive-through lanes would cause with parked vehicles in the adjacent parking areas.

The Applicant has submitted a revised Site Layout Plan (included in the agenda packet), which addresses some of the concerns offered by Staff related to this standard. A portion of this Plan is displayed on the next page for reference (please note that this image of the plan is not to scale). A summary of the proposed changes relevant to this standard are listed in A through D below. Please refer to the associated letter (in red) and highlighted areas on the plan image on the next page to view where on the site these proposed changes would occur.

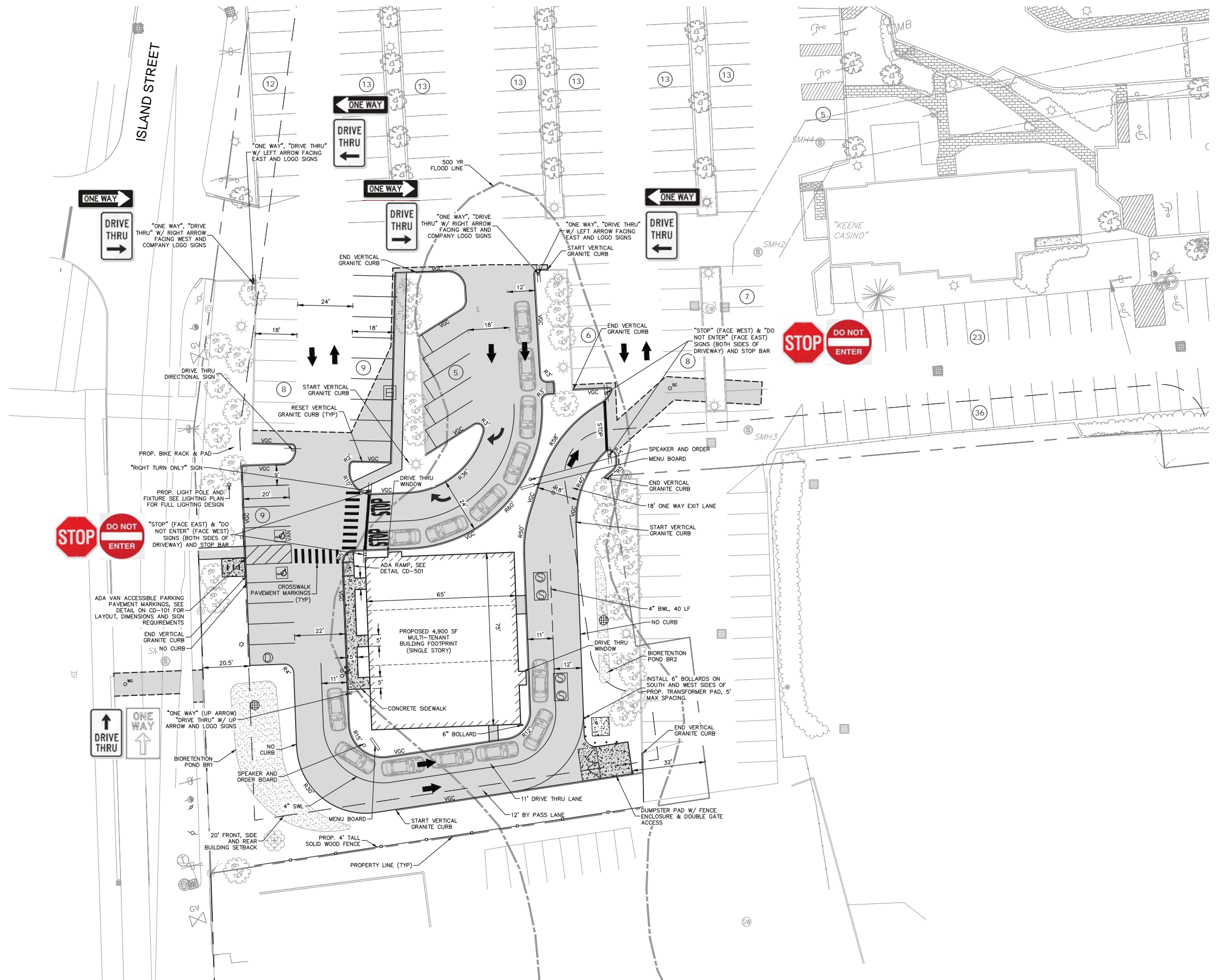
- A. Internal Sidewalk – The Applicant is proposing to install a paved walkway in a parking lot island to provide a path of travel for pedestrians parking onsite to the proposed building. The Applicant has proposed to shift back the stop line for the northern-most drivethrough lane to accommodate a painted crosswalk, which would connect this walkway to the proposed building entrance.
- B. Right-Turn Only – The Applicant is proposing to install a right-turn only sign at the exit of the northern-most drivethrough lane. This may reduce the potential for vehicles to turn left into the southern drive-through lane.
- C. Changes to Parking Space Locations – The Applicant has proposed to remove a space from the row of parking that is adjacent to the northern drivethrough lane to prevent possible conflict of vehicles backing into the travel aisle/drivethrough. The Applicant has also proposed to relocate the two accessible parking spaces a few spaces to the north in an effort to reduce the risk that the vehicles queueing in the southern drivethrough might block these vehicles from backing out. However, there is still the potential for the southern drivethrough lane to cause a conflict with these parked vehicles, as there is not enough space for a parked vehicle to back out if the vehicle queue exceeds the space provided in the drivethrough lane by more than two vehicles. Staff have encouraged the Applicant to consider alternative designs to this southern drivethrough to provide more space for vehicles to queue behind the menu board located on the plan.
- D. Walkway Connecting Island St – The Applicant has proposed to install a walkway that would connect the interior of the site to the sidewalk network on Island Street. This walkway would connect to the front of the proposed building via a crosswalk. In addition, the Applicant has proposed to relocate the bicycle rack and concrete pad to an area directly adjacent to this walkway.

STAFF REPORT

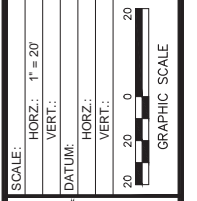


Recommended Motion:

Staff will provide a recommended motion at the August 24, 2020 Planning Board meeting.



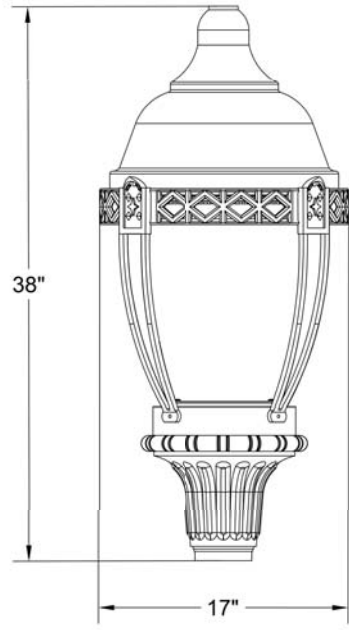
No.	DATE	DESCRIPTION	DESIGNER	REVIEWER
3.	8/4/2020	ADD PEDESTRIAN ACCESS AND FENCE	ARS	BAP
2.	7/27/2020	UPDATE BIKE RACK	ARS	BAP
1.	7/13/2020	REV. PER. 7/6/2020 PLANNING COMMENTS	ARS	BAP



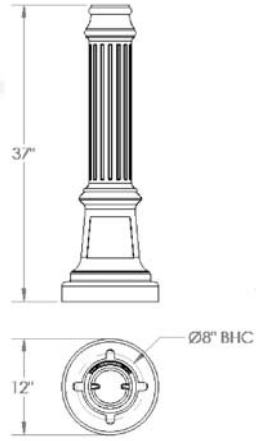
FUSS & O'NEILL
 50 COMMERCIAL STREET
 MANCHESTER, NEW HAMPSHIRE 03101
 603.666.8223
 www.fandob.com

BRADY SULLIVAN PROPERTIES
 SITE LAYOUT PLAN
 MULTI-TENANT USE
 0 ISLAND STREET
 NEW HAMPSHIRE
 KEENE

PROJ. No.: 20191323.A10
 DATE: 6/17/2020Y
CS-102
 33 of 59



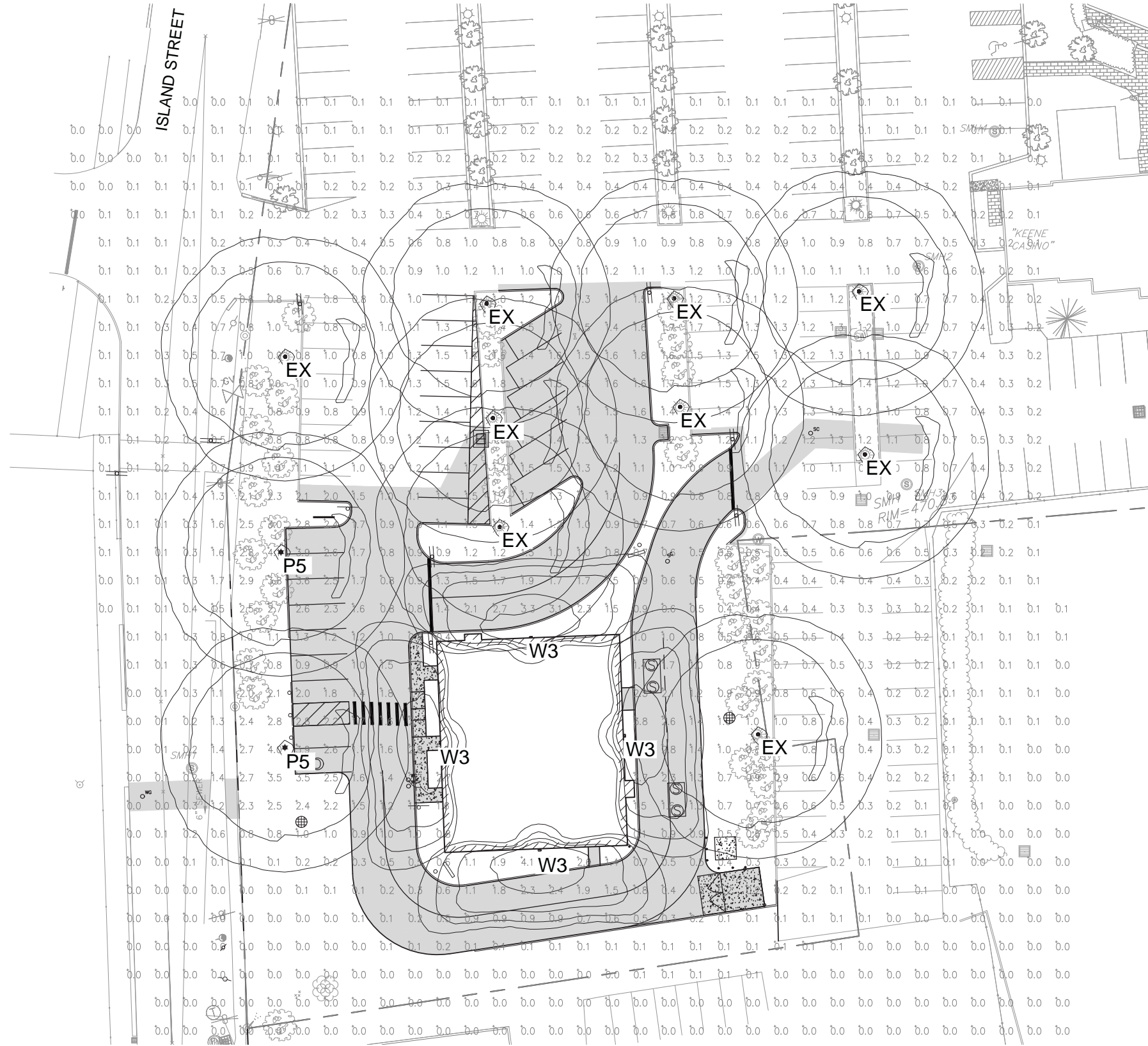
LEXINGTON
 MANUFACTURED BY PEMCO
 LIGHTING PRODUCTS



PLB220 DECORATIVE ALUMINUM
 POLE MANUFACTURED BY PEMCO
 LIGHTING PRODUCTS



ISS IMPACT ELITE LED
 MANUFACTURED BY MCGRAW
 EDISON

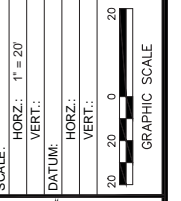


NOTE: FIXTURE TYPE EX. IS THE EXISTING LIGHT TO REMAIN AND ARE INCLUDED FOR LOCATION PURPOSES. ACTUAL
 FIXTURE LUMENS AND PERFORMANCE MAY DIFFER IN THE FIELD.

LIGHTING PARKING AREA
 STATISTICS
 ILLUMINANCE (FC)
 AVERAGE = 1.50
 MAXIMUM = 3.9
 MINIMUM = 0.7
 AVG./MIN RATIO = 2.14
 MAX./MIN RATIO = 5.57

Symbol	Qty	Label	Arrangement	Description
★	9	EX	SINGLE	EXISTING POLE LIGHT
★	2	P5	SINGLE	LEX-NL-80WJK-U-V-A1-T1-BK/ PLB220-4-12-S-188-T300-BK, MT HT, 12'
□	4	W3	SINGLE	ISS-AF-600-LED-E1-SL3/ WALL MTD 12' AFG

No.	DATE	DESCRIPTION	DESIGNER	REVIEWER
1.	7/13/2020	REV. PER. 7/6/2020 PLANNING COMMENTS	ARS	BAP
2.	7/22/2020	REV. LIGHT FIXTURE TO FULL CUTOFF	ARS	BAP



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 50 COMMERCIAL STREET
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 603.668.8233
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BRADY SULLIVAN PROPERTIES
 LIGHTING PLAN
 MULTI-TENANT USE
 0 ISLAND STREET
 NEW HAMPSHIRE
 KEENE

PROJ. No.: 20191323A10
 DATE: 6/17/2020Y
CL-101
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STAFF REPORT

SPR-01-13, Modification #2 –SITE PLAN REVIEW – Indoor Shooting Range, 19 Ferry Brook Rd

Request:

Applicant and owner Cheshire County Shooting Sports Education Foundation proposes to install a 3,300-sf indoor shooting range on the property located at 19 Ferry Brook Road (TMP# 214-021-000). A waiver is requested from Development Standard #6 – Landscaping. This property is located in both the Town of Sullivan and the City of Keene. The portion of the property in Keene is 55-acres in size and is located in the Rural District.

Background:

This property is located about 3.1 miles northeast of Central Square and about 0.25 miles north of NH Rt. 9 in the Rural district. The site is currently used as an outdoor shooting range, with a shooting deck and small, 1,152 square foot clubhouse with access from Ferry Brook Road. The majority of the parcel is located in the Town of Sullivan; however, the portion of the parcel with the clubhouse and shooting range is located in Keene.

This property received conditional site plan approval in 2013 for the construction of a 26,000 square foot indoor shooting range (SPR-01-13). A modification to the site plan was approved by the Planning Board in 2015 (SPR-01-13, Modification #1); however, the building was never constructed and the site plan associated with that project has expired.

The current proposal is to install a 3,300 square foot indoor shooting range that would be connected to the existing clubhouse building by an 8 foot by 8 foot airlock. This use is allowed as an accessory to the main use on the site, which is classified as “outdoor recreational use as a business.”

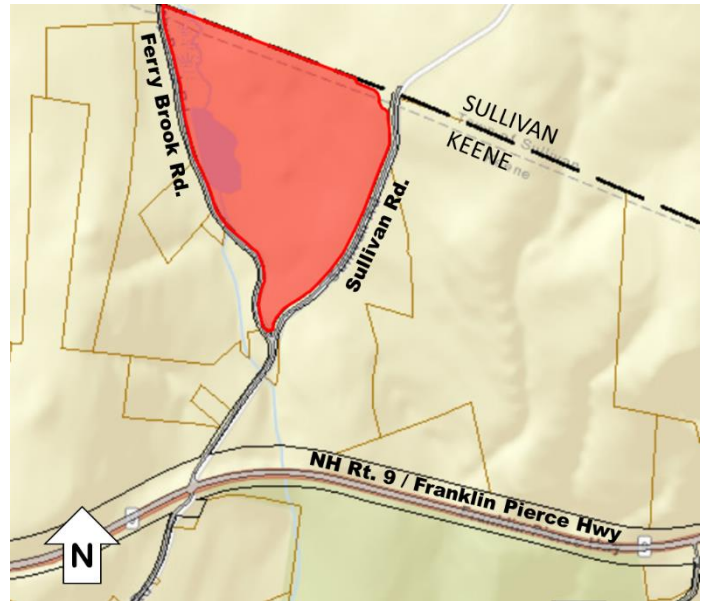


Figure 1. Image showing the parcel located at 19 Ferry Brook Road. The portion of the parcel located in Keene is shaded in red.

Completeness:

The Applicant requests exemptions from providing a Landscaping Plan, a full traffic study, and a soils report. Staff have determined that the requested exemptions would have no bearing on the merits of the application and recommend accepting the application as “complete.”

Departmental Comments:

- **Zoning:** The parking calculation provided by the Applicant shows that 10 spaces are required for 4,600 sf of building area; however, the site data table indicates that building coverage for the lot is 6,770 sf, which would require 14 spaces.
- **Code Enforcement:** Please show the accessible path of travel from the van-accessible parking space to the entrance, as well as any curbing and curb ramp details.
- **Engineering:** After reviewing the revised materials and responses to staff comments, Engineering staff have no remaining questions or comments.
- **Fire & Police:** No comments.

Application Analysis: The following is a review of the Planning Board development standards relevant to this application.

1. **Drainage:** The Applicant proposes to install a level spreader to the west of the proposed building addition to infiltrate runoff from the roof of the new addition, as shown on the “Post Development Drainage Plan” (Sheet D-2). According to the drainage report prepared for this project on July 8, 2020, the post-development condition will consist of runoff sheet-flowing from the roof to the level spreader, resulting in slightly less runoff as compared to pre-development conditions. This standard appears to be met.
2. **Sedimentation and Erosion Control:** The Applicant proposes to install straw wattle or silt fencing along the western (downhill) edge of the project area, between the area to be disturbed and the existing wetland. Details for the straw wattle and silt fence are included on Sheet 5 of the plan set, which also includes “Sediment and Erosion Control Notes” detailing best management practices for installing and maintaining sediment and erosion control measures. This standard appears to be met.
3. **Hillsides:** The proposed project would not impact any precautionary or prohibitive slopes. This standard does not apply.
4. **Snow Storage:** The Applicant notes in the project narrative that snow will be stored on site. There is ample room for snow storage along the edge of the existing driveway and parking area. This standard appears to be met.
5. **Flooding:** There are no floodplains present on this property. This standard does not apply.
6. **Landscaping:** The Applicant has requested a waiver from this standard, which requires the installation of one tree per 10 parking spaces for parking lots of 10 or more parking spaces. The written waiver request, which is attached to this staff report, notes that the parking area for this site is in a location that is not visible from the road and is surrounded by mature vegetation. The Planning Board should consider the Planning Board Waiver Criteria, listed below, in making a decision to grant or deny this waiver request.

“X. WAIVER OF REGULATIONS AND DEVELOPMENT STANDARDS

1. *Unless otherwise set forth in these regulations, the Planning Board may waive strict compliance with the Board’s Development Standards or other regulations on a case-by-case basis, when criteria established for the granting of specific standards are found to be met, and in any case, as long as the granting of such waiver meets each of the following criteria:*
 - a) *That granting the waiver will not be contrary to the spirit and intent of these regulations;*
 - b) *That granting the waiver will not increase the potential for creating adverse impacts to abutters, the community or the environment; and*
 - c) *That granting the waiver has not been shown to diminish the property values of abutting properties.*
 - d) *Consideration will also be given to whether strict conformity with the regulations or Development Standards would pose an unnecessary hardship to the applicant.”*

7. Noise: In a memo to staff dated August 7, 2020, the Applicant states that the noise levels on the site will not increase from the proposed new indoor shooting range. The Applicant further notes that the concrete masonry block walls of the proposed new shooting range provide an effective noise barrier, and that the building would be located about 350 feet from the nearest residence.
8. Screening: The Applicant does not propose any screening as part of this project. There is an existing dumpster located on the site that will be moved to a new location with a concrete pad. This dumpster will not be visible from the road.
10. Lighting: The Applicant proposes to install three pole mounted lights on 15-foot poles in the parking lot area and two wall-mounted lights above the egress doors, as shown in the Lighting Plan attached to this staff report. All lights will be full cut-off LED light fixtures with a bronze finish color. The pole-mounted lights will have a color temperature of 4000K and the wall-pack lights will have a color temperature of 5000K. The pole-mounted lights will be on timers that will turn off at 10:00 pm; however, the wall-pack lights will remain on throughout the night to provide security lighting after hours.

According to the Lighting Plan, the average and minimum illumination levels for the parking area will be 1.84 footcandles (fc) and 0.7 fc, respectively, and would have a uniformity ratio of 2.63. No floodlighting or uplighting is proposed, and no light would cross the property boundary onto an adjacent parcel or the public right of way. This standard appears to be met.

11. Sewer and Water: This site is currently served by an existing well and septic system. The Applicant proposes to continue using the existing well and construct a new septic system, which will require review and approval by the NH Department of Environmental Services Subsurface Systems Bureau. This standard appears to be met.
12. Traffic: The Applicant submitted a traffic study that was prepared in 2013 for a previous site plan application for a 26,000 square foot indoor shooting range with 40 shooting lanes. This traffic study estimates that the average weekday traffic generation would be 70 trips with 10 trips during the peak hour, and that Saturday traffic generation would be between 100-150 vehicle trips with 20 vehicle trips during the peak hour. The 2013 traffic study concludes that the increase in traffic could be accommodated by the existing roadways surrounding the site. However, this building was never constructed. The current request is to construct a 3,300 square foot building with 7 shooting lanes, which is significantly smaller than the previous proposal for which the traffic study was prepared. Based on the results of the 2013 traffic study and the smaller size of the proposed building, the Applicant estimates that the average daily traffic generation will be less than 100 vehicle trips per day, and that peak hour traffic generation will be 15 vehicle trips or less. Staff have reviewed the 2013 traffic study and agree with the Applicant's conclusion that the current proposal would have less of an impact on traffic generation than the previous proposal.

This standard states that all developments must comply with the parking requirements of the Zoning Ordinance. At the time of this staff report, the site plan shows 11 marked parking spaces where 14 spaces are required, as determined by the Zoning Administrator. The Applicant has been notified that three additional spaces will need to be provided to comply with zoning.

13. Comprehensive Access Management: No changes are proposed to the existing gravel driveway, which provides access to the site from Ferry Brook Road. The Applicant proposes to install a bicycle rack to provide bicycle parking for visitors and/or employees who travel to the site by bicycle. This site is not located along a City Express Bus route and it is not in an area where sidewalks are provided. This standard appears to be met.

14. Hazardous or Toxic Materials: The Applicant notes in the project narrative that the shooting range on this site is in compliance with State and federal guidelines for the management of lead on shooting ranges. This standard appears to be met.
15. Filling and Excavation: The Applicant states in the project narrative that no excess material will be brought in or out of the site, and that the only material required for this project is gravel for the building pad. No fill is proposed to be placed in a floodplain, in a wetland area, or on a steep slope. This standard appears to be met.
16. Wetlands: There are wetlands present on the site; however, all of the proposed work is outside the 75-foot buffer surrounding these wetlands. This standard appears to be met.
17. Surface Water: No work is proposed within the 75-foot Surface Water Protection Overlay buffer. This standard appears to be met.
19. Architecture and Visual Appearance: The Applicant proposes to construct a 3,300 sf (110' long by 30' wide) indoor shooting range oriented north-south that will be connected to the existing wood-frame clubhouse by an 8' x 8' airlock. The new shooting range is proposed to be a single-story concrete masonry unit structure painted to match the existing building. The middle section of the new addition, which is about 86' long, would have a shed roof that slopes from east to west with a maximum height of 16'-7 1/4." At the north and south ends of the building, there will be flat sections of roof about 12' long where HVAC and mechanical equipment will be located. Figure 2 shows the west (front) and east (rear) elevations of the proposed building. The full architectural elevations are included as an attachment to this staff report.

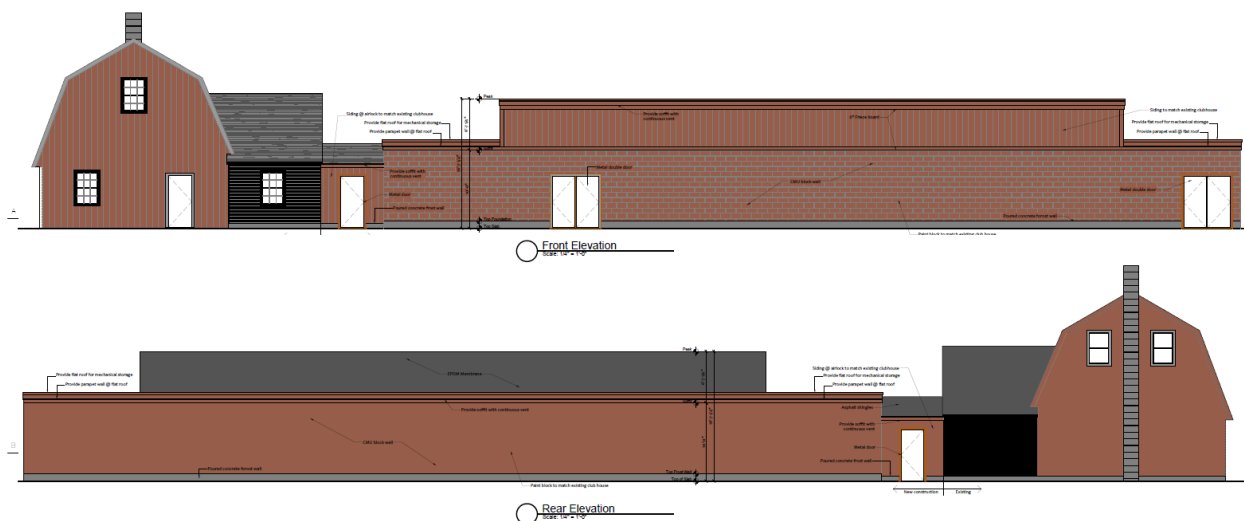


Figure 2. Image of the color architectural elevations for the proposed new shooting range and existing club house at 19 Ferry Brook Road.

Recommended Motion:

If the Board is inclined to approve this request, the following motion is recommended:

Approve SPR-01-13 Modification #2 including granting a waiver from Development Standard #6 – Landscaping, as shown on the plan set identified as “Proposed Indoor Range, Cheshire County Shooting Sports Education Foundation, Ferry Brook Road, Keene, NH” prepared by Brickstone Land Use Consultants dated July 17, 2020 and last revised on August 7, 2020, and on the architectural elevations identified as “Elevations Sections” prepared by Timothy Sampson at a scale of ¼ inch = 1 foot dated July 17, 2020, with the following conditions prior to signature by Planning Board chair:

1. Submittal of a revised site plan to show the provision of 14 parking spaces on the site.
2. Owner’s signature appears on site plan and architectural elevations.

CITY OF KEENE | PLANNING BOARD

SITE PLAN REVIEW / MODIFICATION APPLICATION



This form must be filled out in its entirety. If a box is not checked, staff will assume that the information is not provided and the application is, therefore, not complete. Incomplete applications will not be accepted for review.

A	Project Name Proposed Indoor Shooting Range Cheshire Co, Shooting Sports Educaton Foundation		Date Received/Date of Submission:	
	Tax Map Parcel number(s) 2 1 4 - 0 2 1 -		Community Development Dept. File #:	
Project Address: 19 Ferry Brook Road	Owner	Name/Company: PLEASE PRINT: Cheshire County Shooting Sports Education Foundation Inc		
Zoning District: Rural		Address: PO Box 233 Keene NH 03431		
Acreage/S.F.of Parcel: 43.60 ac / 1899216 sf		Telephone\ Email: 603-352-8563 / rentalskeene@yahoo.com		
Modifications: Is this a modification to a previously-approved site plan? <input type="checkbox"/> No <input checked="" type="checkbox"/> Yes: SPR#: 01-13 Date: 01/18/2013 (plan expired)		SIGNATURE: <i>[Signature]</i> Printed Name: <u>Michael Hagan Chair</u>		
For those sections of the application that are not affected by the proposed modification to the previously approved site plan, you are encouraged to request exemptions in lieu of submitting required documents.	Applicant	Contact Name/Company: PLEASE PRINT: Same as Above		
		Address:		
		Telephone\ Email:		
		SIGNATURE: _____ Printed Name: _____		
B Descriptive Narrative Including				
<input checked="" type="checkbox"/> Type of development <input checked="" type="checkbox"/> Sedimentation Control <input checked="" type="checkbox"/> Scope/scale of development <input checked="" type="checkbox"/> Proposed uses <input checked="" type="checkbox"/> Vegetation <input checked="" type="checkbox"/> Parcel size <input checked="" type="checkbox"/> Location of access points <i>N/A</i> <input type="checkbox"/> Debris management <input checked="" type="checkbox"/> Proposed stormwater, drainage & erosion plan <input checked="" type="checkbox"/> Any other descriptive information <i>N/A</i> <input type="checkbox"/> Disposal proposals for boulders, stumps & debris				
C A complete application must include the following				
<input checked="" type="checkbox"/> Two (2) copies of completed application forms signed and dated <input checked="" type="checkbox"/> Two (2) copies of descriptive narrative <input checked="" type="checkbox"/> Notarized list of all owners of property within 200' - include owner and applicant <input checked="" type="checkbox"/> Two (2) sets of mailing labels, per abutter <input checked="" type="checkbox"/> Seven (7) copies on "D" size paper of plans (24" x 36")		<input checked="" type="checkbox"/> Three (3) copies of "D" size architectural elevations (24" x 36") <input checked="" type="checkbox"/> Plans stamped/signed by reg. professional <input checked="" type="checkbox"/> Two (2) copies on <input checked="" type="checkbox"/> Three (3) copies of all technical reports <input checked="" type="checkbox"/> Two (2) color architectural elevations on 11" x 17" <input checked="" type="checkbox"/> A check to cover the costs of legal notice to advertise the public hearing and mailing notices out to abutters		

**City of Keene
SITE PLAN REVIEW**

**Proposed Indoor Range
For
Cheshire County Shooting Sports Education Foundation, Inc.
Ferry Brook Road**

Property Owner:
Cheshire County Shooting Sports Education Foundation, Inc.
19 Ferry Brook Road
PO Box 233
Keene, NH 03431

July 17, 2020

Narrative

This application is for a proposed indoor shooting and educational building to be located at the Cheshire County Shooting Sports Education Foundation (CCSSEF) at 19 Ferry Brook Road in Keene. The property is approximately 195.5 acres that straddles the town line between Keene and Sullivan, NH. Approximately 44 acres is in the Rural District in Keene with the balance in Sullivan. This proposal is entirely within the Keene portion of the property.

The proposed building will be a one story addition to an existing 1152 sf building. The addition will be 30' x 110' (3300 SF) and will be connected to the existing building with an 8' x 8' airlock. There will be seven lanes for shooting. The existing building contains restrooms, a small food prep area, a meeting room and offices for the organization's use.

The purpose of the building is to provide indoor range space for the Cheshire County Shooting Sports Education Foundation (CCSSEF). The CCSSEF is a nonprofit entity that owns and manages the facility. The mission of the CCSSEF is education in safe, responsible shooting activities and to foster amateur sports competition on both the national and international level. The organization will be responsible for all educational and training events and all activities that foster national and international amateur competition held at CCF&GC.

The foundation has a very active junior program that will utilize the facility. The program averages between 30 – 45 juniors who are active year round in regional and national matches. Over the years the program has produced 8 Collegiate Rifle Team members who have attended college on scholarships, 3 Military Team members and 1 Olympic Development Team member. Also, most of the local police departments in Cheshire County will use the proposed facility for qualification and training of their officers.

The property is on the east side of Ferry Brook Road and has approximately 4500 feet of frontage on the road. The property also has approximately 2000 feet of frontage on the Sullivan Road. The existing outdoor range occupies the south west corner of the property. The proposed new building will be constructed adjacent to the existing outdoor range and will use the same entrance driveway and some of the existing facilities.

The proposed building addition will be built on the north side of the existing main building in an area adjacent to the existing parking lot. This location currently contains an

existing outdoor shooting platform which will be removed. An existing septic system in that area will be removed and replaced with a new septic system to be constructed adjacent to the entrance driveway.

The proposed building is an allowed use as accessory to the main use of an outdoor recreational activity as a business. The site is surrounded by residential uses. However, due to the large size of the property the nearest residence is approximately 350 feet from the proposed building.

WAIVER REQUESTED:

Development Standard 6 – Landscaping

We are requesting the following waiver of the landscaping standard: part b. where 1 tree per 10 parking spaces is required. We are proposing no new landscaping.

Waiver Criteria:

- a. ***That granting the waiver will not be contrary to the spirit and intent of these regulations:***

The spirit and intent of the landscaping standard in this case is to provide screening, shading and an attractive streetscape by planting trees within and adjacent to a commercial parking lot. This existing parking area is located over 300 feet from the public roads adjacent to the site. The existing trees and vegetation between the parking area and the public roads provide ample screening. The parking area is currently not visible from Old Ferry Road or from Sullivan Center Road. The spirit and intent of the standard will be met because the parking lot will not be visible from the road or abutting properties.

- b. ***That granting the waiver will not increase the potential for creating adverse impacts to abutters, the community or the environment:***

This is an existing parking lot surrounded by a forested buffer and is not visible from the public right of way or from adjacent properties. Not planting of two trees (1 tree per 10 parking spaces) will have no effect on abutting properties, the community or the environment.

- c. ***That granting the waiver has not been shown to diminish the property values of abutting properties.***

The existing parking lot is not visible from abutting properties or from the public right of way. The forested buffer is an effective screen and helps to buffer any noise or light from the parking area.

- d. ***Consideration will also be given to whether strict conformity with the regulations of Development Standards would pose and unnecessary hardship to the applicant.***

The planting of additional trees is not necessary at this site. The large lot (195 acres) is mostly wooded. The parking area has existed at this location for decades and is not visible from abutting properties or from the public right of way. The planting of two 3” caliper trees would cost an estimated \$1200 - \$1500 and is an unnecessary expense to the applicant.

DEVELOPMENT STANDARDS:

1. Drainage.

The new building addition will have a shed roof which sheet drains to the south side of the building. A stone infiltration strip will collect much of the roof runoff and infiltrate the stormwater into the ground. Excess runoff will be collected in a long level spreader to be constructed downhill of the addition. The level spreader will also allow infiltration to groundwater. Any excess stormwater flow will overflow the level spreader and sheet drain onto a low area south of the building site. The level spreader is sized to accommodate up to a 50 year design storm with no increase in runoff leaving the site. Please see the attached plans and drainage report for additional information.

2. Sedimentation/Erosion Control.

Sediment and erosion control measures as shown on the attached plans are designed to meet the State of New Hampshire RSA 485-A:17 for control of runoff and sedimentation. Please see attached plans for more details.

3. Hillside Protection.

Not applicable.

4. Snow Storage and Removal.

Snow will be stored on site where shown on the site plan.

5. Flooding.

This property is not identified as being within the floodplain for the City of Keene.

6. Landscaping.

No new landscaping is proposed. No additional parking is planned. This site is in the middle of a large forested area and is not visible from Rt. 9 or from Sullivan Road. The view from Ferry Brook Road is only open at the existing driveway. The entrance driveway and the remainder of the site is wooded so no new trees are planned along Ferry Brook Road or the entrance drive. A waiver is requested from providing new landscaping.

7. Noise.

The existing property is used as an outdoor shooting range. Constructing the building will move some of the activities indoors and will not increase the noise generated on site. It will most likely reduce noise due to many of the uses currently happening outdoors moving indoors.

8. Screening.

No new screening is proposed. The existing dumpsters will be located behind the proposed building and will not be visible from the public way. The proposed building and parking area will be approximately 300 feet from Ferry Brook Road. The existing wooded buffer between Ferry Brook Road and the building site will remain.

9. Air Quality

This project will not affect air quality.

10. Lighting.

On site lighting will be accomplished using high efficiency LED fixtures by Cooper Lighting, Inc. Two pole mounted lights on 15' poles will provide an average 1.84 footcandles throughout the parking area. Parking lot light fixtures will be placed on timers and photocells with lights off after 10:00 PM. Wall mounted fixtures will be added over the two passage doors using full cutoff LED fixtures mounted at 9' height. Wall fixtures will remain on after hours for security lighting around the building. Please see attached plans and specifications for more detail. Fixture cut sheets have also been provided.

11. Sewer and Water.

The proposed new building will be served by the existing on site well. Water usage is limited to on site rest rooms and a small kitchen. The existing well has the capacity to service this use. The building will be served by a new on site septic system. The proposed new system will require approval by the DES Subsurface Bureau.

12. Traffic.

In 2013, a traffic impact study was prepared by Laurie Rauseo, P.E. to determine the impact of a proposed new indoor range approximately 26,000 sf in size with 40 shooting lanes. The report estimated that Saturday would be the peak use day for the range with daily traffic volume from the development estimated to be 100 – 150 vehicle trips per day with 20 vehicle trips during the peak hour. The new proposal is for a 3300 sf building with 7 shooting lanes and a maximum building occupancy of 49 people. We estimate that average vehicle trips per day will be less than 100 cars with 15 vehicle trips during the peak hour. This will result in no significant impacts to safety or capacity on Ferry Brook Road, Sullivan Road or at Rt. 9. Please see the attached January 16, 2013 traffic letter for additional information.

13. Driveways.

The existing range site is serviced by a single curb cut located on Ferry Brook Road. This driveway will be utilized for the proposed new building. The existing driveway is 18 feet paved and can accommodate the proposed traffic for the building.

14. Hazardous and Toxic Materials.

The site has been a shooting range for many years. Management of lead at the range is done in compliance with state and federal guidelines for the operation of a shooting range. The range is in compliance with these standards and continues to monitor their operations as required.

15. Filling/Excavation.

No excess material will be hauled to or from the site. All cut and fill materials will be used on site. The only materials required for this project will be processed gravels for the proposed building pad.

16. Wetlands.

No impacts to existing wetlands are proposed.

17. Surface Waters.

No surface waters will be impacted as part of this application.

18. Stump Dumps.

No stump dumps will be constructed on site.

19. Architectural and Visual Appearance.

The proposed building addition will be a single story CMU building 30 feet wide and 110 feet long. It will have a shed roof over the center portion of the building approximately 86 feet long, 16 feet at the high point and 10 feet at the low point. At each end of the addition there will be a flat roof section approximately 12 feet long which will be used to hold rooftop equipment for ventilation and HVAC. The addition will be connected to the existing building which houses the Foundation offices with an 8' x 8' airlock with a shed roof. The airlock will be wood framed with clapboard siding to match the existing building. The addition will be painted to match the existing building. Please see the attached color building elevations for further detail.

PROPOSED INDOOR RANGE

CHESHIRE COUNTY SHOOTING SPORTS EDUCATION FOUNDATION FERRY BROOK ROAD, KEENE, NH

OWNER:
CHESHIRE COUNTY FISH AND GAME CLUB

19 FERRY BROOK ROAD
PO BOX 233
KEENE, NH 03431
TAX MAP #904-02-001

JULY 17, 2020
REVISED THRU AUGUST 7, 2020

DEVELOPMENT CONSULTANT:

Brickstone
Land Use Consultants, LLC



Site Planning, Permitting and Development Consulting
185 Winchester Street, Keene, NH 03431
Phone: (603) 357-0116

SURVEYOR:

CARDINAL SURVEYING &
LAND PLANNING
Keene, New Hampshire 03431
Tel. (603) 499-6151

ENGINEER:

SVE Associates
Engineering Surveying Landscape Architecture Planning
439 West River Road, Brattleboro, VT 05302
Phone (802) 257-0561 Fax (802) 257-0721
website: www.sveassoc.com

OWNERS CERTIFICATION:

I CERTIFY THAT I AM THE OWNER OF THIS PROPERTY
AND THAT I APPROVE OF THIS SITE PLAN.

OWNER: _____

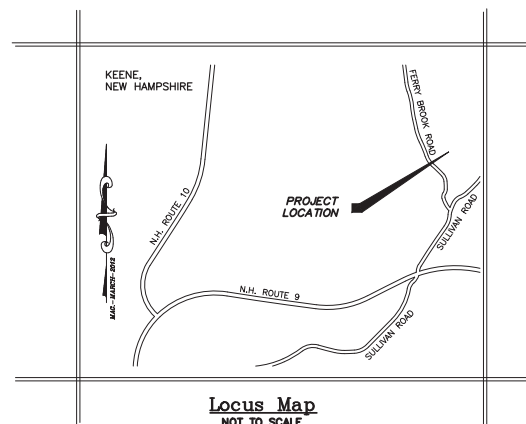
DATED _____

SHEET INDEX

- 10.01 FLAVIN BOUNDARY SURVEY
VICINITY PLAN
1. EXISTING CONDITION PLAN
 2. PROPOSED CONDITION PLAN
 3. SEPTIC PLAN
 4. LIGHTING PLAN
 5. CONSTRUCTION DETAILS

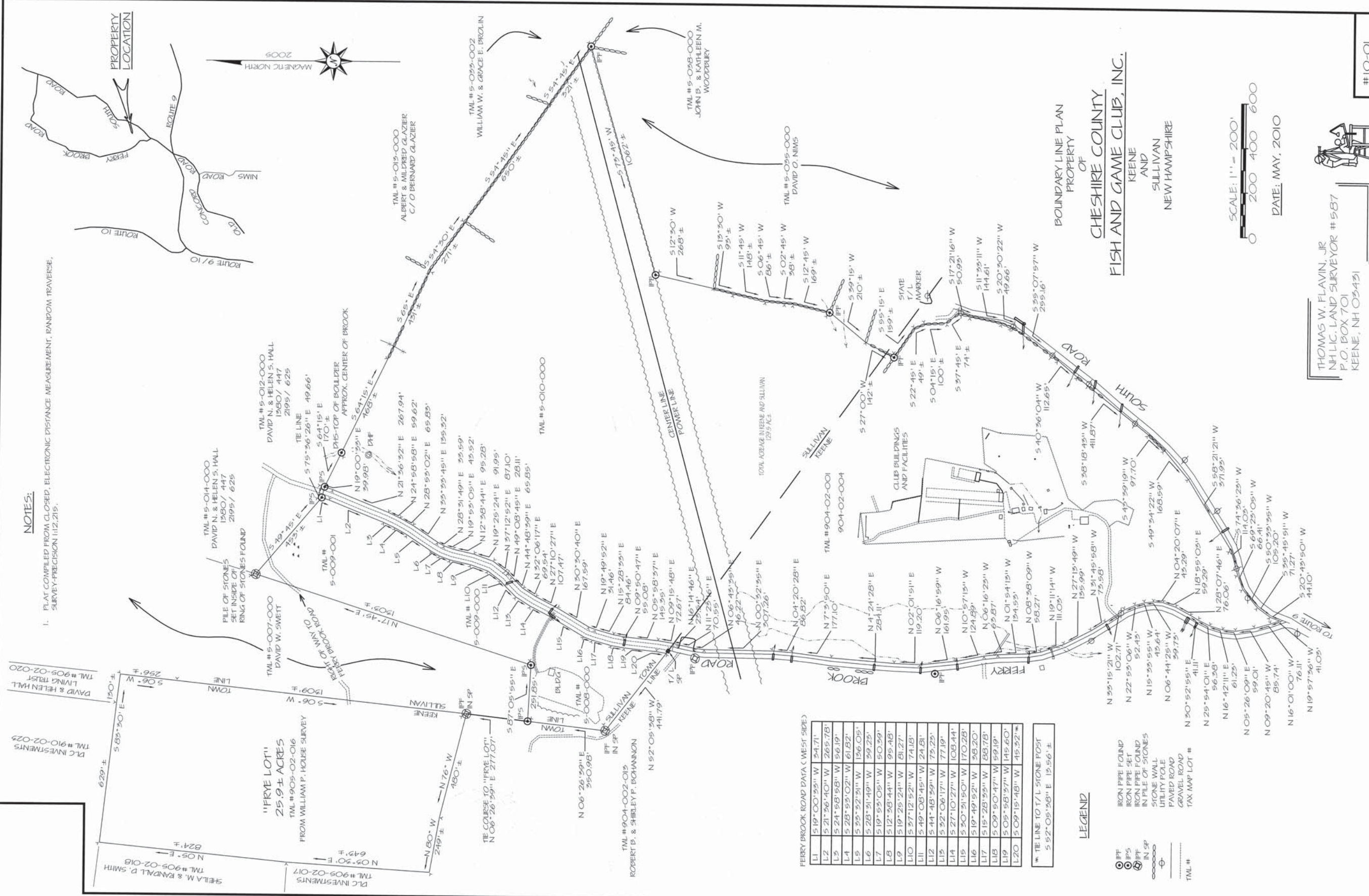
PERMITS REQUIRED:

KEENE PLANNING BOARD
DES SUBSURFACE
BUILDING PERMIT



APPROVED BY THE KEENE PLANNING BOARD	CHAIRMAN _____	DATE _____
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NOTES:
 1. PLAN COMPILED FROM CLOSED, ELECTRONIC DISTANCE MEASUREMENT, RANDOM TRAVERSE.



BOUNDARY LINE PLAN
 OF
FISH AND GAME CLUB, INC.
 KEENE
 AND
 SULLIVAN
 NEW HAMPSHIRE

SCALE: 1" = 200'
 0 200 400 600
 DATE: MAY, 2010

THOMAS W. FLAVIN, JR.
 NH LIC. LAND SURVEYOR #587
 P.O. BOX 701
 KEENE, NH 03443

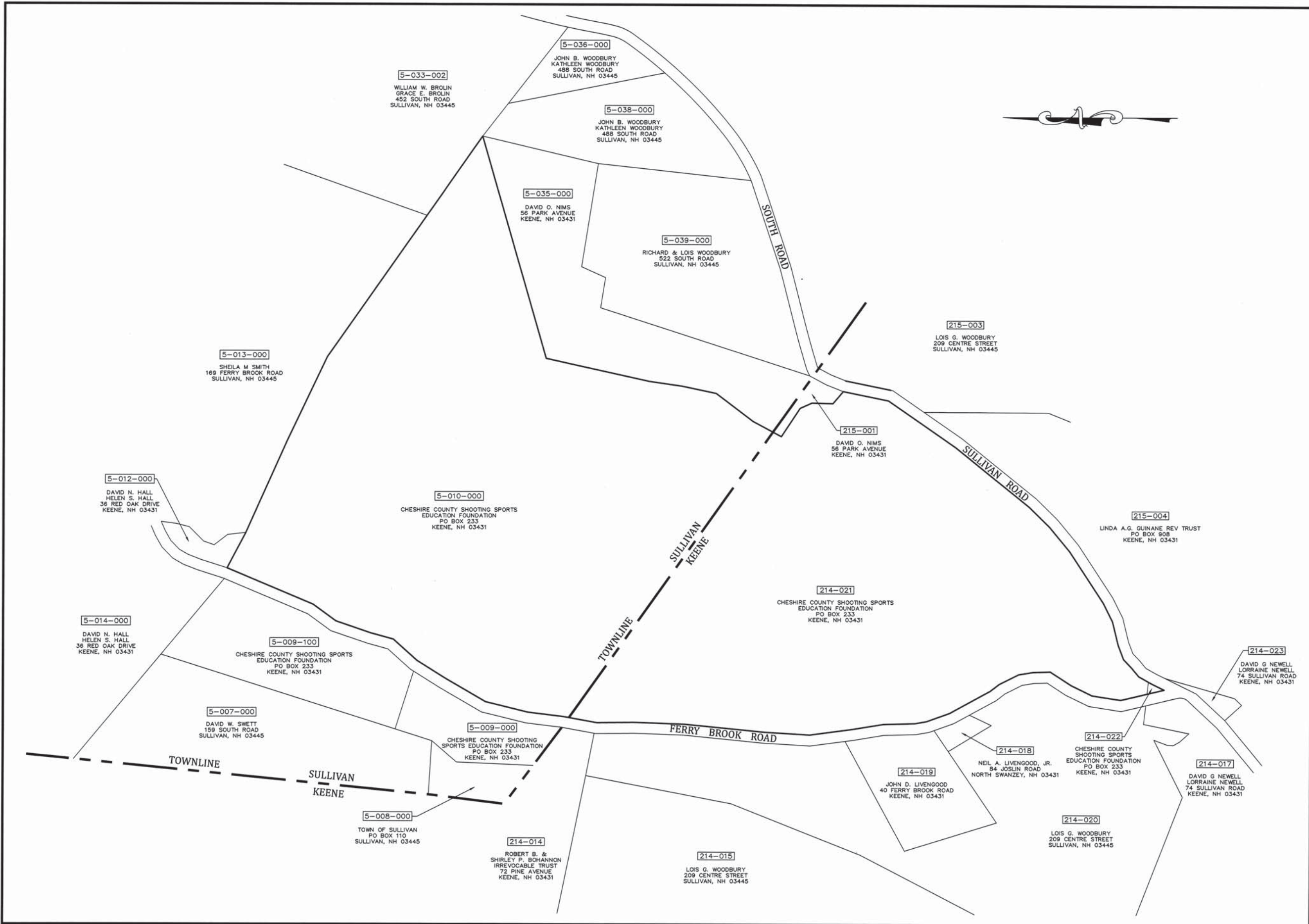
#10-01

FERRY BROOK ROAD DATA (WEST SIDE)

L1	5 19° 00' 55" W	24.71'
L2	5 21° 36' 40" W	265.78'
L3	5 24° 58' 58" W	56.19'
L4	5 28° 55' 02" W	61.82'
L5	5 35° 52' 51" W	156.05'
L6	5 28° 21' 49" W	39.23'
L7	5 19° 53' 05" W	50.39'
L8	5 12° 38' 44" W	95.18'
L9	5 19° 25' 24" W	81.27'
L10	5 27° 12' 52" W	74.18'
L11	5 49° 08' 45" W	24.81'
L12	5 44° 48' 39" W	75.25'
L13	5 32° 06' 17" W	77.19'
L14	5 27° 10' 27" W	108.44'
L15	5 30° 21' 50" W	170.28'
L16	5 19° 49' 52" W	38.20'
L17	5 15° 28' 35" W	88.78'
L18	5 09° 50' 47" W	59.19'
L19	5 05° 58' 57" W	145.60'
L20	5 09° 15' 48" W	45.52'

* TIE LINE TO T/L STONE POST
 5 52° 05' 28" E 13.56' ±

LEGEND
 IPF IRON PIPE FOUND
 IP5 IRON PIPE SET
 IPFF IRON PIPE FOUND IN FILE OF STONES
 S STONE WALL
 U UTILITY POLE
 P PAVED ROAD
 G GRAVEL ROAD
 T TAX MAP LOT #



REVISIONS: DATE:

OWNER/DEVELOPER:
CHESHIRE COUNTY SHOOTING SPORTS EDUCATION FOUNDATION
 PO BOX 233
 KEENE, NH 03431

ENGINEER:

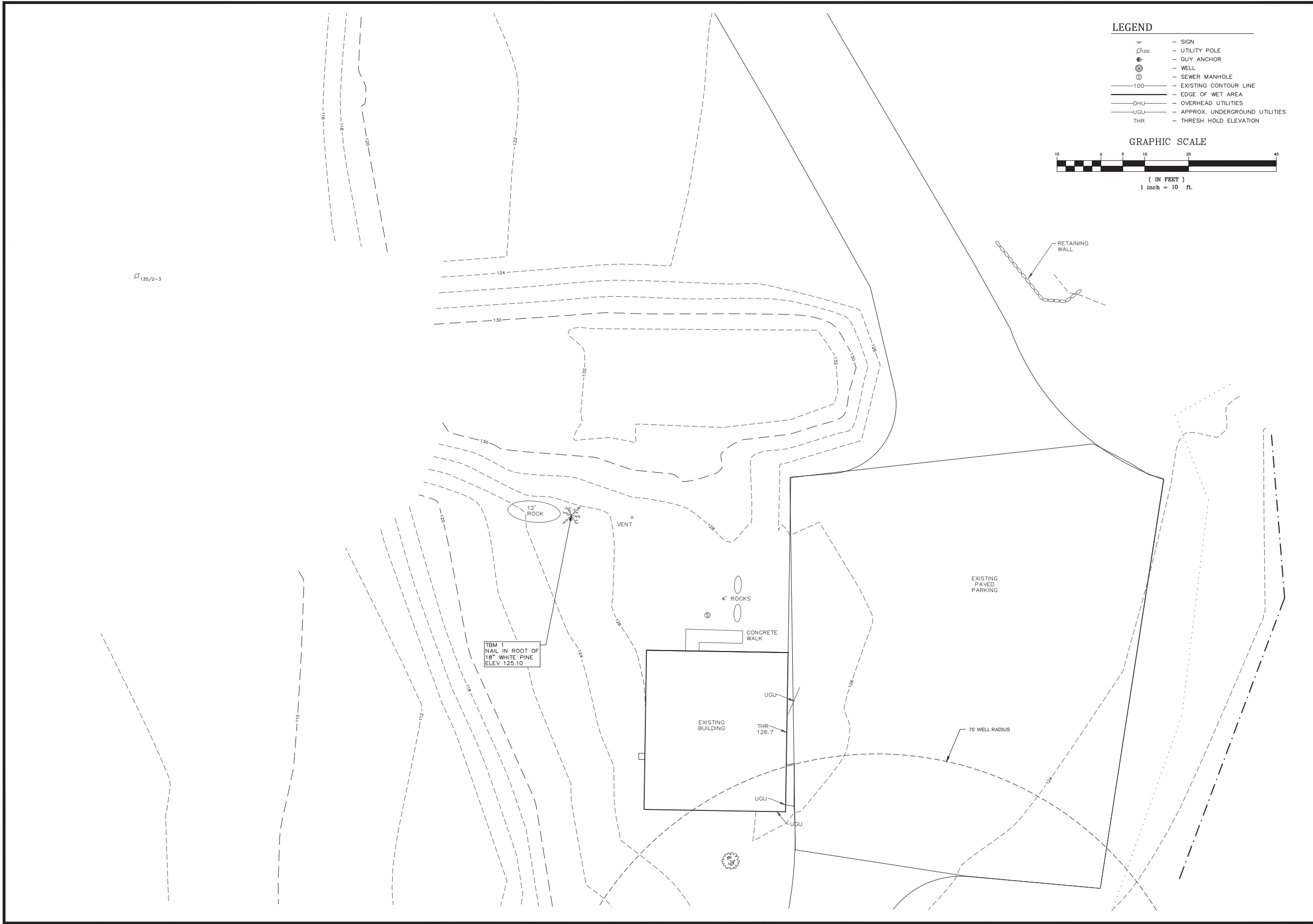
PLANNER:
Brickstone Land Use Consultants, LLC
 Site Planning, Permitting and Development Consulting
 185 Winchester Street, Keene, NH 03431
 Phone: (603) 357-0116

PROPOSED INDOOR SHOOTING RANGE
 19 FERRY BROOK ROAD
 KEENE, NH

VICINITY PLAN

SCALE: 1"=200'

DATE: 7/15/2020



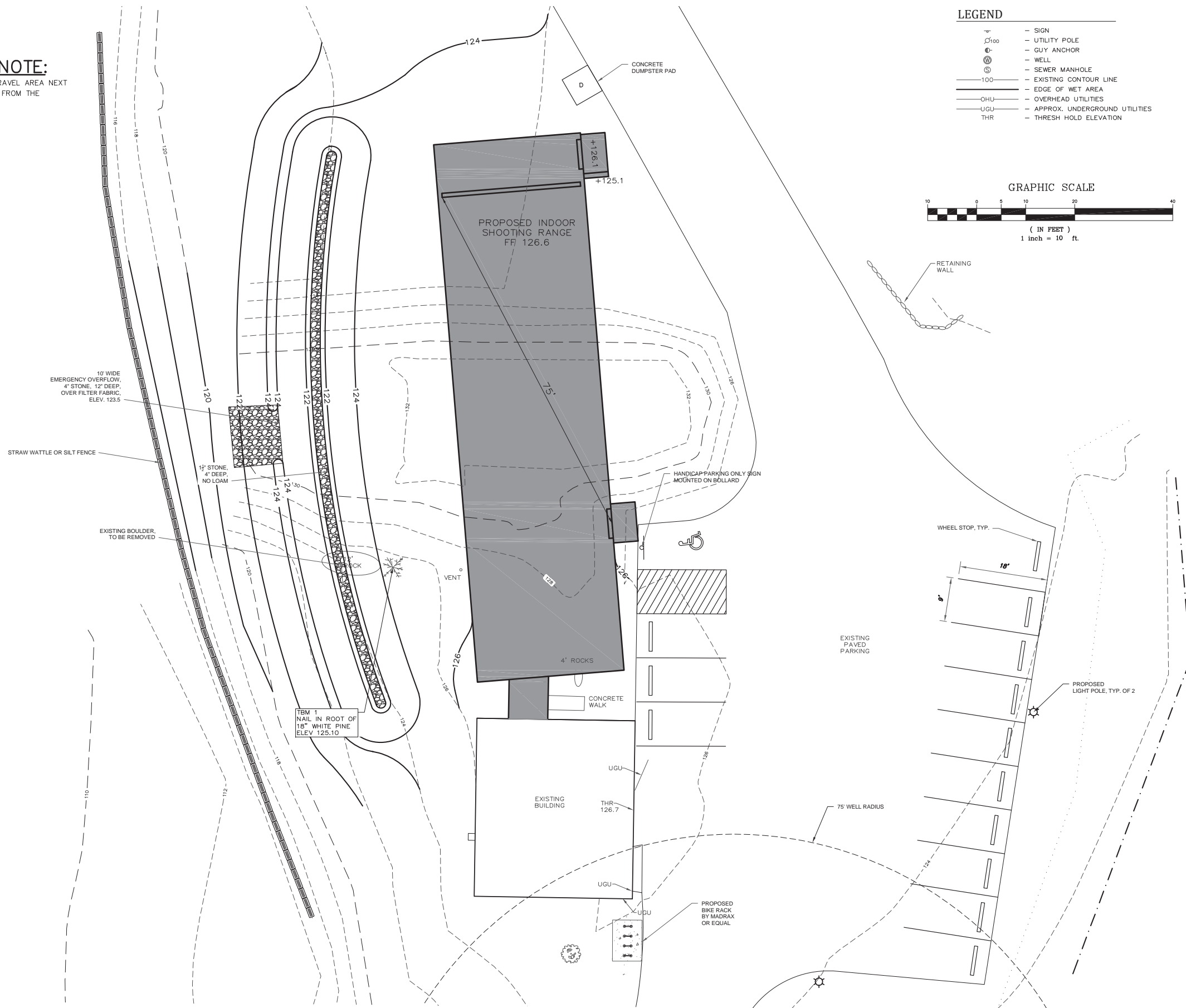
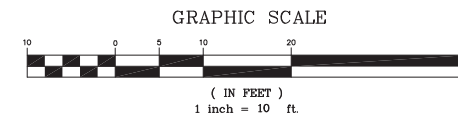
REVISIONS:	DATE:
OWNER/DEVELOPER: CHESHIRE COUNTY SHOOTING SPORTS EDUCATION FOUNDATION PO BOX 233 KEENE, NH 03431	
PLANNER: Brickstone Land Use Consultants, LLC <small>Site Planning, Permitting and Development Consulting 185 Winchester Street, Keene, NH 03431 Phone: (603) 357-0116</small>	
PROPOSED INDOOR SHOOTING RANGE 19 FERRY BROOK ROAD KEENE, NH	
EXISTING CONDITION PLAN	
SCALE: 1"=10'	
DATE: 5/18/2020	
SHEET 1	

OVERFLOW PARKING NOTE:

OVERFLOW PARKING WILL BE PROVIDED IN THE GRAVEL AREA NEXT TO THE LEACHFIELD AND ACROSS THE DRIVEWAY FROM THE LEACHFIELD.

LEGEND

- SIGN
- UTILITY POLE
- GUY ANCHOR
- WELL
- SEWER MANHOLE
- EXISTING CONTOUR LINE
- EDGE OF WET AREA
- OVERHEAD UTILITIES
- APPROX. UNDERGROUND UTILITIES
- THRESH HOLD ELEVATION



REVISIONS: DATE:
 ADDED WHEEL STOPS,
 BOLLARD & ADA SPACE
 8/7/20
 ADDED PARKING SPACES
 8/13/20

OWNER/DEVELOPER:
**CHESHIRE COUNTY
 SHOOTING SPORTS
 EDUCATION FOUNDATION**
 PO BOX 233
 KEENE, NH 03431

ENGINEER:
SVE
 SVE Associates
 P.O. Box 1818
 439 West River Road
 Brattleboro, VT 05302
 T 802.257.0561
 www.sveassoc.com

PLANNER:
**Brickstone
 Land Use Consultants, LLC**
 Site Planning, Permitting and Development Consulting
 185 Winchester Street, Keene, NH 03431
 Phone: (603) 357-0116

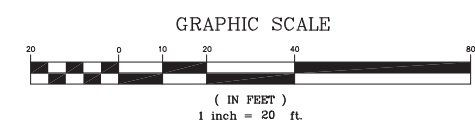
**PROPOSED INDOOR
 SHOOTING RANGE**
 19 FERRY BROOK ROAD
 KEENE, NH

**PROPOSED
 CONDITION
 PLAN**

SCALE: 1"=10'

DATE: 7/17/2020

SHEET 2



OWNER/DEVELOPER:
**CHESHIRE COUNTY
 SHOOTING SPORTS
 EDUCATION FOUNDATION**
 PO BOX 233
 KEENE, NH 03431

PLANNER:
**Brickstone
 Land Use Consultants, LLC**
Site Planning, Permitting and Development Consulting
 185 Winchester Street, Keene, NH 03431
 Phone: (603) 357-0116

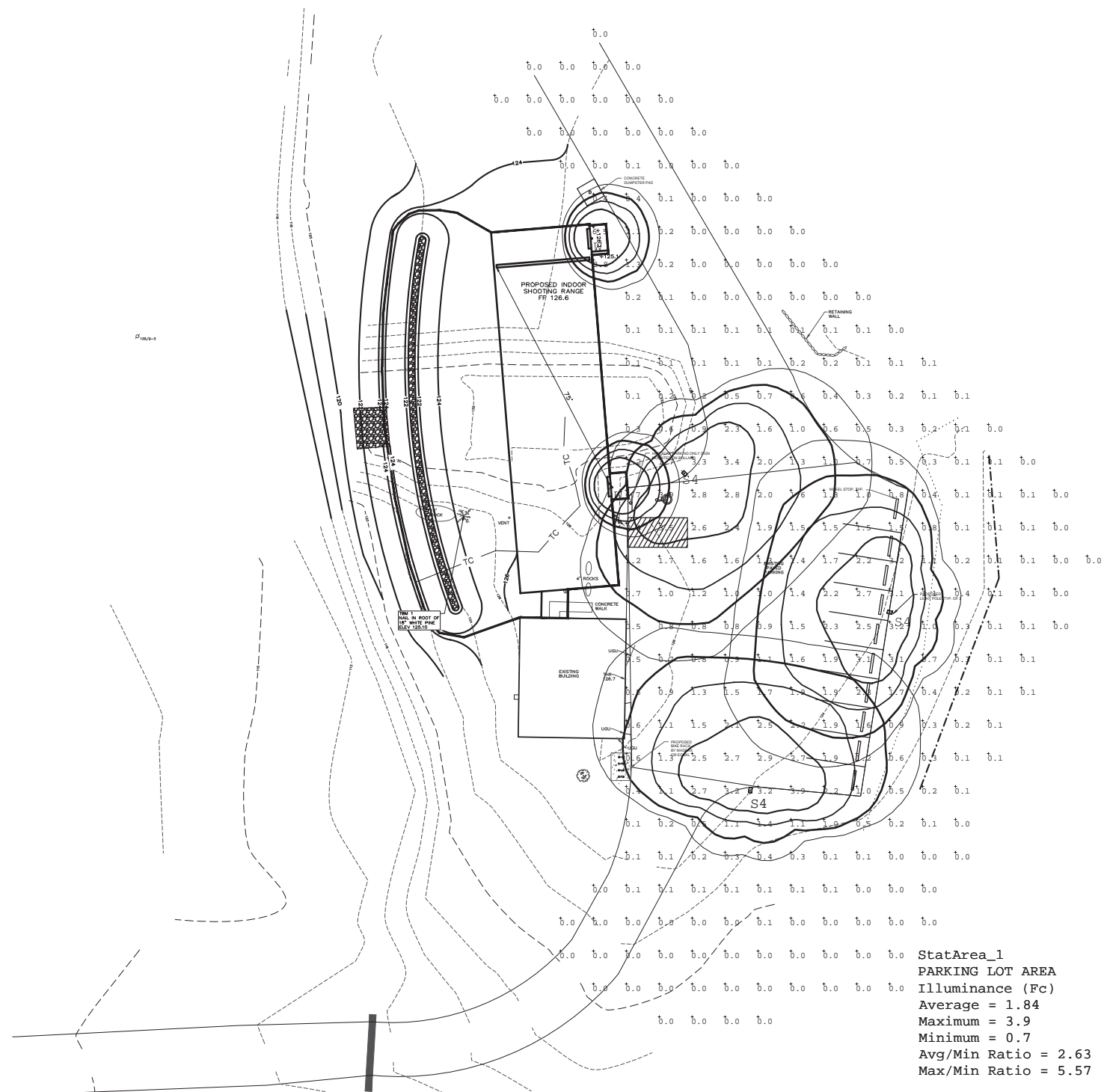
PROPOSED INDOOR
 SHOOTING RANGE
 19 FERRY BROOK ROAD
 KEENE, NH

**SEPTIC
 PLAN**

SCALE: 1"=10'

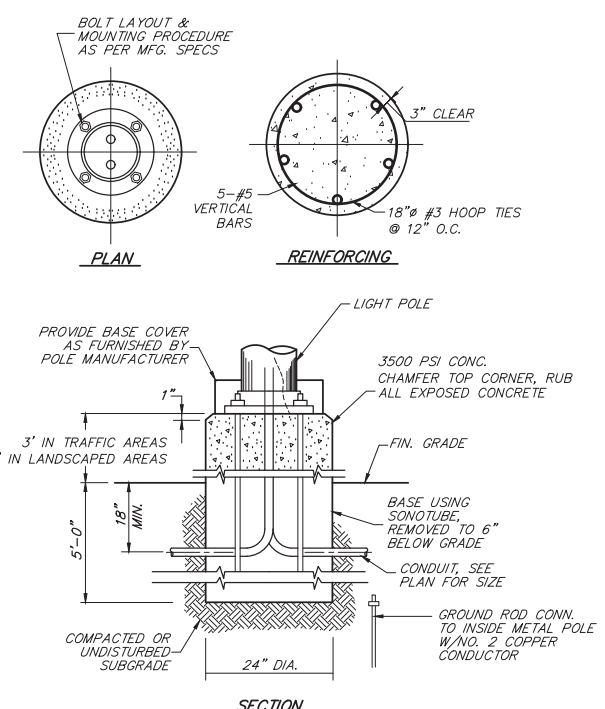
DATE: 7/17/2020

SHEET 3



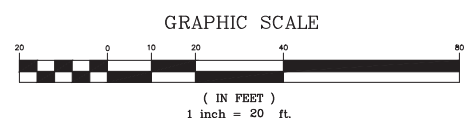
StatArea_1
 PARKING LOT AREA
 Illuminance (Fc)
 Average = 1.84
 Maximum = 3.9
 Minimum = 0.7
 Avg/Min Ratio = 2.63
 Max/Min Ratio = 5.57

Luminaire Schedule				
Symbol	Qty	Label	Arrangement	Description
→	3	S4	SINGLE	PRV-C15-D-UNV-T4-BZ/ SSS4A15SFN1 (15' AFG)
□	2	W	SINGLE	XTOR1B/ WALL MTD 9' AFG



NOTE: LIGHT POLE BASE LOCATIONS:
 1. TRAFFIC AREAS: 4 FEET FROM FACE OF POLE
 BASE TO PAVEMENT EDGE.
 2. PEDESTRIAN AREAS: 2 FEET FROM FACE OF POLE
 BASE TO SIDEWALK/TRAVEL WAY EDGE.

LIGHT POLE BASE DETAIL
 NOT TO SCALE



OWNER/DEVELOPER:
CHESHIRE COUNTY SHOOTING SPORTS EDUCATION FOUNDATION
 PO BOX 233
 KEENE, NH 03431

ENGINEER:
SVE
 SVE Associates
 P.O. Box 1818
 439 West River Road
 Brattleboro, VT 05302
 T 802.257.0561
 www.sveassoc.com

PLANNER:
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 Site Planning, Permitting and Development Consulting
 185 Winchester Street, Keene, NH 03431
 Phone: (603) 357-0116

PROPOSED INDOOR SHOOTING RANGE
 19 FERRY BROOK ROAD
 KEENE, NH

LIGHTING PLAN

SCALE: 1"=20'

DATE: 7/14/2020

SHEET 4

GENERAL CONSTRUCTION NOTES:

1. THE CONTRACTOR SHALL CALL DIG-SAFE, AT 1-888-344-7233 AT LEAST 72 HOURS BEFORE THE START OF EXCAVATION.
2. THE CONTRACTOR IS EXPECTED TO BE AWARE OF AND COMPLY WITH ALL PERMITS AND PERMIT CONDITIONS.
3. ALL TRENCHING, EXCAVATION, SHEETING, SHORING, ETC. SHALL COMPLY WITH THE MOST CURRENT OSHA REGULATIONS.
4. THE CONTRACTOR SHALL NOTIFY SVE ASSOCIATES IF FIELD CONDITIONS VARY FROM THAT SHOWN ON THE PLAN(S). THE CONTRACTOR'S WORK SHALL NOT VARY FROM THE PLAN(S) UNLESS SO AUTHORIZED BY SVE ASSOCIATES.
5. ALL WORK SHALL BE COMPLETED IN ACCORDANCE WITH SITE PLANS AND SPECIFICATIONS PROVIDED OR IN ACCORDANCE WITH NH DEP'T OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, LATEST EDITION.
6. IN CASE OF CONFLICTS, THE MOST STRINGENT INTERPRETATION OF THE PLANS, SPECIFICATIONS, LOCAL OR STATE REGULATIONS, OR PERMIT CONDITIONS SHALL APPLY. THE ENGINEER SHALL BE THE DETERMINANT AS TO WHAT APPLIES.
7. ALL KNOWN SUBSURFACE UTILITIES AND STRUCTURES HAVE BEEN INDICATED ON THE PLAN(S) AS ACCURATELY AS POSSIBLE. THE EXACT LOCATION MAY VARY AND THE CONTRACTOR IS CAUTIONED TO PROCEED WITH CARE.
8. CONTRACTOR SHALL VERIFY ALL BENCH MARKS, INVERTS, PIPES AND STRUCTURES ELEVATIONS PRIOR TO START OF WORK. IMMEDIATELY NOTIFY SVE ASSOCIATES IF THE FIELD INFORMATION DOES NOT MATCH PLAN INFORMATION.
9. THE OWNER WILL PROVIDE BENCH MARKS. THE CONTRACTOR WILL BE RESPONSIBLE FOR ALL OTHER LAYOUT AND FOR REPLACEMENT OF LAYOUT COMPLETED BY THE OWNER.
10. CONTRACTOR SHALL PROVIDE A FULL SET OF AS-BUILT DRAWINGS TO THE OWNER WITH SWING TIES OR COORDINATES, LOCATING ALL VALVES, FITTINGS, STRUCTURES, PIPES, ETC. THE AS-BUILTS SHALL INDICATE MATERIALS, PIPE LENGTHS INSTALLED, ALL INVERTS, AND ALL STRUCTURE ELEVATIONS. ACCEPTANCE OF THE WORK IS SUBJECT TO ACCEPTANCE OF THE AS-BUILTS BY THE ENGINEER AND OWNER.
11. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL DE-WATERING AT NO ADDITIONAL COST TO THE OWNER.
12. ALL SURFACES SHALL BE GRADED TO DRAIN.
13. THE CONTRACTOR SHALL RESTORE ALL DISTURBED SURFACES TO THEIR ORIGINAL CONDITION OR BETTER. ALL NEW AND EXISTING PIPES AND STRUCTURES SHALL BE CLEANED.

SEDIMENT AND EROSION CONTROL NOTES:

1. INSTALL ALL SEDIMENT & EROSION CONTROL MEASURES IN ACCORDANCE WITH MANUFACTURER'S DIRECTION OR DETAILS PROVIDED. PERIMETER CONTROLS MUST BE INSTALLED PRIOR TO EARTH MOVING OPERATIONS.
2. THE CONTRACTOR IS RESPONSIBLE FOR ALL EROSION CONTROL. THE CONTRACTOR SHALL TAKE ALL MEASURES NEEDED TO MINIMIZE EROSION TO THE GREATEST EXTENT POSSIBLE, AT NO ADDITIONAL COST TO THE OWNER, REGARDLESS OF DETAIL SHOWN ON THESE PLANS.
3. CONTRACTOR SHALL INSPECT AND REPAIR ALL SEDIMENT AND EROSION CONTROL MEASURES DAILY WHILE UNDER CONSTRUCTION, THEN AFTER EACH RAINFALL OR 0.5" IN 24 HOURS AND NOT LESS THAN ONCE A WEEK THEREAFTER UNTIL ALL UPHILL SOILS ARE WELL STABILIZED.
4. SEED, FERTILIZE & MULCH ALL FINISH GRADED AREAS WITHIN 72 HOURS OF FINISH GRADING.
5. SEDIMENT CONTROLS AND/OR SILT FENCES SHALL BE REPLACED WHEN CLOGGED AND NO LONGER FUNCTIONAL.
6. SEDIMENT CONTROLS AND/OR SILT FENCES SHALL REMAIN IN PLACE UNTIL ALL UPHILL VEGETATED AREAS ARE STABILIZED.
7. ALL SOIL STOCKPILES SHALL BE SEEDED AND MULCHED IF LEFT IN PLACE MORE THAN 21 DAYS.
8. SEEDING OF ALL DISTURBED AREAS SHALL BE COMPLETED NOT LATER THAN OCTOBER 15TH.
9. STABILIZATION OF ALL WORK AREAS SHALL BE COMPLETED NOT MORE THAN 45 DAYS FOLLOWING THE START OF WORK.
10. ALL SOIL SLOPES STEEPER THAN 3:1 SHALL BE COVERED WITH EROSION CONTROL FABRIC, S150 FROM NORTH AMERICAN GREEN OR APPROVED EQUAL.
11. STABILIZE ALL DRAINAGE SWALES, LEVEL SPREADERS, BERMS, AND DITCHES PRIOR TO DIRECTING RUNOFF TO THEM.
12. CONTRACTOR SHALL IMMEDIATELY REPAIR OR REPLACE SEDIMENT AND EROSION CONTROLS AS REQUESTED BY THE ENGINEER.

SITE DATA TABLE

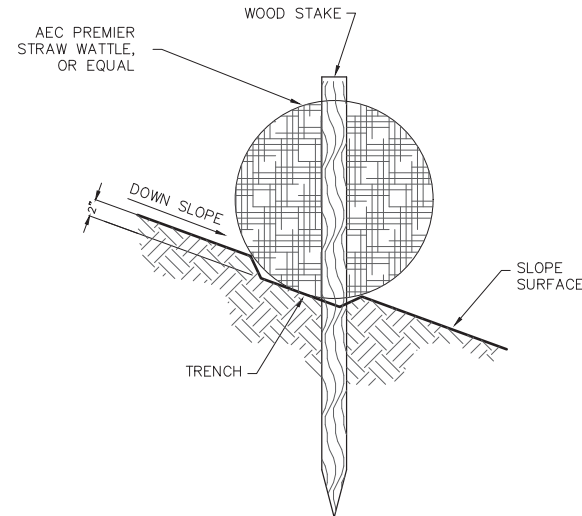
KEENE TAX MAP #: 904-02-001-0000,
SULLIVAN TAX MAP #: 5-010-000
5,641,020 SQ. FT. ± 129.5± ACRES

ZONE:		RURAL	
LOT SIZE:	AVAILABLE: 129.5	REQUIRED: 5 AC	
FRONTAGE:	> 50 FEET	50 FEET	
LOT WIDTH:	> 200 FEET	200 FEET	
BLDG. HEIGHT:	ALLOWED: 35 FT	PROPOSED: < 35 FEET	
BUILDING SETBACKS:			
FRONT:	50'		
REAR:	50'		
SIDES:	50'		
LOT COVERAGE:			
BUILDINGS:	MAXIMUM: 10% (239,580 S.F./ 5.5 AC)	PROPOSED: 0.2% (6,770 S.F./ 0.15 AC)	
GRAVEL/PAVEMENT:	20% (479,160 S.F./ 11 AC)	1.9% (47,500 S.F./ 1.1 AC)	
TOTAL IMPERMEABLE:	20% (479,160 S.F./ 11 AC)	2.3% (54,270 S.F./ 1.25 AC)	
PARKING:			
9' X 18':	REQUIRED: 1 PER 500 SF ASSEMBLY SPACE	PROPOSED: 10 SPACES	
	4,600 SF/500 = 10		
A.D.A. ACCESSIBLE:	1	1 ADA	

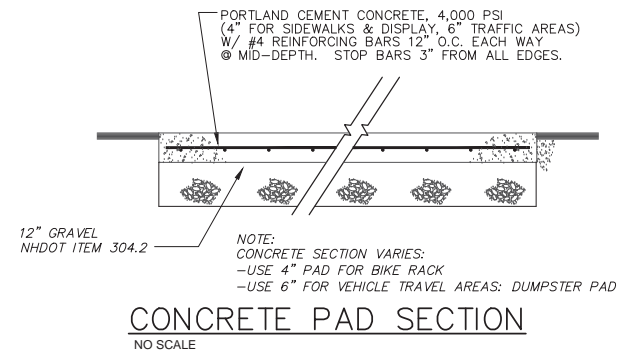
SEQUENCE OF WORK

THE SEQUENCE OF WORK SHALL BE FOLLOWED WITHIN EACH PHASE OF THE PROJECT. AT NO TIME OR PLACE SHALL PROJECT PHASING SUPERCEDE SOUND SEDIMENT AND EROSION CONTROL PLANNING.

1. INSTALL SILT FENCE IN ACCORDANCE WITH MANUFACTURER'S DIRECTIONS, IN LOCATIONS DETAILED ON THIS PLAN OR AS ORDERED BY THE ENGINEER.
2. INSTALL NEW LEACHFIELD & LEVEL SPREADER.
3. REMOVE EXISTING LEACHFIELD.
4. CONSTRUCT NEW ADDITION IN ACCORDANCE WITH APPROVED PLANS.
5. LOAM AND SEED DISTURBED AREAS, STABILIZE SLOPES WITH MATTING WHERE SPECIFIED.
6. REMOVE SILT FENCE AFTER ALL UPHILL SOILS ARE STABILIZED.



STRAW WATTLE
NOT TO SCALE



SEED SPECIFICATIONS

PERMANENT SEED:

ALL MOWABLE AREAS: PARK SEED NHDOT TYPE 15 (CONSERVATION MIX ACCEPTABLE, AS APPROVED BY ENGINEER)

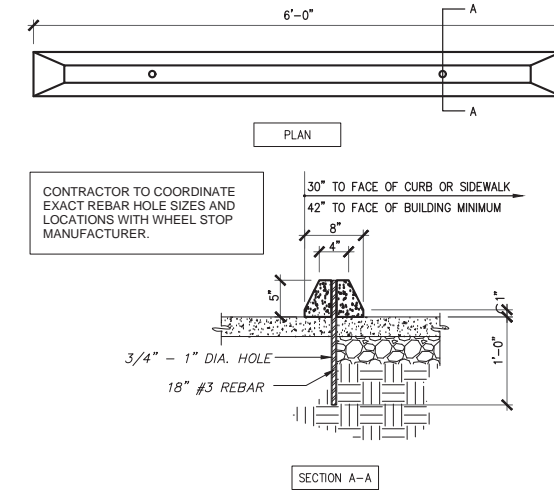
CREeping RED FESCUE	40 LB/AC
PERENNIAL RYEGRASS	50 LB/AC
KENTUCKY BLUEGRASS	25 LB/AC
REDTOP	5 LB/AC
TOTAL:	120 LB/AC

ALL SLOPES 5:1 OR STEEPER: SLOPE SEED NHDOT TYPE 45 (OR OTHER WILDFLOWER MIX APPROVED BY ENGINEER)

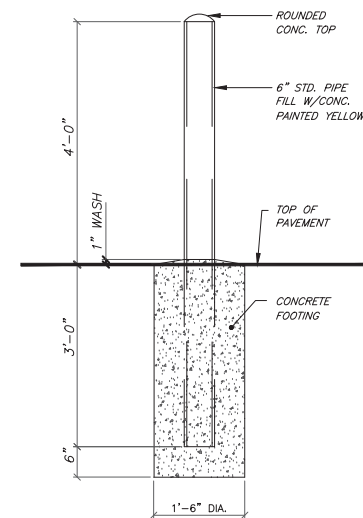
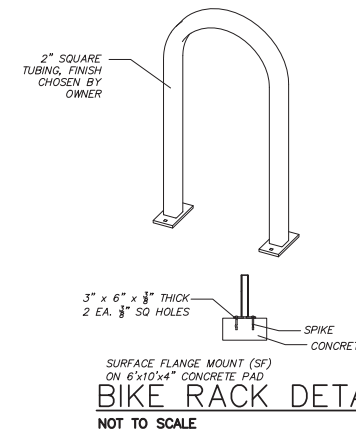
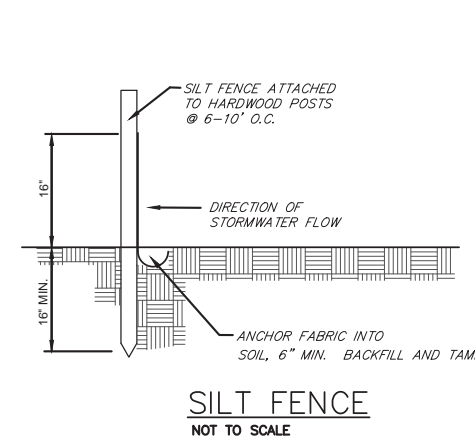
CREeping RED FESCUE	35 LB/AC
PERENNIAL RYEGRASS	30 LB/AC
REDTOP	5 LB/AC
ALSIKE CLOVER	5 LB/AC
LANCE-LEAVED COREOPSIS	5 LB/AC
OXEYE DAISY	3 LB/AC
BUTTERFLY WEED	3 LB/AC
BLACKEYED SUSAN	3 LB/AC
WILD LUPINE	3 LB/AC
TOTAL:	95 LB/AC

PROJECT SPECIFIC NOTES:

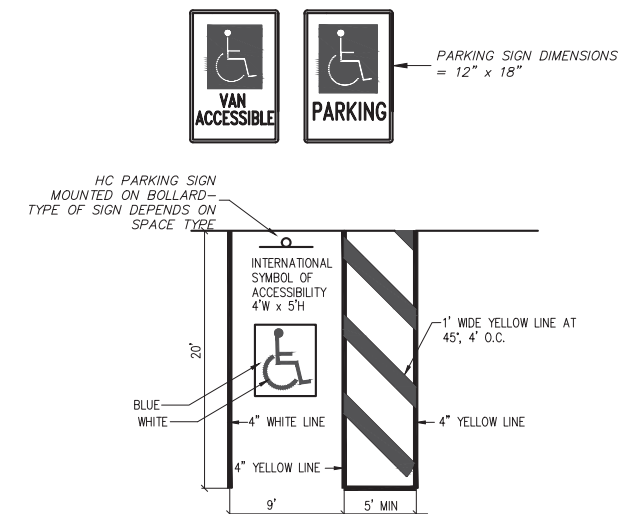
1. SEEDING OF ALL DISTURBED AREAS SHALL BE COMPLETED NOT LATER THAN OCTOBER 15TH.
2. SEEDING OF ALL FINISHED AREAS SHALL BE COMPLETED NOT MORE THAN 72 HOURS AFTER FINISH GRADING.
3. STABILIZATION OF ALL WORK AREAS SHALL BE COMPLETED NOT MORE THAN 45 DAYS FOLLOWING THE START OF WORK.
4. ALL NEW EXTERIOR LIGHTS SHALL BE SHIELDED TO PROTECT AGAINST ADDED LIGHT POLLUTION.
5. STABILIZE ALL DRAINAGE SWALES PRIOR TO DIRECTING RUNOFF TO THEM.



PRECAST CONCRETE WHEEL STOP
NOT TO SCALE



BOLLARD DETAIL
NOT TO SCALE



WHERE ACCESSIBLE PARKING SPACES ARE ADJACENT TO LANDSCAPED ISLANDS AND PROTECTED BY CURB, PLACE SIGN TWO FEET INSIDE BACK OF CURB.

WHERE ACCESSIBLE SPACES ARE ADJACENT TO A SIDEWALK, PLACE SIGN AT FACE OF CURB, INSIDE CONCRETE FILLED GALVANIZED STEEL BOLLARD (SEE DETAIL).

HANDICAP PARKING SPACE DETAIL
NOT TO SCALE

REVISIONS: DATE:
ADDED WHEEL STOPS,
BOLLARD & ADA SPACE
8/7/20

OWNER/DEVELOPER:
CHESHIRE COUNTY
SHOOTING SPORTS
EDUCATION FOUNDATION
PO BOX 233
KEENE, NH 03431

ENGINEER:
SVE
SVE Associates
P.O. Box 1818
439 West River Road
Brattleboro, VT 05302
T 802.257.0561
www.sveassoc.com

PLANNER:
Brickstone
Land Use Consultants, LLC
Site Planning, Permitting and Development Consulting
185 Winchester Street, Keene, NH 03431
Phone: (603) 357-0116

PROPOSED INDOOR
SHOOTING RANGE
19 FERRY BROOK ROAD
KEENE, NH

CONSTRUCTION
DETAILS

SCALE: AS NOTED

DATE: 7/17/2020

SHEET 5

These drawings are LIMITED SCOPE and are intended only to describe general design intent, scale, overall spatial relationships and material where indicated. These drawings shall be considered preliminary for purposes of design review, comment, or budget pricing only, unless expressly released for other purposes as indicated in the issue log. The architect assumes responsibility for errors in the information provided, and not for omissions.

Architect:
Timothy Sampson
 NCARB, LEED AP
 103 Roxbury Street
 Suite 206
 Keene, NH
 603 769 7736



Engineer:

SAMPSON ARCHITECTS
 2019 W0# 19-057

Prepared For
CCSSEF
 19 Ferrybrook Road
 Keene, NH 03431

PROJECT
Indoor Shooting Range
 19 Ferrybrook Road
 Keene, NH 03431

TITLE
Elevations Sections

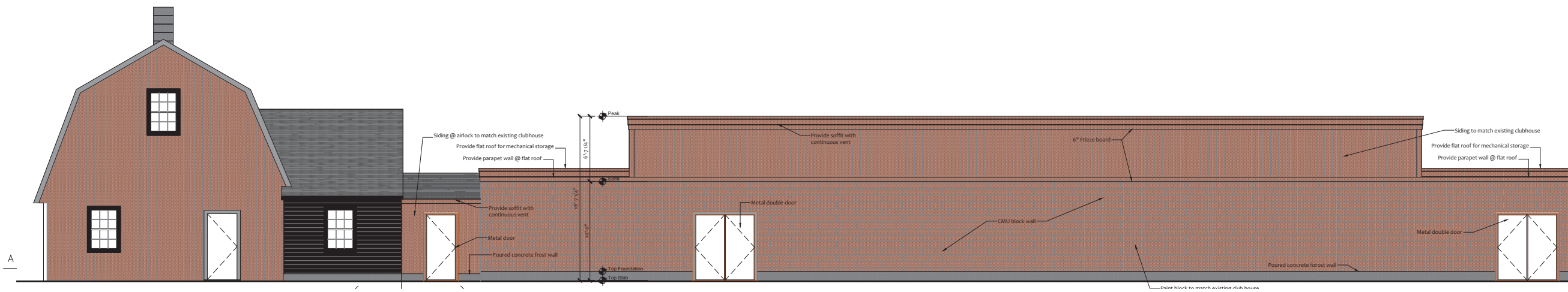
Date:	Revisions:
7.17.20	Planning Board
-	-
-	-

SCALE As Noted

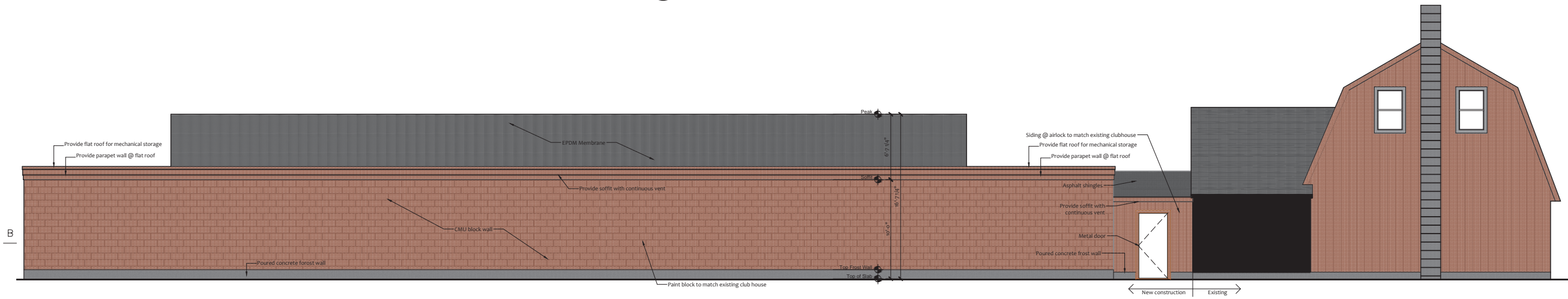
DATE 7.16.20

SHEET NUMBER

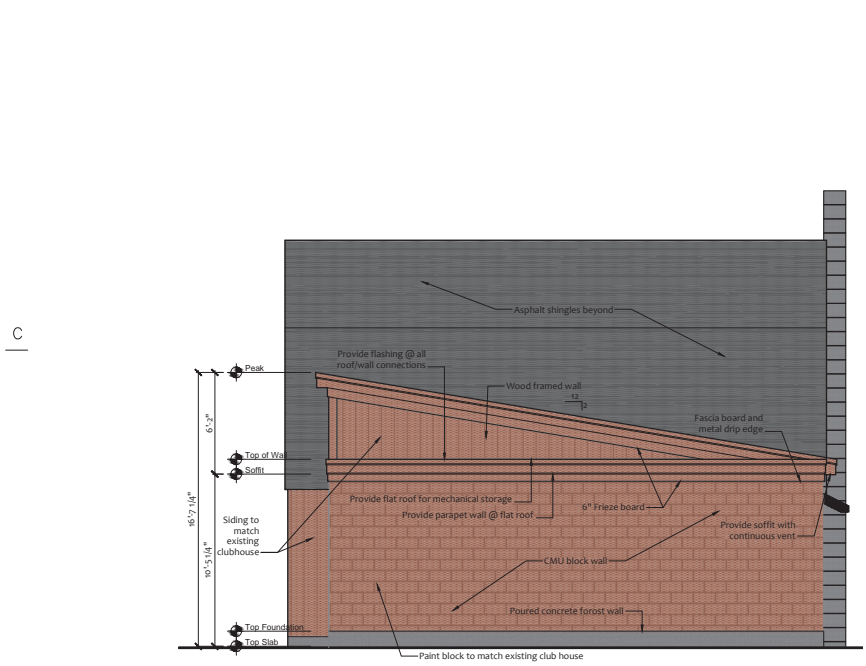
A2
 54 of 59



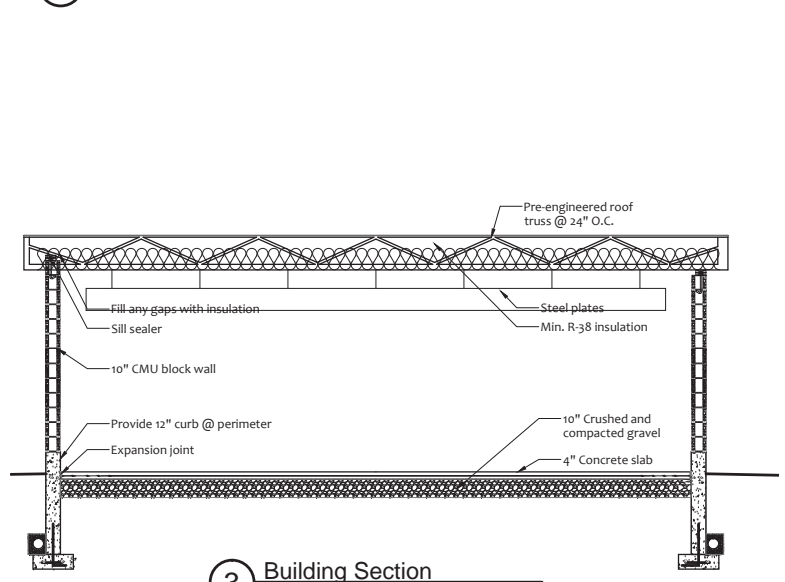
Front Elevation
 Scale: 1/4" = 1'-0"



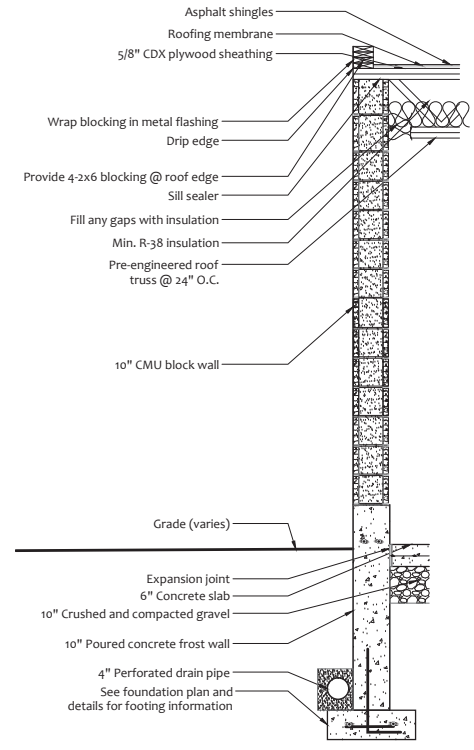
Rear Elevation
 Scale: 1/4" = 1'-0"



Right Elevation
 Scale: 1/4" = 1'-0"



3 Building Section
 Scale: 1/4" = 1'-0"



Wall Section @ Flat Roof
 Scale: 1/2" = 1'-0"

Project		Catalog #		Type	
Prepared by		Notes		Date	



Lumark

PRV / PRV-XL Prevail LED

Area / Site Luminaire

Typical Applications

Outdoor • Parking Lots • Walkways • Roadways • Building Areas

Interactive Menu

- Ordering Information page 2
- Mounting Details page 3
- Optical Configurations page 3
- Product Specifications page 3
- Energy and Performance Data page 4
- Control Options page 5

Product Certifications



Product Features

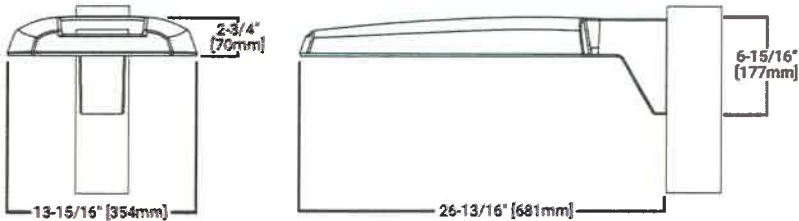


Quick Facts

- Lumen packages range from 7,100 - 48,600 lumens (50W - 350W)
- Replaces 70W up to 1,000W HID equivalents
- Efficacies up to 148 lumens per watt
- Energy and maintenance savings up to 85% versus HID solutions
- Standard universal quick mount arm with universal drill pattern

Dimensional Details

Prevail



Prevail XL



DESCRIPTION

The patented Lumark Crosstour™ LED Wall Pack Series of luminaires provides an architectural style with super bright, energy efficient LEDs. The low-profile, rugged die-cast aluminum construction, universal back box, stainless steel hardware along with a sealed and gasketed optical compartment make the Crosstour impervious to contaminants. The Crosstour wall luminaire is ideal for wall/surface, inverted mount for façade/canopy illumination, post/bollard, site lighting, floodlight and low level pathway illumination including stairs. Typical applications include building entrances, multi-use facilities, apartment buildings, institutions, schools, stairways and loading docks test.

SPECIFICATION FEATURES

Construction

Slim, low-profile LED design with rugged one-piece, die-cast aluminum hinged removable door and back box. Matching housing styles incorporate both a small and medium design. The small housing is available in 12W, 18W and 26W. The medium housing is available in the 38W model. Patented secure lock hinge feature allows for safe and easy tool-less electrical connections with the supplied push-in connectors. Back box includes three half-inch, NPT threaded conduit entry points. The universal back box supports both the small and medium forms and mounts to standard 3-1/2" to 4" round and octagonal, 4" square, single gang and masonry junction boxes. Key hole gasket allows for adaptation to junction box or wall. External fin design extracts heat from the fixture surface. One-piece silicone gasket seals door and back box. Minimum 5" wide pole for site lighting application. Not recommended for car wash applications.

Optical

Silicone sealed optical LED chamber incorporates a custom engineered mirrored anodized reflector providing high-efficiency illumination. Optical assembly includes impact-resistant tempered glass and meets IESNA requirements for full cutoff compliance. Available in seven lumen packages; 5000K, 4000K and 3000K CCT.

Electrical

LED driver is mounted to the die-cast housing for optimal heat sinking. LED thermal management system incorporates both conduction and natural convection to transfer heat rapidly away from the LED source. 12W, 18W, 26W and 38W series operate in -40°C to 40°C [-40°F to 104°F]. High ambient 50°C models available. Crosstour luminaires maintain greater than 89% of initial light output after 72,000 hours of operation. Three half-inch NPT threaded conduit entry points allow for thru-branch wiring. Back box is an authorized

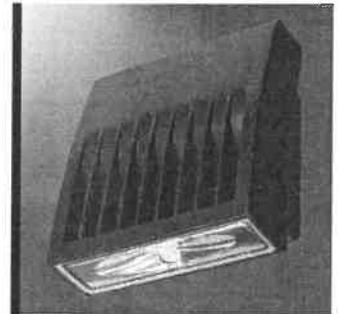
electrical wiring compartment. Integral LED electronic driver incorporates surge protection. 120-277V 50/60Hz or 347V 60Hz models.

Finish

Crosstour is protected with a Super durable TGIC carbon bronze or summit white polyester powder coat paint. Super durable TGIC powder coat paint finishes withstand extreme climate conditions while providing optimal color and gloss retention of the installed life.

Warranty

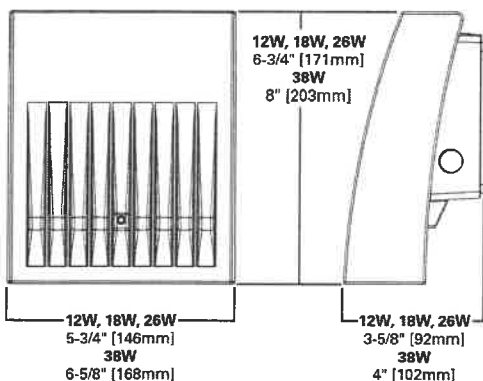
Five-year warranty.



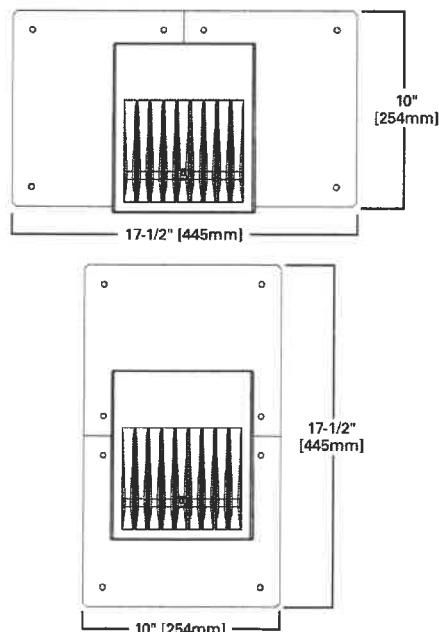
XTOR CROSSTOUR LED

APPLICATIONS:
WALL / SURFACE
POST / BOLLARD
LOW LEVEL
FLOODLIGHT
INVERTED
SITE LIGHTING

DIMENSIONS



ESCUTCHEON PLATES



CERTIFICATION DATA

UL/cUL Wet Location Listed
LM79 / LM80 Compliant
ROHS Compliant
ADA Compliant
NOM Compliant Models
IP66 Ingressed Protection Rated
Title 24 Compliant
DesignLights Consortium® Qualified*

TECHNICAL DATA

40°C Maximum Ambient Temperature
External Supply Wiring 90°C Minimum

EPA

Effective Projected Area (Sq. Ft.):
XTOR1B, XTOR2B, XTOR3B=0.34
XTOR4B=0.45

SHIPPING DATA:

Approximate Net Weight:
3.7 - 5.25 lbs. [1.7 - 2.4 kgs.]

January 16, 2013

David Bergeron
Brickstone Land Use Consultants, LLC
182 Winchester Street
Keene, NH 03431

RE: Traffic Analysis, CCSEF Indoor Range
19 Ferry Brook Road, Keene, NH

Dear Mr. Bergeron:

As per your request, I have conducted a brief traffic study of the above referenced project. It is proposed to construct an indoor range as part of the Cheshire County Fish & Game Club. The indoor range is known as the Cheshire County Shooting Sports Education Foundation (CCSSEF).

Proposed Development

The club operates an existing outdoor range located on the east side of Ferry Brook Road just west of Sullivan Road and north of Route 9 in northeastern Keene. The club is a private facility providing various types of outdoor ranges for its approximately 700 members. It is proposed to construct a 26,000± square foot indoor shooting range on the same site. Access to the indoor range will be provided via the existing 18 foot wide gravel driveway on Ferry Brook Road.

Existing Roadway Conditions

Ferry Brook Road is a municipally maintained road beginning in Keene at Sullivan Road and ending in Sullivan at South Road. Ferry Brook Road essentially parallels Sullivan Road/South Road for its entire length. Ferry Brook Road is approximately 20 feet wide plus gravel shoulders at the site driveway. It is posted for 30 miles per hour in Keene and 25 miles per hour in Sullivan. Sight distance for the site driveway appears to be adequate based on the posted speed limit. Ferry Brook Road is currently not a through street since a May flooding event damaged the road crossing over Ferry Brook. The Town of Sullivan is working on plans to reconstruct the bridge. In 2009, the average daily volume on Ferry Brook Road was reported to be 110 vehicles per day over Ferry Brook in Keene. The area is very rural with the range occupying most of the property along the east side of Ferry Brook Road between Sullivan Road and the town line. There are no homes on the west side of Ferry Brook Road between Sullivan Road and the range driveway. Three homes are located beyond the range and before the closed bridge.

Sullivan Road carries traffic between Route 9 and the center of Sullivan. At Ferry Brook Road, Sullivan Road is 18-22 feet wide. Ferry Brook Road intersects Sullivan Road just north of Route 9. There are two homes located between Route 9 and Ferry Brook Road. There is no traffic volume reported on Sullivan Road in the public record, however, it is estimated that volumes are below 500 vehicles per day. At the intersection with Route 9, Sullivan Road forms a four-legged intersection since it continues to the south in Keene. Route 9 is a state maintained

east-west highway. STOP signs provide traffic control for traffic entering Route 9 from either direction on Sullivan Road. At the intersection, Route 9 provides for one travel lane and paved shoulder eastbound and two travel lanes and a shoulder westbound. The second travel lane on Route 9 westbound is a climbing lane which begins just east of Sullivan Road due to a vertical grade. The posted speed limit on Route 9 at Sullivan Road is 55 miles per hour. In 2009, the average daily volume on Route 9 was reported to be 7400 vehicles per day east of Sullivan Road in Keene. Sight distance on Route 9 at Sullivan Road (north leg) appears to be at least 1000 feet in either direction. Advance intersection signs are posted on both directions of Route 9 with a street name plaque identifying Sullivan Road as the cross street.

Site Trip Generation

Traffic counts were not conducted at the existing outdoor range since winter season use is limited. The purpose of this analysis is to estimate the number of additional trips generated by the proposed CCSSEF indoor range. Typically, trip generation for new developments is based on data found in the ITE Trip Generation manual.¹ Unfortunately, there is no data available for indoor ranges. To estimate trip generation, the expected daily activity for the proposed indoor range was discussed with Mr. Ed Csenge, Project Manager, CCSSEF. The following operational characteristics were identified:

1. The indoor range will provide 40 lanes along with a multi-purpose room, 24 seat classroom, a kitchen and office areas.
2. It is expected that one or two employees will be on-site during normal operations.
3. The indoor range will be open to members during a typical weekday and often used for collegiate shooting matches on weekends. Training and other events may be open to the public.
4. Instructional classes are currently held in a small building at the existing club. These classes will relocate to the new indoor range.
5. The existing club has peak activity on Saturdays and the same is expected for the indoor range. During a typical Saturday, members arrive and leave throughout the day starting around 8 am and ending around 3-4 pm with peak activity around noon.
6. While the indoor range has 40 lanes, these are not used all at the same time. Many lanes are kept available in case of problems with other lanes. Perhaps 20 lanes in operation would be considered "capacity" for typical member usage.
7. The typical recreational shooter might spend one to two hours at the facility. During a collegiate match, students will spend an entire Saturday at the facility.
8. Spectator attendance at these events is limited. Total attendance for a match is estimated at 50 people. When these events are held, the indoor range may be closed to members.

On a typical weekday, it is estimated that about 50 people will visit CCSSEF throughout the day. Average vehicle occupancy is estimated at 1.5 persons per vehicle. Therefore, trip generation for CCSSEF on a typical weekday is estimated at approximately 70 trips (35 enter, 35 exit). Peak hour traffic is estimated at 10 vehicles (5 enter, 5 exit).

On a Saturday, during a collegiate shooting match, there may be 50 attendees at a match plus member usage and/or training after the match in the late afternoon and evening. It is estimated that 100 people will visit on a typical Saturday and 150 people may visit on a peak

¹ Trip Generation, 8th Edition, Institute of Transportation Engineers (ITE), 2008

letter to Mr. Bergeron
RE: CCSSEF Indoor Range, Keene
January 16, 2012
page 3

Saturday. Average vehicle occupancy is likely to be higher on Saturdays since many activities would involve parents with children, coaches with students, friends attending a class, etc. According to national statistics, social and recreational trips average 2.3 persons per vehicle. For this analysis, average vehicle occupancy for the indoor range traffic on a Saturday is conservatively estimated at 2.0 persons per vehicle. Therefore, trip generation for CCSSEF on a Saturday is estimated at approximately 100-150 trips (50-75 enter, 50-75 exit). Peak hour traffic is estimated at 20 vehicles (10 enter, 10 exit).

Site Trip Distribution

Trip generation is used to determine the number of trips entering and exiting the site. Trip distribution is used to describe where those trips will travel on the roadway network. According to the Fall 2012 newsletter, "Cheshire County Fish and Game Club, believed to be the largest sporting club of this type in southwestern New Hampshire, enjoys membership and visits from residents in Cheshire, Hillsborough, Merrimack, and Sullivan Counties in NH; Orleans, Rutland, and Windham Counties in VT; several towns in Connecticut, Massachusetts, and New York; and seven other states". Given the regional nature of the facility, nearly all of the traffic to and from the site is expected to travel via Route 9 to Sullivan Road to Ferry Brook Road. Only residents of Sullivan would find it convenient to access the site via Sullivan Road from the north.

Summary

The proposed CCSSEF indoor range, on the site of the existing Cheshire County Fish & Game Club, is estimated to generate 70 weekday trips, 100 trips on a typical Saturday and 150 trips on a peak Saturday. Most of the CCSEF traffic will access the site via Route 9 to Sullivan Road to Ferry Brook Road. Route 9 is a well-maintained state highway while Sullivan and Ferry Brook Roads are local, municipally managed roads with very low traffic volumes and a narrower traveled way. The estimated traffic increase for the proposed CCSEF indoor range is also low and can be accommodated on the surrounding roadways.

I trust this information has been helpful. Please call me if you have any questions.

Sincerely,



Laurie M. Rauseo, P.E., PTOE
NH PE #8396

