



## **City of Keene Planning Board**

### **AGENDA**

**Monday, November 25, 2024      6:30 PM      City Hall, 2<sup>nd</sup> Floor Council Chambers**

- I. **Call to Order** – Roll Call
- II. **Minutes of Previous Meeting** – October 28, 2024
- III. **Final Vote on Conditional Approvals**
- IV. **Public Hearings**
  - a. **PB-2024-15 – Major Site Plan – Monadnock Conservancy, 0 Ashuelot St** - Applicant the Monadnock Conservancy, on behalf of owner JRR Properties LLC, proposes to construct a ~6,215-sf office building and make associated site improvements on the parcel at 0 Ashuelot St (TMP #567-001-000). The parcel is ~3.53-ac in size and is located in the Commerce District.
  - b. **PB-2024-16 – Major Site Plan – Kia, 440 Winchester St** - Applicant Hutter Construction Corp, on behalf of owner 434-440 Winchester LLC, proposes to construct a new ~15,365-sf Kia dealership in place of the existing ~9,950-sf building and make associated site modifications on the property at 440 Winchester St (TMP #115-004-000). A Surface Water CUP is requested for temporary impacts within the 30' wetland buffer. Waivers are requested from Section 20.14.3.D, Section 21.7.3.C, and Section 21.7.4.A.1 of the LDC to allow for parking in front of the building and allowed lighting levels. The parcel is 2.23-ac in size and is located in the Commerce Limited District.
- V. **Master Plan Update** (<https://keenemasterplan.com/>)
- VI. **Training on Site Development Standards** – Snow Storage, Screening, & Landscaping
- VII. **Staff Updates**
- VIII. **New Business**
- IX. **Upcoming Dates of Interest**
  - Joint Committee of the Planning Board and PLD – December 9<sup>th</sup>, 6:30 PM
  - Planning Board Steering Committee – December 3<sup>rd</sup>, 11:00 AM
  - Planning Board Site Visit – December 11<sup>th</sup>, 8:00 AM – To Be Confirmed
  - Planning Board Meeting – December 16<sup>th</sup>, 6:30 PM

1 **City of Keene**  
2 **New Hampshire**

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5 **PLANNING BOARD**  
6 **MEETING MINUTES**  
7

8 **Monday, October 28, 2024**

**6:30 PM**

**Council Chambers,  
City Hall**

**Members Present:**

Harold Farrington, Chair  
Roberta Mastrogiovanni, Vice Chair  
Mayor Jay V. Kahn  
Councilor Michael Remy  
Sarah Vezzani  
Armando Rangel  
Kenneth Kost  
Michael Hoefler, Alternate  
Stephon Mehu, Alternate

**Staff Present:**

Mari Brunner, Senior Planner  
Megan Fortson, Planner

**Members Not Present:**

Ryan Clancy  
Randyn Markelon, Alternate  
Tammy Adams, Alternate

9 **I) Call to Order**

10 Chair Farrington called the meeting to order at 6:30 PM and a roll call was taken  
11

12 **II) Minutes of the Previous Meeting – September 23, 2024**  
13

14 Chair Farrington offered the following corrections:

15 Line 183 – add the word “access”

16 After Line 197 – add the phrase “the Chairman closed the public hearing”

17 After Line 234 – insert the phrase “original proposal by the applicant”

18 Line 254 – delete the word “but” not parallel to the wheels

19 Line 445 – “not parallel to the wheels”  
20

21 A motion was made by Mayor Kahn that the Planning Board accept the September 23, 2024  
22 meeting minutes as amended. The motion was seconded by Roberta Mastrogiovanni and was  
23 unanimously approved by roll call vote.  
24

25 **III) Final Vote on Conditional Approvals**  
26

27 The Chairman stated, as a matter of practice, the Board will now issue a final vote on all  
28 conditionally approved plans after all of the “conditions precedent” have been met.



29 This final vote will be the final approval and will start the 30-day appeal clock.  
30 He asked whether there were any applications tonight that are ready for a final vote.  
31 Ms. Brunner stated there are two applications that are ready for a final vote this evening.  
32 The first is project is CLSS-CUP-03-23, Congregate Living and Social Services Conditional Use  
33 Permit, for the Keene Serenity Center located at 24 Vernon Street. Ms. Brunner stated this  
34 application came before the committee almost a year ago, and the only condition that they had to  
35 meet was getting their license, which they received in September. They have now met all their  
36 conditions of approval and are looking for final approval.  
37 A motion was made by Roberta Mastrogiovanni that the Planning Board issue final approval for  
38 CLSS-CUP-03-23. The motion was seconded by Kenneth Kost and carried on a unanimous vote.  
39  
40 The second project for final approval was the Cottage Court Conditional Use Permit project PB-  
41 2024-09 for 30 High Street. This was to add a third unit within the existing building of a two-unit  
42 structure. There were three conditions attached to this application.  
43 The first condition was the owner's signature appears on the proposed plot plan, the second was  
44 submittal of five color paper copies of the approved plan, and the third was security in an amount  
45 acceptable to the Community Development Department to cover the cost of landscaping. Ms.  
46 Brunner stated that all three of those conditions have been met.  
47 A motion was made by Roberta Mastrogiovanni that the Planning Board issue final approval for  
48 PB-2024-09. The motion was seconded by Kenneth Kost and carried on a unanimous vote.

49

50 **IV) Public Hearings**

51 a. **PB-2024-13 – 2-Lot Subdivision – Habitat for Humanity, 0 Old Walpole Rd –**  
52 Applicant Monadnock Land Planning, on behalf of owner Monadnock Habitat for Humanity  
53 Inc., proposes to subdivide the ~7.18-ac parcel at 0 Old Walpole Rd (TMP #503-005-000) into  
54 two lots ~2.35-ac and ~4.83-ac in size. The parcel is located in the Rural District.

55

56

57 A. Board Determination of Completeness

58

59 Planner Mari Brunner recommended the Board accept the application as complete.

60

61 A motion was made by Roberta Mastrogiovanni to accept Application PB-2024-13 as complete.  
62 The motion was seconded by Kenneth Kost and was unanimously approved.

63

64 B. Public Hearing

65 Mr. Dave Bergeron of Monadnock Land Planning began by introducing Matt Keenan  
66 Monadnock Habitat for Humanity.

67

68 Mr. Keenan indicated Monadnock Habitat for Humanity is a nonprofit 501C3 affiliate of the  
69 International Habitat for Humanity. Their mission is to build affordable workforce housing so  
70 that it can be turned over through a no interest mortgage to a partner family.

71 The applicant puts in 300 hours of sweat equity to build their home. Mr. Keenan stated they  
72 fundraise to purchase land and materials and rely on volunteer labor to keep the cost down,  
73 which makes the home affordable for these partner families.  
74

75 He noted they have been in the area since 1991 and have built 16 homes in the southwestern part  
76 of the state. The most recent house they built was in 2020. He added that when they move  
77 applicants into homes it frees up homes for Keene Housing Authority.  
78

79 Mayor Kahn stated during the site visit the Board learned about the number of years covenants  
80 are placed on the properties by virtue of habitat being the developer. He asked for added  
81 clarification on this issue. Mr. Keenan explained because it is affordable housing, Habitat for  
82 Humanity holds the mortgage. The applicant pays them through no interest mortgage payments  
83 on a monthly basis. Habitat for Humanity has first right of refusal to purchase the home if the  
84 applicant needs to relocate or can't make the payments, and then they turn it into a home for  
85 another affordable housing family.  
86

87 He added in the mortgage they write deed restrictions for the applicant to sell, but they also give  
88 Habitat for Humanity the opportunity to take the home back and put another family in the home.  
89

90 The Mayor clarified that this remains affordable housing beyond the life of the first owner. Mr.  
91 Keenan answered in the affirmative. There are provisions in place so that the homeowner can't  
92 sell the property and take all the savings. He added there is a silent mortgage Habitat for  
93 Humanity gets back if anything happens, and they have to sell. If Habitat for Humanity can't  
94 afford to purchase the home back from the partner family with the first right of refusal, the  
95 provision goes away. The partner family can sell the home to whoever they want, but Habitat for  
96 Humanity gets that silent mortgage as funding for the next home.  
97

98 Mr. Kost asked whether or not there is a time period of commitment as a requirement so that a  
99 homeowner cannot sell the home for profit. Mr. Keenan stated it is for the length of the  
100 mortgage, which is typically 30 years. He added since 1991 the first mortgage was just paid off  
101 in August. This is the first family that has gone through the whole process.  
102

103 Mr. Bergeron addressed the Board next and referred to Old Walpole Road as well as the street  
104 which heads towards Surry on the map. It is a seven-acre parcel with frontage along Old Walpole  
105 Road. There are wetlands delineated on the property. There are two homes being proposed for  
106 the site. The first lot would be 2.3 acres in size and the second lot would be about 4.8 acres in  
107 size. He noted that some steep slopes are located to the rear of the site.  
108

109 As far as water and sewer for these lots – there is access to water and sewer on site.  
110 Test pits have been completed to determine where those septic systems can be located. Areas on  
111 both lots that will be able to support septic systems and will be part of a State Subdivision  
112 approval, which will be also submitted for this property.  
113

114 For fire protection, there is a fire hydrant directly across the street. Mr. Bergeron noted the entire  
115 site is currently wooded and described the area that would be cleared where the building sites  
116 and the septic systems would go, while the remainder of the lot would remain fully wooded.

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There will also be some wooded area along the front of the property that will be maintained. There will be a single driveway that will come into property line, which will service both house sites. This concludes Mr. Bergeron’s presentation.

Mr. Kost noted all the wooded areas, steep slopes, and wetlands, and he asked whether the applicant was planning on placing any protections, such as a conservation easement. Mr. Bergeron stated he had a discussion with the City Assessor about placing a conservation easement on the land and whether it would be economically feasible for such a small piece of property. He indicated the issue with most conservation easements is that there needs to be someone who maintains that easement. Most of the time, the person who maintains the easement is from the Forest Society or a similar group; however, this is such a small piece of property, so it would be difficult to get someone interested in it.

Mr. Bergeron stated at the very least they have talked about placing deed restrictions which will prevent further clearing of a lot beyond what is developable area.

Staff comments were next.

Ms. Brunner addressed the Board. She stated staff has determined this application will have no regional impact and noted this is something the Board would need to address.

Next, Ms. Brunner addressed the following items:

Character of Land for Subdivision – She indicated the applicant did a good job explaining the application. This is a steep lot with wetlands and the developable area is limited, but there is room for two house lots. Staff recommend a condition of approval for a storm water management plan for each lot prior to a building permit being issued. Also, she stated that the wetland buffer should be flagged and inspected by staff prior to site development.

Scattered or Premature Development – Ms. Brunner stated this area of the city is already developed and there is no concern regarding emergency access. Both sites can support on-site septic systems and well.

Preservation of Existing Features – As mentioned by the applicant, there are steep slopes, wetlands, tree cover, and a stonewall that runs around the property. All of these would be preserved, except for the trees in the development area.

Fire Protection and Water Supply - There is a fire hydrant near the property, so Fire Department staff had no concerns.

Utilities – Applicant is not proposing to use city water or sewer. The lots can support septic and a well. With respect to the septic, New Hampshire DES approval will be required, because both new lots would be under five acres, which is a recommended condition of approval.

Shared Driveway – There is a recommended condition of approval regarding this issue. Ms. Brunner noted that the Board may want to consider requiring a condition of approval submittal

162 of language for an easement document so that the neighbors will have clear easement language  
163 in place for use of that shared driveway.

164  
165 Ms. Brunner reviewed motion language. This concluded staff comments.

166  
167 Mayor Kahn stated in a previous iteration, there was concern about storm water getting onto  
168 abutting properties on Butternut Drive and asked whether this has been reviewed by staff. Ms.  
169 Brunner stated that because she knows who the applicant is and what they are proposing, she  
170 would be surprised if there was a large storm water impact. However, if the lots were to be sold  
171 it would be a different issue, hence the reason for the requirement for a storm water management  
172 plan.

173  
174 The Chair asked for public comment next.

175  
176 Mr. Nick Bergman of 122 Butternut Drive addressed the Board. He stated he was following up  
177 on discussion at prior meetings, and he felt the applicants were good stewards and have  
178 considered concerns from neighbors. They have reduced the number of houses, and will be using  
179 a shared driveway and locating buffers along the front and rear of the site. He stated he was  
180 happy with this proposal.

181  
182 Mr. Bergeron stated he has a general layout for what the lot would look like and has a pretty  
183 good idea what the size of buildings would be and where the driveway is going to be located.  
184 He added their drainage plan will not increase runoff from this property; in addition, what exists  
185 now would be slightly reduced once this development is completed. This is because they are  
186 going to handle the storm water for this development on site.

187  
188 Mr. Jeff Kaiser of 118 Butternut Drive was the next speaker. He thanked Habitat for Humanity  
189 for listening to the neighbors and doing such a good job of working with them to come up with a  
190 plan, which he felt is a lot better than the one from a year ago. He felt if this plan is not  
191 approved, anything else that is proposed would be a lot worse, and a different applicant would  
192 not work as well with the neighbors as this applicant did.

193  
194 Mr. Kevin of Thatcher of 28 Evans Circle stated he would also like to speak in support of this  
195 application. He stated as a civil engineer the standard of care that the applicant has put in for a  
196 two-lot subdivision is above the norm. He felt this is in keeping with providing affordable  
197 workforce housing and would be a great benefit to the city.

198  
199 With no further comment, the Chair closed the public hearing.

200  
201 C. Board Discussion and Action

202 A motion was made by Roberta Mastrogiovanni that the Planning Board approve PB2024-13 as  
203 shown on the plan set identified as “Subdivision Plan Prepared for Habitat for Humanity, INC.”  
204 prepared by David A Mann Survey at a scale of 1 inch = 80 feet, dated September 3, 2024 with  
205 the following conditions:  
206

- 207 1. Prior to final approval and signature by the Planning Board Chair, the following conditions  
208 precedent shall be met:
- 209 a. The owner’s signature appears on the plan.
  - 210 b. Inspection of lot monuments by the Public Works Director or their designee following  
211 their installation or the submittal of a security deposit in an amount deemed satisfactory  
212 to the Public Works Director to ensure that the monuments will be set.
  - 213 c. Subdivision approval from the New Hampshire Department of Environmental Services  
214 shall be obtained.
  - 215 d. Submittal of four (4) full sized paper copies, two (2) mylar copies, and a digital copy of  
216 the final plan set.
  - 217 e. Submittal of a check in the amount of \$51.00 made out to the City of Keene to cover  
218 recording fees.
- 219
- 220 2. After final approval and signature by the Planning Board Chair, the following conditions shall  
221 be met:
- 222 a. Prior to the issuance of a building permit for new residential construction, a stormwater  
223 management plan shall be submitted to the Community Development Department for  
224 review and approval by the City Engineer.
  - 225 b. At the City Engineer’s discretion, the 75-ft. wetland buffer shall be flagged and inspected  
226 by the Community Development Director or their designee prior to site development.
  - 227 c. C. Submittal of draft easement language for approval of a shared driveway.
- 228

229 The motion was seconded by Mayor Kahn.

230  
231 Mr. Hoefler agreed with staff that he sees no regional impact from this application.

232  
233 Ms. Vezzani thanked everyone for their comments. She felt it was great they were able to come  
234 back to meet the needs of the neighbors and saw no issue with moving forward with this  
235 proposal.

236  
237 Chair Farrington stated that from his perspective he felt it is an excellent proposal and appears to  
238 meet all the standards set forth for subdivision. It also addresses the priority of the neighbors -  
239 storm water impact.

240  
241 The motion made by Roberta Mastrogiovanni carried on a unanimous roll call vote.

242  
243  
244 **b. PB-2024-14 – Cottage Court Conditional Use Permit, Hillside Protection**  
245 **Conditional Use Permit, & Major Site Plan – Timberlane Woods Development, 0 Drummer**  
246 **Rd** - Applicant Fieldstone Land Consultants, on behalf of owner Christopher Farris, proposes a  
247 Cottage Court Development consisting of 6 buildings and a total of 36 units on the parcel at 0  
248 Drummer Rd (TMP #515-015-000). A Hillside Protection Conditional Use Permit is requested  
249 for impacts to steep slopes. The parcel is ~13.1-ac in size and is located in the Low Density  
250 District.

251  
252 Ms. Vezzani asked to be recused from this application.

253 Councilor Remy asked whether a vote needed to be taken for the recusal request. Ms. Brunner  
254 stated there is no vote required for land use boards, but an advisory vote could be requested and  
255 it is up to the individual board member to request recusal.  
256

257 A. Board Determination of Completeness

258 Planner Megan Fortson addressed the Board and stated the applicant has requested an exemption  
259 from submitting a traffic study and other technical reports and analysis. After reviewing each  
260 request, staff have made the preliminary determination that the requested exemptions would have  
261 no bearing on the application and recommend that the Board grant the requested exemptions and  
262 accept the application as complete.  
263

264 A motion was made by Roberta Mastrogiovanni to accept Application PB-2024-14 as complete.  
265 The motion was seconded by Mayor Kahn and was unanimously approved by roll call vote.  
266

267 B. Public Hearing

268 Mr. John Noonan from Fieldstone Consultant addressed the Board on behalf of Christopher  
269 Farris. Mr. Noonan stated the proposal is for six buildings with 36 units. This application was  
270 previously approved for five buildings with 30 units. It was approved as a conservation  
271 residential development with nine acres being placed in conservation. However, with the recent  
272 cottage court development being adopted, the applicant is back with six buildings and 36 units.  
273

274 He indicated previously there were garages proposed in front of each unit. This proposal has 46  
275 parking spaces with no garages (one parking space per bedroom). The units would be one-  
276 bedroom, two-bedroom, and studio/efficiency apartments.  
277

278 There will be one central driveway, 24 feet wide, coming off Timberlane Drive. All parking will  
279 be located behind the buildings. There was a waiver request approved as part of the prior  
280 proposal which was granted to locate parking in front of a building, which Mr. Noonan referred  
281 to on the plan. He noted there won't be any more tree clearing as was indicated during the site  
282 visit. A 32-foot buffer will be maintained along Timberlane Drive. The western portion  
283 containing nine acres will remain undeveloped.  
284

285 Mr. Noonan went on to say the applicant would own the property, maintain all the buildings, as  
286 well as maintain pavement, storm water and infrastructure for water and sewer.  
287

288 A grading and drainage plan has been submitted, which is the same as before. Collecting  
289 stormwater with closed drainage, open drainage along the upper portions into the closed drainage  
290 and ultimately into a detention basin.  
291

292 A utility plan has also been submitted and was also submitted with the prior plan, except this  
293 utility plan will tie into the buildings in the upper portion of the plan.  
294

295 Mr. Noonan stated the applicant is also proposing a ground mounted solar array, and the building  
296 to the southeast would have roof mounted solar arrays.  
297



298 Landscaping Plan – Each lot would have bike racks, trees to meet the requirements, and shrubs  
299 in each parking area. The transformer on site would be screened by a dwarf spruce. The central  
300 dumpster would be enclosed by a fence.

301  
302 Lighting Plan – Mr. Noonan stated there would be 14-foot-tall pole mounted lights, with the rest  
303 being wall packs residential style lighting. Lighting would be fully cut off LED fixtures.

304  
305 Mr. Noonan stated fire access plans were also submitted that shows trucks can get into and out of  
306 the site. The plan also notes that a 200-foot fire hose can access the rear of the site.

307  
308 The next exhibit plan is in reference to slope impacts. The prior proposal had 20,000 square feet  
309 for each lot which did not reach that threshold. With the new plan, the impact stands at 54,000  
310 square feet and a Hillside Conditional Use Permit application has been submitted. Mr. Noonan  
311 noted these are all precautionary slopes (15% to 20%).

312  
313 Architectural Renderings – Elevations for the buildings have been submitted. Mr. Noonan stated  
314 the applicant is willing to design the buildings to meet the cottage court overlay district  
315 standards. The previous plan called for town house style homes, 2.5 stories, and 30-foot roof  
316 peak height.

317  
318 Mr. Christopher Farris addressed the Board and stated the if the Board has an opinion, he was  
319 flexible with the final appearance of the building as they want it to fit in with the neighborhood.

320  
321 This concluded the applicant's comments.

322  
323 Mayor Kahn stated removing the first floor garages lowers the profile and allows for surface  
324 parking, which he felt was an improvement. He asked how the remaining parts of the driveway  
325 would address capacity for guest parking or additional parking on the property.

326 Mr. Noonan stated along each side of the driveway there is a larger gravel shoulder along this  
327 area where the dumpster is located, which could be used for parking.

328  
329 The Mayor asked whether 24 feet is sufficient for two way traffic. Mr. Noonan stated that 24 feet  
330 meets the city standard for a town road and is wide enough for two-way traffic.

331  
332 Mr. Kost noted the addition of another building would cause more runoff and asked how that is  
333 being handled. Mr. Noonan stated their drainage report included the impervious areas that  
334 increased compared to the prior plan. The way that is being handled is with the use of swales on  
335 either side, and they are also providing a treatment swale at the top and at the bottom of the  
336 driveway and sediment forebay. In addition, main pond areas were increased in size and well as  
337 the detention pond which was also increased in size. Ultimately, this gets submitted for the  
338 Alteration of Terrain Permit which is reviewed by DES.

339  
340 Chair Farrington clarified that the elevation with the stairways at the front is not going to be  
341 included. Mr. Farris agreed.

342

343 Mr. Hoefler stated he prefers this design compared to the original design. He asked how the solar  
344 provides relief to the users, from a net metering standpoint. Mr. Farris stated their intention with  
345 this development is to provide affordable housing and all utility costs will be included in the rent.  
346 The solar would just contribute to that overall savings.

347  
348 Chair Farrington noted building three and asked whether it could be rotated 90° or if there are  
349 reasons why it is in its current position. Mr. Noonan stated that in order to work more with the  
350 slope, it is designed to follow the contours of the land. If it is located at the end of the parking  
351 lot, you would have one side that cuts into the hillside and the other side would drop way off.  
352 Mr. Noonan stated they are trying to minimize the slope impacts.

353  
354 The Chair asked how addresses for each property would be handled for emergency services  
355 purposes.

356  
357 Mr. Noonan stated each building unit would have a 911 address of its own. The interior  
358 driveways would not be named. It would be building one through six and unit numbers.  
359 This would be turned into the city to decide how they want it numbered.

360  
361 Mayor Kahn asked whether the applicant was willing to be flexible on the type of housing they  
362 are providing based on housing trends in the community where demand might be for more two-  
363 bedroom compared to one-bedroom units. Mr. Farris stated the original plan was all two-  
364 bedroom units. He stated it was probably feasible, based on the size of the buildings, to obtain  
365 that.

366  
367 This would, however, encompass using the half-story attic space which they were not planning  
368 on utilizing. He added the program they would be using requires a 25% minimum of two-  
369 bedroom units, but this can be changed based on need. The Mayor asked staff what this process  
370 would look like.

371  
372 Staff comments were next.

373  
374 Ms. Fortson addressed the Board and stated, as indicated by the applicant, this is a 13.1-acre tract  
375 which has about 818 feet of frontage along Timberlane Drive and 160 feet of frontage on  
376 Drummer Road. The site is currently forested and undeveloped, although logging has taken place  
377 on the eastern end of the property near Timberlane Drive. Wetlands are present towards the  
378 western portion of the parcel and steep slopes are present on the northeastern portion of the site.

379  
380 As indicated by the applicant, this parcel was previously the subject of a Major Site Plan &  
381 Conservation Residential Development (CRD) Subdivision application in 2022. The applicant  
382 has subsequently received three extensions to the timeframe to satisfy the precedent conditions  
383 of approval. Cottage Court Development (CCD) was not an option in 2022 and the applicant is  
384 coming back to the Board to utilize this option so they don't need to have multiple lots or place  
385 land in conservation.

386  
387 The proposed development will consist of six town-home style buildings containing a total of 36  
388 units ranging from studio to two-bedroom units. This proposed development would impact more



389 than 20,000 sf of precautionary slopes (15-25% grade), which requires the applicant obtain a  
390 Hillside Protection CUP. Cottage Court Standards outlined in Article 17 of the LDC states if a  
391 development is proposing more than five units, it would require site plan review.  
392

393 Ms. Fortson noted staff has made a preliminary evaluation that the proposed development does  
394 not appear to have the potential for regional impact, but the Board will need to make a final  
395 determination on that issue.  
396

397 Ms. Forston noted there will be 54,075 sf of impact on precautionary slopes, and the vegetation  
398 clearing seems to be limited to the area required for construction. The plan indicates the site will  
399 be developed based on Best Management Practices for DES Alteration of Terrain Permit  
400 requirement for site stabilization, erosion control measures and stormwater management. Staff  
401 recommend a precedent condition of approval regarding security to cover maintenance of erosion  
402 control measures. Staff also recommend a condition be included regarding a pre-construction  
403 meeting and an inspection of erosion control measures prior to start of site work.  
404

405 Precautionary Slope Standards: Ms. Fortson stated as mentioned previously the property owner  
406 logged the proposed development area prior to the issuance of the previous Planning Board  
407 conditional approval in 2022, which is allowed under the Hillside Protection requirement. As  
408 part of this application, the owner will be installing private utility lines in the areas of  
409 precautionary slopes. She noted the Board will need to make a determination as to whether or not  
410 the applicant has sufficiently demonstrated that this site is being developed in the best way  
411 possible.  
412

413 View Clearing: The proposed conditions plan on Sheet 3 of the plan set delineates the edge of  
414 the tree clearing. The project narrative states that the extent of this clearing was based on the  
415 location of the 30' surface water buffer along the western portion of the proposed development  
416 area. The narrative also states that no additional clearing will be required for the site to be  
417 developed. This standard appears to be met.  
418

419 Drainage & Stormwater Management: The narrative states that the impervious surfaces on the  
420 site will have stormwater runoff treated and retained on site to meet or reduce the runoff when  
421 compared to the undeveloped state of the property. Planning staff recommend that the Board  
422 consider including the submittal of an approved AOT permit application as a precedent condition  
423 of approval for this application. This standard appears to be met.  
424

425 Ms. Fortson next reviewed Cottage Court CUP Regulations:  
426 Multi-family uses or town-style homes are permitted in the Low Density District as long as the  
427 units are attached horizontally and contain between 4-6 total dwelling units per building. The  
428 applicant is proposing six buildings with 36 units. The proposed conditions plan shows the  
429 accessory structures and features to be incorporated into the development, including mail kiosks,  
430 a community garden, dumpster area, playground, dog park, small-scale ground-mounted solar  
431 array, and a picnic area. This standard appears to be met.  
432

433 The project narrative states that the development will be located on a single lot and will be  
434 owned by a single entity who will be responsible for all maintenance. Units will be rented out to

435 individuals as apartments. The studio apartments are going to be about 512 square feet in size,  
436 the one-bedroom apartments will be 640 square feet and the two-bedroom apartments will be 800  
437 square feet in size.

438  
439 Under the Dwelling Unit Size for Cottage Court CUP, all new units in the development could  
440 have a maximum average size of 1,250 square feet of gross floor area per unit and a maximum  
441 footprint of 900-sf per unit. The information provided by the applicant indicates that the total  
442 gross floor area of all units is 23,872 square feet, which averages out to 663 gross floor area per  
443 unit. The narrative states that the footprint of each individual unit will not exceed 900 square  
444 feet. Staff recommend that the submittal of architectural elevations and floor plans prior to the  
445 issuance of a building permit be included as a condition of approval. This standard appears to be  
446 met.

447  
448 Parking: The plan shows 46 surface parking spaces, which is one space per bedroom. This  
449 standard appears to be met.

450  
451 Building Separation: The two closest buildings will be about 11 feet apart and the Fire Marshal  
452 had no concerns. This standard appears to be met.

453  
454 Driveways: Driveways for two-way traffic need a minimum width of 20' and a maximum width  
455 of 24'. The maximum width is going to be 24' and a minimum width of 22'. This standard  
456 appears to be met.

457  
458 Screening: The project narrative states that the existing vegetative buffer in the development area  
459 is going to remain along Timberlane. Staff recommend a precedent condition of approval to  
460 maintain a vegetative buffer along the perimeter of the developed area. This standard appears to  
461 be met

462  
463 Snow Storage: Snow storage areas have been identified on the plan and excess snow is going to  
464 be removed from the site. That standard appears to be met.

465  
466 Landscaping: This standard requires that one tree be installed for every 10 parking spaces under  
467 Article 9 of the Land Development Code. The applicant is proposing to install five trees to meet  
468 that requirement, as well as some additional landscaping. This standard appears to be met.

469  
470 Screening: The applicant is going to be maintaining a vegetated buffer around the perimeter of  
471 the site. Staff recommend that this be shown on an updated plan and included as a precedent  
472 condition of approval.

473  
474 Lighting: This standard appears to be met.

475  
476 Water and Sewer: This standard appears to be met

477  
478 Traffic and Access Management: The applicant submitted an updated traffic study, which  
479 engineering staff did review and had no concerns about the increase in the number of vehicles for  
480 30 versus 36 units.

481  
482 Filling and Excavation: That standard was met  
483  
484 Surface Waters and Wetlands: Ms. Fortson stated the only thing that staff recommend as a  
485 condition of approval is that the 30-foot buffer be flagged and subject to inspection by the  
486 Community Development Director prior to the commencement of site work.  
487  
488 Architecture and Visual Appearance: Mr. Noonan did say that they requested a waiver from this  
489 section of the code. This would be the requirement that all required parking be located to the side  
490 or rear of buildings. Ms. Fortson noted in Article 17, which are the Cottage Court Development  
491 standards, if there is going to be parking located between a unit and the road it needs to be  
492 screened in some way; it can be screened with landscaping in accordance with Article 9 of the  
493 LDC, which is where the parking lot landscaping standards are located.  
494 Ms. Fortson noted this waiver was not necessary, because Article 20 and Article 17 conflict as no  
495 parking is actually located between the units and the road.  
496  
497 Article 17 takes precedence over Article 20.  
498  
499 Ms. Fortson reviewed the motion language and noted the one related to a cut sheet for an updated  
500 wall pack light fixture – 1. E. is no longer necessary, given that they have already addressed that.  
501  
502 Ms. Brunner addressed the Board next and noted what Ms. Fortson stated about Article 17  
503 clearly states that in the instance where there is a conflict between Article 17 and the site  
504 development standards, the standard in Article 17 prevails. Based on the topography of the site,  
505 the building where there is parking proposed to be between the building and the road would not  
506 be visible from the road. Thus, staff concluded that a waiver was not required.  
507  
508 The next issue Ms. Brunner referred to was in reference to Cottage Court, which has a parking  
509 minimum and a parking maximum. The applicant stated clearly that there are 46 bedrooms and  
510 46 parking spaces, which is the maximum number of parking spaces that are allowed under the  
511 Cottage Court Ordinance. The gravel shoulders along the driveway are technically not parking  
512 spaces. They cannot provide more than 46 parking spaces without going to the ZBA to obtain a  
513 variance. This concluded staff comments.  
514  
515 Chair Farrington noted the traffic study from VHP came in just a couple weeks ago and asked if  
516 staff were able to review the report. Ms. Forston answered in the affirmative and stated it was  
517 sent to engineering staff who did not see any concern. The Chair clarified it is well below the  
518 100 trips. Staff agreed.  
519  
520 Mayor Kahn asked staff what procedure would be followed if the applicant, based on market  
521 conditions, changes the number of specific bedroom sizes. Ms. Forston stated what the Board is  
522 doing is approving the number of units not the layout of those units. If they want to change the  
523 number of units, they would need to come back before the Board. Mayor Kahn stated he was  
524 questioning the change to the number of beds. Ms. Brunner stated in that instance, regarding the  
525 actual mix of the types of units, any change to a conditional use permit must come back to the  
526 Planning Board. A change of that level would have to come back to the Planning Board.

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Mr. Kost referred to the precautionary slopes standard where it *says the Board will need to make a determination as to whether or not the applicant sufficiently demonstrated it is the most suitable portion of the site*, and asked whether the Board can assume that this is the best place to build. Additionally, since there is going to be some impact to 54,000 square feet of slope area – he stated he is not seeing where the mitigation is. He asked how any kind of erosion or runoff would be prevented.

Ms. Brunner responded by saying this site is the appropriate location, and that standard is concerned with a site impacting more than 20,000 square feet of precautionary slopes. What the standard is asking the Board to evaluate is whether there is a location on the site that would not impact steep slopes. The answer for this property, after looking at the plans, is that the proposed area would be the least impactful.

Mr. Noonan added that he would like to review their existing conditions. He stated a large portion of the property close to where the wetlands are located was not surveyed because of its size. The applicant knew he was not going to develop that area, even though there is frontage off Drummer Road. He noted the darker grey areas on the plan, which are the precautionary slopes.

He noted the three plateaus and stated this is the area they targeted as the best spot to place the buildings. The buildings were laid out to work as much as they could to work with that land.

After constructing the site and ensuring that there is no erosion or impact on slopes post construction, conditions will be reviewed with the Alteration Terrain Permit. There will be inspections done during construction. Steeper slopes, 3:1 or greater, would have erosion control matting.

The Chair asked for public comment next.

Mr. David Ploppert addressed the Board. He stated he worked on the Timberlane Drive project for two years and there was an incredible amount of dynamite blasting used to put in all the cellar holes on the left side of the site. He felt a project of this size with the mountain that has a ledge will increase veins of water running down that slope.

He also questioned how this project is going to impact the value of homes already on Timberlane Drive.

Mr. Kevin Thatcher of 28 Evans Circle addressed the Board next. He stated he is a civil engineer for NHDES Alteration of Terrain Bureau. He stated he would like to address the Board in that capacity. He stated he was able to perform a preliminary review based on the information in the staff report. Mr. Thatcher stated this project is not compliant with DES Regulations regarding stormwater quality treatment. He stated he raises these concerns for the benefit of the applicant and the Board so that they can address the changes that need to be made. Specifically, a significant portion of this site is a tributary to the detention basin. He added that a detention basin is not an approved stormwater quality management practice with DES; it is only a conveyance for quantity control not quality control.

573

574 Mr. Thatcher added that each of the buildings seem to have a stone drippage associated with  
575 them. These details were not provided in the staff report. From a state perspective, he pointed out  
576 single and duplex-style residences are exempt from stormwater quality treatment, but multi  
577 family, town homes, and apartment buildings are not exempt. The project must ensure that drip  
578 edges meet treatment standards regarding water coming off the roofs.

579

580 Lastly, Mr. Thatcher noted there is one treatment swale proposed, but it isn't clear if this meets  
581 DES Standards.

582

583 Mr. Mark Vanson of 62 Meetinghouse Road stated he wanted to follow up on Mayor Kahn's  
584 question regarding overflow parking. He felt one car per bedroom, even though it is the standard,  
585 seems "tight." A working couple in a one-bedroom could have two cars. A two-bedroom with a  
586 teenage driver could have three cars. He asked that the gravel aprons, which are not technically  
587 parking spaces, be considered a quantifiable measure of the capacity of this area to handle  
588 visitor, resident and service parking for this development. If the gravel area parking cannot  
589 handle this type of parking, there will likely be parking happening on Timberlane Drive.  
590 Timberlane Drive has restricted parking during the winter months, and its capacity to manage  
591 vehicular travel is limited.

592

593 Sarah Vezzani of 464 Elm Street addressed the Board next. Ms. Vezzani stated she has owned  
594 her home for 17 years and has not had any water issues in her basement. Since the clearing  
595 across the street, she has had more than a foot of water. She stated she contacted the city, and no  
596 one can say if the instances of water in her basement are related exactly to this project; however,  
597 the situation does seem curious because it occurred directly after the clearing across the way. She  
598 indicated stormwater is a concern for her  
599 and hoped she would not suffer a \$60,000 loss to her finished basement, which is not covered by  
600 insurance. She asked that this be noted on the record that this was an issue that occurred after this  
601 clearing.

602

603 With no further comment, the Chair closed the public hearing.

604

605 The Chair asked Mr. Noonan to comment on the concerns that were just raised.

606 Mr. Noonan address the first comment regarding the ledge. He stated when test pits were done  
607 on the site, there was no ledge encountered. The owner has done additional digging on site and  
608 investigative work. Afterward, the owner found only boulders throughout the site and did not  
609 find any ledge. In addition, after the initial approval, he performed excavations to see if there  
610 was any ledge and none was found. He added that when you travel up to Drummer Hill there is a  
611 substantial ledge. The Chair asked whether they anticipate any use of dynamite.

612

613 Mr. Noonan stated, based on what was found with test pits and what the owner of the property  
614 has investigated further, they don't anticipate blasting. If any blasting was to be undertaken, it  
615 would be through Fire Department review; however, they do not foresee blasting on the site.

616

617 The next comment Mr. Noonan addressed was in reference to the statements Mr. Thatcher had  
618 made regarding the design for Alteration of Terrain. He stated they are asking for that as a

619 condition of approval, and they would design it to meet state standards. There could be more  
620 treatment required. He noted the swales being proposed on site and additional space where more  
621 treatment swales could be located.

622  
623 As far as clearing, the property went from a wooded lot to being cleared. Once an approval and  
624 an alteration of terrain permit is issued, there would be a design to slow down and reduce runoff  
625 from the site. He added he did not know where this abutter's house was located in relation to this  
626 site.

627  
628 Councilor Remy stated this is the first time he has heard from a civil engineer who happens to  
629 work for the state. When reviewing this process, including a condition of approval, he inquired  
630 whether the Board should consider his testimony. Ms. Fortson stated the Planning Board  
631 obviously is not trained to know what the operation of an Alteration of Terrain Permit is going to  
632 require. If the applicant applied for the permit, and it was going to significantly impact their  
633 proposed design, then staff would need to look at the changes that are proposed. Staff would then  
634 determine if the changes would require a return to the Planning Board for review. Pending staff  
635 determination, it could be reviewed by the Minor Project Review Committee as a minor site plan  
636 application or even reviewed administratively by staff.

637  
638 Mr. Noonan stated the city is copied on that Alteration of Terrain application, and this would be  
639 prior to the applicant coming back for a final approval. Staff would see if there were any major  
640 changes to the site plan prior to the final plans being submitted.

641  
642 Mayor Kahn clarified these units would not have basements and would have concrete footings;  
643 in addition, any disruption would be limited to removal of boulders on the site. Mr. Noonan  
644 agreed there would be no basement and removal of boulders would be done to put in footings.

645  
646 The Mayor further clarified stormwater runoff would be reviewed by the State. Mr. Noonan  
647 stated this would be reviewed as part of the Alteration of Terrain Permit. He also asked for  
648 clarification that the water flow from the site is not going to be disrupted because of the  
649 detention pond, which will give greater capacity for stormwater runoff than there currently is on  
650 the site. Mr. Noonan stated the design, after the Alteration of Terrain permit process, will be  
651 implemented to treat stormwater runoff and match as close as possible or create a slight  
652 reduction from current runoff conditions. The design would be in the pre-development, or  
653 wooded, condition, so the system ultimately will be sized larger if the applicant showed the  
654 conditions as already cleared. The Mayor clarified, again, that the applicant's intent with the  
655 design is that it would reduce the amount of runoff from the site currently. Mr. Noonan answered  
656 in the affirmative and stated this would be a requirement of the Alteration of Terrain Permit.

657  
658 Mr. Hofer stated he hears the concerns about parking and felt it is going to be a tight parking  
659 situation; however, the site is also constrained because the maximum amount of parking for the  
660 use of a cottage court overlay has been met.

661  
662 The Chair noted the site has been cleared and, at least anecdotally, has affected the flow of water  
663 down the hill. It will be sometime between now and when the storm water drainage system is in  
664 place. The Chair asked, in the meantime, if there was anything that could be done to reduce some



665 of the drainage. Mr. Noonan stated it has been three years since the site was cleared and there has  
666 been a substantial amount of vegetation that has already grown over the entire site; therefore, he  
667 felt it was completely stabilized at this point. There are small trees across the entire site. He did  
668 not feel a stormwater management system would warrant much from what is there today. The  
669 Chair asked that the applicant be mindful of this issue as they proceed when developing the site.  
670

671 Mayor Kahn referred to the concern raised about quantifying the amount of gravel parking the  
672 site may provide and asked if this is something that can be done. Mr. Noonan stated he would be  
673 hesitant to say how many spaces there are available along the side. He stated the applicant is  
674 showing that they are meeting the maximum of the one per bedroom parking requirement. He  
675 felt indicating they are adding ten more spaces on the side would make the site plan not  
676 compliant.  
677

678 Ms. Fortson added this becomes an issue for a few reasons. First, being above the maximum  
679 number of parking spaces would require that they go before the Zoning Board for a variance to  
680 allow for more parking spaces. You will also then be potentially creating parking spaces that  
681 don't meet the parking lot design standards.  
682

683 The Mayor asked whether the parking lots were going to be paved. Mr. Noonan agreed they  
684 would be paved. The Mayor asked whether the applicant has considered permeable surface for  
685 greater capacity on the site. Mr. Noonan stated they have not considered permeable surfaces; it  
686 would be standard asphalt. All the impervious surfaces would be running into treatment swales  
687 and then into a detention pond. He stated the applicant is trying to keep costs down as much as  
688 possible, because the ultimate goal of the project is providing affordable and workforce housing.  
689

690 The Mayor asked staff, when the Department of Environmental Services review is conducted,  
691 whether this could be a possible suggestion they might make to achieve certain mitigation.  
692 Ms. Brunner stated she has never seen that as a requirement or a suggestion. She stated the way  
693 she believes it works is that there are certain standards that you have to meet, and then it is up to  
694 the applicant to design a plan that meets those standard, which is what gets reviewed by the state.  
695

### 696 C. Board Discussion and Action

697 A motion was made by Roberta Mastrogiovanni that the Planning Board approve PB-2024-14 as  
698 shown on the plan set identified as "Multi-Family Residential Development, Tax Map 515, Lot  
699 15, Timberlane Woods" prepared by Fieldstone Land Consultants at varying scales on  
700 September 20, 2024 and last revised on October 14, 2024 with the following conditions:  
701

702 1. Prior to final approval and signature of the plans by the Planning Board Chair, the following  
703 conditions precedent shall be met:

704 a. Owner's signature appears on the plan.

705 b. Submittal of five paper copies and a digital copy of the final plan set stamped by an  
706 engineer and certified wetlands scientist licensed in the State of NH.

707 c. Submittal of an updated proposed conditions plan with a 30' "no-cut buffer" labeled  
708 along the boundaries of the proposed development area.

709 d. Submittal of five full sized, color paper copies and a digital copy of the final elevations  
710 stamped by a NH-licensed architect showing each building façade with the proposed exterior  
711 building materials/finish colors labeled.

712 e. Submittal of an updated cut sheet for a wall-pack light fixture that complies with the  
713 lighting standards outlined under Article 21.7 of the LDC.

714 f. Submittal of an approved Alteration of Terrain Permit application to the Community  
715 Development Department.

716 g. Submittal of a security deposit to cover the cost of sediment and erosion control,  
717 landscaping, and as-built plans in a form and amount acceptable to the City Engineer.

718

719 2. After final approval, the following conditions subsequently shall be met:

720 a. Prior to the commencement of site work, a preconstruction meeting will be scheduled  
721 with Community Development Staff.

722 b. Prior to the commencement of site work, the erosion and sediment control measures  
723 shall be inspected by the Community Development Department to ensure compliance with this  
724 application and all City of Keene regulations.

725 c. Prior to the commencement of site work, the 30' wetlands buffer shall be flagged and  
726 inspected by the Community Development Department to ensure compliance with the Surface  
727 Water Protection Ordinance and all City of Keene regulations.

728 d. Following the installation of landscaping, the applicant shall contact Community  
729 Development Staff to schedule an initial landscaping inspection.

730 e. After 1 full growing season (a minimum of 1 year), the applicant shall contact  
731 Community Development staff to conduct a final landscaping inspection.

732 The motion was seconded by Kenneth Kost.

733

734 Mr. Kost stated this project provides much needed affordable housing, which is important. He  
735 stated the concern about storm water runoff and downhill erosion has been raised, and stated he  
736 wanted to make sure everything is in place between state reviews and the city engineers.

737 He added he supports the city in its desire for wanting housing, but he wanted to make sure we  
738 are not damaging or causing issues in the excitement of getting housing.

739

740 Ms. Brunner stated she just spoke to the applicant, and the plans don't have sufficient area to  
741 place a 30-foot buffer around the entire development area. The applicant is asking that this  
742 condition be modified to take out "30 feet" and to say a "no cut buffer" around the development  
743 area. To get a 30-foot buffer around the entire development they would have to change their  
744 entire design.

745

746 The Chair asked where the 30-foot requirement came from. Ms. Brunner stated it might have  
747 come from the CRD approval. It is carryover from that application. What staff intend with that  
748 condition is to ensure the vegetation there today would remain.

749

750 She felt if it is labeled as a no cut buffer, the buffer would meet the intent without having to  
751 specify the 30-feet.

752

753 Mr. Noonan agreed and added when they had the CRD Subdivision, he noted how the buildings  
754 were located, and the garages were almost directly on to the driveway.



755 Hence, there was 30-feet right at the entry of the driveway. With the new plan that includes the  
756 rotation of the buildings, the drainage and utilities, they are not able to get 30 feet through this  
757 entire area. Mr. Noonan added there is no 30-foot buffer requirement and it is not a regulation.  
758

759 Mayor Kahn felt this is a development the city needs. Regarding storm water runoff, there is a  
760 net improvement, which needs to be and will be verified independently. The storm water runoff  
761 will be verified by the Department of Environmental Services, and the city is dependent on their  
762 review.  
763

764 The Chair asked whether any Board wanted to address regional impact. Mr. Mehu stated this  
765 plan doesn't meet regional impact.  
766

767 Mr. Hoefler felt this was a challenging lot and is glad there is a developer who is willing to take  
768 on this development. He felt there are protections in place to make sure the stormwater will  
769 meet state standards. He stated he had no concerns proceeding with a vote tonight.  
770

771 Robert Mastrogiovanni made the motion to amend 1c. to read as follows:

- 772 1. Submittal of an updated proposed conditions plan with a "no-cut buffer" labeled along  
773 the boundaries of the proposed development area.
- 774 2. The amended motion was seconded by Mayor Kahn  
775

776 The original motion carried on a unanimous roll call vote.

777 The amended motion carried on a unanimous roll call vote.  
778  
779

## 780 **V. Advice & Comment**

781 **a. Cottage Court Development & Surface Water CUP – 0 Court St**, TMP #228-016-  
782 000 – Fieldstone Land Consultants, on behalf of owner Guitard Homes LLC, is seeking input  
783 from the Planning Board regarding a potential Cottage Court Development & Surface Water  
784 Conditional Use Permit (CUP). The parcel is located in the Low Density District.  
785

786 Mr. John Noonan addressed the committee again. Mr. Noonan stated this proposal is for 36  
787 individual homes. The property is located on 0 Court Street. He noted a cul-de-sac that comes in  
788 and extends to the back of the property. There is a wetland that bisects a portion of the property,  
789 and the applicant is looking to build in three phases.  
790

791 The phases would include the following:

- 792 1. The first phase would be coming off Court Street using the turnaround.
- 793 2. The second phase would be crossing the wetland and using that proportion of property.
- 794 3. The third phase would be crossing the wetland to the rear of the property.  
795

796 Mr. Noonan stated the applicant has met with city staff and reviewed the three styles of homes  
797 that they are looking to build. Specifically, cottage style must meet the 900 square feet gross  
798 floor area or less and maximum of 1,250 square feet total gross. He presented a rendering showing  
799 the three home styles they were looking at.  
800

801 Mr. Noonan stated there is a right of way for this subject lot that crosses through the Keene  
802 Center land, which they are not proposing to use. Ultimately, they would be using the Court  
803 Street entrance of the proposed road. Water and sewer services would come off Court Street and  
804 they would also be using the water extension at the right of way. The Fire Department is satisfied  
805 with the access to hydrants located in this area for their use.  
806

807 Mr. Noonan stated they would be impacting over 100,000 square feet of wetlands overall, and  
808 this application would also include applying for an Alteration of Terrain Permit and a Wetland  
809 Permit for the three crossings. There would be two permanent crossings and one temporary  
810 crossing, if water does not extend from a hydrant located at the end of the property.  
811

812 The homes would be two-bedroom homes. At this point the applicant is in the preliminary stages  
813 of meeting the College Court Ordinance for density. Some of the homes would have the option  
814 of a detached garage or shed as part of the property. He stated they would be coming back soon  
815 before the Board with a full site plan application.  
816

817 Mayor Kahn asked how many units would be in each phase. Mr. Noonan stated phase one would  
818 have 13 units, phase two would have 12 units, and phase three would have six units. The Mayor  
819 clarified there will be a traffic study as part of this application. Mr. Noonan answered in the  
820 affirmative. The Mayor asked if all the units would be in two levels. Mr. Noonan stated two of  
821 the three styles will be two stories, and one will be a single story. The Mayor asked whether  
822 these homes would be in private ownership. Mr. Noonan stated it would be a homeowners'  
823 association with the terms that the units cannot be rented. The Mayor felt this is something the  
824 community would benefit from and asked for a price range for these home.  
825

826 Applicant Mike Guitard addressed the Board and called attention to the different styles of homes  
827 being considered. He stated the houses could be less than \$300,000 (market driven concept). The  
828 Mayor asked whether soil tests have been completed. Mr. Guitard answered in the negative but  
829 stated that soil tests will be completed. The applicant added development would all be slab on  
830 grade with no basement.  
831

832 Mr. Kost asked what kind of parking is being offered. Mr. Guitard stated it would be one per  
833 bedroom. Mr. Noonan stated they are on the agenda with the Conservation Commission at their  
834 next meeting to address the wetland crossing.  
835

836 Ms. Vezzani asked the applicant to make sure they meet the maximum square footage average.  
837 She also asked the applicant to keep in mind an attractive screening between the homes.  
838 She also asked the applicant to consider traffic, which is going to increase in the area, and have a  
839 study completed by a NH Licensed engineer.  
840

841 The Mayor asked if the attached garages are considered under the Cottage Court Ordinance. Ms.  
842 Brunner stated garages do not count towards the gross floor area.  
843

844 Mr. Mehu stated he likes this concept. He noted the footprint has been increased and felt a  
845 variety in the neighborhood won't be a bad thing.  
846

847 Ms. Mastrogiovanni asked whether there was a cap on the value of homes under the Cottage  
848 Court Ordinance. Ms. Brunner stated there is no cap, but it is addressed through the size of the  
849 home.

850  
851 **VI. Master Plan Update (<https://keenemasterplan.com/>)**

852 Ms. Brunner stated, since the last update, the future summit was held on October 5<sup>th</sup>. There was a  
853 lot of feedback received from participants. The vision report was updated based on the feedback.  
854 “Heathy Community” was changed to “Vibrant Neighborhood.” There was also a call for  
855 “Educated Citizenry” based on feedback received at the summit.

856  
857 The steering committee is currently in the process of finalizing the task forces for the six pillars  
858 related to the Master Plan Update. The task forces will start meeting in January. Their work will  
859 be shared with the Steering Committee and the Planning Board to finalize the Master Plan. For  
860 those who don’t have online access, there will be public open houses conducted at the library for  
861 the discussion boards on December 11, 12 and 13.

862  
863  
864 **VII. Training on Site Development Standards – Snow Storage & Removal, Landscaping, &**  
865 **Screening**

866 Item was not discussed

867  
868 **VIII. Staff Updates**

869 Ms. Brunner stated there is a training opportunity coming up. The training is the Municipal Land  
870 Use Law Conference on Saturday November 16, from 9:00 am to 3:15 pm, held at Southern NH  
871 University. Keene is an NHMA member, and the registration fee is \$115. The Board has a small  
872 budget which could fund two attendees.

873  
874 **IX. New Business**

875 None

876  
877 **X. Upcoming Dates of Interest**

- 878 • Joint Committee of the Planning Board and PLD – Tuesday, November 12th, 6:30 PM  
879 • Planning Board Steering Committee – November 12th, 11:00 AM  
880 • Planning Board Site Visit – November 20th, 8:00 AM – To Be Confirmed  
881 • Planning Board Meeting – November 25th, 6:30 PM

882  
883 The Chair reminded the Board to let staff know if they cannot attend a site visit, so the date can  
884 be changed to meet quorum.

885  
886 With no further business, Chair Farrington adjourned the meeting at 9:25 PM.

887  
888 Respectfully submitted by,  
889 Krishna Pahl, Minute Taker

890  
891 Reviewed and edited by,  
892 Emily Duseau, Planning Technician



**CITY OF KEENE**  
NEW HAMPSHIRE

**MEMORANDUM**

**TO:** Planning Board  
**FROM:** Community Development Staff  
**DATE:** November 18, 2024  
**SUBJECT:** Agenda Item III - Final Vote on Conditional Approvals

**Recommendation:**

To grant final approval for any projects that have met all their “conditions precedent to final approval.”

**Background:**

This is a standing agenda item in response to the “George Stergiou v. City of Dover” opinion issued by the NH Supreme Court on July 21, 2022. As a matter of practice, the Planning Board issues a final vote on all conditionally approved projects after the “conditions precedent to final approval” have been met. This final vote will be the final approval and will start the 30-day appeal clock.

**As of the date of this packet, there are no applications ready for final approval.**

If any projects meet their conditions precedent between date of this packet and the meeting, they will be identified and discussed during this agenda item.

All Planning Board actions, including final approvals, are posted on the City of Keene website the day after the meeting at [KeeneNH.gov/planning-board](https://www.keeneNH.gov/planning-board).

# STAFF REPORT

## PB-2024-15 – SITE PLAN REVIEW – MONADNOCK CONSERVANCY HEADQUARTERS – 0 ASHUELOT ST

### **Request:**

Applicant the Monadnock Conservancy, on behalf of owner JRR Properties LLC, proposes to construct a ~6,215-sf office building, create parking, install water and sewer utilities, and make stormwater drainage improvements on the site at 0 Ashuelot St (TMP #567-001-000). The parcel is ~3.53-ac in size and is located in the Commerce District.

### **Background:**

The purpose of this application is to construct a 6,215 SF, single story, office building with associated site improvements for the new headquarters of the Monadnock Conservancy. The proposed headquarters will provide office space for 25 occupants and include a community room. The majority of the subject parcel is located within the 100-year floodplain; therefore the project will require compensatory flood storage to offset the construction and site development.

The subject parcel is the former location of an overflow parking lot for the Colony Mill marketplace. The parking lot has since been removed and replaced with turf grass. The parcel is located on the west side of Ashuelot St, adjacent to the Ashuelot River. The parcel is approximately 500 ft from the intersection with West St. with the Mascoma Bank to the south and Harper Acres, a Keene Housing development, to the north.



Fig 1: 0 Ashuelot St. outlined in yellow

The current owner of the property intends to subdivide the parcel at a future date and donate approximately 1 acre in the northeastern corner of the parcel to the Monadnock Conservancy for this proposed development. The remainder of the parcel is intended to be donated to the City of Keene to provide additional green space connection along the Ashuelot River. A portion of the required compensatory flood storage will be located on part of the parcel to be donated to the City. An agreement between the Monadnock Conservancy and the City for the flood storage structures is being negotiated.

## STAFF REPORT

The project will be required to obtain an Alteration of Terrian permit from the NH Department of Environmental Services as well as a Floodplain Development Permit. In addition to flood storage improvements, an existing City-owned storm drainpipe that runs through the parcel will be replaced with a riparian drainage swale that will support both stormwater and floodwater volume management.

### **Determination of Regional Impact:**

After reviewing the application, staff have made a preliminary evaluation that the proposed site plan does not appear to have the potential for “regional impact” as defined in RSA 36:55. The Board will need to make a final determination as to whether the proposal, if approved, could have the potential for regional impact.

### **Completeness:**

The applicant has requested an exemption from submitting a traffic study, historic evaluation, screening analysis, architectural & visual appearance analysis, and other technical reports and analyses. After reviewing each exemption request, staff have determined that granting the request would have no bearing on the merits of the application and recommend that the Board accept the application as complete.

**Application Analysis:** The following is a review of the Planning Board development standards relevant to this application.

- 21.2 **Drainage:** The proposed stormwater management system includes detention basins, grass swales, infiltration trenches and an emergency spillway. The proposed spillway will direct flow as needed across the subject parcel to reduce volume and velocity. The applicant states in their narrative that that runoff will be managed onsite with no adverse impacts to downstream abutters. It appears that this standard has been met.
- 21.3 **Sediment & Erosion Control:** The applicant states in their narrative that the site is relatively flat which reduces concerns related to erosion concerns. The application proposes the installation of erosion control measures such as silt fence as needed. A stabilized construction entrance is also proposed. All control measures will be maintained and repaired based on best management practices. It appears that this standard has been met.
- 21.4 **Snow Storage & Removal:** The applicant states in their narrative that snow storage is proposed to the west of the parking area. If snowfall exceeds snow storage on site, any additional snow will be removed from the site. It appears that this standard has been met.
- 21.5 **Landscaping:** The application is required to install one tree per ten parking spaces in the parking area. The applicant proposes to install five Red Maple and one Multi-Leader Birch to meet this requirement. A rhododendron is proposed to be installed next to the proposed power transformer to provide screening. The proposed parking area will need to be screened from the residential parcel to the north. The screening requirement includes the installation of ornamental and shades trees, evergreens, and ornamental shrubs. The applicant has proposed a selection ornamental shrubs and grasses such as Inkberry Holly and Rhododendrons as well as Switch Grass to meet this requirement. A condition of



## STAFF REPORT

approval related to the financial security for the proposed landscaping is included in the suggested motion language. It appears that this standard has been met.

21.6 Screening: The applicant states in their narrative that the dumpster area will be enclosed by a 7'6" tall solid wood enclosure with an attached shed. The heat pumps will be screened with matching wood fencing. A rhododendron is proposed to be installed to screen the transformer from the public right of way. The applicant is proposing to screen the parking area from the residentially-zoned parcel to the north as required by section 9.4.4 of the Land Development Code utilizing a landscape buffer of trees, ornamental shrubs, and native grasses. It appears that this standard has been met.

21.7 Lighting: The application proposes to install seven pole-mounted parking area light fixtures and 11 canopy lights. The parking area fixtures will be installed on 12' tall pole mounts. An additional six wall mounted lights and 15 bollard lights are also proposed. All light fixtures are full cut-off with a CRI of greater than 70 and a color temperature of 3,000K. The applicant has also submitted a reduced lighting plan for security purposes with the required 50% reduction in lighting level shown. It appears that this standard has been met.

21.8 Sewer & Water: The applicant states in their narrative that the development will connect to City water and sewer with a new set of services. The water service will be a 1 ½" diameter line. The sewer service will be a 4" diameter. A separate fire suppression water service is not proposed as fire suppression is not required. It appears that this standard has been met.

21.9 Traffic & Access Management: The applicant states in their narrative that the proposed use will not generate a significant increase in the total vehicle trips per day. The previous use of the property was a 3.5-acre overflow parking area for a shopping center. The ITE trip generation manual estimates an additional 24 new vehicle trips during weekday peak.

The site design includes the installation of 25 parking spaces, including two accessible parking spaces and a 24' wide drive aisle. Granite curbing is proposed as wheel stops along all parking spaces. The street access for the parking area will be located at the northeast corner of the property, to the north of the proposed office building. A turn around is located at the far end of the parking area and a truck turning plan has been submitted to ensure that emergency vehicles have sufficient area to navigate the site.

Pedestrian access is located off Ashuelot Street with concrete walkways connecting the site to the City's sidewalk system. A bike rack is proposed to be installed by the front entrance of the office building. An existing concrete apron located at the southeast corner of the property left over from the parking lot is proposed to remain. It appears that this standard has been met.

21.10 Filling & Excavation: In order to achieve the required flood compensation, significant excavation and earthwork will be required. The project will require an Alteration of Terrian permit from DES for disturbance over 100,000 SF. The property is located near West Street which will provide a clear path for trucks to travel to and from the site. It appears that this standard has been met.

## STAFF REPORT

- 21.11 Surface Waters & Wetlands: The applicant states in their narrative that there will not be any work on the site that will impact the Ashuelot River. The proposed flood storage swale will require the removal of an existing 15" culvert that is part of the City's stormwater management system in the area. The applicant is proposing to connect the riparian swale used for flood storage to the existing City culvert system and upgrade the City system as needed. No impacts to surface waters or natural wetlands are proposed as part of this application. It appears that this standard has been met.
- 21.12 Hazardous & Toxic Materials: The applicant states in their narrative that there are no known hazardous or toxic materials on site and the proposed use does not include hazardous or toxic materials. It appears that this standard has been met.
- 21.13 Noise: The applicant states in their narrative that the proposed use will generate noise that is typical for an office building. It appears that this standard has been met.
- 21.14 Architecture & Visual Appearance: The applicant states in their narrative that the proposed office building will "mimic the Monadnock Conservancy's values." (see Fig 2.) The submitted elevations propose a single-story office building with a standing seam metal roof with solar panels. The building façade will be sections of slate siding with accent sections of wood siding.



Fig 2: Office building rendering

The east elevation (see Fig. 3) will face Ashuelot Street and depicts a mix of the three building façade materials, wood, slate, and metal. A wooden fence will also be located on this elevation to screen the proposed heat pumps from view of the public right-of-way.



# STAFF REPORT

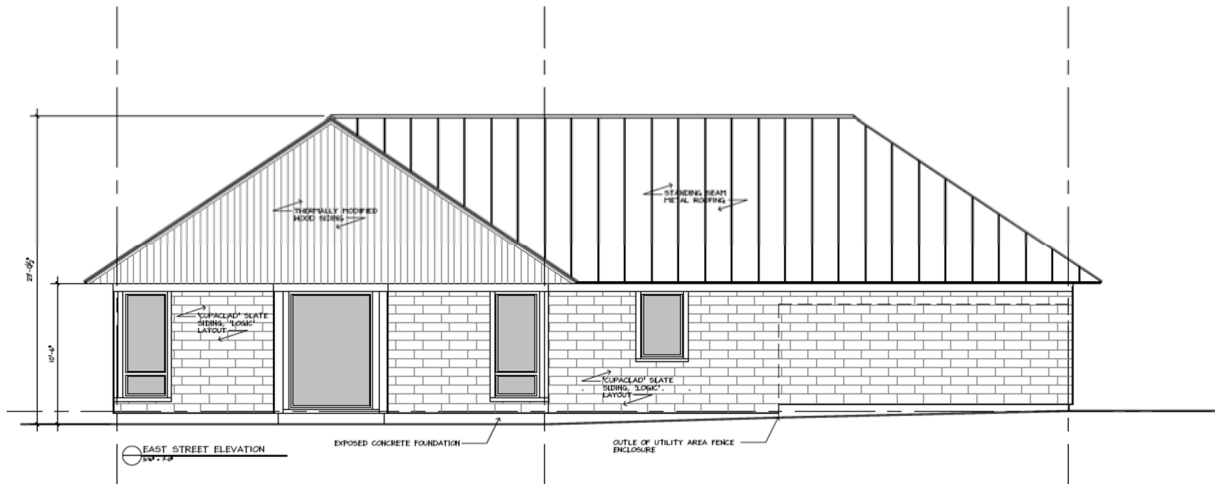


Fig 3: 0 East elevation facing Ashuelot St.

The Board should consider the following standards as part of their review of the City's Architectural and Visual Appearance standards to determine if this standard has been met.

Section 21.14.2 Visual Interest:

- A. *"Front facades and exterior walls shall be articulated to express an architectural identity to avoid a uniform appearance, and architectural details shall give the impression of being integral to and compatible with the overall design."*
- B. *"Structures shall have architectural features (e.g. dominant gable ends, cornices, granite sills, arched openings, large windows framed with architecturally consistent trim, etc.) and patterns that provide visual interest at the pedestrian scale, reduce massive aesthetic effects, and harmonize with the City's distinctive architectural identity, unique character, and prevailing scale."*

## **Recommended Motion:**

If the Board is inclined to approve this request, the following motion is recommended:

***"Approve PB-2024-15 as shown on the plan set identified as "Monadnock Conservancy Headquarters" prepared by SVE Associates at varying scales on October 18, 2024 and last revised on November 12, 2024 with the following conditions:***

1. ***Prior to final approval and signature of the plans by the Planning Board Chair, the following conditions precedent shall be met:***
  - a. ***Owner's signature appears on the plan.***
  - b. ***Submittal of five paper copies and a digital copy of the final plan with the AOT permit number included as a note on the site plan.***
  - c. ***Submittal of a security to cover the cost of sediment and erosion control, landscaping, and as-built plans in a form and amount acceptable to the City Engineer.***

## STAFF REPORT

2. ***Subsequent to final approval, the following conditions shall be met:***
  - a. ***Prior to the commencement of site work, a preconstruction meeting will be scheduled with Community Development Staff.***
  - b. ***Prior to the commencement of site work, the erosion and sediment control measures shall be inspected by the Community Development Department to ensure compliance with this application and all City of Keene regulations.***
  - c. ***Following the installation of landscaping, the applicant shall contact Community Development Staff to schedule an initial landscaping inspection.***
  - d. ***After 1 full growing season (a minimum of 1 year), the applicant shall contact Community Development Staff to conduct a final landscaping inspection."***



# City of Keene, NH Site Plan Application

If you have questions about how to complete this form, please call: (603) 352-5440 or email: [communitydevelopment@keenenh.gov](mailto:communitydevelopment@keenenh.gov)

## SECTION 1: PROJECT INFORMATION

<b>PROJECT NAME:</b> Monadnock Conservancy Headquarters		<b>TYPE OF APPLICATION BEING SUBMITTED:</b>	
<b>PROJECT ADDRESS(ES):</b> 0 Ashuelot Street		<input checked="" type="checkbox"/> MAJOR PROJECT APPLICATION <input type="checkbox"/> MINOR PROJECT APPLICATION	
<b>EXISTING OR PREVIOUS USE:</b> undeveloped		<b>PROPOSED USE:</b> office	
<b>GROSS FLOOR AREA OF NEW CONSTRUCTION</b> (in square feet) 6,215 sf		<b>GROSS FLOOR AREA OF EXISTING BUILDINGS/STRUCTURES</b> (in square feet) 0 sf	
<b>AREA OF PROPOSED NEW IMPERVIOUS SURFACES</b> (in square feet) 19,830 sf		<b>TOTAL AREA OF LAND DISTURBANCE</b> (in square feet) 73,000 sf	

## SECTION 2: CONTACT INFORMATION

PROPERTY OWNER	APPLICANT
<b>NAME/COMPANY:</b> JRR Properties, LLC	<b>NAME/COMPANY:</b> Ryan Owens/Monadnock Conservancy
<b>MAILING ADDRESS:</b> P.O. Box 323, Keene, NH 03431	<b>MAILING ADDRESS:</b> P.O. Box 337, Keene, NH 03431
<b>PHONE:</b> 603-252-3916	<b>PHONE:</b> 603-357-0600 ext 103
<b>EMAIL:</b> hanna@nhlandlaw.com	<b>EMAIL:</b> ryan@monadnockconservancy.org
<b>SIGNATURE:</b>	<b>SIGNATURE:</b> Digitally signed by Ryan Owens Date: 2024.10.14 16:50:57 -04'00'
<b>PRINTED NAME:</b> Thomas R. Hanna, manager and attorney	<b>PRINTED NAME:</b> Ryan Owens

AUTHORIZED AGENT (if different than Owner/Applicant)	FOR OFFICE USE ONLY:	
<b>NAME/COMPANY:</b> Liza Sargent/SVE Associates	<b>TAX MAP PARCEL #(s):</b> 5 lot 001 000 000 000	
<b>MAILING ADDRESS:</b> P.O. Box 1818, Brattleboro, VT 05302	-----	
<b>PHONE:</b> 802-257-0561	<b>PARCEL SIZE:</b> 3.53 ac	<b>DATE STAMP:</b> 
<b>EMAIL:</b> lsargent@sveassoc.com	<b>ZONING DISTRICT:</b> Commerce	
<b>SIGNATURE:</b> Liza Sargent Digitally signed by Liza Sargent Date: 2024.10.15 13:32:13 -04'00'	<b>PROJECT #:</b> PB-2024-15	
<b>PRINTED NAME:</b> Liza Sargent		

PROJECT NARRATIVE

**Monadnock Conservancy Headquarters**  
**0 Ashuelot Street**  
**Owner: JRR Properties, LLC**  
**Applicant: Monadnock Conservancy**

October 18, 2024

SVE Associates, on behalf of the applicant the Monadnock Conservancy, is submitting this site plan application to construct an office building. The project consists of construction of a 6,215-sf office building, parking, water and sewer connection & associated stormwater drainage treatment and detention. The proposed headquarters is sized for up to 25 occupants, there is an equal amount of parking spaces. Almost the entire parcel is within the 100-year floodplain, therefore the project requires flood storage compensation.

SVE Associates has met with the former City Engineer, now Director of Public Works on the joint project of replacing the existing City Storm Drainpipe that runs through the parcel and replacing a portion of it with a riparian drainage swale that will accomplish both the required flood storage compensation volume and maintain the City's stormwater drainage. Flood storage compensation volumes are accounted for above the seasonal high-water table and do not include the volume of excavation required to maintain the City's stormwater drainage system.

Sediment and erosion control for the development will be a row of silt fence or straw wattles. Contractor will maintain and remove accumulated sediment and debris as necessary.

The proposed plan complies with all City Development Standards:

1.) Drainage & Stormwater Management:

There will be no net increase in stormwater runoff. See attached drainage narrative. Stormwater will be treated and detained with the infiltration basin constructed above the 10-year flood elevation, as required by NHDES Alteration of Terrain.

2.) Sedimentation/ Erosion Control:

The site is relatively flat, minimizing the potential for erosion problems. Regardless, the Contractor is to install, monitor, and repair erosion control measures on a regular basis. These instructions are included in the notes on Sheet N-1 and details on Sheet C-4.

3.) Snow Storage and Removal:

Snow storage is proposed to the west of the proposed parking. If snowfall exceeds available snow storage, snow will be trucked offsite.

**SVE Associates**

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Engineering \* Surveying \* Landscape Architecture \* Planning  
439 West River Road, Brattleboro, VT 05302 Phone: (802) 257-0561 Fax (802) 257-0721 E-mail [svek@sveassoc.com](mailto:svek@sveassoc.com)

4.) Landscaping:

One birch cluster and three red maples are to be planted to screen the parking and transformer.

5.) Screening:

The new dumpster location will be screened from view by the dumpster enclosure. The transformer will be screened by a rhododendron and a birch cluster. The heat pumps will be screened by a fence. Fence will have steel post and thermally modified wood siding.

6.) Lighting:

There are 7 pole mounted lights, 11 canopy lights proposed, 6 wall mounted lights and 15 bollard lights proposed. All lights are full cut-off.

7.) Sewer & Water:

The site will connect to the municipal water and sewer. For an office with 25 occupants, design flow is 5 gpd/100 sf is 311 gpd. Sewer service is a 4" diameter SDR35 and domestic water is a 1 1/2" diameter CTS service. No fire suppression system required for single story, business use, under 12,000 sf with occupant load of less than 300.

8.) Traffic & Access Management:

No significant changes to amount or timing of trips to the site. From ITE trip generation manual estimates, there will be 24 additional vehicle trip ends on a weekday. For reference, this parcel used to be a 3.5-acre overflow parking lot for the nearby shopping center. Both vehicular and pedestrian access will be off of Ashuelot Street. A bike rack is located at the front entrance and is accessed along the proposed sidewalk.

9.) Filling and Excavation:

There will be compensatory flood plain excavation for the proposed filling within the 100-year flood plain.

10.) Surface Waters & Wetlands:

The property abuts the Ashuelot River to the west and the City's drainage swale to the north. Changes are proposed to the drainage swale that will affect a wetland (re-grading to remove the existing culvert). No changes are proposed that would affect the Ashuelot River.

11.) Hazardous & Toxic Materials:

Not applicable.

12.) Noise:

The proposed use will generate noise typical of an office space.

**SVE Associates**

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Engineering \* Surveying \* Landscape Architecture \* Planning  
439 West River Road, Brattleboro, VT 05302 Phone: (802) 257-0561 Fax (802) 257-0721 E-mail [svek@sveassoc.com](mailto:svek@sveassoc.com)

13.) Architecture and Visual Appearance:

New building's style and visual appearance will mimic the Monadnock Conservancy's values. See elevations for more detail.

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**SVE Associates**

Engineering \* Surveying \* Landscape Architecture \* Planning  
439 West River Road, Brattleboro, VT 05302 Phone: (802) 257-0561 Fax (802) 257-0721 E-mail [svek@sveassoc.com](mailto:svek@sveassoc.com)

# MONADNOCK CONSERVANCY HEADQUARTERS

0 ASHUELOT STREET, KEENE, NEW HAMPSHIRE

PROPERTY OWNER:  
**JRR PROPERTIES, LLC**

P.O. BOX 323  
KEENE, NEW HAMPSHIRE 03431

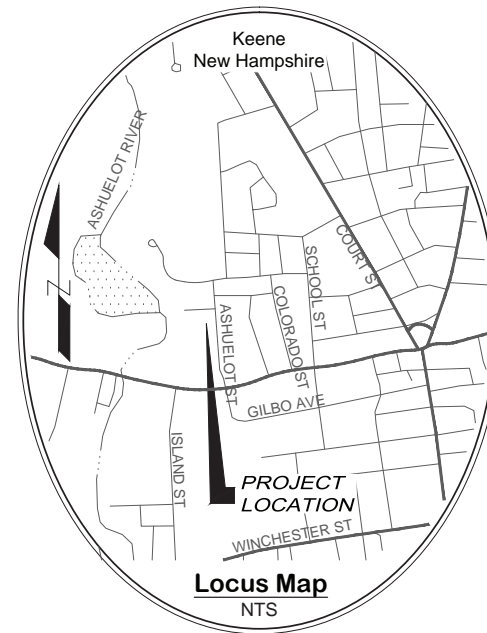
APPLICANT:  
**MONADNOCK CONSERVANCY**  
15 EAGLE COURT, 2ND FLOOR  
KEENE, NEW HAMPSHIRE 03431

NH FISH AND GAME AOT PERMIT  
CONDITIONS RELATED TO THREATENED &  
ENDANGERED WILDLIFE NOTES:

- WOOD TURTLES AND COMMON NIGHTHAWKS ARE WITHIN THE PROJECT VICINITY. FLYERS SHALL BE PROVIDED TO CONTRACTORS TO AID IN IDENTIFICATION THAT SHALL INCLUDE NHFG CONTACT INFORMATION.
- IF WOOD TURTLES ARE LOCATED WITHIN THE PROJECT SITE, THEY SHALL BE SAFELY RELOCATED OUTSIDE OF THE CONSTRUCTION AREA AND CONTACT NHFG IMMEDIATELY- MELISSA DOPPERALSKI 603-479-1129 OR JOSH MEGYESY AT 978-578-0802.
- CONTACT NHFG IMMEDIATELY-MELISSA DOPPERALSKI 603-479-1129 OR WILDLIFE ADMINISTRATOR AT 603-271-2461 IF COMMON NIGHTHAWKS ARE OBSERVED NESTING IN CONSTRUCTION AREAS AND AVOID DISTURBING BIRDS ON NEST. REFERENCE THE PROJECT NAME AND LOCATION IN COMMUNICATIONS.
- IN ORDER TO AVOID TURTLES FROM NESTING WITHIN DISTURBED SITE, CONTRACTOR SHALL INSTALL TEMPORARY BARRIERS (E.G. SILT FENCE) AROUND CONSTRUCTION AREAS BY MAY 15TH ACTIVITY TO EXCLUDE TURTLES FROM ACTIVE CONSTRUCTION AREAS. SEE SHEET C-1.
- NO SUMPS SHALL BE INCLUDED IN CATCH BASINS FOR THE PROTECTION OF WILDLIFE.
- DUE TO THE PROXIMITY OF THE SITE TO THE ASHUELOT RIVER AND KNOWN STATE AND FEDERALLY-LISTED FRESHWATER MUSSEL POPULATIONS, THE USE OF FERTILIZERS SHALL BE AVOIDED TO THE EXTENT POSSIBLE. IF FERTILIZER IS NECESSARY, A SLOW RELEASE NITROGEN FERTILIZER SHALL BE USED.
- ALL MANUFACTURED EROSION AND SEDIMENT CONTROL PRODUCTS, EXCEPT FOR SILT FENCE INSTALLED IN ACCORDANCE WITH ENV-WQ 1506.04, UTILIZED FOR, BUT NOT LIMITED TO, SLOPE PROTECTION, RUNOFF DIVERSION, SLOPE INTERRUPTION, PERIMETER CONTROL, INLET PROTECTION, CHECK, DAMS, AND SEDIMENT TRAPS SHALL NOT CONTAIN WELDED PLASTIC, PLASTIC, OR MULTI-FILAMENT OR MONO-FILAMENT POLYPROPYLENE NETTING OR MESH.
- ALL OBSERVATIONS OF THREATENED OR ENDANGERED SPECIES SHALL BE REPORTED IMMEDIATELY TO THE NEW HAMPSHIRE FISH AND GAME DEPARTMENT NONGAME AND ENDANGERED WILDLIFE ENVIRONMENTAL REVIEW PROGRAM BY PHONE AT 603-271-2461 AND BY EMAIL AT NHFGreview@wildlife.nh.gov. EMAIL SUBJECT LINE: NHB24-3137\_0\_ASHUELOT STREET MONADNOCK CONSERVANCY HEADQUARTERS\_WILDLIFE SPECIES OBSERVATION. PHOTOGRAPHS SHALL BE PROVIDED FOR VERIFICATION AS FEASIBLE.
- THE NEW HAMPSHIRE FISH AND GAME DEPARTMENT SHALL HAVE ACCESS TO THE PROPERTY DURING THE TERM OF THE PERMIT.

INDEX OF PLANS

- ASBUILT PLAN (BY HUNTLEY SURVEY & DESIGN, PLLC)
- N-1 NOTES & LEGEND
- C-1 SITE PLAN (1"=20')
- C-2 GRADING & DRAINAGE PLAN (1"=10')
- C-3 CONSTRUCTION DETAILS
- C-4 CONSTRUCTION DETAILS
- C-5 CONSTRUCTION DETAILS
- C-6 CONSTRUCTION DETAILS
- LA-1 LANDSCAPING PLAN
- LT-1 LIGHTING PLAN
- LT-2 REDUCED LIGHTING PLAN



SVE PROJECT #: K2781  
PREPARED BY

**Wood Turtle**  
(New Hampshire Species of Special Concern)

- Neck and forelimbs are orange.
- Characterized by its highly sculpted shell with each larger scute taking on an irregular pyramidal shape.
- Adults can be 5-6 inches long.

Please report sightings to NH Fish and Game at RAARP@wildlife.nh.gov or at 603-271-2461. Photo documentation, location, and date/time of observation is helpful.

NOTE: It is illegal to remove a wood turtle from the wild (RSA 207:1, FIS 804.02).

**REPORT OBSERVATIONS OF COMMON NIGHTHAWKS**  
The NH Fish & Game Department is requesting observations of Common Nighthawk that could be encountered onsite. Report sightings immediately to NHFG Wildlife Specialist at 603-271-2461 (M-F 8:30 to 4:30) or via email at NHFGWildlife@wildlife.nh.gov. Please include the following information:

- **When Encountered**
  - Perched on a tree limb or other structure
  - In flight
  - Multiple individuals (if so, note the sex and age of each)
  - Wingspan is 7.5-12.5 inches
  - Tail is deeply forked with a wide notch
  - Tail is not "bald" but rather has a dark band on the outer edge
  - Noisy in an open area (grass, bare dirt, open woods)
  - Incubation is for 18-20 days - another 30 plus days for fledging
- An adult during a broken wing display if driven from the nest.
- Adults may also hiss and open their mouth at you if in distress.



*Liza Sargent* 11/12/24  
LIZA P. SARGENT DATE  
R.C.E. NUMBER: 13365

Land Surveyor:  
**Huntley Survey & Design, PLLC**  
659 West Road  
Temple, NH 03084  
PHONE (603) 381-3227  
www.Huntleysurvey.com

Civil Engineer:  
**SVE Associates**  
439 West River Road  
P.O. Box 1818  
Brattleboro, VT 05302  
PHONE (802) 257-0561

Architect:  
**Sheldon Pennoyer Architects**  
64 North Main Street  
Suite 1  
Concord, NH 03301  
PHONE (603) 856-8994

APPROVED BY THE APPLICANT:

\_\_\_\_\_  
JRR PROPERTIES, LLC  
ON \_\_\_\_\_

INSPECTION PERMISSION: UPON APPROVAL OF THIS SITE PLAN, THE OWNER GRANTS PERMISSION FOR THE MEMBERS OR AGENTS OF THE KEENE PLANNING BOARD TO INSPECT THIS SITE AS NECESSARY.

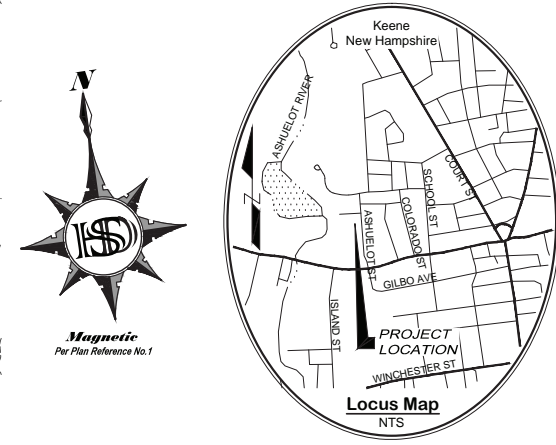
APPROVED BY THE KEENE PLANNING BOARD

\_\_\_\_\_  
ON \_\_\_\_\_

CERTIFIED BY CHAIRMAN \_\_\_\_\_ 33 of 92

October 18, 2024  
revised thru November 12, 2024





**Plan References**

- REFERENCES INCLUDE ALL INFORMATION REFERRED TO ON ANY OF THE FOLLOWING PLANS
- ALTA/ACSM LAND TITLE SURVEY OF MAYO TWO, LLC, ASHUELOT STREET PROPERTY, DATED JUNE 2004; BY ROGER T. MONSELL, CLOUGH HARBOUR & ASSOCIATES LLP (Provided by Client)

**Notes**

- THE BEARINGS SHOWN ON THIS PLAN ARE BASED ON A MAGNETIC OBSERVATION PER PLAN REFERENCE NO.1 AND SERVE ONLY TO DEFINE ANGULAR RELATIONSHIPS.
- THE BOUNDARY LINES SHOWN ON THIS PLAN WERE CALCULATED FROM CONTROL POINTS SHOWN ON PLAN REFERENCE NO.1. THE BOUNDARY WAS SLIGHTLY ADJUSTED TO MATCH PIPES FOUND. A BOUNDARY SURVEY WAS NOT PERFORMED
- TOPOGRAPHY SHOWN ON THIS PLAN IS FROM AN ACTUAL FIELD SURVEY BY HUNTLEY SURVEY & DESIGN, PLLC PERFORMED DURING THE MONTH OF MAY, 2021, SUPPLEMENTED BY POINT CLOUD DATA OBTAINED FROM NH GRANIT FOR AREAS OUTSIDE THE IMMEDIATE SURVEY LIMITS. THE VERTICAL DATUM IS NAVD 88 OBTAINED BY CONVERTING THE NGVD 1929 BENCHMARKS INDICATED ON PLAN REFERENCE NO.1. CONTOUR INTERVAL IS ONE (1) FOOT.
- THE PARCEL SHOWN IS PARTIALLY WITHIN IN ZONE AE, A FEMA SPECIAL FLOOD HAZARD AREA WITH AN ANNUAL 1% CHANCE OF FLOODING, PER FEMA FIS 33005CV001A, SHEET 20P, CROSS SECTIONS BR & BS, ALSO AS SHOWN ON FEMA FLOOD PANEL 33005C0266E, EFFECTIVELY DATED MAY 23, 2006. THE FLOOD HAZARD (100 YEAR FLOOD) ELEVATION IS 474.8' NAVD88. THE 10 YEAR FLOOD LINE HAS BEEN INTERPOLATED FROM THE SAME FIS CROSS SECTIONS TO BE 473.7.
- WATER ELEVATION AT SHORE OF ASHUELOT RIVER IN THE SURVEYED AREA ON JUNE 18, 2024 WAS APPROXIMATELY 471.4'
- UNDERGROUND UTILITIES, STRUCTURES AND FACILITIES HAVE BEEN PLOTTED FROM DATA OBTAINED FROM FIELD SURVEY OF SURFACE LOCATIONS & INFORMATION SHOWN ON PLAN REFERENCE No.1 THEIR EXISTENCE MUST BE CONSIDERED APPROXIMATE. THERE MAY BE OTHER UNDERGROUND UTILITIES THE EXISTENCE OF WHICH ARE NOT KNOWN. THE SIZE AND LOCATION OF ALL UTILITIES AND STRUCTURES MUST BE VERIFIED PRIOR TO ANY AND ALL CONSTRUCTION. CALL DIG-SAFE PRIOR TO ANY CONSTRUCTION.
  - A. WASTEWATER SEWER LINES AND STORM SEWER LINES ARE SHOWN FROM LOCATION OF SURFACE STRUCTURES AND MEASURED INVERTS.
  - B. WATER LINES ARE SHOWN FROM LIMITED DATA OBTAINED FROM CITY GIS AND A SINGLE SURFACE STRUCTURE.
  - C. GAS LINES ARE SHOWN FROM TIES AND GIS DATA OBTAINED FROM LIBERTY GAS.
  - D. SUBSURFACE ELECTRIC LINE LOCATIONS WERE NOT INVESTIGATED OR MARKED OUT BY THE PROPER AUTHORITIES, AND IF THEY EXIST, ARE NOT SHOWN.

**Surveyor's Certification**

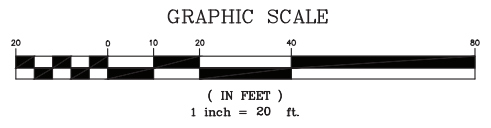
PURSUANT TO RSA 676: 18 III AND RSA 672: 14, I CERTIFY THAT THIS SURVEY AND PLAT WERE PRODUCED BY ME OR THOSE UNDER MY DIRECT SUPERVISION FROM A TOTAL STATION AND DATA COLLECTOR TRAVERSE THAT MEETS OR EXCEEDS NH LAN 500 AND THE ALLOWABLE RELATIVE POSITIONAL ACCURACY FOR URBAN AREAS AS REQUIRED BY THE STATE OF NEW HAMPSHIRE IN TABLE 500.1, "ACCURACY MEASUREMENTS. LOCAL ACCURACY OF CONTROL SUPPORTING THE SURVEY," AND IS BASED ON INFORMATION RECORDED AT THE CHESHIRE COUNTY REGISTRY OF DEEDS AS REFERENCED HEREON, INFORMATION PROVIDED BY THE CLIENT AND PHYSICAL EVIDENCE FOUND.

THIS IS A TOPOGRAPHIC SURVEY AND IS SUBSTANTIALLY CORRECT TO THE BEST OF MY KNOWLEDGE AND BELIEF. ALL DIMENSIONS ARE SUBJECT TO THE ERROR OF CLOSURE PREVIOUSLY STATED.

**Symbol Legend**

	CATCH BASIN
	DRAIN MANHOLE
	STORM SEWER LINE
	HYDRANT
	WATER VALVE
	CURB STOP
	WATER LINE
	SEWER MANHOLE
	SEWER CLEANOUT
	SANITARY SEWER LINE
	LIGHT POLE
	SIGN
	POST/BOLLARD
	CHAIN LINK FENCE
	TREE LINE
	EDGE OF PAVEMENT
	EDGE OF GRAVEL
	IRON PIN/PIPE
	SURVEY STATION
	KEENE ENGINEERING DEPARTMENT
	CHESHIRE COUNTY REGISTRY OF DEEDS
	TAX MAP PARCEL NUMBER
	DEED VOLUME & PAGE

NO.	DATE	REVISION	BY
3	8/29/2024	ADD TOPO & WETLANDS N OF SITE	RJH
2	6/20/2024	ADD TOPO ON STREET AND AT RIVER	RJH
1	3/26/2024	ADD 10 YEAR FLOOD LINE	RJH



**Asbuilt Plan**  
 LAND OF  
**JRR Properties LLC**  
 located at  
 Tax Map Parcel No. 567-001  
 0 Ashuelot Street, Keene, Cheshire County, New Hampshire  
 Book 2805, Page 630

Scale 1"= 20'  
 Surveyed 05/2021 Plan prepared 05/18/2021  
 Project No. H20-067 Cad File No. H20-067B.dwg

**Huntley Survey & Design, PLLC**  
 NH & VT Land Surveying, Wetlands & NH Septic System Design  
 659 West Road, Temple, NH 03084 (603) 924-1669 www.huntleysurvey.com





## GENERAL CONSTRUCTION NOTES:

1. THE CONTRACTOR SHALL CALL DIG-SAFE, AT 1-888-344-7233 AT LEAST 72 HOURS BEFORE THE START OF EXCAVATION.
2. THE CONTRACTOR IS EXPECTED TO BE AWARE OF AND COMPLY WITH ALL PERMITS AND PERMIT CONDITIONS.
3. ALL TRENCHING, EXCAVATION, SHEETING, SHORING, ETC. SHALL COMPLY WITH THE MOST CURRENT OSHA REGULATIONS.
4. THE CONTRACTOR SHALL NOTIFY SVE ASSOCIATES IF FIELD CONDITIONS VARY FROM THAT SHOWN ON THE PLAN(S). THE CONTRACTOR'S WORK SHALL NOT VARY FROM THE PLAN(S) UNLESS SO AUTHORIZED BY SVE ASSOCIATES.
5. ALL WORK SHALL BE COMPLETED IN ACCORDANCE WITH SITE PLANS AND SPECIFICATIONS PROVIDED OR IN ACCORDANCE WITH NH DEPT' OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, LATEST EDITION.
6. IN CASE OF CONFLICTS, THE MOST STRINGENT INTERPRETATION OF THE PLANS, SPECIFICATIONS, LOCAL OR STATE REGULATIONS, OR PERMIT CONDITIONS SHALL APPLY. THE ENGINEER SHALL BE THE DETERMINANT AS TO WHAT APPLIES.
7. ALL KNOWN SUBSURFACE UTILITIES AND STRUCTURES HAVE BEEN INDICATED ON THE PLAN(S) AS ACCURATELY AS POSSIBLE. THE EXACT LOCATION MAY VARY AND THE CONTRACTOR IS CAUTIONED TO PROCEED WITH CARE.
8. CONTRACTOR SHALL VERIFY ALL BENCH MARKS, INVERTS, PIPES AND STRUCTURES ELEVATIONS PRIOR TO START OF WORK. IMMEDIATELY NOTIFY SVE ASSOCIATES IF THE FIELD INFORMATION DOES NOT MATCH PLAN INFORMATION.
9. THE OWNER WILL PROVIDE BENCH MARKS. THE CONTRACTOR WILL BE RESPONSIBLE FOR ALL OTHER LAYOUT AND FOR REPLACEMENT OF LAYOUT COMPLETED BY THE OWNER.
10. CONTRACTOR SHALL PROVIDE A FULL SET OF AS-BUILT DRAWINGS TO THE OWNER WITH SWING TIES OR COORDINATES. LOCATING ALL VALVES, FITTINGS, CORPORATIONS, STRUCTURES, PIPES, ETC. THE AS-BUILTS SHALL INDICATE MATERIALS, PIPE LENGTHS INSTALLED, ALL INVERTS, AND ALL STRUCTURE ELEVATIONS. ACCEPTANCE OF THE WORK IS SUBJECT TO ACCEPTANCE OF THE AS-BUILTS BY THE ENGINEER AND OWNER.
11. MONUMENTATION THAT HAS BEEN DISTURBED SHALL BE RESET BY A NEW HAMPSHIRE LICENSED LAND SURVEYOR AT NO COST TO THE OWNER.
12. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL DE-WATERING AT NO ADDITIONAL COST TO THE OWNER.
13. ALL CASTINGS AND VALVE BOXES SHALL BE SET FLUSH IN PAVEMENT AND WALKS, UP 0.1 FEET IN VEGETATED SURFACES.
14. ALL SURFACES SHALL BE GRADED TO DRAIN.
15. ALL TREES WHOSE ROOTS HAVE BEEN DAMAGED SHALL BE REMOVED AT THE CONTRACTOR'S EXPENSE.
16. THE CONTRACTOR SHALL RESTORE ALL DISTURBED SURFACES TO THEIR ORIGINAL CONDITION OR BETTER. ALL NEW AND EXISTING PIPES AND STRUCTURES SHALL BE CLEANED. ALL SIGNS SHALL BE REPLACED. ALL DAMAGED VEGETATION SHALL BE REPLACED.
17. ALL CURB SHALL BE SET SO THAT ENDS ABUT OR ARE TIPPED DOWN, 6" MINIMUM LENGTH, FLUSH WITH PAVEMENT.
18. UNLESS OTHERWISE NOTED, ALL CURB RADII TO BE FACE OF CURB.

## SEDIMENT AND EROSION CONTROL

1. INSTALL ALL SEDIMENT & EROSION CONTROL MEASURES IN ACCORDANCE WITH MANUFACTURER'S DIRECTION OR DETAILS PROVIDED. PERIMETER CONTROLS MUST BE INSTALLED PRIOR TO EARTH MOVING OPERATIONS.
2. THE CONTRACTOR IS RESPONSIBLE FOR ALL EROSION CONTROL. THEY SHALL TAKE ALL MEASURES NEEDED TO MINIMIZE EROSION TO THE GREATEST EXTENT POSSIBLE, AT NO ADDITIONAL COST TO THE OWNER, REGARDLESS OF DETAIL SHOWN ON THESE PLANS.
3. CONTRACTOR SHALL INSPECT AND REPAIR ALL SEDIMENT AND EROSION CONTROL MEASURES DAILY WHILE UNDER CONSTRUCTION, THEN AFTER EACH RAINFALL OF 0.5" IN 24 HOURS AND NOT LESS THAN ONCE A WEEK THEREAFTER UNTIL ALL UPHILL SOILS ARE WELL STABILIZED.
4. CONTRACTOR SHALL MAINTAIN INSPECTION LOGS ON SITE AS REQUIRED BY THE EPA STORMWATER POLLUTION PREVENTION PLAN. INSPECTION LOGS SHALL BE AVAILABLE FOR VIEWING BY THE APPROPRIATE AUTHORITIES UPON REQUEST.
5. SEED & MULCH ALL FINISH GRADED AREAS WITHIN 72 HOURS OF FINISH GRADING. ROADWAY STABILIZED W/IN 72 HOURS OF ACHIEVING FINISH GRADE.
6. SEDIMENT CONTROLS AND/OR SILT FENCES SHALL BE REPLACED WHEN CLOGGED AND NO LONGER FUNCTIONAL.
7. SEDIMENT CONTROLS AND/OR SILT FENCES SHALL REMAIN IN PLACE UNTIL ALL UPHILL VEGETATED AREAS ARE STABILIZED.
8. ALL SOIL STOCKPILES SHALL BE SEEDED AND MULCHED IF LEFT IN PLACE MORE THAN 21 DAYS.
9. SEEDING OF ALL DISTURBED AREAS SHALL BE COMPLETED NOT LATER THAN OCTOBER 15TH.
10. STABILIZATION OF ALL WORK AREAS SHALL BE COMPLETED NOT MORE THAN 45 DAYS FOLLOWING THE START OF WORK.
11. ALL SOIL SLOPES STEEPER THAN 3:1 SHALL BE COVERED WITH EROSION CONTROL FABRIC, S150
12. STABILIZE ALL DRAINAGE SWALES, BASINS, BERMS, AND DITCHES PRIOR TO DIRECTING RUNOFF TO THEM.
13. CONTRACTOR SHALL IMMEDIATELY REPAIR OR REPLACE SEDIMENT AND EROSION CONTROLS AS REQUESTED BY THE ENGINEER.
14. LIMIT THE AREA OF DISTURBANCE TO SMALLEST PRACTICAL AREA.
15. LOT CONSTRUCTION SHALL NOT COMMENCE UNTIL AFTER THE ASSOCIATED ROADWAY AND DRAINAGE IS COMPLETE AND STABLE.

## PROJECT SPECIFIC NOTES:

1. ALL AREAS TO BE VEGETATED SHALL RECEIVE A MINIMUM OF 6" OF LOAM, SEED AND MULCH.
2. SEEDING OF ALL DISTURBED AREAS SHALL BE COMPLETED NOT LATER THAN OCTOBER 15TH.
3. SEEDING OF ALL FINISHED AREAS SHALL BE COMPLETED NOT MORE THAN 72 HOURS AFTER FINISH GRADING.
4. STABILIZATION OF ALL WORK AREAS SHALL BE COMPLETED NOT MORE THAN 45 DAYS FOLLOWING THE START OF WORK.
5. ALL NEW EXTERIOR LIGHTS SHALL BE SHIELDED TO PROTECT AGAINST ADDED LIGHT POLLUTION.
6. STABILIZE ALL DRAINAGE SWALES PRIOR TO DIRECTING RUNOFF TO THEM.

## SEQUENCE OF WORK

THE SEQUENCE OF WORK SHALL BE FOLLOWED WITHIN EACH PHASE OF THE PROJECT. AT NO TIME OR PLACE SHALL PROJECT PHASING SUPERCEDE SOUND SEDIMENT AND EROSION CONTROL PLANNING.

1. INSTALL SILT FENCE IN ACCORDANCE WITH MANUFACTURER'S DIRECTIONS, IN LOCATIONS DETAILED ON THIS PLAN OR AS ORDERED BY THE ENGINEER.
2. CONSTRUCT THE STABILIZED CONSTRUCTION ENTRANCE TO PREVENT TRACKING OF SEDIMENT OFFSITE.
3. INSTALL SILT SACKS IN CATCH BASINS.
4. CLEAR AND GRUB THE SITE.
5. PLACE FILL TO BRING SITE ABOVE THE 100 YEAR FLOOD ELEVATION. EXCAVATE FLOOD STORAGE COMPENSATION.
6. INSTALL WATER SERVICE FOLLOWED BY OTHER UTILITIES.
7. CONSTRUCT ACCESS DRIVE IN ACCORDANCE WITH APPROVED PLANS.
8. CLEAR & GRUB BUILDING/PARKING AREA. CONSTRUCT NEW PARKING LOT AND BUILDING.
9. LOAM AND SEED DISTURBED AREAS, STABILIZE SLOPES WITH MATTING WHERE SPECIFIED.
10. REMOVE SILT FENCE AFTER ALL UPHILL SOILS ARE STABILIZED.

## STABILIZATION DEFINITION:

AN AREA SHALL BE CONSIDERED STABLE IF ONE OF THE FOLLOWING HAS OCCURED:

1. BASE COURSE GRAVELS HAVE BEEN INSTALLED IN AREAS TO BE PAVED;
2. A MINIMUM OF 85% VEGETATED GROWTH HAS BEEN ESTABLISHED;
3. A MINIMUM OF 3" OF NON-EROSIVE MATERIAL SUCH STONE OR RIPRAP HAS BEEN INSTALLED;
4. EROSION CONTROL BLANKETS HAVE BEEN PROPERLY INSTALLED.

## A.D.A. ACCESSIBILITY NOTES:

ALL CONSTRUCTION SHALL COMPLY WITH DEPARTMENT OF JUSTICE 28 CFR PART 36, A.D.A. STANDARDS FOR ACCESSIBLE DESIGN. THIS INCLUDES, BUT IS NOT LIMITED TO THE FOLLOWING REQUIREMENTS:

### PARKING SPACES AND ACCESS AISLES:

1. PARKING SPACES AND ACCESS AISLES SHALL HAVE SURFACE SLOPES NOT EXCEEDING 1:50 (2%) IN ANY DIRECTION.
2. MINIMUM PARKING SPACE WIDTH SHALL BE 8 FT.
3. MINIMUM ACCESS AISLE WIDTH SHALL BE 5 FT. (8 FT. FOR VAN ACCESSIBLE SPACES).
4. ACCESSIBLE SPACES SHALL BE DESIGNATED AS RESERVED BY A SIGN SHOWING THE SYMBOL OF ACCESSIBILITY. VAN ACCESSIBLE SPACES SHALL BE FURTHER DESIGNATED AS SUCH BY APPROPRIATE SIGNAGE.

### ACCESSIBLE ROUTES:

5. AT LEAST ONE ACCESSIBLE ROUTE SHALL BE PROVIDED FROM PUBLIC TRANSPORTATION STOPS, A.D.A. PARKING, PASSENGER LOADING ZONES, AND PUBLIC STREETS OR SIDEWALKS, TO AN A.D.A. BUILDING ENTRANCE.
6. AT LEAST ONE ACCESSIBLE ROUTE SHALL CONNECT A.D.A. ACCESSIBLE BUILDINGS, ACCESSIBLE ELEMENTS AND FACILITIES (MAILBOXES, TRASH RECEPTACLES, COMMON AREAS), AND A.D.A. PARKING THAT ARE ON THE SAME SITE.
7. MAXIMUM SLOPE OF SURFACES ADJACENT TO AN ACCESSIBLE ROUTE SHALL NOT EXCEED 1:20 (5%).
8. CURB RAMP FLARES SHALL NOT EXCEED A SLOPE OF 1:12 (8.33%).
9. MAXIMUM CROSS-SLOPE ALONG ANY PORTION OF THE ACCESSIBLE ROUTE SHALL NOT EXCEED 1:50 (2%).
10. TRANSITIONS FROM RAMPS AND WALKS SHALL BE FLUSH AND FREE OF ABRUPT CHANGES.

### RAMPS:

11. ANY PART OF AN ACCESSIBLE ROUTE WITH A SLOPE GREATER THAN 1:20 (5%) SHALL BE CONSIDERED A RAMP.
12. THE LEAST POSSIBLE SLOPE SHALL BE USED FOR ANY RAMP.
13. MAXIMUM SLOPE OF ANY RAMP SHALL BE 1:12 (8.33%).
14. MAXIMUM RISE OF ANY RAMP SHALL BE 30 IN. ANY RAMP HAVING A RISE GREATER THAN OR EQUAL TO 6 IN. SHALL HAVE AT LEAST ONE HANDRAIL.
15. RAMPS SHALL HAVE LEVEL LANDINGS AT BOTTOM AND TOP. LANDINGS SHALL BE AS WIDE AS THE RAMP AND AT LEAST 60 IN. LONG.
16. OUTDOOR RAMPS AND THEIR APPROACHES SHALL BE DESIGNED SO THAT WATER WILL NOT ACCUMULATE ON WALKING SURFACES.

IN THE EVENT THAT THESE REQUIREMENTS CONFLICT WITH DESIGN PLANS, OR IF FIELD CONDITIONS RENDER THESE UNATTAINABLE, CONTACT THE ARCHITECT AND/OR ENGINEER PRIOR TO BEGINNING WORK.

## WINTER CONSTRUCTION REQUIREMENTS:

1. ALL PROPOSED VEGETATED AREAS THAT DO NOT EXHIBIT A MINIMUM OF 85% VEGETATIVE GROWTH BY OCTOBER 15TH, OR WHICH ARE DISTURBED AFTER OCTOBER 15TH, SHALL BE STABILIZED BY SEEDING AND 1) INSTALLING EROSION CONTROL BLANKETS ON SLOPES GREATER THAN 3:1, or 2) PLACING 3 TO 4 TONS OF MULCH PER ACRE, SECURED WITH ANCHORED NETTING. THE INSTALLATION OF EROSION CONTROL BLANKETS AND NETTING SHALL NOT OCCUR OVER ACCUMULATED SNOW OR ON FROZEN GROUND AND SHALL BE COMPLETED IN ADVANCE OF SPRING THAW OR SPRING MELT EVENTS.
2. ALL DITCHES OR SWALES WHICH DO NOT EXHIBIT A MINIMUM OF 85% VEGETATIVE GROWTH BY OCTOBER 15TH, OR WHICH ARE DISTURBED AFTER OCTOBER 15TH, SHALL BE STABILIZED TEMPORARILY WITH STONE OR EROSION CONTROL BLANKETS APPROPRIATE FOR THE DESIGN FLOW CONDITIONS.
3. AFTER OCTOBER 15TH, INCOMPLETE ROAD OR PARKING SURFACES WHERE WORK HAS STOPPED FOR THE WINTER SEASON SHALL BE PROTECTED WITH A MINIMUM OF 3 INCHES OF CRUSHED GRAVEL MEETING NHDOT ITEM 304.3 SPECIFICATIONS.

## DUST CONTROL:

DUST CONTROL SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE NEW HAMPSHIRE STORMWATER MANUAL, VOLUME 3: EROSION AND SEDIMENT CONTROLS DURING CONSTRUCTION.

1. PHASE CONSTRUCTION AND SEQUENCE EARTH DISTURBANCE ACTIVITIES TO REDUCE THE AREA OF LAND DISTURBED AT ANY ONE TIME.
2. MAINTAIN AS MUCH NATURAL VEGETATION AS IS PRACTICABLE.
3. USE TRAFFIC CONTROL TO RESTRICT TRAFFIC TO PREDETERMINED ROUTES.
4. USE TEMPORARY MULCHING, PERMANENT MULCHING, TEMPORARY VEGETATIVE COVER, PERMANENT VEGETATIVE COVER TO REDUCE THE NEED FOR DUST CONTROL.
5. APPLY WATER, OR OTHER DUST INHIBITING AGENTS OR TACKIFIERS, AS APPROVED BY THE NHDES.

## ALTERATION OF TERRAIN NOTES:

1. THIS PROJECT IS REQUIRED TO BE MANAGED IN A MANNER THAT MEETS THE REQUIREMENTS AND INTENT OF RSA 430: 53 AND CHAPTER AGR 3800 RELATIVE TO INVASIVE SPECIES.
2. PERIMETER CONTROLS SHALL BE INSTALLED PRIOR TO EARTH MOVING OPERATIONS.
3. TEMPORARY WATER DIVERSION (SWALES, BASINS, ETC.) MUST BE USED AS NECESSARY UNTIL AREAS ARE STABILIZED.
4. FUGITIVE DUST MUST BE CONTROLLED IN ACCORDANCE WITH ENV-A 1000. PRECAUTIONS TO PREVENT, ABATE AND CONTROL FUGITIVE DUST SHALL INCLUDE BUT NOT BE LIMITED TO THE FOLLOWING:
  - A. THE USE OF WATER OF HYDROPHILIC MATERIAL ON OPERATIONS OR SURFACES OR BOTH;
  - B. THE APPLICATION OF WATER OF HYDROPHILIC MATERIAL, OR TARPS OR OTHER SUCH COVERS TO MATERIAL STOCKPILES;
  - C. THE USE OF HOODS, FANS, FABRIC FILTERS, OR OTHER DEVICES TO ENCLOSE AND VENT AREAS WHERE MATERIALS PRONE TO PRODUCING FUGITIVE DUST ARE HANDLED;
  - D. THE USE OF CONTAINMENT METHODS FOR SANDBLASTING OR SIMILAR OPERATIONS; AND
  - E. THE USE OF VACUUMS OR OTHER SUCTION DEVICES TO COLLECT AIRBORNE PARTICULATE MATTER.
3. RUNOFF MUST BE DIRECTED TO TEMPORARY PRACTICES UNTIL STORMWATER BMPS ARE STABILIZED.

## PERMITS REQUIRED:

1. NHDES ALTERATION OF TERRAIN
2. EPA STORMWATER POLLUTION PREVENTION PLAN
3. CITY FLOODPLAIN DEVELOPMENT PERMIT
4. CITY SITE PLAN PERMIT
5. CITY EXCAVATION PERMIT (WATER & SEWER SERVICES).

100 YEAR FLOODPLAIN: 474.8

10 YEAR FLOODPLAIN: 473.7

## SITE DATA TABLE

TAX MAP #:	567-001-000-000-000, 153,767 SQ. FT. ± 3.53 ACRES		
ZONE:	COMMERCE		
LOT SIZE:	AVAILABLE: 3.53 AC	REQUIRED: 15,000 SF	AFTER SUBDIVISION: 47,237 SF/1.08± AC
FRONTAGE:	377'	50'	185'
LOT WIDTH:	360'	50'	185'
BLDG. HEIGHT:	ALLOWED: 35'	PROPOSED: 26'	
BUILDING SETBACKS:	FRONT: 20'	REAR: 20'	SIDES: 20'
PAVEMENT SETBACKS:	PARKING AREA ≤ 10,000 SF PARKING LOT FRONT/REAR/SIDES: 8'		
LOT COVERAGE:	MAXIMUM: 80% (123,013 SF/2.82 AC)	PROPOSED: 4% (6,595 SF/0.15 AC)	AFTER SUBDIVISION: 14% (6,595 SF/0.15 AC)
BUILDINGS:	80% (123,013 SF/2.82 AC)	2% (2,845 SF/0.07 AC)	6% (2,845 SF/0.07 AC)
PAVEMENT:	80% (123,013 SF/2.82 AC)	6% (9,440 SF/0.22 AC)	20% (9,440 SF/0.22 AC)
TOTAL IMPERMEABLE:	80% (123,013 SF/2.82 AC)	6% (9,440 SF/0.22 AC)	20% (9,440 SF/0.22 AC)
PARKING	REQUIRED 9'x 18': 4 SPACES/1000 GFA=25 ADA ACCESSIBLE: 2	PROPOSED 25 (OF WHICH 2 ARE ADA ACCESSIBLE) 2	
LANDSCAPING	REQUIRED 1 TREE/10 SPACES =3	PROPOSED 4 TREES	

## PROPERTY OWNER:

JRR PROPERTIES, LLC  
P.O. BOX 323  
KEENE, NH 03431

## APPLICANT:

MONADNOCK CONSERVANCY  
15 EAGLE COURT, 2ND FLOOR  
KEENE, NH 03431

## SEED SPECIFICATIONS

### TEMPORARY SEED

PERENNIAL RYE GRASS

### PERMANENT SEED:

ALL MOWABLE AREAS: PARK SEED NHDOT TYPE 15  
(CONSERVATION MIX ACCEPTABLE, AS APPROVED BY ENGINEER)

CREEPING RED FESCUE	40 LB/AC
PERENNIAL RYEGRASS	50 LB/AC
KENTUCKY BLUEGRASS	25 LB/AC
REDTOP	5 LB/AC
TOTAL:	120 LB/AC

## LEGEND

	HYDRANT
	EXISTING CATCH BASIN
	PROPOSED CATCH BASIN
	CULVERT END SECTION
	SEWER MANHOLE
	GATE VALVE
	DRAIN MANHOLE
	HANDICAP PARKING
	LANDSCAPE POST LIGHT
	LIGHT POLE
	EXTERIOR RECESSED LED LIGHT
	EXTERIOR WALL SCONCE
	SEWER LINE
	WATER LINE
	STORM DRAIN LINE
	SILT FENCE
	100 YEAR FLOODPLAIN BOUNDARY
	WETLAND BOUNDARY
	SIGN
U.N.O.	UNLESS NOTED OTHERWISE
N.I.C.	NOT IN CONTRACT
T.B.R.	TO BE REMOVED
O.A.E.	OR APPROVED EQUAL
S.C.E.	STABILIZED CONSTRUCTION ENTRANCE
I.C.C.	INTEGRAL CONCRETE CURB
V.C.C.	VERTICAL CONCRETE CURB
V.G.C.	VERTICAL GRANITE CURB
T.D.	TIP-DOWN
L.S.M.	LOAM, SEED & MULCH



Liza Sargent 11/12/24

LIZA P. SARGENT DATE  
R.C.E. NUMBER: 13365

NO.	DATE	REVISION	CHK	LPS
1	01-NOV-24			
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# SVE

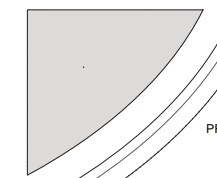
Engineering  
Planning  
Landscape Architecture  
Surveying

SVE Associates  
P.O. Box 1818  
439 West River Road  
Brattleboro, VT 05302  
T 802.257.0561  
F 802.257.0721  
www.sveassoc.com

## NOTES & LEGEND

MONADNOCK CONSERVANCY  
HEADQUARTERS

MONADNOCK CONSERVANCY  
15 EAGLE CT, 2ND FLOOR  
KEENE, NEW HAMPSHIRE



PROJ. #:  
K2781

DATE:  
10-OCT-24

DESIGN: LPS SHEET  
DRAWN: LPS  
CHECKED: LPS  
35 of 92

# N-1





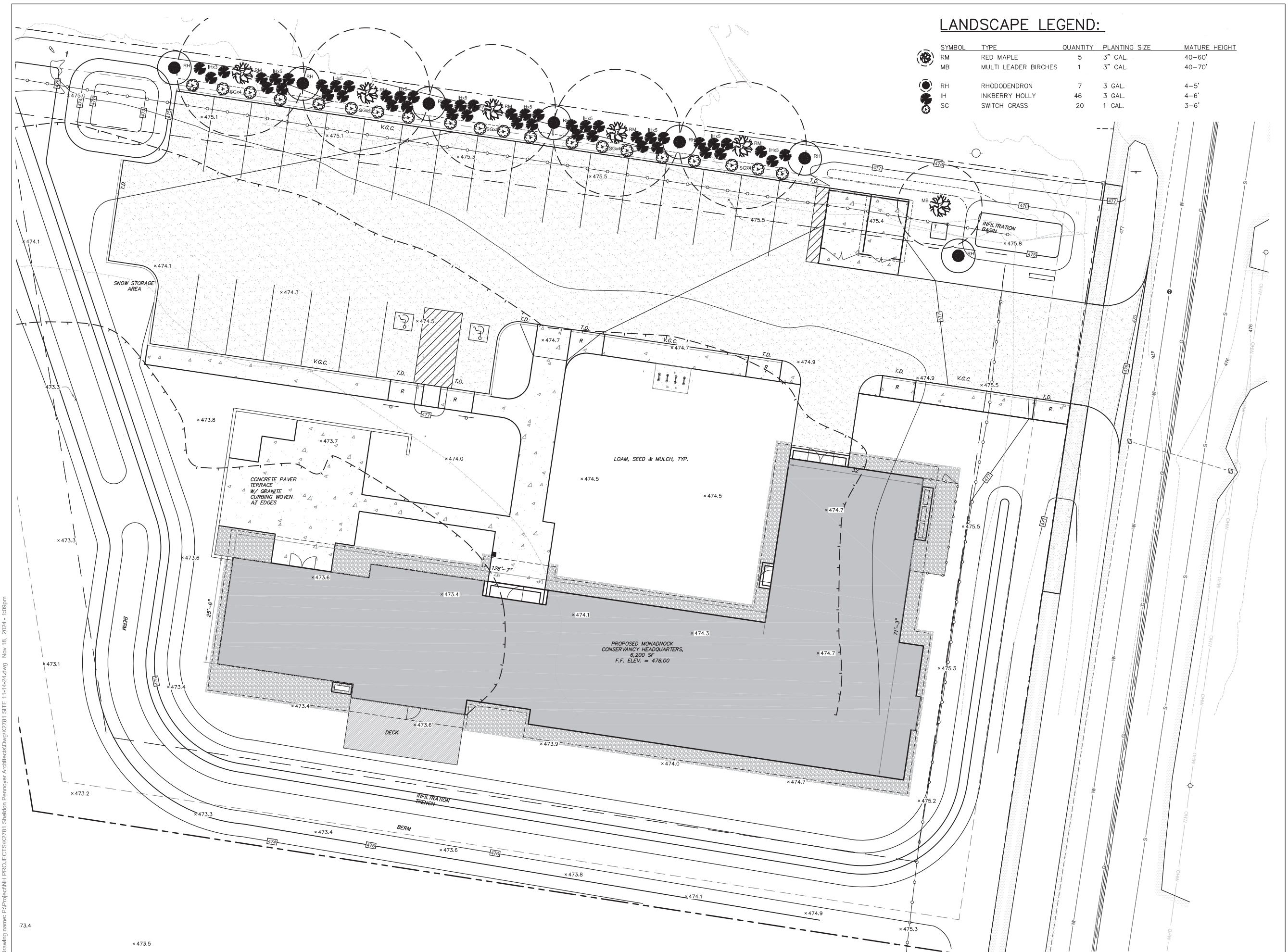






**LANDSCAPE LEGEND:**

SYMBOL	TYPE	QUANTITY	PLANTING SIZE	MATURE HEIGHT
	RED MAPLE	5	3" CAL.	40-60'
	MULTI LEADER BIRCHES	1	3" CAL.	40-70'
	RHODODENDRON	7	3 GAL.	4-5'
	INKBERRY HOLLY	46	3 GAL.	4-6'
	SWITCH GRASS	20	1 GAL.	3-6'



NO.	REVISION	DATE	CHK
		LPS	LPS
1	ADDED FINISH GRADING	04-NOV-24	LPS
2	ADDITIONAL LANDSCAPING	18-NOV-24	LPS

**SVE**  
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**LANDSCAPING PLAN**  
 MONADNOCK CONSERVANCY HEADQUARTERS  
 MONADNOCK CONSERVANCY  
 15 EAGLE CT, 2ND FLOOR  
 KEENE, NEW HAMPSHIRE

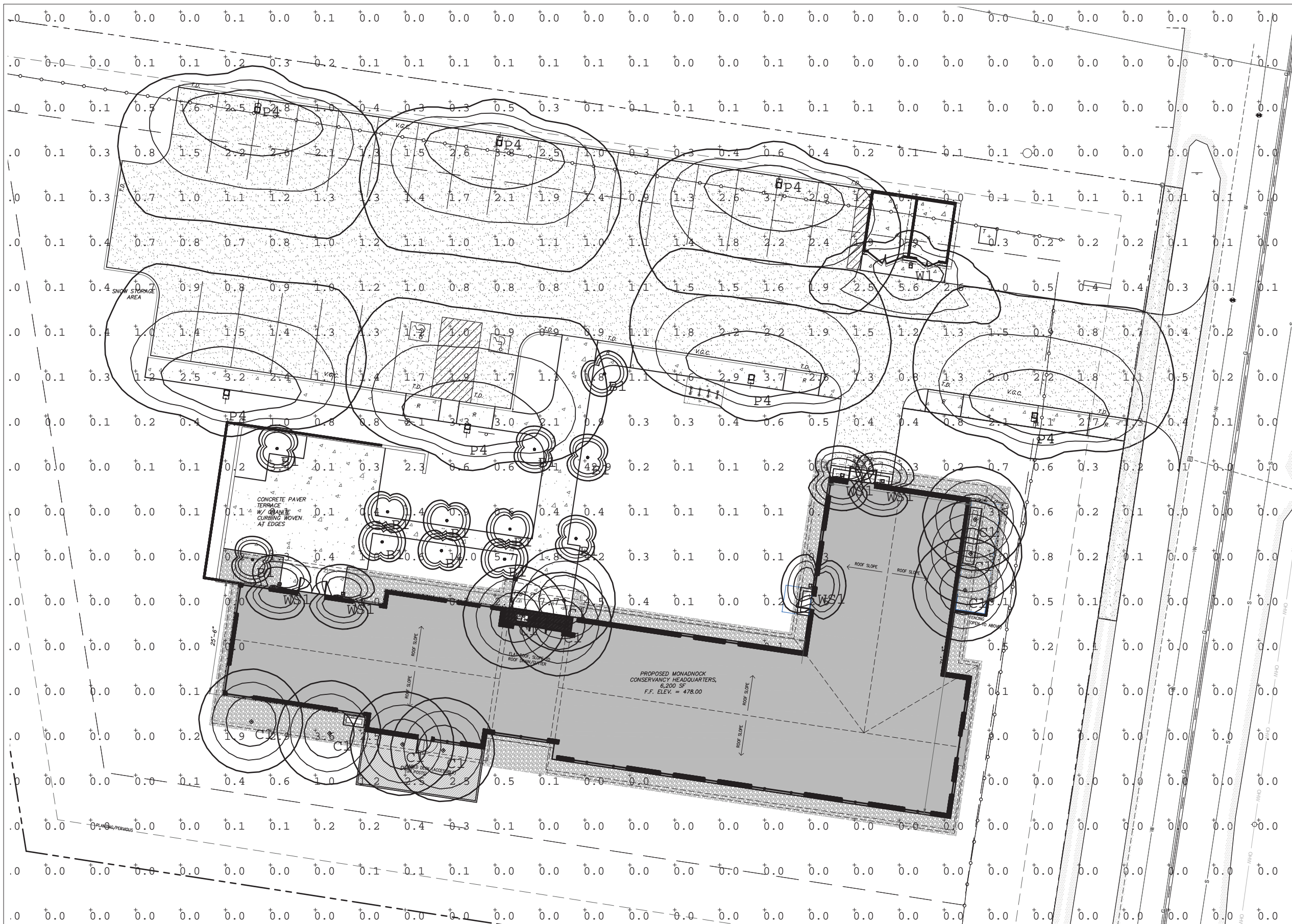
0 5' 10' 20'  
 GRAPHIC SCALE 1" = 10'

PROJ. #:  
K2781  
 DATE:  
10-OCT-24

Drawing name: P:\Project\NH PROJECTS\K2781 - Sheldon Pinnoyer Arch\Hecsd\K2781 SITE 11-14-24.dwg Nov 18, 2024 - 1:09pm



Drawing name: P:\Projects\NH PROJECTS\K2781\Sheldon Penneyr Architect\DWG\K2781 SITE 11-7-24.dwg Nov 12, 2024 - 9:56am



Symbol	Qty	Label	Arrangement	Description	Tag	LED	Luminaire Lumens	Luminaire Watts	TOTAL Watts
CL	15	CL	Single	MFC 6643-308E		0.900	849	13.2356	198.524
CL	11	CL	Single	COVER HOLES/SPRINKLER (ROSLIM-300K SELECTIONS) // WITH BRACKET OR OTHER COMPATIBLE HOUSING	CANOPY LIGHT // ADVISE IF DECORATIVE OVERLAY NEEDED	0.900	924	7.4	107.7
P4	7	P4	Single	ROOSER UNDER-SHAFT-TYPE-160	MOUNTED ON 1" VALANTRY POLE: D8110-450123-DL-PP-ROOSER-BE-FRC-AN	0.900	1599	33	231
W1	1	W1	Single	MFC-TWL-F3-161-175-308X-UNV-WK-CXK	WALL MTD TO SEED, 8" SPD	0.900	1212	10	10
W1	5	W1	Single	MFC-WB-W1819-WK	WALL MTD 6" SPD	0.900	446	12.7897	63.948

Label	CalcType	Units	Avg	Max	Min	Avg/Min	Max/Min
ENTRANCE AREA	Illuminance	fc	2.31	42.8	2.2	19.9	21.9
PARKING LOT	Illuminance	fc	1.56	5.6	0.7	2.23	8.00

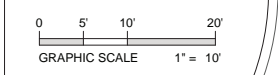
FOR PERMITTING

NO.	REVISION	DATE	DWN	CHK

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 P.O. Box 1818  
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 Brattleboro, VT 05302  
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 F 802.257.0721  
 www.sveassoc.com

**LIGHTING PLAN**  
 MONADNOCK CONSERVANCY HEADQUARTERS  
 MONADNOCK CONSERVANCY  
 15 EAGLE CT, 2ND FLOOR  
 KEENE, NEW HAMPSHIRE



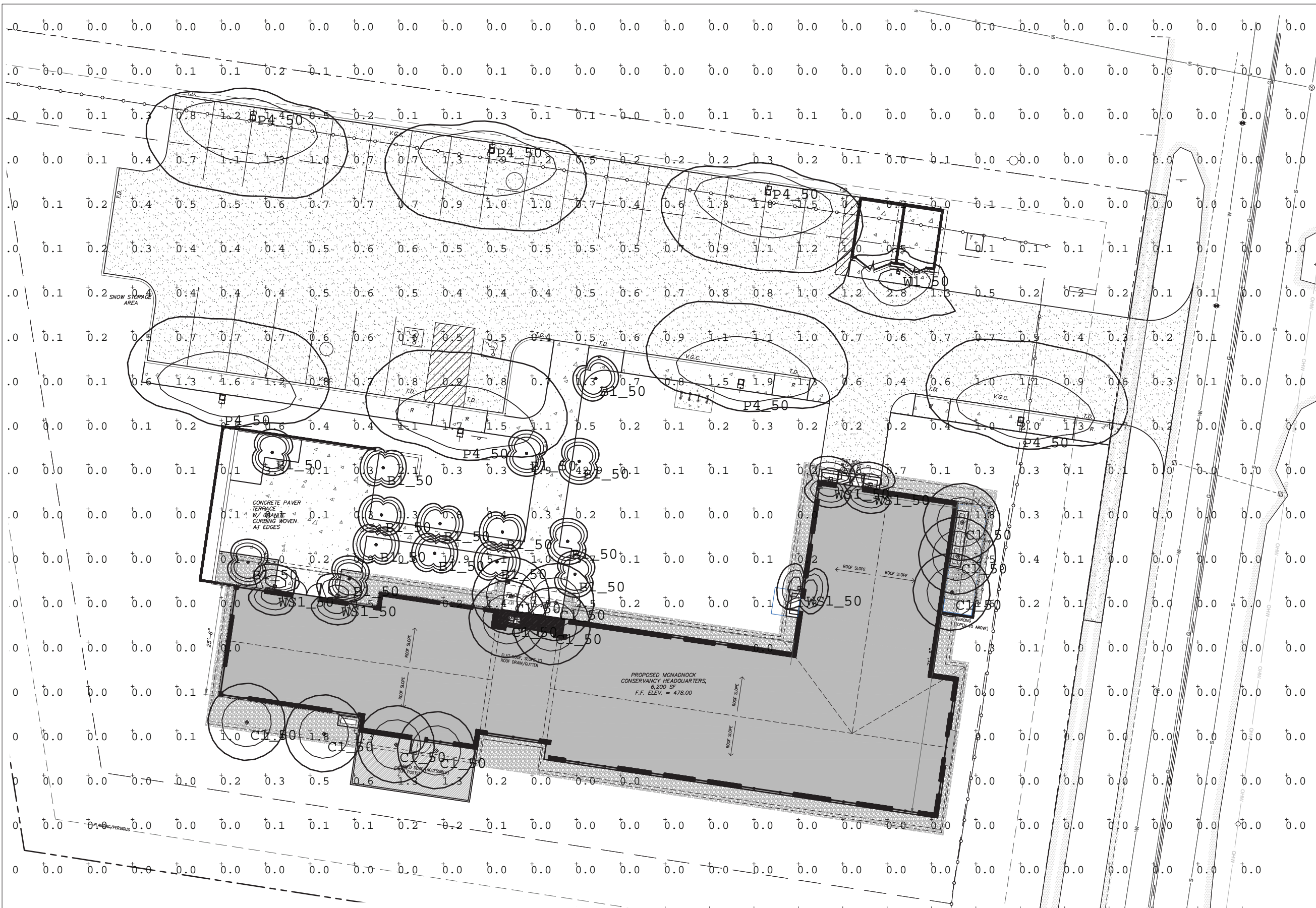
PROJ. #: K2781  
 DATE: 10-OCT-24

DESIGN: LPS SHEET  
 DRAWN: LPS  
 CHECKED: LPS

39 of 42 **LT-1**



Drawing name: P:\Project\NH PROJECTS\K2781\_Sheldon\_Pennoyer\_Architects\DWG\K2781 SITE 11-7-24.dwg Nov 12, 2024 - 9:56am



Symbol	Qty	Label	Arrangement	Description	Fix	SLP	Luminaire Lumens	Luminaire Watts	Total Watts
⊙	15	SI_50	Single	WACI 841-3082	DIMMED SS	0.950	549	13.2336	198.504
⊙	11	CI_50	Single	COOPER BQ44-SPRODLINE (SOLA-3008 SELECTIONS) // WITH BRACKET OR OTHER COMPARABLE MOUNTING	DIMMED SS // CANOPY LIGHT // ADVISE IF DECORATIVE OVERLAY NEEDED	0.450	926	9.8	107.8
⊙	7	PI_50	Single	COOPER CALM-BATA-830-0-SLA-82	DIMMED SS // MOUNTED ON 12" VALMONT POLE: 06330-000110-DI-PP-	0.450	3599	93	231
⊙	1	WE_50	Single	WACI 841-3082	DIMMED SS // WALL MTD TO 880V, 8" APD	0.450	1212	10	10
⊙	1	WS1_50	Single	WACI 841-3082	DIMMED SS // WALL MTD 8" APD	0.450	446	12.1897	83.948

Label	CalcType	Units	Avg	Max	Min	Avg/Min	Max/Min
ENTIRE AREA	ILLUMINANCE	FC	0.59	42.9	0.0	82.9	9.73
PARKING LOT	ILLUMINANCE	FC	0.97	8.8	0.0	2.97	9.73

EXTERIOR LIGHTING TO BE REDUCED BY 50% BETWEEN 10PM AND 6AM PER TOWN STANDARDS WITH A LIGHTING CONTROL PANEL/TIME CLOCK

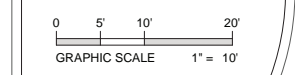
FOR PERMITTING

NO.	REVISION	DATE	DWN	CHK

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 Landscape Architecture  
 Surveying

SVE Associates  
 P.O. Box 1818  
 439 West River Road  
 Brattleboro, VT 05302  
 T 802.257.0561  
 F 802.257.0721  
 www.sveassoc.com

**REDUCED LIGHTING PLAN**  
 MONADNOCK CONSERVANCY HEADQUARTERS  
 MONADNOCK CONSERVANCY  
 15 EAGLE CT, 2ND FLOOR  
 KEENE, NEW HAMPSHIRE



PROJ. #: K2781  
 DATE: 10-OCT-24  
 DESIGN: LPS  
 DRAWN: LPS  
 CHECKED: LPS  
 SHEET 40 OF 42 **LT-2**















SP ARCHITECTS

WWW.SP-ARCHITECTS.COM

603-856-8994  
64 North Main St., Suite 201  
Concord, NH 03301

MONADNOCK CONSERVANCY  
OFFICE HEADQUARTERS

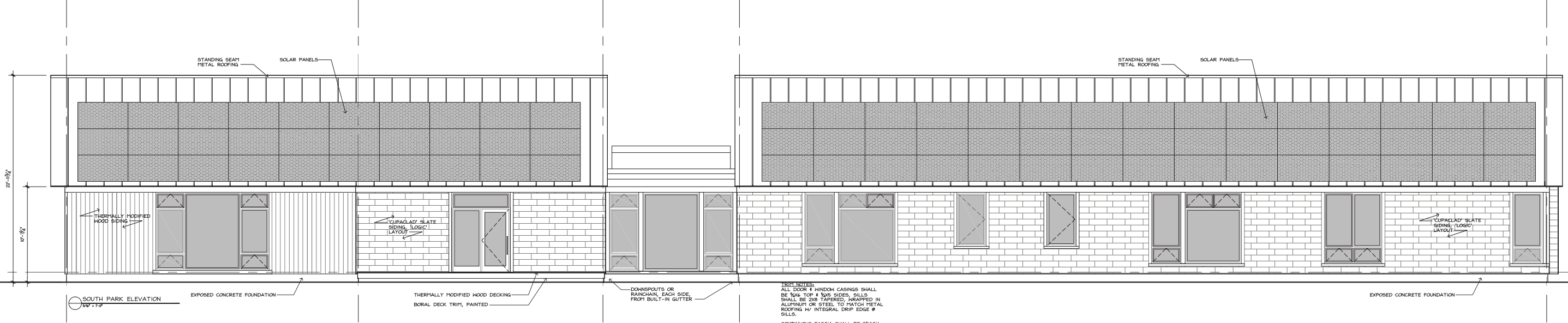
ASHUELOT STREET  
KEENE, NEW HAMPSHIRE 03431

EXTERIOR ELEVATIONS

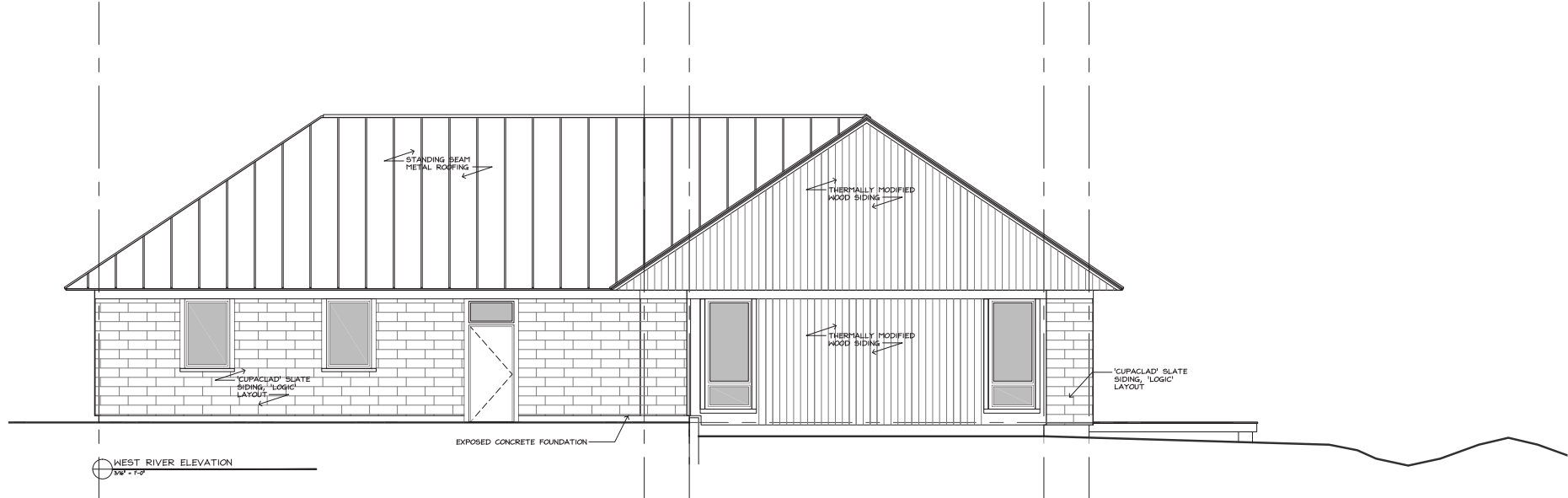
DRAWN BY: RMF  
CHECKED BY: SKP  
DATE:  
REVISIONS:

DRAWING NO:

A2.1



**FINISH NOTES:**  
 ALL DOOR & WINDOW CASINGS SHALL BE 1 1/2\"/>



WEST RIVER ELEVATION  
WV-102

SOUTH PARK ELEVATION  
SP-102



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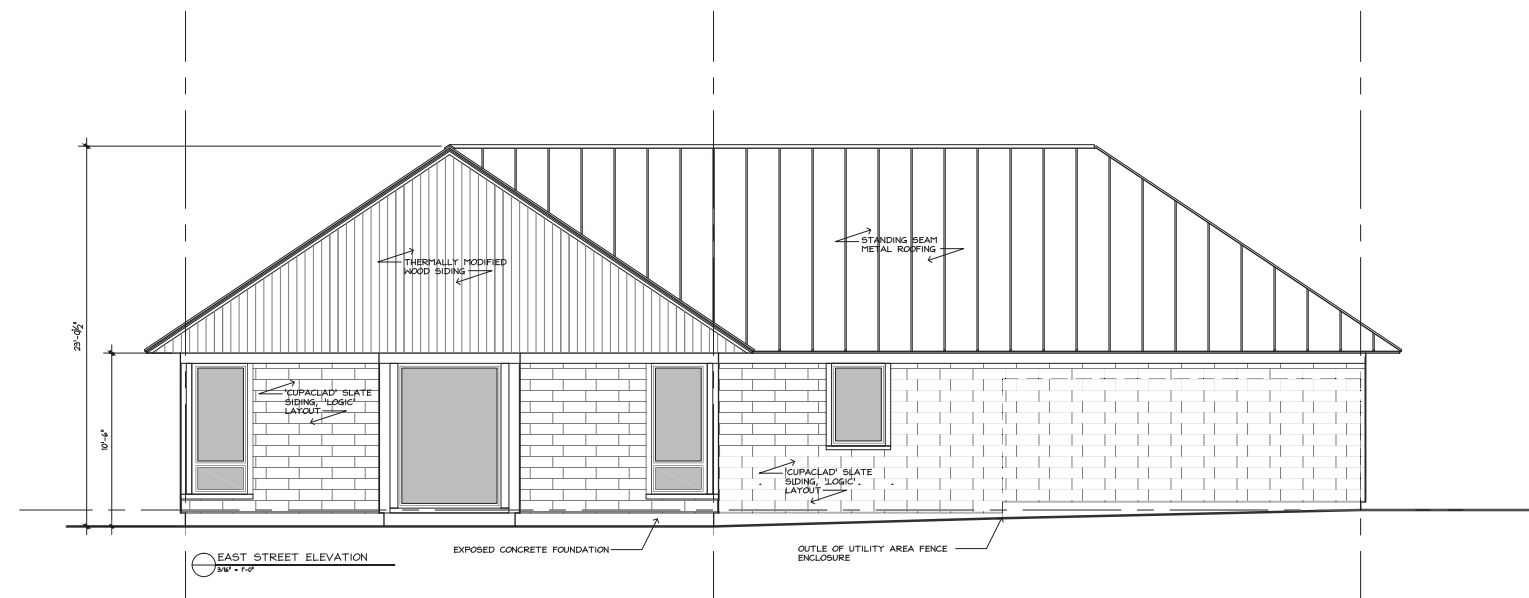
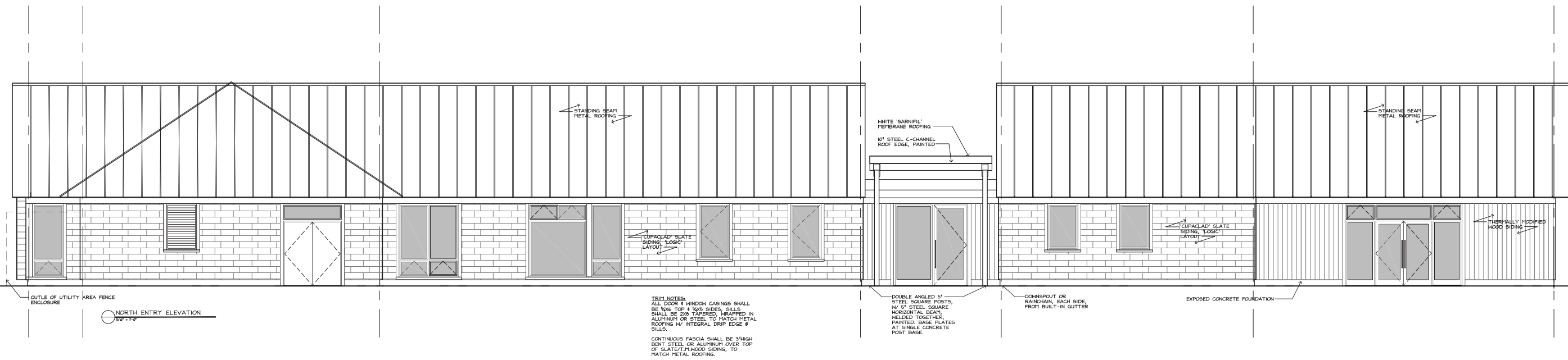
ASHUELOT STREET  
KEENE, NEW HAMPSHIRE 03431

EXTERIOR ELEVATIONS

DRAWN BY: RMF  
CHECKED BY: SKP  
DATE:  
REVISIONS:

DRAWING NO:

A2.2



THERMALLY MODIFIED WOOD SIDING



WOOD FENCE WITH STEEL VIEW WINDOWS



LOOKING NORTHEAST FROM THE CITY PARK







SLATE SIDING



THERMALLY MODIFIED WOOD SIDING



LOOKING SOUTHWEST TOWARDS THE MAIN ENTRANCE



# SVE Associates

---

Engineering \* Surveying \* Landscape Architecture \* Planning

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October 18, 2024

Mari Brunner, Senior Planner  
City of Keene, Community Development  
3 Washington Street  
Keene, NH 03431

**RE: Floodplain Development, Monadnock Conservancy Headquarters, 0 Ashuelot Street**

Dear Mari:

The Monadnock Conservancy would like to build their headquarters at 0 Ashuelot Street, on approximately 1 acre of the currently undeveloped 3.5-acre parcel owned by JRR Properties, LLC. This property is also the location of a former parking lot for overflow parking at a nearby shopping center.

Based on FIRM Panel 33005C0266E, the majority of the property is located within the 100-year floodplain associated with the Ashuelot River and the 100-year floodplain elevation for the site is 474.8. The proposed building will be constructed on fill to bring it above the 100-year floodplain elevation, with the finish floor elevation proposed at 478.0. The existing and proposed topography are shown on the attached plans. All topography is based on field survey and not the flood maps.

The existing grade of the building site for the pavilion and stage varies from approximately 473.4 to 475.2. The finished floor of the new building will be raised to be more than one foot above the 100-year floodplain elevation as required by Article 23 – Floodplain Regulations of the City of Keene Land Development Code. Since this building site is below the 100-year floodplain elevation, floodplain compensation is required for its construction.

Flood storage will not be reduced by the proposed project, as compensatory storage will be provided by removing the existing 15” diameter corrugated metal culvert that runs from the north property line in the southerly direction to approximately the center of the property and replacing it with a riparian drainage swale. The volume of flood storage compensation is only from 474.8 down to 471.8. The shallowest depth to the seasonal high-water table was located in test pit #3 at 34” below existing grade,  $474.3 - 2.8 = 471.5$ .

Wetlands exist on the adjacent property and are shown on the attached plan. No wetlands will be impacted by the proposed development, however there will be temporary wetland impacts for removing the existing 15” CMP culvert and constructing the riparian drainage swale.

If there are any questions regarding the submitted material or more information is needed, please do not hesitate to contact me at 802-257-0561 or via email at [lsargent@sveassoc.com](mailto:lsargent@sveassoc.com).

Sincerely,  
**SVE Associates**

*Liza Sargent*

Liza Sargent, PE

**Drainage Narrative**  
for the  
**Monadnock Conservancy Headquarters**

**0 Ashuelot Street  
Keene, N.H.**

**Dated: 10/14/24**

**Revised: 11/11/24**

**Prepared For:**  
Monadnock Conservancy  
P.O. Box 337  
Keene, NH 03431

**Prepared By:**  
SVE Associates  
P.O. Box 1818  
Brattleboro, VT 05302



Liza Sargent, P.E.  
PE No. 13365

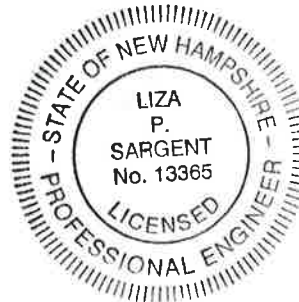
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P.O. Box 1818  
439 West River Road  
Brattleboro, VT 05302

Tel: 802.257.0561

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## **1.0 Introduction**

This Drainage Narrative documents the drainage impacts associated with the proposed development of the former parking lot off Ashuelot Street in Keene, NH. The property is currently owned by JRR Properties, LLC and contains grass lawn. JRR Properties is looking to donate a portion of the land to the Monadnock Conservancy to construct their headquarters and donate the remaining land to the City of Keene.

## **2.0 Existing Conditions**

### **2.1 Site Characteristics**

The site was previously an asphalt parking lot. In 2021, JRR Properties obtained an Alteration of Terrain permit to remove the asphalt and planted grass. It has been maintained as grass lawn since. The property primarily slopes internally, toward the existing City storm drain which runs north to south. The property abuts the City Park to the west. *Land west of River Street and around Castle Street slope towards the drainage ditch that runs generally north to south that drains to the City culvert that runs through the subject property.*

### **2.2 Soil Characteristics**

Review of the Natural Resources Conservation Service Web Soil Survey indicates the project area is Occum Fine Sandy Loam. A Site-Specific Soil Survey was performed by Christopher Guida, Certified Soil Scientist. The soils are considered to have a hydrologic classification of C “because of the suspected pre-disturbed parent materials having a range of HSG from B to D and almost the entire site being in the floodplain”.

### **2.3 Flood Plain**

Review of the Flood Insurance Rate Map (FIRM) for the City of Keene, New Hampshire, Community Panel 33005C0266E effective date: May 23, 2006, indicates the subject property falls primarily within zone AE, the 100-year floodplain, elevation 474.8.

## **2.4 Existing Hydrology**

The drainage area studied for this project is approximately 16+/- acres in size. Hydrological calculations indicate the expected runoff generated from the existing property. The majority of stormwater runs overland to the low point on the property and into the existing 15" diameter City Storm drain. A much smaller portion of the stormwater runs from the City sidewalk to the existing catch basin on Ashuelot Street.

The table below summarizes the results of the existing runoff calculations for the property. Flow is represented in cubic feet per second (CFS).

Design Storm	Existing 100R	Existing 101R
Q <sub>25</sub>	0.07	12.90

## **3.0 Developed Conditions**

### **3.1 Design Objectives**

The objective of this Stormwater Management Report is to analyze the pre- and post-development stormwater runoff conditions and impacts to downgradient properties for the proposed development of the property. There will be approximately +/-19,830 square feet of added impervious area. Stormwater generated on the developed site will be: (a) flow off the south/east side of the roof and be routed to a stormwater infiltration basin that runs around the building, (b) sheet flow from the north/west side of the roof, across the concrete sidewalks/patio to the asphalt driveway/parking lot, along the curb to the infiltration basin, (c) the driveway/sidewalk access onto Ashuelot Street will drain to the existing City catch basin, and (d) the re-graded lawn space/ flood storage compensation area will drain to where the existing City storm drain is to be removed.

### **3.2 Developed Hydrology**

The limited drainage area studied for the developed hydrology consists of the same overall area examined in the existing hydrology.

### **3.3 Summary of Post Developed Hydrology (CFS)**

Design Storm	Developed 100R	Developed 101R
Q <sub>25</sub>	0.55	11.32

Design Storm	Existing 100R	Developed 100R	Existing 101R	Developed 101R	Δ
Q <sub>25</sub>	0.07	0.55	12.90	11.32	-1.1

## **4.0 Stormwater Management Standards**

The Surface Water Drainage System is consistent with the City of Keene Land Development Regulations.

- A. There is no significant increase in runoff off the site in the post development condition.
- B. The stormwater runoff will be treated in the infiltration basin; therefore, no degradation of water quality is expected.
- C. *Stormwater runoff that drained to the 15" CMP that is to be removed and replaced with the riparian swale, headwall and 15" HDPE will maintain the existing drainage pattern.*

## **5.0 Conclusion**

This Drainage Narrative has been prepared to document the stormwater impacts associated with the development at the former parking lot, now lawn space at 0 Ashuelot Street in Keene. The calculation presented have been completed in accordance with the City of Keene Land Development Regulations. The proposed storm drainage system was designed for the 25-year design storm. The conclusion based on system design indicates the onsite runoff will be managed and there will be no adverse impacts to downstream abutters.

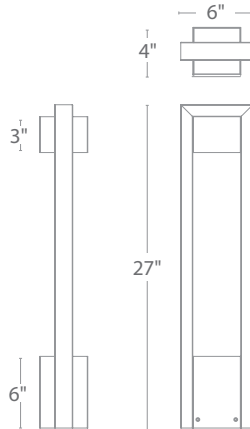


# ARCHETYPE 12V/120V/277V LED BOLLARD

6641/6642/6643

# WAC

## LANDSCAPE LIGHTING



Fixture Type:

Catalog Number:

Project: \_\_\_\_\_

Location: \_\_\_\_\_

### PRODUCT DESCRIPTION

Sleek linear design blends seamlessly into pathways while providing soft, even illumination

### FEATURES

- IP66 rated, Protected against powerful water jets
- Factory sealed water tight fixtures
- Mounting accessories included
- UL & cUL 1838 Listed (12V); 1598 Listed (120V/277V)

### SPECIFICATIONS

#### 6641

**Input:** 9-15VAC (Transformer is required)  
**Power:** 5.5W / 6.0VA  
**Brightness:** Up to 150 lm  
**CRI:** 90  
**Rated Life:** 60,000 hours  
**Dimming:** MLV dimming only

#### 6642

**Input:** 120V  
**Power:** 12.5W  
**Brightness:** Up to 390 lm  
**CRI:** 90  
**Rated Life:** 60,000 hours  
**Dimming:** TRIAC or ELV dimming

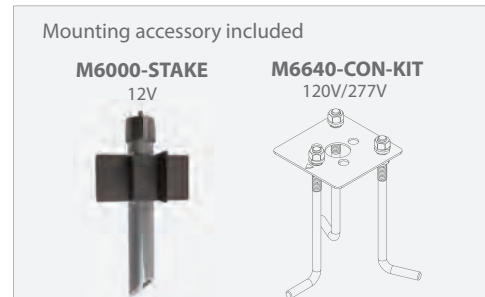
#### 6643

**Input:** 277V  
**Power:** 10.5W  
**Brightness:** Up to 430 lm  
**CRI:** 90  
**Rated Life:** 60,000 hours  
**Dimming:** ELV dimming only

Model	Color Temp	Finishes
<b>6641</b> 12V	<b>27</b> 2700K Warm White	<b>BZ</b> Bronze on Aluminum
	<b>30</b> 3000K Pure White	<b>BK</b> Black on Aluminum
<b>6642</b> 120V	<b>27</b> 2700K Warm White	<b>BZ</b> Bronze on Aluminum
	<b>30</b> 3000K Pure White	<b>BK</b> Black on Aluminum
<b>6643</b> 277V	<b>27</b> 2700K Warm White	<b>BZ</b> Bronze on Aluminum
	<b>30</b> 3000K Pure White	<b>BK</b> Black on Aluminum

**6643-\_\_\_BZ**

Example: **6643-30BZ**



wacighting.com  
 Phone (800) 526.2588  
 Fax (800) 526.2585

Headquarters/Eastern Distribution Center  
 44 Harbor Park Drive  
 Port Washington, NY 11050

Central Distribution Center  
 1600 Distribution Ct  
 Lithia Springs, GA 30122

Western Distribution Center  
 1750 Archibald Avenue  
 Ontario, CA 91760

Project		Catalog #		Type	
Prepared by		Notes		Date	



# McGraw-Edison

## GALN Galleon II

Area / Site Luminaire

### Product Features



### Product Certifications



### Interactive Menu

- Ordering Information [page 2](#)
- Mounting Details [page 3](#)
- Optical Distributions [page 5](#)
- Product Specifications [page 5](#)
- Energy and Performance Data [page 6](#)
- Control Options [page 11](#)

### Quick Facts

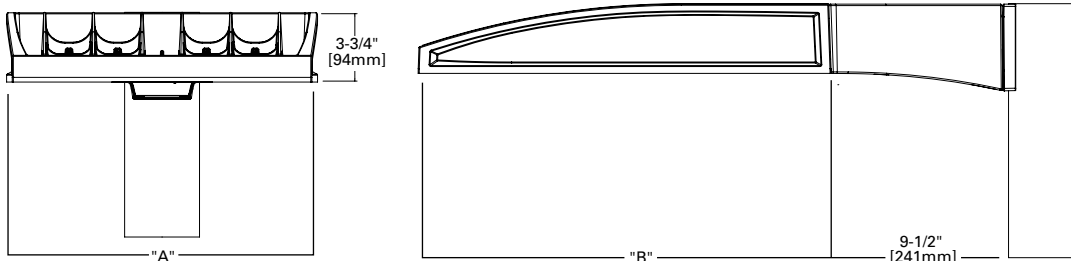
- Lumen packages range from 3,300 - 73,500 (33W - 552W)
- 17 optical distributions
- Efficacy up to 159 lumens per watt

### Connected Systems

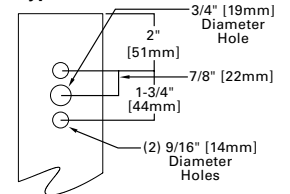
- Wavelinx LITE Wireless
- Wavelinx PRO Wireless

### Dimensional Details

#### Standard Pole Mount Arm



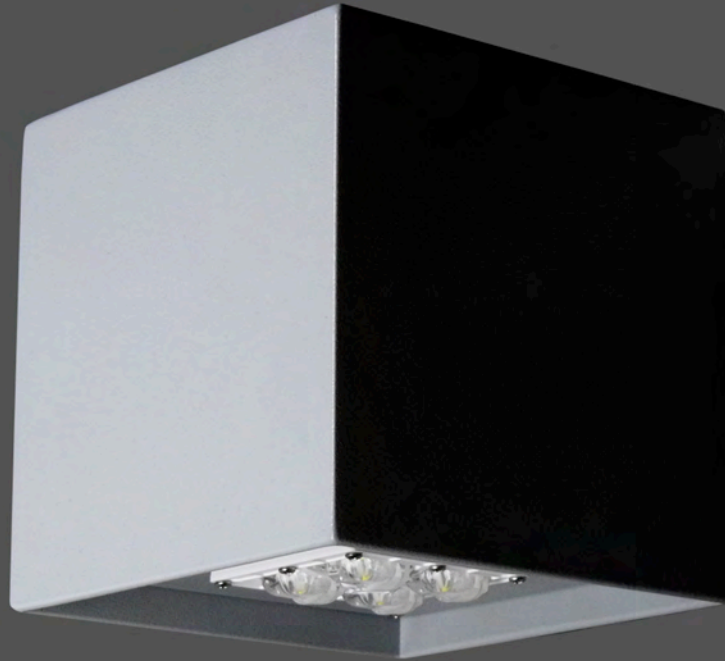
#### Pole Drilling Pattern Type "N"



Number of Light Squares	Width "A"	Housing Length "B"	Weight with Standard or QM Arm	EPA with Standard or QM Arm
1-4	16"	22"	29 lb	0.95
5-6	22"	22"	39 lb	0.95
7-9	22"	28-1/8"	48 lb	1.1

**NOTES:**  
For arm selection requirements and additional line art, see Mounting Details section.

**NOTES:**  
1. Visit <https://www.designlights.org/search/> to confirm qualification. Not all product variations are DLC qualified.  
2. IDA Certified (3000K CCT and warmer only, fixed mounting options)



LED WATTAGE CHART

	16L
175 milliamps	10w (1206-1273 Lumens)
400 milliamps	21w (2478-2911 Lumens)
530 milliamps	28w (3508-3776 Lumens)
700 milliamps	36w (4385-4720 Lumens)
1050 milliamps	56w (6022-6482 Lumens)

**Form**

- Elegant Rectilinear Extruded Aluminum Housing
- Corrosion Resistant Stainless Steel External Hardware
- Sleek, Low Profile Housing
- Spec Grade Performance
- Engineered For Optimum Thermal Management
- 8 Architectural Finishes
- Standard, RAL Colors Available

**Function**

- Micro Optics IES Distributions T2, T3, T4
- 0-10V Dimming Drivers THD @ Max Load < 15%
- Power factor @ Max Load < 0.95
- Amber, 2700K, 3000K, 3500K, 4000K, or 5000K
- 16L LED Configuration
- 10-56 Watts
- CRI 70, 80, or 90
- Extruded Aluminum Heat Sink
- 5 Mils Powder Coat

**Reliability**

- Silicone Micro Optics
- 5 Year Standard Warranty
- IP67 Optics
- Reduces Energy Consumption And Costs Up To 65%

**BUY AMERICAN**

To ensure the latest BAA/TAA/BABA Standards are being met, please select BAA, TAA, or BABA in the options section. Please contact the factory before placing an order for any NLS products requesting BAA (Buy American Act), TAA (Trade American Act), or BABA (Build America, Buy America).



# STAFF REPORT

## PB-2024-16 – SITE PLAN – KIA SITE REDEVELOPMENT, 440 WINCHESTER ST

### **Request:**

Applicant Hutter Construction Corp, on behalf of owner 434-440 Winchester LLC, proposes to construct a new ~15,365-sf Kia dealership in place of the existing ~9,950-sf building and make associated site modifications on the property at 440 Winchester St (TMP #115-004-000). A Surface Water CUP is requested for temporary impacts within the 30' wetland buffer. Waivers are requested from Article 20.14.3.D, Article 21.7.3.C, and Article 21.7.4.A.1 of the LDC to allow for parking in front of the building and higher lighting levels. The parcel is 2.23-ac in size and is located in the Commerce Limited District.

### **Background:**

The Kia Dealership at 440 Winchester St (TMP #115-004-000) is located to the south of the Route 101 & Winchester St (Route 10) intersection. Car dealerships owned by the same entity, 434-440 Winchester LLC, abut the subject parcel to the north and south. A mix of residential and commercial uses are located to the west near Wetmore St & Fairbanks St while vacant land is located to the east.

The subject parcel is 2.23-ac in size with ~134 feet of frontage along Winchester St. Access to the site is from a curb cut shared between the two properties to the south located at 446 & 452 Winchester St. The rear (eastern) portion of the site has an area of wetlands, which requires that a 30' surface water buffer be maintained in the Commerce Limited District where this parcel is located. Figure 2 shows the existing layout of the Kia site.



Figure 1. Aerial imagery from 2020 showing the parcel at 440 Winchester St.

As part of this application, the applicant proposes to demolish the existing ~9,950-sf building and construct a new ~15,365-sf building behind the existing building. Construction will be completed in phases with the new building being constructed first and the old building being demolished afterwards to allow the car dealership to remain open for as long as possible. Associated site

## STAFF REPORT

improvements will include the installation of new landscaping on the western portion of the site near Winchester St, the addition of new parking spaces in the area where the existing building is located, and the reconfiguration of the layout of existing parking spaces at the front of the site and around the perimeter of the new building.

Due to the fact that the entire site is located within the floodplain, the applicant is proposing to lower the grading of the site in the area where the existing building is located near Winchester St as well as at the rear of the site within the 30' surface water buffer. A Surface Water Conditional Use Permit (CUP) was initially submitted as part of this application; however, it was later discovered that this is unnecessary because Article 11.5.L of the Land Development Code (LDC) allows for excavation within the surface water buffer as part of the construction of compensatory flood storage subject to the issuance of a Floodplain Development Permit.

The applicant has requested waivers from Article 20.14.3.D of the LDC to allow for parking in front of the new building and Article 21.7.3.C & Article 21.7.4.A.1 of the LDC to allow for lighting levels above the required maximums.

### **Determination of Regional Impact:**

After reviewing the application, staff have made a preliminary evaluation that the proposed site plan does not appear to have the potential for "regional impact" as defined in RSA 36:55. The Board will need to make a final determination as to whether the proposal, if approved, could have the potential for regional impact.

### **Completeness:**

The applicant has requested exemptions from submitting a traffic analysis, soil analysis, historic evaluation, and screening analysis. After reviewing each request, staff have made the preliminary determination that granting the requested exemptions would have no bearing on the merits of the application and recommend that the Board accept the application as "complete."

### **Departmental Comments:**

- **Code Enforcement Staff Comments:** Building Permit & Floodplain Development Permit applications will need to be submitted for the proposed site redevelopment.
- **Zoning Staff Comments:**
  - A detailed maintenance plan for the porous pavement will need to be submitted to ensure that this area of the site does not become impervious.
  - The applicant should be prepared to discuss the proposed alternative interior landscape design that deviates from the requirements for parking lots with 50+ spaces per Article 9.4.5.B of the LDC.

**Application Analysis:** The following is a review of the Planning Board development standards relevant to this application.

**Article 21.2 – Drainage & Stormwater Management:** The project narrative states that the applicant is proposing to use the existing drainage structures on the northern portion of the site that outlet into an existing drainage ditch near the northeastern corner of the site. Note #4 on the proposed conditions plan on Sheet 3 of the plan set shows that the amount of green space on the site will



## STAFF REPORT

be increased from ~23.9% to 27.6%. An additional ~8.6% of the site will be covered with pervious asphalt, increasing the overall amount of green space on the site by ~12.3%.

During the review of this application, Zoning Staff requested that the applicant submit a detailed maintenance plan for the porous pavement proposed to be installed behind the new building on the rear portion of the site to ensure that it does not become impervious. The applicant has submitted an inspection and maintenance manual, which is included as an attachment to this staff report. The plan was reviewed by the City Engineer, who did not have any concerns. The second page of the manual recommends that inspections be performed every couple of weeks and after large storm events within the first year following construction to ensure that the site and slopes remain stabilized.

Planning Staff recommend that the Board include a condition of approval that all inspection reports be submitted to the Community Development Department for review and approval by the City Engineer's Office one year after the completion of construction. To ensure that these inspections are completed, Planning Staff also recommend that the Board require the submittal of a security to cover the cost of performing all inspections during the course of this year as a condition of approval. In addition, staff recommend that the submittal of an annual maintenance report to the Community Development Department and the submittal of an updated grading plan with a note added stating that an annual inspection and maintenance report for the porous pavement on the rear portion of the site shall be submitted to the Community Development Department be included as conditions of approval. This standard appears to be met.

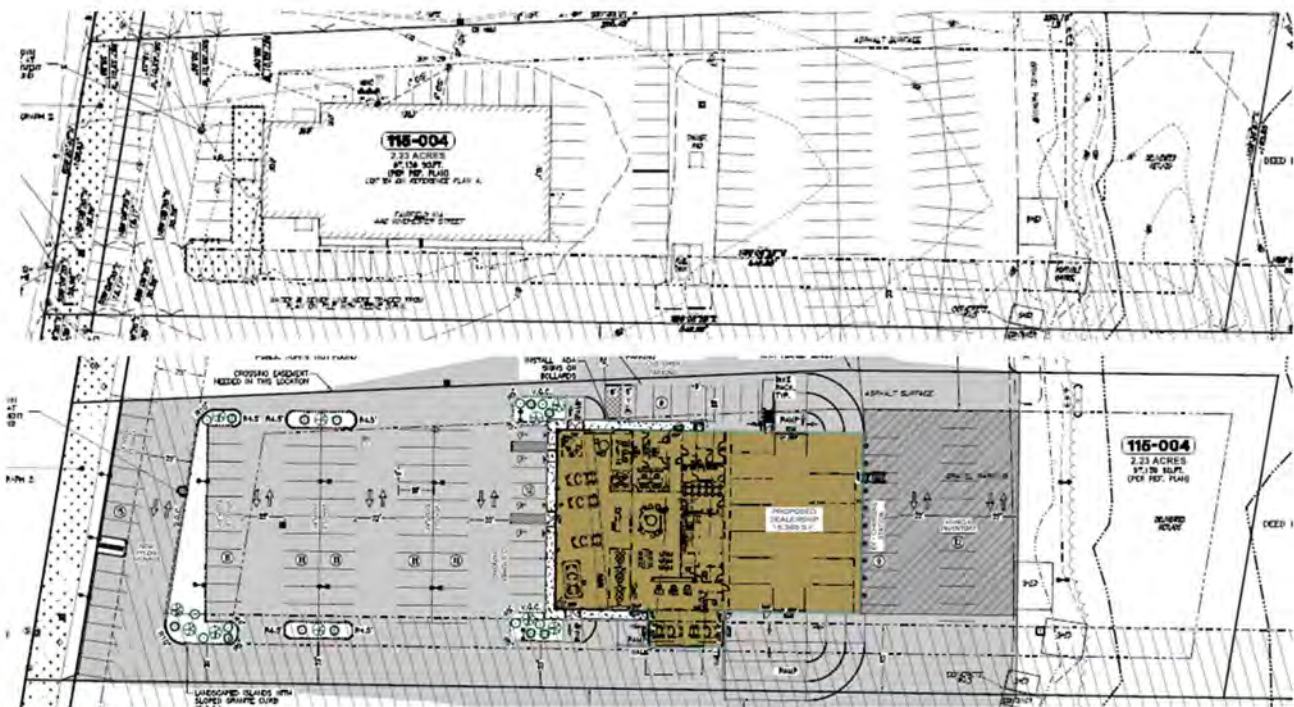


Figure 2. (Top) A snippet of the existing conditions plan for the Kia site. Figure 3. (Bottom) A snippet of the proposed conditions plan for the Kia site.

**Article 21.3 - Sediment & Erosion Control:** The project narrative states that the proposed sediment and erosion control measures include the installation of silt fencing, catch basin silt socks, and rip rap aprons, which are shown on the grading plan on Sheet 4 of the plan set. This plan shows



## STAFF REPORT

the installation of the double row of silt fencing within the 30' surface water buffer at the rear of the site, which can be seen in Figures 2 & 3. The installation of erosion control measures within the surface water buffer would normally require the submittal of a Surface Water CUP as part of the application materials; however, because this fencing is being installed to protect the adjacent wetland from excavation within the buffer related to compensatory flood storage, no CUP is required per [Section 11.5.L](#) of the LDC.

Planning Staff recommend that the Board include a condition of approval related to the flagging and inspection of the 30' buffer by Community Development Staff prior to the commencement of site work. Additionally, the submittal of a security for the installation and maintenance of the erosion control measures during construction is also recommended to be included as a condition of approval. This standard appears to be met.

**Article 21.4 - Snow Storage & Removal:** The proposed conditions plan on Sheet 3 of the plan set shows a snow storage area near the northeastern corner of the parcel and states that snow will not be stored within the wetlands or surface water buffer and will be hauled off the site, if needed. This standard appears to be met.

**Article 21.5 - Landscaping:** The landscaping plan on Sheet 7 of the plan set shows the installation of rhododendron, winterberry, and nannyberry viburnum shrubs in landscaping islands on the western portion of the site near Winchester St. Article 9.4 of the LDC outlines the design standards for perimeter and internal parking lot landscaping areas and typically requires that trees and other shrubs are installed on site to screen the parking lot from view of adjacent properties and the public right-of-way. Article 9.4.4.A.6 & Article 9.4.5.B.5 of this article grant the Planning Board the authority to approve an alternative landscaping plan, if they feel that the proposed design generally meets the intent of Article 9.4.

The project narrative states that the applicant does not want trees installed near the building due to leaves and birds causing issues with new car finishes. The Board will need to determine if the landscaping shown on Sheet 7 of the plan set meets the intent of the parking lot landscaping requirements outlined under Article 9.4 of the LDC. Planning Staff recommend that the Board include conditions of approval related to the submittal of a security to cover the cost of the landscaping and its installation as well as the performance of an initial inspection following the installation of landscaping and a final landscaping after 1 growing season (a minimum of 1 year).

**Article 21.6 - Screening:** The project narrative states that the mechanical equipment will be set back 10' from the edge of the roof and that the proposed building design incorporates a roof parapet that will range from ~8" tall at the front of the building to ~40" tall at the rear of the building as shown in the elevations included as an attachment to this staff report. This standard appears to be met.

**Article 21.7 - Lighting:** The photometric plan on Sheet 6 of the plan set shows that the applicant is proposing to install a total of thirteen, 20'-tall full cut-off pole light fixtures with a color temperature of 3,000K and a CRI of 80. An additional 8 full cut-off wall pack light fixtures with a color temperature of 3,000K and a CRI of 80 will be installed at a maximum height of 14' around the perimeter of the new building. Note #6 on the photometric plan states that the average illumination levels will not exceed an average of 1-fc between the hours of 10:00 pm and 6:00 am.

## STAFF REPORT

The applicant has requested a waiver from Article 21.7.3.C of the LDC to allow for lighting levels that exceed 0.1-fc at the property line and 1-fc at the right-of-way line. An additional waiver from Article 21.7.4.A.1 of the LDC has also been requested to allow for an average lighting level above 3.5-fc in the display parking spaces at the front of the site adjacent to Winchester St. The written waiver requests are included in the project narrative that was submitted by the applicant. In determining whether or not to grant the requested waivers, the Board will need to review the waiver criteria outlined under Article 26.12.14.A of the LDC, which are included below.

### **Article 26.12.14 - Waiver Criteria:**

- A. *"Unless otherwise set forth in this LDC, the Planning Board may grant a waiver from strict compliance with provisions of the Site Development Standards in Article 21 or site plan review standards in Section 26.12, on a case by case basis, so long as the Board finds, by majority vote, that:*
- 1. Strict conformity would pose an unnecessary hardship to the applicant and the waiver would not be contrary to the spirit and intent of the regulations; or,*
  - 2. Specific circumstances relative to the site plan, or conditions of the land in such site plan, indicate that the waiver will properly carry out the spirit and intent of the regulations.*
  - 3. In granting a waiver, the Planning Board may require any mitigation that is reasonable and necessary to ensure that the spirit and intent of the standard being waived will be preserved, and to ensure that no increase in adverse impacts associated with granting the waiver will occur."*

**Article 21.8 - Sewer & Water:** The City's GIS Database shows that the site is currently served by City water and sewer utilities. The project narrative states that the new building will require extensions of the water and sewer lines on the site, which will be coordinated during the demolition process of the existing building. This standard appears to be met.

**Article 21.9 - Traffic & Access Management:** The project narrative states that there are no new permanent curb cuts proposed as part of this project and that there will be no increase in traffic generation expected as a result of the project. The grading plan shows the construction of a temporary stabilized construction entrance/exit onto the site from Winchester St. Engineering Staff reviewed the proposed installation of this temporary curb cut as part of this application and had no comments. As mentioned previously in this staff report, Planning Staff recommend including a condition of approval related to the submittal of security for the installation and maintenance of sediment and erosion control measures.

The project narrative states that the construction process will be phased so that the new building will first be constructed behind the existing building and then the old dealership will be demolished at the front of the site. This will allow the car dealership to remain open for as long as possible during the construction process. In order to ensure that the site will maintain safe and adequate access during the construction process, Planning Staff recommend including a condition of approval related to the submittal of a construction phasing plan to the Community Development Department for review and approval by City Staff prior to the issuance of a building permit. Additionally, it is recommended that a condition of approval related to holding a pre-construction meeting with City Staff prior to the issuance of a building permit also be included.

## STAFF REPORT

In addition, the applicant is proposing to add parking spaces in the footprint of the existing building and reconfigure the layout of existing spaces along the northern and eastern (rear) sides of the building. Given that access to the rear of the site will require that vehicles travel over the adjacent parcels to the north and south, which are currently owned by the same entity, 434-440 Winchester LLC, Planning Staff recommend that the Board include conditions of approval related to the submittal of draft access easement language as well as any other necessary easements or required legal instruments to the Community Development Department for review by the City Attorney's Office. This standard appears to be met.

**Article 21.10 - Filling & Excavation:** The project narrative states that the site falls entirely within the 100-year floodplain, which will require that the site be regraded and fill be added to certain areas. The area of the existing building and a section of the rear of the site will be lowered in grade to provide flood compensation. The proposed conditions plan shows the installation of double silt fencing in a north-south orientation within the 30' surface water buffer at the rear of the site. The applicant is aware that a Floodplain Permit will need to be obtained as part of this project. Planning Staff recommend including a condition of approval related to the submittal of an approved Floodplain Development Permit.

**Article 21.11 - Surface Waters & Wetlands:** As mentioned previously, the proposed conditions plan shows the location of wetlands and the 30' surface water buffer on the rear (easternmost) portion of the site. Other than the installation of silt fencing within the 30' buffer, all other site work appears to be taking place outside of the wetlands buffer. Planning Staff recommend that a condition of approval be included related to the flagging and inspection of the 30' surface water buffer prior to the commencement of site work. This standard appears to be met.

**Article 21.12 - Hazardous & Toxic Materials:** The project narrative states that items considered toxic, including oils, lubricants, brake fluid, etc., will be stored inside the building and handled according to state regulations. This standard appears to be met.

**Article 21.13 - Noise:** The narrative states that there will not be no difference in the amount of noise generated on the site. This standard appears to be met.



*Figure 4. Snippets of the proposed renderings for the Kia primary façade.*

**Article 21.14 - Architecture & Visual Appearance:** The applicant submitted renderings and elevations showing the proposed design for the building exterior, which will feature metal paneling in a gray, black, silver, and white color scheme with glass accents. The project narrative includes a description of the building's design and how the architect feels that it complies with

## STAFF REPORT

the standards. In this section of the narrative, the architect states that a breakup of the massing of the north building façade, which is over 150'-long, will be achieved using a change in materials.

Similarly, a breakup in the massing of the south façade will be achieved with a building projection and material change. The west façade where the main entrance will be located will have a framed portal that will bring the curtainwall massing down to a pedestrian scale and provide visual interest. This section of the narrative also states that the "façade material palette" was dictated by Kia. In reviewing this application, the Board will need to make a determination as to whether or not the proposed building design complies with the standards outlined under this section of the code.

In addition to the exterior building design, the applicant is requesting a waiver from Article 20.14.3.D of the LDC to allow for the creation of additional parking at the front of the new building. In making a determination as to whether or not to grant the requested waiver, the Board will need to evaluate the criteria included previously in this staff report.

### **Recommended Motion:**

If the Board is inclined to approve this request, the following motion is recommended:

***"Approve PB-2024-16 as shown on the plan set identified as "Site Redevelopment Plans, Fairfield Kia of Keene" prepared by Fieldstone Land Consultants PLLC, at varying scales on October 18, 2024 and last revised on November 11, 2024 with the following conditions:***

- 1. *Prior to final approval and signature of the plans by the Planning Board Chair, the following conditions precedent shall be met:***
  - a. Owner's signature appears on the title page and proposed conditions plan.***
  - b. Submittal of five paper copies and a digital copy of the final plan set and elevations.***
  - c. Submittal of a security to cover the cost of sediment and erosion control measures, landscaping, drainage inspections, and as-built plans in a form and amount acceptable to the Community Development Director and City Engineer.***
  - d. Submittal of an updated grading plan with a note added stating that annual drainage inspections shall be performed, and documentation of such shall be submitted to the Community Development Department.***
  - e. Submittal of draft easement language and any other legal instruments required for this application to the Community Development Department for review by the City Attorney's Office.***
- 2. *Subsequent to final approval and signature of the plans, the following conditions shall be met:***
  - a. Prior to the issuance of a building permit, the applicant shall:***
    - i. Schedule a pre-construction meeting with City Staff to review the project.***
    - ii. Submit a construction phasing plan showing the proposed traffic, access, and safety measures that will be put in place during the project for review and approval by City Staff.***

## STAFF REPORT

- b. Prior to the commencement of site work, the applicant shall:**
  - i. Submit an approved Floodplain Development Permit.**
  - ii. Flag the 30' surface water buffer and contact Planning staff to inspect to ensure compliance with the approved site plan and all City of Keene regulations.**
- c. Prior to the issuance of a Certificate of Occupancy for the new building, the applicant shall submit recorded copies of all necessary easements to the Community Development Department.**
- d. One year after the completion of construction, copies of all drainage reports and inspection materials shall be submitted to the Community Development Department.**
- e. Annual maintenance inspections shall be performed, and documentation of such shall be submitted to the Community Development Department.**
- f. After landscaping has been installed, the applicant shall schedule an initial landscaping inspection with Community Development Staff.**
- g. After 1 full growing season (a minimum of 1 year), Community Development Staff shall be contacted to schedule a final landscaping inspection."**



# City of Keene, NH

## Site Plan Application

If you have questions about how to complete this form, please call: (603) 352-5440 or email: [communitydevelopment@keenenh.gov](mailto:communitydevelopment@keenenh.gov)

### SECTION 1: PROJECT INFORMATION

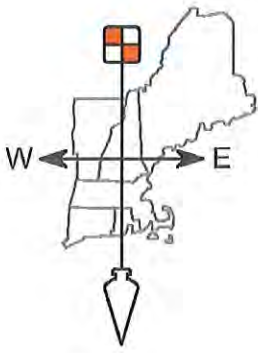
<b>PROJECT NAME:</b> FAIRFIELD'S KIA REDEVELOPMENT		<b>TYPE OF APPLICATION BEING SUBMITTED:</b> <input checked="" type="checkbox"/> MAJOR PROJECT APPLICATION <input type="checkbox"/> MINOR PROJECT APPLICATION
<b>PROJECT ADDRESS(ES):</b> 440 WINCHESTER ST., KEENE, NH 03431		
<b>EXISTING OR PREVIOUS USE:</b> EXISTING KIA CAR DEALERSHIP	<b>PROPOSED USE:</b> KIA CAR DEALERSHIP	
<b>GROSS FLOOR AREA OF NEW CONSTRUCTION</b> (in square feet) 15,365 S.F.	<b>GROSS FLOOR AREA OF EXISTING BUILDINGS/STRUCTURES</b> (in square feet) 9,950 S.F.	
<b>AREA OF PROPOSED NEW IMPERVIOUS SURFACES</b> (in square feet) 66,197 S.F.	<b>TOTAL AREA OF LAND DISTURBANCE</b> (in square feet) 99,000 S.F.	

### SECTION 2: CONTACT INFORMATION

PROPERTY OWNER	APPLICANT
<b>NAME/COMPANY:</b> 434-440 WINCHESTER, LLC	<b>NAME/COMPANY:</b> HUTTER CONSTRUCTION CORP.
<b>MAILING ADDRESS:</b> P.O. BOX 684, KEENE, NH 03431	<b>MAILING ADDRESS:</b> P.O. BOX 257, 810 TURNPIKE ROAD, NEW IPSWICH, NH 03071
<b>PHONE:</b> 603-352-5900	<b>PHONE:</b> 603-878-2300
<b>EMAIL:</b> cmccauley@brattautomall.com	<b>EMAIL:</b> nt@hutterconstruction.com
<b>SIGNATURE:</b> <i>John Noonan SEE LOA</i>	<b>SIGNATURE:</b> <i>John Noonan SEE LOA</i>
<b>PRINTED NAME:</b> CHRISTIAN MCCAULEY	<b>PRINTED NAME:</b> NELS TRAFFIE

AUTHORIZED AGENT (if different than Owner/Applicant)	FOR OFFICE USE ONLY:	
<b>NAME/COMPANY:</b> FIELDSTONE LAND CONSULTANTS, PLLC	<b>TAX MAP PARCEL #(s):</b> 115-004000000000	
<b>MAILING ADDRESS:</b> 45 ROXBURY ST., KEENE, NH 03431	-----	
<b>PHONE:</b> 603-672-5456	<b>PARCEL SIZE:</b> 2.23ac	<b>DATE STAMP:</b>
<b>EMAIL:</b> jenoonan@fieldstonelandconsultants.com	<b>ZONING DISTRICT:</b> Commerce Limited	
<b>SIGNATURE:</b> <i>John Noonan</i>	<b>PROJECT #:</b> PB-2024-16	
<b>PRINTED NAME:</b> JOHN NOONAN		By _____





# FIELDSTONE

LAND CONSULTANTS, PLLC

Surveying ♦ Engineering  
Land Planning ♦ Septic Designs

206 Elm Street, Milford, NH 03055 - Phone: 603-672-5456 - Fax: 603-413-5456  
www.FieldstoneLandConsultants.com

## Site Plan Narrative

Fairfield's KIA  
Building and Site Renovation  
Tax Map Parcel 115-004  
440 Winchester Street  
Keene, New Hampshire

November 11, 2024

### **Project Narrative:**

Fieldstone Land Consultants, on behalf of Fairfield's Kia, is submitting this narrative as part of the Site Plan Review submission. The applicant wishes to raze the existing dealership building and construct a new building directly behind the existing building. The parking lot will be reconfigured to accommodate the new building layout. The plan is to keep the existing dealership open during the majority of the construction phase of the new building.

The site is located at 440 Winchester Street on Tax Map Parcel 115-004-000-000-000, and is owned by 434-440 Winchester LLC, and operated as the Fairfield Kia dealership. The parcel is 2.23 acres in size with 133.9 feet of frontage on Winchester Street and bordered by Fairfield GMC/Cadillac dealership to the north and Keene Mitsubishi dealership to the south; both abutting properties owned by the same owner.

The property is located within the 100-year flood plain, Zone AE, as is common in this section of Keene. The flooding source is the backwater condition of the Ashuelot River and water from the City tax ditch system that connects to Ash Swamp Brook. The proposed building and parking lot will encroach the 100-year floodplain and require a Floodplain Development Permit along with FEMA approval. Any fill that is proposed will be offset by a flood compensation grading that will be based on total volume calculations. The finish floor elevations of the new building will be a minimum of 1 foot above the 100 Year floodplain elevation (100 Year Flood Elevation = 471.2'; Proposed Finish Floor Elevation = 472.2'). The flood compensation will be spread out over the site with a lowering of the grades where the existing building sits, along with lowering grades to the rear of the site.

The proposed improvements will fall within the existing footprint of the dealership's land disturbance with impervious areas being very similar in the pre and post development conditions. The access for this business will remain the same, where an easement is provided along the front of the lot for an access route to the site. The site does not have a curb cut onto Winchester Street/NH Route 10, and relies on the existing crossing easement on the properties to the north and south. The existing parking spaces at the west and north side of the business will be formalized and reconstructed, as many parking spaces are currently un-striped spaces. Access to the rear of the building will remain

unchanged by using the drive aisles between each dealership.

The wetland to rear of the property appears to have been graded as a flood compensation basin or stormwater basin many years ago. There are no proposed impacts to this jurisdictional wetland. There are impacts to the buffer of the surface water and therefore a Conditional Use Permit application has been provided.

Below is an outline of the Site Development Standards on the application and how each is addressed.

**21.2 Drainage & Stormwater Management:**

The stormwater is managed by utilizing the existing drainage structures on the north side of the site which outlet to drainage ditch at the rear of the property. As the impervious areas are almost identical in the pre and post conditions, we have proposed an area in the rear parking as porous asphalt. The porous asphalt serves two purposes; 1. It will reduce the overall impervious to be under the new threshold in the LDC. 2. The porous asphalt will act as a stormwater detention and treatment system. There will be an underdrain to take treated water from under the pavement section and outlet to the drainage ditch to the north.

**21.3 Sediment and Erosion Control:**

Sediment and erosion control measures are outlined and detailed on the site plans. There will be a combination of silt fence, catch basin silt socks, and rip rap apron.

**21.4 Snow Storage & Removal**

The site will use small areas at the rear of the lot for snow storage and excess snow removed as needed. The methods used will match the existing process for snow management. The two adjacent properties are owned by the same company and will utilize snow storage areas across the properties and be hauled off site during excessive storm events.

**21.5 Landscaping**

The site will have landscaping installed per the landscaping plan. The applicant does not want trees near the building due to leaves and birds causing issues with new car finishes. We are requesting the board to approve an alternative landscaping plan based on the unique use of the property.

**21.6 Screening**

There is a dumpster enclosure to the rear of the site. This location is not visible from the public way and the dumpster will be enclosed in a fence enclosure to keep the area looking neat. HVAC equipment is shown on the architectural plans and more than 10 feet from the edge of the roof edge.

**21.7 Lighting**

The lighting is shown on the lighting plan and meets the City standards. Wall mounted lights will be used at building doorways and pole mounted lights in the parking lot. All fixtures are energy efficient LEDs that are full-cutoff. It is noted for hours of operation and 50% of the lights will be on motion sensors to provide security lighting. The lights will also reduce to 50% operation after the business hours.

**21.8 Sewer & Water**

The site is currently serviced by municipal water and sewer. The new building will require extensions of the water and sewer lines on the site. This will be coordinated during the demolition process of the existing building.

**21.9 Traffic & Access Management**

All traffic and access to the site will continue to be from the dealerships to the north and south. There are no proposed new curb cuts. No change from the original approved site plan. We request an Exemption from submitting a formal Traffic Analysis, as there are slightly less parking spaces when

comparing the submitted formal parking spaces and the informal total parked cars at the dealership.

**21.10 Filling & Excavation**

This site falls within the 100 year floodplain and will require fill within the floodplain. The fill will be offset by excavation to provide compensatory storage on site for the floodwaters. The area of the existing building will be lowered in elevation to provide compensation, along with lowering the grades to the rear parking lot.

**21.11 Surface Water & Wetlands**

The jurisdictional wetlands were delineated by the wetland scientist recently on 10/11/2024. The lower grades that define the wetland appear to be a historic flood compensation basin. There are no proposed impacts to this wetland. There will be double rows of silt fence installed adjacent to the wetland area. The paving and grading extents were revised to limit impacts to the resource buffer and ensure no trees were removed within the buffer (this eliminated 12 parking spaces).

**21.12 Hazardous & Toxic Materials**

The items considered toxic would be stored inside the building and handled according to state regulations; these items would include automotive fluids for the service bay (oils, lubricants, brake fluid, etc.).

**21.13 Noise**

There would not be a difference in the noise levels generated by the proposed dealership versus the existing dealership. The service bays are all inside the building, located at the rear of the building.

**21.14 Architecture & Visual Appearance**

The new building will be a modern car dealership that sits back further from the street. The architect has provided the description below in the visual appearance:

- The overall height, placement, materials and massing of the proposed structure is similar to the adjacent GMC dealership.
- While the building is a little over 150'-0" long there is a material change on the North Façade which limits the single wall material to 140'-0" and the South Façade has a building projection along with a material change.
- The main entrance has a framed ACM portal which brings the curtainwall massing down to a pedestrian scale while providing visual cues for the entrance.
- The glass curtain wall and ACM cornice along the Front / West Façade are an architectural feature and provide visual interest.
- The main façade material palette is dictated by Kia which entails glass curtainwall and ACM accents, the balance of the building is a flat metal panel similar to the GMC dealership. The ACM and metal panels have low gloss finish, and the glass curtainwall uses standard glazing.
- Metal panel samples have been provided with the application.

**Waiver Request 1 (Parking in Front of Building):**

**§ 20.14.3.D Site Design and Relationship to Surrounding Community:** *States All required off-street parking shall be to the side or rear of buildings...* We are requesting the waiver from the LDC Section 20.14.3.D for the requirement that parking be located to the side or rear of a building. The existing lot has frontage on Winchester Street (aka NH Route 10) and currently has parking in front of the building. The use as a car dealership will not change and this business relies on visibility of the vehicles for sale. The parking required for the business per LDC is 22 spaces and 41 spaces are provided to the side and rear of the proposed building.

*A. Specific circumstances relative to the subdivision, or conditions of the land in such subdivision, indicate that the waiver will properly carry out the spirit and intent of the regulations; The spirit and intent of the ordinance will be met as the proposed parking will be similar to other car dealerships in the area and the current business has parking in front of the building.*

*B. Granting the waiver will not increase the potential for creating adverse impacts to abutters, the community or the environment; The granting of this waiver will not create any adverse impacts to the abutters, community or environment. The parking will be located to accommodate the unique construction sequence and allow for floodplain compensatory storage to be met. The owner needs the existing business to remain open during construction for the greatest extent possible. This means the new building will be constructed behind the existing building. Once the building are rear parking lot are complete, the existing building will be demolished. The front parking lot will be lower in grade to allow for flood compensation and collection of stormwater runoff.*

*C. Consideration will also be given as to whether strict conformity with the regulations would pose an unnecessary hardship to the applicant; If the regulation was enforced with strict conformity, the applicant would have an unnecessary hardship placed upon them. The hardship would be in the form of re-design, having to close the business during construction in order to construct the building in the front of the lot, and the business would be hindered by the parking lot behind the building. The business model for a car dealership relies on the vehicles for sale being on display and visible to the public as they pass by. We believe that the strict conformity of the regulation would inflict unnecessary hardships on the business and the regulation does not fit with this business use. The section of Winchester Street has historically been used for car dealerships with parking in front of the buildings, and the proposed use would continue that historic use.*

**Waiver Request 2 (Lighting Levels above 0.1 Fc at Property Line):**

**§ 21.7.3.C Lighting:** *States The maximum level of any light fixture cannot exceed 0.1-footcandle measured at the property line and cannot exceed 1-Fc measured at the right-of-way: We are requesting the waiver from the LDC Section 21.7.3.C to allow for lighting levels above 0.1 Fc at the property line and 1-Fc at the right-of-way line.*

*A. Specific circumstances relative to the subdivision, or conditions of the land in such subdivision, indicate that the waiver will properly carry out the spirit and intent of the regulations; The spirit and intent of the ordinance will be met as the proposed parking lot illumination will be similar to the existing condition and will not impact the public. The property line that has an excess of 0.1-Fc is a shared property line with the same owner as the subject lot, with the same use (car dealership).*

*B. Granting the waiver will not increase the potential for creating adverse impacts to abutters, the community or the environment; The granting of this waiver will not create any adverse impacts to the abutters, community or environment. The abutting property owner is the same owner as the subject lot and has the same business use (car dealership). The increase of light at the right-of-way will be onto Winchester Street/NH Route 10, where other street lighting exists, as was not modeled in this site design. It is likely that the lighting on NH Route 10 already exceeds 1.0 Fc and this project's lighting would not change the existing condition of the right-of-way lighting. The existing lights in the right-of-way to the north and south properties would remain unchanged with the proposed plan.*

*C. Consideration will also be given as to whether strict conformity with the regulations would pose an unnecessary hardship to the applicant; If the regulation was enforced with strict conformity, the applicant would have an unnecessary hardship placed upon them. The hardship would be in the form of maintaining the existing fixtures in the right-of-way in front of the business and reducing the lighting along the north side of the building. The design does not impact the welfare or safety of the public, and therefore placing a hardship on the applicant would have no practical gain for the public.*

**Waiver Request 3 (Average Parking Lot Lighting Levels above 3.5 Fc):**

**§ 21.7.4.A.1 Lighting, Parking Lots:** *States Average illumination levels of parking lot lighting shall not exceed 3.5 footcandles:* We are requesting the waiver from the LDC Section 21.7.4.A.1 to allow for lighting levels above 3.5-Fc in the parking lots to allow for greater visibility of the cars on display.

*A. Specific circumstances relative to the subdivision, or conditions of the land in such subdivision, indicate that the waiver will properly carry out the spirit and intent of the regulations; The spirit and intent of the ordinance will be met as the proposed parking lot will have higher illumination levels that are common for the automobile dealerships. The lighting would benefit the business by showcasing cars and allowing for potential customers to look at vehicles past daylight hours. The type of business is a specific circumstance that would benefit from allowing lighting levels to exceed the 3.5-Fc limit.*

*B. Granting the waiver will not increase the potential for creating adverse impacts to abutters, the community or the environment; The granting of this waiver will not create any adverse impacts to the abutters, community or environment. The lighting in the parking lot will only be slightly over the limit and will benefit the abutters, whom are the same owner as the subject lot. The car dealerships, community, or environment in this area will not have any adverse impacts due to the proposed lighting.*

*C. Consideration will also be given as to whether strict conformity with the regulations would pose an unnecessary hardship to the applicant; If the regulation was enforced with strict conformity, the applicant would have an unnecessary hardship placed upon them. The hardship would be in the form of re-design, and having less than ideal lighting for the business use, making vehicles for sale less visible to potential customers. As there are no impacts to the public, environment and the direct abutters are also car dealerships, we do not believe that strict conformity with the regulation is warranted.*

**Surface Water Conditional Use Permit 11.6.1**

We are requesting a Surface Water Conditional Use Permit to allow for the flood compensation grading within and outlet of stormwater culvert within the wetland buffer. There are no proposed impacts to trees or the wetland resource itself.

Below is an outline of the LDC permit standards for a Conditional Use Permit (CUP), followed by the permit criteria and how each item has been addressed.

**LDC Section 11.6.2: Conditional Use Permit Standards:**

**A. The proposed use cannot be located in a manner to avoid encroachment:**

The existing limit of pavement has been held in the proposed site plans; however, the grades will be lowered to provide flood storage compensation. The grading proposed will not impact any of the existing trees and will only impact the wetland buffer that is currently gravel parking spaces. The proposed underdrain has been located to outlet at the same location as an existing 18-inch culvert, thus reducing the impact to the buffer at the greatest extent possible.

**B. Encroachment into the buffer area has been minimized:** As mentioned above, the impact will to the buffer will be for flood compensation grading and installation of an underdrain culvert. These items have been revised to minimize the impact to the buffer and not impact the jurisdictional wetland resource.

**C. The nature, design, siting, and scale of the proposed use will avoid the potential for adverse impacts to the surface water resource:** The pavement to the rear of the site will be porous asphalt to serve as a stormwater management system and reduce impervious coverage. The nature of this pavement is to reduce runoff from the paved surfaces and reduce stormwater velocities. Currently this same area is standard pavement, so the proposed design will avoid adverse impacts to the wetland resource. It should be noted that the wetland resource is likely an original excavation done for stormwater and floodplain mitigation on the site. There will be double rows of silt fence to further protect the resource and ensure the vegetated buffer is not impacted during construction.

**D. The surface water buffer area shall be left in a natural state:** The encroachment area will be temporarily impacted by installing the underdrain outlet and by lowering grades adjacent to the existing tree-line. The disturbed areas will be restored with a conservation seed mix and allowed to return to the natural state.

**E. The Planning Board may consider the following:**

- 1. The size, character, and quality of the surface water being encroached upon:** The area of the outlet of the underdrain is clearly a manmade ditchline for draining the property. The wetland area in the middle of the lot appears to be originally dug out for stormwater and/or floodplain compensation measures. The resource will not be impacted and only the buffer will be temporarily impacted.
- 2. The location and connectivity of the surface water in relation to other surface waters:** The drainage ditch ultimately connects to the ditches that run along Cromwell Drive and go under Krif Road to outlet into Ash Swamp Brook. The wetland resource at the rear of the lot is lower in grade and does not have an outlet; it is an isolated ponding area. During large flooding events this area would be connected to the backwater flooding conditions of the Ashuelot River.
- 3. The nature of the ecological and hydrological functions served by the surface water:** The functions of the resource will not change from the existing condition.
- 4. The nature of the topography, slopes, soils and vegetation in the buffer:** The slopes, soils and vegetation within the buffer area will not change. The grades will be slightly lower than the existing condition for flood compensation, however, the topography will be very similar to the existing condition.
- 5. The role of the surface water buffer in mitigating soil erosion, sediment and nutrient transport, groundwater recharge, flood storage and flow dispersion:** The porous asphalt drainage system will support groundwater recharge and reduce stormwater runoff from the parking lot. Soil erosion, sediment and nutrient transport will be reduced from the current scenario. The lower grades across the site will provide flood storage on site.
- 6. The extent to which the surface water buffer serves as wildlife habitat or travel corridor:**



The buffer area is directly adjacent to a parking lot and the trees will not be removed. There will not be any adverse impacts or changes to wildlife habitat or travel routes.

7. **The rate, timing and volume of stormwater runoff and its potential to influence water quality:** The rate, timing and volume of stormwater will be mitigated by the use of deep sump catch basins, oil/debris hoods, and the porous asphalt pavement. The overall lot coverage is approximately equal in the existing and proposed scenarios, with impervious coverage being reduced in the proposed condition.
8. **The sensitivity of the surface water and the surface water buffer to disruption from changes in grade, or plant and animal habitat in the buffer zone:** The grades will be slightly lower within the buffer, however, the existing tree-line is being maintained. The temporary impacts for grading and the culvert installation will not disrupt the animal habitat in the buffer zone. The area to be regraded within the buffer will be seeded with a conservation seed mix.

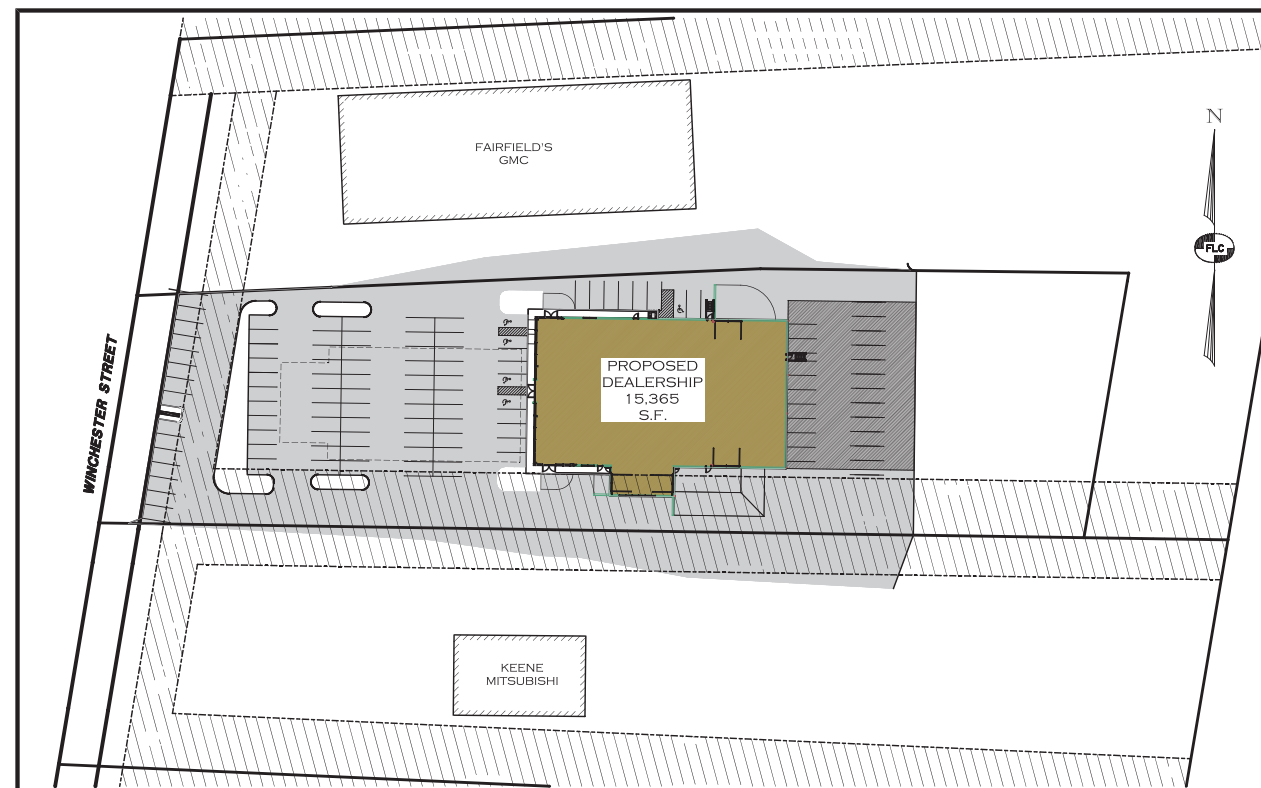
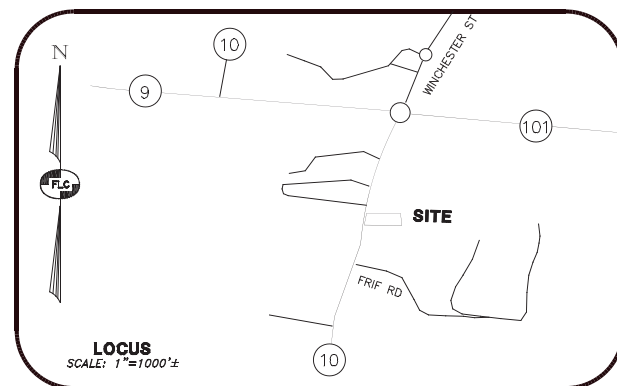
# SITE REDEVELOPMENT PLANS

## *FAIRFILED KIA OF KEENE*

- TAX MAP 115, LOT 004 -  
 (440 WINCHESTER STREET)  
 KEENE, NEW HAMPSHIRE

OCTOBER 18, 2024

LAST REVISED: NOVEMBER 11, 2024



SCALE: 1" = 60'

SHEET INDEX		
PAGE	SHEET	TITLE
1	CV-1	COVER SHEET
2	EX-1	EXISTING CONDITIONS PLAN
3	SP-1	SITE PLAN
4	GR-1	GRADING & DRAINAGE PLAN
5	UT-1	UTILITY PLAN
6	LT-1	LIGHTING PLAN
7	LS-1	LANDSCAPING PLAN
8	DT-1	EROSION CONTROL DETAILS
9	DT-2	CONSTRUCTION DETAILS
10	DT-3	CONSTRUCTION DETAILS
11	DT-4	CONSTRUCTION DETAILS (SEWER)

**PREPARED FOR:**  
**HUTTER CONSTRUCTION CORP.**  
 P.O. BOX 257, NEW IPSWICH, NH 03071

**LAND OF:**  
**434-440 WINCHSTER STREET**  
 P.O. BOX 684, KEENE, NH 03431

1. THE LOCATION OF THE UTILITIES SHOWN ARE APPROXIMATE. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO LOCATE AND PRESERVE ALL UTILITY SERVICES.

2. THE CONTRACTOR IS RESPONSIBLE FOR CONTACTING AND COORDINATING WITH ALL JURISDICTIONAL AGENCIES AND UTILITY COMPANIES PRIOR TO AND DURING CONSTRUCTION.

3. THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND PROPOSED WORK PRIOR TO CONSTRUCTION.

CONTACT DIG SAFE 72 HOURS PRIOR TO CONSTRUCTION  
**DIGSAFE.COM**  
 OR DIAL 811  
 KNOW WHAT'S BELOW

Surveying ♦ Engineering ♦ Land Planning ♦ Permitting ♦ Septic Designs

FIELDSTONE

LAND CONSULTANTS, PLLC

206 Elm Street, Milford, NH 03055  
 Phone: (603)-672-5456 Fax: (603)-413-5456  
 www.FieldstoneLandConsultants.com

LAND-OWNER SIGNATURE

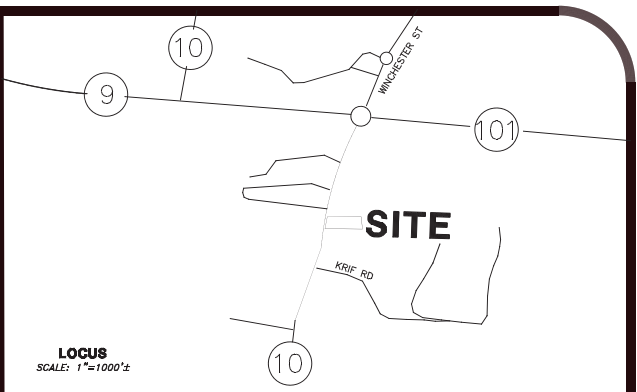
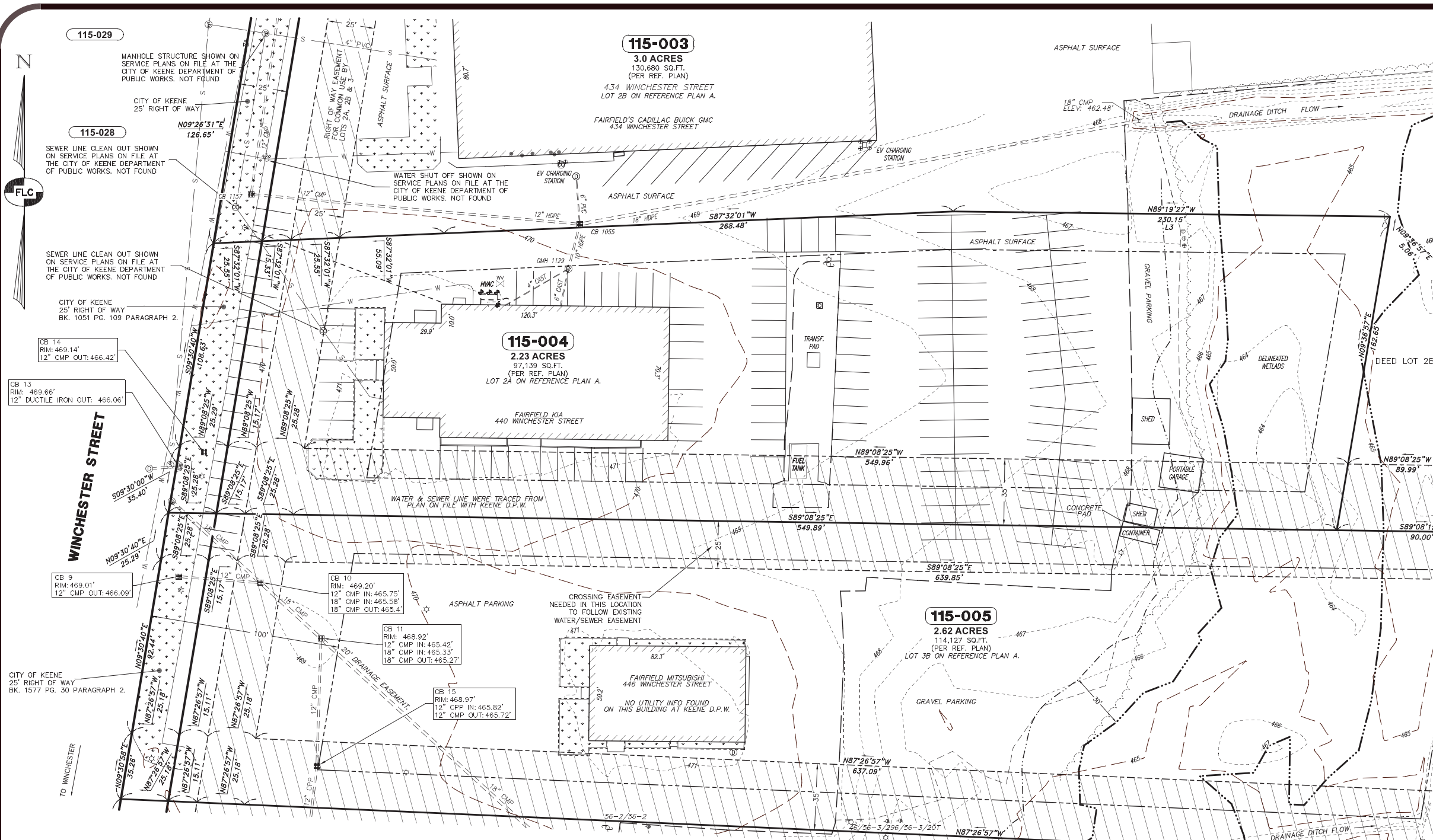
OWNER: \_\_\_\_\_ DATE: \_\_\_\_\_

APPROVED BY THE KEENE PLANNING BOARD

ON: \_\_\_\_\_ CERTIFIED BY \_\_\_\_\_ AND \_\_\_\_\_  
 CHAIRMAN: \_\_\_\_\_ SECRETARY: \_\_\_\_\_

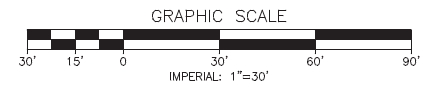
REV.	DATE	DESCRIPTION	C/O	DR	CK
A	11/11/24	REVS PER STAFF COMMENTS		CJC	JEN

FILE: 3762CV01.dwg PROJ. NO. 3762.01 SHEET: CV-1 PAGE NO. 11 OF 11



- NOTES:**
- THE OWNER OF RECORD FOR TAX MAP PARCEL 115-004-000-000-000 IS 434-440 WINCHESTER, LLC. DEED REFERENCE IS BOOK 1577, PAGE 30 RECORDED IN THE C.C.R.D. 10/1/1996.
  - THE PURPOSE OF THIS PLAN IS TO SHOW THE EXISTING CONDITIONS OF TAX MAP PARCEL 115-004.
  - THE TOTAL AREA OF TAX MAP PARCEL 115-004 IS 2.23+ ACRES PER CITY OF KEENE TAX MAPS AND REFERENCE PLANS. ACCESS TO THE BUSINESS IS THROUGH THE PROPERTIES DIRECTLY NORTH AND SOUTH OF THE SUBJECT LOT. FRONTAGE IS 133.91 FT. ON NH RT. 10.
  - ZONING FOR THE PARCELS IS COMMERCE LIMITED.
    - FRONTAGE = 100 FT. (133.9' EXISTING)
    - FRONT SETBACK = 100 FT.
    - REAR SETBACK = 20 FT.
    - SIDE SETBACK = 20 FT.
    - MAXIMUM BUILDING COVERAGE = 40%
    - EXISTING BUILDING = 9,950 SF.
    - MAXIMUM IMPERVIOUS COVERAGE (INCLUDING STRUCTURES) = 70% (67,997 SF)
    - EXISTING IMP = 89,363 SF (71%)
    - MINIMUM GREEN/OPEN = 30% (EXISTING GREEN=23.9%)
    - HEIGHT LIMIT = 35 FT. (2-STORIES).
  - THE EXISTING CONDITIONS OF THE PROPERTY ARE BASED ON THE SURVEY PLAN BY DIBERNARDO ASSOCIATES, LLC, DATED JUNE 28, 2017. PROPERTY BEARINGS ARE BASED ON NH STATE PLAN AND ELEVATIONS ON NAVD89 DATUM. UNDERGROUND UTILITIES ARE APPROXIMATE. WETLAND DELINEATION WAS DONE BY THIS OFFICE IN THE MONTH OF OCTOBER, 2024.
  - THE LOTS LIE WITHIN THE BOUNDARY OF THE 100 YEAR FLOOD HAZARD PER FLOOD INSURANCE RATE MAP FOR THE CITY OF KEENE, CHESHIRE COUNTY, COMMUNITY 330023, PANEL NUMBER 330050266E DATED MAY 23, 2006. BASED ON THIS PANEL, THE ESTIMATED ELEVATION OF THE 100 YEAR FLOOD AT THIS LOCATION IS 471.2 FT.

- REFERENCE PLANS:**
- "PLAN SHOWING EXISTING CONDITIONS OF PROPERTY OF - 434-440 WINCHESTER, LLC. - 434 WINCHESTER STREET - KEENE, N.H. 03431", SCALE: 1"=30' AND DATED JUNE 28, 2017 BY DIBERNARDO ASSOCIATES, LLC
  - "EXISTING PLAN", SCALE: 1"=50' AND DATED OCTOBER 20, 2017 BY BRICKSTONE LAND USE CONSULTANTS.



REV.	DATE	DESCRIPTION	C/O	DR	CK
A	11/11/24	REVS PER STAFF COMMENTS			

**EXISTING CONDITIONS PLAN**  
**TAX MAP 115, LOT 004**  
**440 WINCHESTER STREET**  
**KEENE, NH 03431**  
 PREPARED FOR:  
**FAIRFIELD KIA**  
**440 WINCHESTER ST., KEENE, NH 03431**  
 LAND OF:  
**434-440 WINCHESTER, LLC**  
**P.O. BOX 684, KEENE, NH 03431**

SCALE: 1" = 30'      NOVEMBER 11, 2024

Surveying ♦ Engineering ♦ Land Planning ♦ Permitting ♦ Septic Designs

**FIELDSTONE**  
**LAND CONSULTANTS, PLLC**

206 Elm Street, Milford, NH 03055  
 Phone: (603) 672-5456      Fax: (603) 413-5456  
 www.FieldstoneLandConsultants.com

- LEGEND:**
- EXISTING FEATURES**
- RIGHT-OF-WAY LINE
  - BOUNDARY LINE
  - ABUTTING LOT LINE
  - BUILDING SETBACK LINE
  - EDGE OF PAVEMENT
  - EDGE OF GRAVEL
  - CURB LINE
  - EDGE OF TREE LINE
  - EXISTING EASEMENT LINE
  - 1000' 10' CONTOUR INTERVAL
  - 998' 2' CONTOUR INTERVAL
  - UNDERGROUND DRAINAGE LINE
  - OVERHEAD UTILITY LINE
  - UNDERGROUND UTILITY LINE
  - GAS LINE
  - WATER LINE
  - SEWER LINE

- CATCH BASIN DATA:**
- CB 1055  
RIM=469.02  
INV. IN (NW)=465.57  
INV. IN (S)=465.42  
INV. IN (W)=465.44  
INV. OUT (E)=465.26  
SUMP=464.74
  - CB 1157  
RIM=468.99  
INV. IN (N)=465.99  
INV. OUT (E)=465.44  
SUMP=464.34
  - DMH 1129  
RIM=470.41  
INV. IN (SW)=466.91  
INV. IN (W)=467.33  
INV. OUT (N)=466.71  
SUMP=465.33

- G.B.(F) GRANITE BOUND FOUND
- D.H.(F) DRILL HOLE FOUND
- L.P.W.(F) IRON PIN FOUND
- L.P.P.(F) IRON PIPE FOUND
- D.H.(PRP) DRILL HOLE PER REF. PLAN
- L.P.W.(TRB) IRON PIN TO BE SET
- UTILITY POLE & GUY WIRE
- CATCH BASIN (SQUARE)
- DRAIN MAN-HOLE
- SEWER MAN-HOLE
- WATER HYDRANT
- WATER VALVE
- WATER SHUT-OFF
- WELL
- ELECTRIC PEDESTAL
- SINGLE SIGN POST
- EXISTING BUILDING

**CERTIFICATION:**

WETLANDS WERE DELINEATED IN ACCORDANCE WITH THE US ARMY CORPS OF ENGINEERS' 1987 WETLAND DELINEATION MANUAL Y-87-1 AND REGIONAL SUPPLEMENTS FOR NORTHEAST AND NORTHCENTRAL REGION AND FIELD INDICATORS FOR HYDRIC SOILS IN NEW ENGLAND BY KENNETH M. ROBINSON, C.W.S. IN OCTOBER OF 2024.



DATE: 10/18/24

- PERMITS REQUIRED:**
- CITY OF KEENE PLANNING BOARD SITE PLAN REVIEW.
  - CITY OF KEENE FLOODPLAIN DEVELOPMENT PERMIT.
  - CITY OF KEENE BUILDING PERMIT.
  - FEMA LETTER OF MAP REVISION (LOMR-F) & ELEVATION CERTIFICATE.

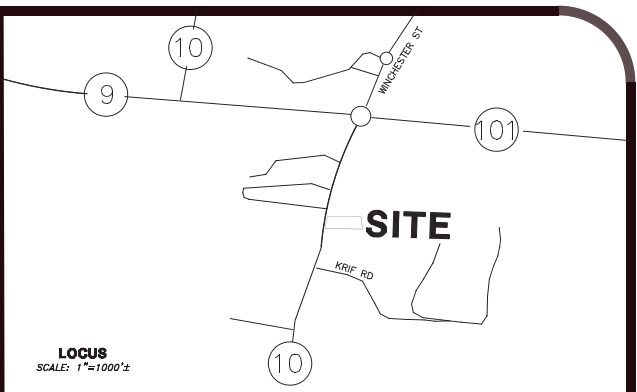
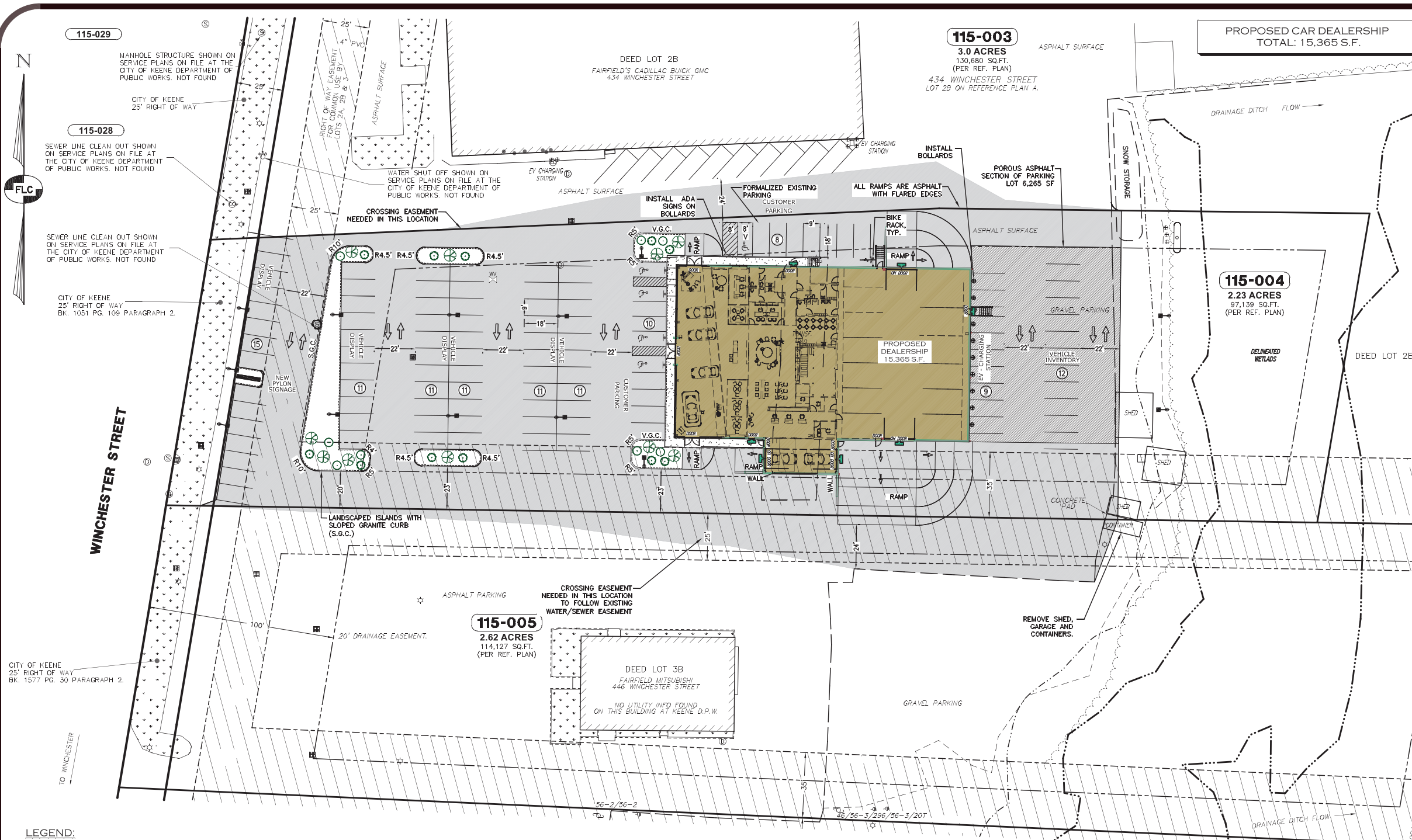
**LAND-OWNER SIGNATURE**

OWNER: \_\_\_\_\_ DATE: \_\_\_\_\_

**APPROVED BY THE KEENE PLANNING BOARD**

ON: \_\_\_\_\_ CERTIFIED BY: \_\_\_\_\_ AND  
 CHAIRMAN: \_\_\_\_\_ SECRETARY: \_\_\_\_\_





- SITE PLAN NOTES:**
- THE OWNER OF RECORD FOR TAX MAP PARCEL 115-004-000-000-00 IS 434-440 WINCHESTER, LLC. DEED REFERENCE IS BOOK 1577, PAGE 30 RECORDED IN THE C.C.R.D. 10/1/1996.
  - THE PURPOSE OF THIS PLAN IS TO SHOW THE REDEVELOPMENT PLAN FOR THE FAIRFIELD'S KIA CAR DEALERSHIP BUILDING WITH SITE IMPROVEMENTS AND DETERMINATION OF FLOOD COMPENSATION GRADING.
  - THE TOTAL AREA OF TAX MAP PARCEL 115-004 IS 2.23± ACRES PER CITY OF KEENE TAX MAPS AND REFERENCE PLANS. ACCESS TO THE BUSINESS IS THROUGH THE PROPERTIES DIRECTLY NORTH AND SOUTH OF THE SUBJECT LOT. FRONTAGE IS 133.91 FT. ON NH RT. 10.
  - ZONING FOR THE PARCELS IS **COMMERCE LIMITED** PROPOSED:
    - FRONTAGE = 100 FT. (133.9' EXISTING)
    - FRONT SETBACK = 100 FT.
    - REAR SETBACK = 20 FT.
    - SIDE SETBACK = 20 FT.
  - MAXIMUM BUILDING COVERAGE = 40%  
 EXISTING BUILDING = 9,950 SF, PROPOSED BUILDING = 15,365 (15.8%)  
 -MAXIMUM IMPERVIOUS COVERAGE (INCLUDING STRUCTURES) = 70% (67,997 SF)  
 EXISTING IMP = 69,363 SF (71%), PROPOSED IMP = 62,076 SF (63.9%), POROUS SURFACE = 8,396 SF (8.6%)  
 -MINIMUM GREEN/OPEN = 30%. (EXISTING GREEN=23.9%, PROPOSED GREEN =27.6%)  
 -HEIGHT LIMIT = 35 FT. (2-STORIES). (PROPOSED BLDG 25'-8", ONE-STORY)
  - THE EXISTING CONDITIONS OF THE PROPERTY ARE BASED ON THE SURVEY PLAN BY DIBERNARDO ASSOCIATES, LLC DATED JUNE 28, 2017. PROPERTY BEARINGS ARE BASED ON NH STATE PLAN AND ELEVATIONS ON NAVD88 DATUM. UNDERGROUND UTILITIES ARE APPROXIMATE. WETLAND DELINEATION WAS DONE BY THIS OFFICE IN THE MONTH OF OCTOBER, 2024.
  - THE LOTS LIE WITHIN THE BOUNDARY OF THE 100 YEAR FLOOD HAZARD PER FLOOD INSURANCE RATE MAP FOR THE CITY OF KEENE, CHESHIRE COUNTY, COMMUNITY 330023, PANEL NUMBER 3300502666 DATED MAY 23, 2006. BASED ON THIS PANEL, THE ESTIMATED ELEVATION OF THE 100 YEAR FLOOD AT THIS LOCATION IS 471.2 FT.
  - THE EXISTING BUILDING WILL BE DEMOLISHED FOLLOWING THE FULL CONSTRUCTION OF THE PROPOSED BUILDING. THIS WILL ALLOW THE DEALERSHIP TO REMAIN OPEN DURING CONSTRUCTION OF THE NEW BUILDING.
  - PARKING CALCULATIONS:**  
 EXISTING PARKING = 150 SPACES (APPROX-MANY SPACES UNSTRIPPED)  
 4 SPACES/1000 SF INDOOR SALES: 4300/1000 x 4 = 17.2 (18 SPACES)  
 4 SPACES/SERVICE BAY: 1 SERVICE BAY x 4 = 4 SPACES  
 TOTAL REQUIRED = 22 SPACES; TOTAL PROVIDED = 109 SPACES (29 SPACES TO SIDE/REAR)  
 ADA SPACES REQ'D = 5 (1 VAN); ADA SPACES PROVIDED = 5 (1 VAN)
  - THE BUILDING FINISH FLOOR ELEVATION IS 472.2', PROVIDING 1 FOOT ABOVE BASE FLOOD ELEVATION (B.F.E. =471.2').
  - THE PROPOSED FLOOD COMPENSATION FOR THE FILL PLACED IN THE FLOODPLAIN IS BASED ON A TOTAL VOLUME COMPENSATION AT 471.2' IN ELEVATION, TO BE APPROVED BY THE KEENE FLOODPLAIN ADMINISTRATOR.  
 CUT VOLUME : 73,131.0 C.F., 2,708.6 C.Y. (TOTAL FLOOD COMP.)  
 FILL VOLUME : 59,991.4 C.F., 2,221.9 C.Y. (TOTAL FILL)
  - ANY SIGNS SHOWN WILL BE PERMITTED SEPARATELY AND DETAILS PROVIDED AS PART OF THE BUILDING PERMIT APPLICATION.
  - WHEN SNOW IS EXCESSIVE, IT WILL BE HAULED OFF SITE. SNOW STORAGE WILL NOT BE DUMPED INTO THE ADJACENT WETLANDS OR THE BUFFER AREA.

REV.	DATE	DESCRIPTION	C/O	DR	CK
A	11/11/24	REVS PER STAFF COMMENTS			

**SITE PLAN**  
**TAX MAP 115, LOT 004**  
**440 WINCHESTER STREET**  
**KEENE, NH 03431**  
 PREPARED FOR:  
**FAIRFIELD KIA**  
**440 WINCHESTER ST., KEENE, NH 03431**  
 LAND OF:  
**434-440 WINCHESTER, LLC**  
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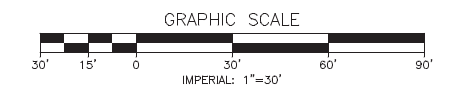
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- LEGEND:**
- |                              |                          |                                   |
|------------------------------|--------------------------|-----------------------------------|
| <b>EXISTING FEATURES</b>     | <b>PROPOSED FEATURES</b> | <b>UTILITIES</b>                  |
| RIGHT-OF-WAY LINE            | EDGE OF PAVEMENT         | UTILITY POLE                      |
| BOUNDARY LINE                | PAVEMENT SAWCUT LINE     | UTILITY BOX LOCATION              |
| ABUTTING LOT LINE            | PARKING COUNT            | WATER SERVICE SHUT-OFF            |
| BUILDING SETBACK LINE        | EDGE OF PAVEMENT         | FLARED END SECTION (FES)          |
| EDGE OF PAVEMENT             | CAPE COD BEAM (CCB)      | TRAFFIC FLOW (NOT PAINTED ARROWS) |
| EDGE OF GRAVEL               | 2 FT. CONTOUR            | SIGN                              |
| CURB LINE                    | 10 FT. CONTOUR           | STONE CHECK DAM                   |
| EDGE OF TREE LINE            | SPOT ELEVATION           | INLET PROTECTION                  |
| EXISTING EASEMENT LINE       | STORM WATER DRAINAGE     | PROPOSED BUILDING                 |
| 1000 10' CONTOUR INTERVAL    | STORM DRAIN MANHOLE      |                                   |
| 998 2' CONTOUR INTERVAL      | LIMITS OF CLEARING       |                                   |
| UNDERGROUND DRAINAGE LINE    | PROPOSED RETAINING WALL  |                                   |
| OH OVERHEAD UTILITY LINE     | TEMPORARY SILT FENCE     |                                   |
| U/C UNDERGROUND UTILITY LINE | POLE MOUNTED LIGHT       |                                   |
| G GAS LINE                   | UNDERGROUND ELECTRIC     |                                   |
| W WATER LINE                 | OH OVERHEAD ELECTRIC     |                                   |
| S SEWER LINE                 | W WATER LINE             |                                   |
|                              | S SEWER LINE             |                                   |
|                              | S SEWER MANHOLE          |                                   |

- REFERENCE PLANS:**
- "PLAN SHOWING EXISTING CONDITIONS OF PROPERTY OF - 434-440 WINCHESTER LLC. - 434 WINCHESTER STREET - KEENE, N.H. 03431", SCALE: 1"=30' AND DATED JUNE 28, 2017 BY DIBERNARDO ASSOCIATES, LLC
  - "EXISTING PLAN", SCALE: 1"=50' AND DATED OCTOBER 20, 2017 BY BRICKSTONE LAND USE CONSULTANTS.



**CERTIFICATION:**  
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  - CITY OF KEENE FLOODPLAIN DEVELOPMENT PERMIT;
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  - FEMA LETTER OF MAP REVISION (LOMR-F) & ELEVATION CERTIFICATE;

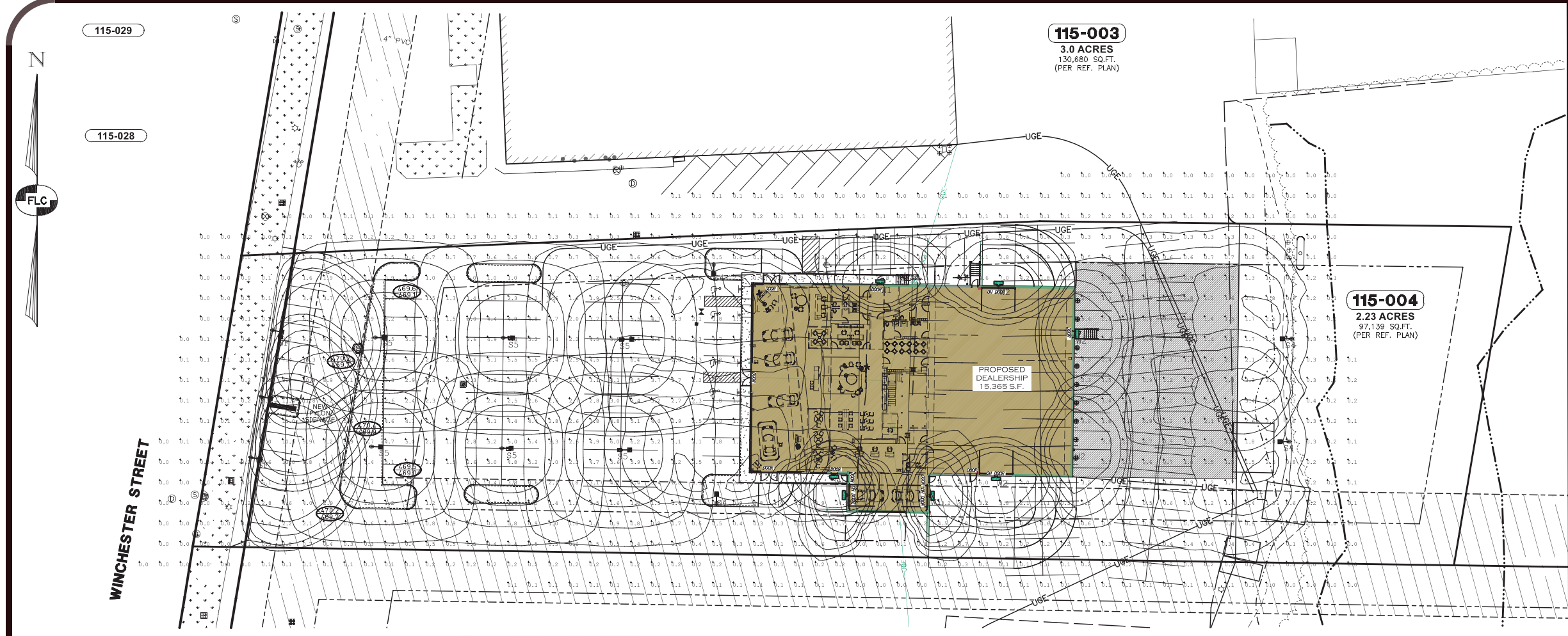
<b>LAND-OWNER SIGNATURE</b>	
OWNER: _____	DATE: _____
<b>APPROVED BY THE KEENE PLANNING BOARD</b>	
ON: _____	CERTIFIED BY _____ AND _____
CHAIRMAN: _____	SECRETARY: _____











- DESIGN NOTES:**
1. LIGHTING REQUIREMENTS:
 

PARKING LOTS	REQUIRED	PROPOSED
AVERAGE	3.5 Fc MAX	SEE EACH SECTION
MINIMUM	0.33 Fc	- Fc
U. RATIO (AVG/MIN)	5:1	- Fc
MAX AT PROP. LINE	0.1 Fc	- Fc
MAX AT R.O.W.	1.0 Fc	- Fc
SIDEWALKS		
MINIMUM	0.5 Fc	N.A.
MAXIMUM	5.0 Fc	N.A.
LUMENS	1200 L MAX	N.A.
  2. ALL PROPOSED LIGHTING MUST BE GREATER THAN 70 (CRI) COLOR RENDERING INDEX.
  3. ALL PROPOSED LIGHTING MUST BE 3000 KELVIN COLOR-TEMPERATURE OR LESS.

- LIGHTING NOTES:**
1. LIGHTING SHALL BE INSTALLED AND ARRANGED SO AS NOT TO REFLECT OR CAUSE GLARE UPON ABUTTING LAND, HIGHWAYS AND ROADS.
  2. ALL FIXTURES ARE FULL CUTOFF, LED FIXTURES. FLOOD LIGHTING AND UP-LIGHTING ARE PROHIBITED.
  3. LIGHTING IS PROVIDED VIA BUILDING MOUNTED LIGHTS AND POLE MOUNTED LIGHTS.
  4. MOUNTING HEIGHT OF ALL PROPOSED WALL MOUNT LIGHTING FIXTURES SHALL BE 10 & 14 FEET ABOVE FINISH GRADE ON THE BUILDINGS. LOCATED AT EACH BUILDING UNIT.
  5. POLE MOUNTED LIGHTS ARE TO BE MOUNTED 20.0 FEET ABOVE FINISH GRADE.
  6. ALL LIGHTS ARE TO BE SETUP ON PHOTOCELLS TO AUTOMATICALLY TURN OFF DURING DAYLIGHT HOURS. TIMER SHALL BE INSTALLED TO LIMIT HOURS FROM 6 AM - 10 PM. IF OPERATING 24/7 THE LIGHTING MUST REDUCE BY 50% FOR SECURITY LIGHTING. HALF OF THE WALLPACKS SHALL BE INSTALLED WITH MOTION SENSORS. THE AVERAGE FOOTCANDLE RATING SHALL NOT EXCEED 1.0 Fc BETWEEN 10PM - 6 AM.
  7. ALL FIXTURES AND HARDWARE ARE TO BE DARK BRONZE IN COLOR TO MATCH.
  8. LIGHT FIXTURES ARE AVAILABLE THROUGH EXPOSURE 2 LIGHTING. ANY CHANGE IN FIXTURE MUST BE APPROVED BY THE OWNER, DESIGN ENGINEER, AND CITY OF KEENE.
  9. EXISTING UNDERGROUND ELECTRICAL LINES ARE APPROXIMATE, ELECTRICAL COMPANY NEEDS TO VERIFY THE LOCATION OF ELECTRICAL LINES.
  10. THE LIGHT TRESPASS ONTO ABUTTING PROPERTIES ARE PROPERTIES OWNED BY THE SAME ENTITY.

**V-Locity Small (VALS)**  
Outdoor LED Area Light

OVERVIEW	
Lumen Package	6,000 - 27,000
Wattage Range	36 - 178
Efficacy Range (LPW)	142 - 171
Weight lbs(kg)	20 (9.1)
Control Options	IMSBT, ALB, ALS, 7-Pin, PCI



FRONT DISPLAY SPACES ADJACENT TO ROADWAY	DISPLAY PARKING LOT INCLUDES CUSTOMER PARKING	BACK PARKING LOT
ILLUMINANCE (Fc)	ILLUMINANCE (Fc)	ILLUMINANCE (Fc)
Average = 7.78	Average = 3.25	Average = 1.65
Maximum = 11.8	Maximum = 6.4	Maximum = 5.4
Minimum = 2.8	Minimum = 1.3	Minimum = 0.5
Avg/Min Ratio = 2.78	Avg/Min Ratio = 2.50	Avg/Min Ratio = 3.30
Max/Min Ratio = 4.21	Max/Min Ratio = 4.92	Max/Min Ratio = 10.80

Symbol	Qty	Label	Arrangement	Description	Tag	CRI	CCT
⬇	4	S4	Single	VALS-12L-4F-UNV-30K8-CXX-SA-IMBTL1 / 4SQ B3 S11G17.5 S AB 4BC	17.5FT POLE ON 2.5FT BASE	80	3000K
⬇	6	S5	Single	VALS-12L-5QN-UNV-30K8-CXX-SA-IMBTL1 / 4SQ B3 S11G17.5 S AB 4BC	17.5FT POLE ON 2.5FT BASE	80	3000K
⬇	3	SF	Single	VALS-12L-M-UNV-30K8-CXX-SA-IMBTL1 / 4SQ B3 S11G17.5 S AB 4BC	17.5FT POLE MOUNTED ON 2.5FT BASE	80	3000K
⬆	4	W1	Single	LWP2-50W-WW-OPT-SEN-3MRLV-5	WALL MOUNTED 10' AFG - REQUIRES NEC-8RC	80	3000K
⬆	4	W2	Single	LWP2-50W-WW-OPT-SEN-3MRLV-5	WALL MOUNTED 14' AFG - REQUIRES NEC-8RC	80	3000K

**LWP2 SERIES** Outdoor Lighting

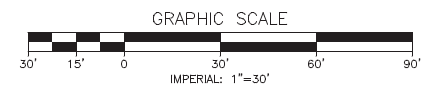
- LWP2-50WW
- LWP2-50NW
- LWP2-50CW



Performance Table: LWA-50W

MODEL NO.	Wattage	Voltage	Lumens	Color Temp.	BUG Rating	LPW
LWP2-50WW	50W	120~277V	5000LM	3000K	B2-U1-G1	100
LWP2-50NW	50W	120~277V	5500LM	4000K	B2-U1-G1	110
LWP2-50CW	50W	120~277V	5500LM	5000K	B2-U1-G1	110

PROPOSED CAR DEALERSHIP TOTAL: 15,365 S.F.



REV.	DATE	REVS PER STAFF COMMENTS DESCRIPTION	C/O	DR	CK
A	11/11/24	REVS PER STAFF COMMENTS			
			CJC	JEN	

**LIGHTING PLAN**  
TAX MAP 115, LOT 004  
440 WINCHESTER STREET  
KEENE, NH 03431  
PREPARED FOR:  
**FAIRFIELD KIA**  
440 WINCHESTER ST., KEENE, NH 03431  
LAND OF:  
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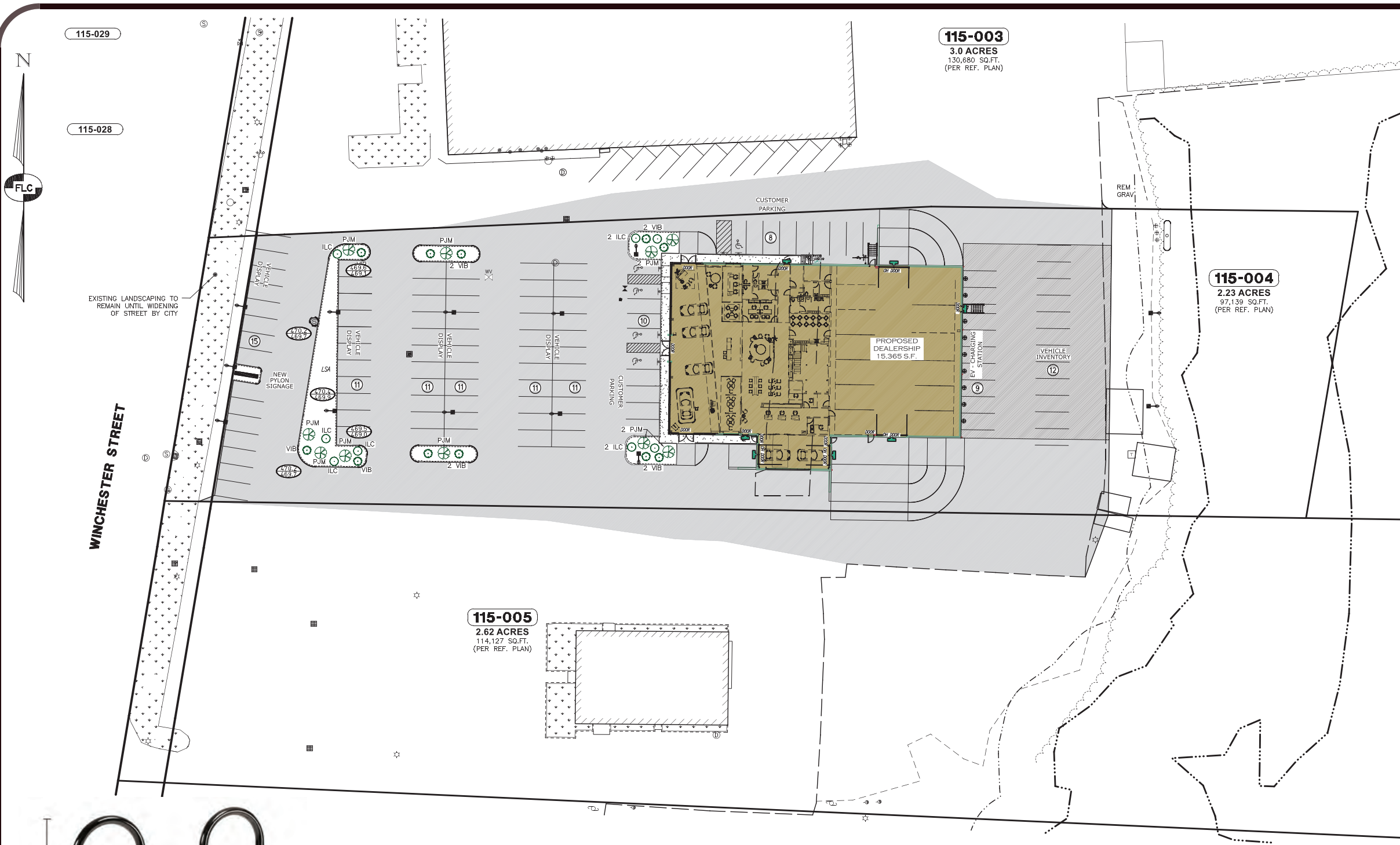
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Nov 12, 2024 - 2:15pm - FLC-31  
P:\0\_FLC\PROJECTS\3762\3762.01.DWG



**DESIGN NOTES:**

1. LANDSCAPING REQUIREMENTS:

TREES (SITE)	REQUIRED	PROPOSED
1/10 PARKING SPACE (112 SP)	12 TREES	0 TREES
PARKING PERIMETER TREES	N/A	N/A

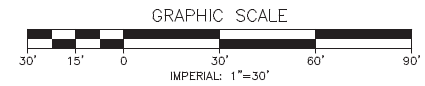
  

SHRUBS:	REQUIRED	PROPOSED
EVERGREENS	SCREENING	0
NON-EVERGREEN	N/A	30

PARKING OVER 50 SPACES:	REQUIRED	PROPOSED
1 TREE PER 10 SPACES	-	0
10% PARKING AREA LANDSCAPED	1,296 SF	2,946 SF
PARKING ISLAND SIZE MIN.	95 SF	306 SF

- LANDSCAPING NOTES:**
- THE LANDSCAPE CONTRACTOR IS RESPONSIBLE FOR CONTACTING DIG-SAFE AND FOR VERIFICATION OF ALL UTILITIES AND SHALL NOTIFY THE OWNERS REPRESENTATIVE OF ANY CONFLICTS PRIOR TO COMMENCING.
  - EXISTING TREES TO REMAIN SHALL BE PRESERVED AND PROTECTED DURING CONSTRUCTION. TEMPORARY FENCING SHALL BE INSTALLED PRIOR TO THE START OF SITE WORK TO PROTECT ROOT MASSES.
  - EXISTING TREES THAT ARE TO BE REMOVED, SHALL BE REMOVED ENTIRELY FROM THE SITE, INCLUDING STUMPS. NO STUMP-DUMPS ARE ALLOWED ON SITE.
  - UNTIL ALL GRADING AND CONSTRUCTION HAS BEEN COMPLETED WITHIN THE IMMEDIATE AREA NO PLANT MATERIAL SHALL BE INSTALLED.
  - UNLESS OTHERWISE NOTED OR APPROVED, ALL TREES MUST BE BALLED AND BURLAPPED.
  - ALL PLANT MATERIALS INSTALLED SHALL MEET OR EXCEED THE SPECIFICATIONS OF "THE AMERICAN STANDARDS FOR NURSERY STOCK" BY THE AMERICAN ASSOCIATION OF NURSERMEN.
  - ANY PROPOSED PLANT MATERIAL SUBSTITUTIONS MUST BE APPROVED BY THE OWNER'S REPRESENTATIVE.
  - ALL PLANT MATERIAL SHALL BE GUARANTEED BY THE INSTALLER FOR ONE YEAR FOLLOWING DATE OF ACCEPTANCE. ANY PLANT MATERIAL THAT IS SIGNIFICANTLY DAMAGED, MISSING, DISEASE RIDDEN, OR DEAD SHALL BE ABATED WITHIN 1-YEAR OR BEFORE THE END OF THE FOLLOWING PLANTING SEASON, WHICHEVER OCCURS FIRST.
  - IN AREAS OF STONE MULCH LAY 6 MIL SHEETS OF "VISQUEEN" TYPE POLYETHYLENE ON COMPACTED SUBGRADE BEFORE PLACING STONE, MINIMUM 6" OVERLAP. PERFORATE SHEETING IN PLANTING BEDS BEFORE PLACING STONE.
  - UNLESS OTHERWISE NOTED LOAM AND SEED ALL DISTURBED AREAS WITH A MINIMUM 4" OF SUITABLE LOAM. SLOPES GREATER THAN 3:1 SHALL BE PROTECTED WITH AN EROSION CONTROL BLANKET. SEE SITE PLAN.
  - WHERE APPLICABLE, THE CONTRACTOR SHALL HAVE ALL FALL TRANSPLANTING HAZARD PLANTS DUG IN THE SPRING AND STORED FOR FALL PLANTING.
  - PLANTS SHALL BE INSTALLED WITHIN ONE YEAR OF COMMENCEMENT OF CONSTRUCTION
  - ALL LANDSCAPING SHALL BE LOCATED AND MAINTAINED SO AS NOT TO IMPACT THE LINES OF SIGHT AT THE ENTRANCE AND INTERNAL INTERSECTIONS TO PROVIDE SAFE PASSAGE OF PEDESTRIANS, BICYCLISTS, AND MOTORISTS.
  - ALL LANDSCAPED AREAS WILL BE MAINTAINED TO HAVE A SUFFICIENT AMOUNT OF WATER TO MAINTAIN VIABILITY EITHER BY IRRIGATION OR BY OTHER MEANS.
  - PROPOSED PLANTINGS SHALL NOT CONFLICT WITH SNOW STORAGE AREAS, LIGHT FIXTURES OR UNDERGROUND UTILITIES.
  - ALL LANDSCAPING APPROVED AS PART OF THE SITE PLAN SHALL BE CONSIDERED AS ELEMENTS OF THE SITE, IN THE SAME MANNER AS PARKING, BUILDING MATERIALS, AND OTHER SITE DETAILS. ANY CHANGES WILL REQUIRE APPROVAL BY THE OWNER AND CITY COMMUNITY DEVELOPMENT DIRECTOR.
  - BIKE RACKS ARE PROPOSED AT THE REAR OF THE BUILDING.
  - ALL HVAC EQUIPMENT SHALL BE ROOF MOUNTED OR NOT BE VISIBLE FROM THE PUBLIC RIGHT OF WAY.
  - THE LANDSCAPING PLAN IS BASED ON "ALTERNATIVE DESIGN" TO WORK WITH THE CAR DEALERSHIP USE, SUCH AS NO TREES (LEAVES, BIRD DROPPINGS, BUGS) AND MORE VISIBILITY TO PASSERS BY.



REV.	DATE	DESCRIPTION	C/O	DR	CK
A	11/11/24	REVS PER STAFF COMMENTS		CJC	JEN

**LANDSCAPING PLAN**  
**TAX MAP 115, LOT 004**  
**440 WINCHESTER STREET**  
**KEENE, NH 03431**  
 PREPARED FOR:  
**FAIRFIELD KIA**  
**440 WINCHESTER ST., KEENE, NH 03431**  
 LAND OF:  
**434-440 WINCHESTER, LLC**  
**P.O. BOX 684, KEENE, NH 03431**

SCALE: 1" = 30'      NOVEMBER 11, 2024

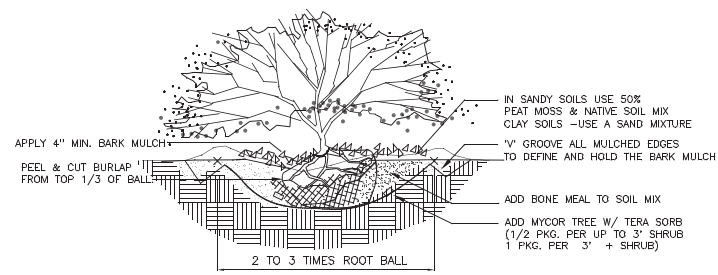
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 Phone: (603) 672-5456      Fax: (603) 413-5456  
 www.FieldstoneLandConsultants.com



- INSTALL 1/2" STAINLESS BOLTS INTO DROP-IN ANCHORS BY HILT.
- SET TWO 8# SONOTUBES FILLED WITH CONCRETE TO CONNECT BASE PLATES TO.

**BIKE RACK DETAIL**      1      LS-1



**B & B SHRUB PLANTING DETAIL**      2      LS-1

**PLANT SYMBOLS**

- PJM
- ILC
- VB

**PLANT LIST & NAME**

LIMITS OF CLEARING - PROPOSED TREELINE

P.J.M. RHODODENDRON SHRUB (10) 2 GALLON CONTAINER, 1.5' - 2'H

ILEX VERTICILLATA (WINTERBERRY SHRUB) (8) 2 GALLON CONTAINER, 1.5' - 2'H

VIURNUM LENTAGO (NANNYBERRY VIURNUM) (11) 2 GALLON CONTAINER, 1.5' - 2'H

**QTY.**

BUFFER AROUND WETLANDS





SOUTH WEST RENDERING



NORTH WEST RENDERING



NORTH EAST RENDERING



SOUTH EAST RENDERING

**ARCHITECT:**  
**MPA**architect PLLC  
 p: 603-769-1826  
 a: P.O. Box 305  
 New Ipswich, NH 03071  
 e: Marcus@MP-Architect.com

---

**PROJECT:**  
**Fairfield Kia**  
 440 Winchester St, Keene, NH 03431

**PROJECT ADDRESS:**  
 440 Winchester St, Keene, NH 03431

**DESIGN BUILDER:**  
 Hutter Construction Corporation  
 PO Box 257, 810 Turnpike Road New Ipswich, NH 03071

**CLIENT:**  
**Fairfield Kia**  
 440 Winchester St, Keene, NH 03431

**CLIENT ADDRESS:**  
 440 Winchester St, Keene, NH 03431

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**SEAL:**

**REVISIONS:**

**DRAWING INFO:**

**DATE:**  
 10.01.24

**PHASE:**  
 PROGRESS

**TITLE:**

**EXTERIOR  
 RENDERINGS**

**NUMBER:**

**A-200**  
 79 of 92



EXTERIOR MATERIAL SCHEDULE			
MARK	DESCRIPTION	MFR	MODEL & COLOR
E-1	ALUMINUM COMPOSITE MATERIAL	ALPOLIC	ALPOLIC FR, COLOR: TOB BLACK
E-2	ALUMINUM COMPOSITE MATERIAL	ALPOLIC	ALPOLIC FR, COLOR: BSX SILVER METALLIC
E-3	INSULATED METAL PANEL	CHIEF BUILDINGS	COLOR: WHITE
E-4	PAINT	BENJAMIN MOORE	COLOR: HC-169 COVENTRY GRAY
E-6	METAL	ELEVATE	PREFINISHED ALUMINUM, BLACK TO MATCH ACM
E-7	METAL	CHIEF BUILDINGS	PREFINISHED ALUMINUM, WHITE TO MATCH WALL PANELS

SHEET NOTES	
#	DESCRIPTION
001	WALL MOUNTED LIGHTING, GARDCO PURE FORM, REF: SITE LIGHTING PLAN
002	BUILDING SIGNAGE, REF: KIA EXTERIOR SIGNAGE DESIGN INTENT DOCUMENTS
003	HOLLOW METAL DOOR, PAINTED
004	ALUMINUM, INSULATED, HIGH SPEED OVERHEAD DOOR WITH VISION LIGHTS, PAINTED
005	ALUMINUM AND GLASS, INSULATED, HIGH SPEED OVERHEAD DOOR, CLEAR ANODIZED FINISH
006	ALUMINUM STOREFRONT WINDOW, KAWNEER 451T SYSTEM 2" X 4 1/2" FRAME DEPTH, CENTER GLAZED, CLEAR ANODIZED FINISH
007	ALUMINUM STOREFRONT DOOR, KAWNEER 350T ENTRANCE, NARROW STYLE WITH 451T FRAMING SYSTEM, 2" X 4 1/2" FRAME DEPTH, CENTER GLAZED, CLEAR ANODIZED FINISH
008	ALUMINUM CURTAINWALL, KAWNEER 1620UT, 2" X 8-1/4" FRAME DEPTH, OUTSIDE GLAZED, CLEAR ANODIZED FINISH
010	GALV. METAL GUARD RAIL, PAINTED
011	ASPHALT RAMP
012	SLOPE OF RAMP BEYOND
013	SLOPE OF ROOF BEYOND
014	ROOF TOP UNIT ON INSULATED CURB, REF: MECHANICAL, VERIFY SIZE AND LOCATION

**ARCHITECT:**  
MPA Architect PLLC  
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New Ipswich, NH 03071  
e: Marcus@MP-Architect.com

**PROJECT:**  
**Fairfield Kia**  
440 Winchester St, Keene, NH 03431

**DESIGN BUILDER:**  
Hutter Construction Corporation  
PO Box 257, 810 Turnpike Road New Ipswich, NH 03071

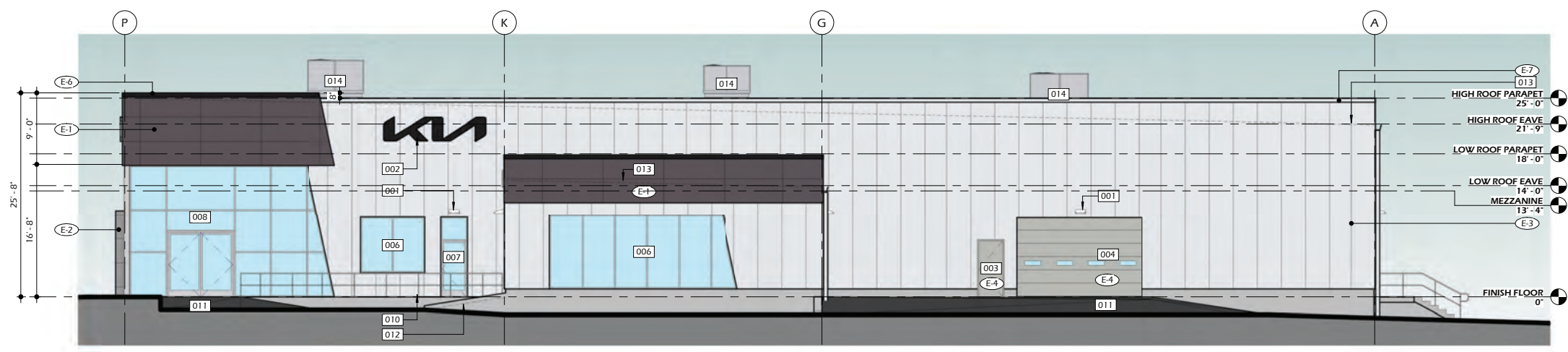
**CLIENT:**  
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**REVISIONS:**

**DRAWING INFO:**  
DATE: 10.01.24  
PHASE: PROGRESS  
TITLE: ARCHITECTURAL EXTERIOR ELEVATIONS  
NUMBER: A-201



**A3 SOUTH ELEVATION**  
SCALE: 1/8" = 1'-0"



**A1 WEST ELEVATION**  
SCALE: 1/8" = 1'-0"

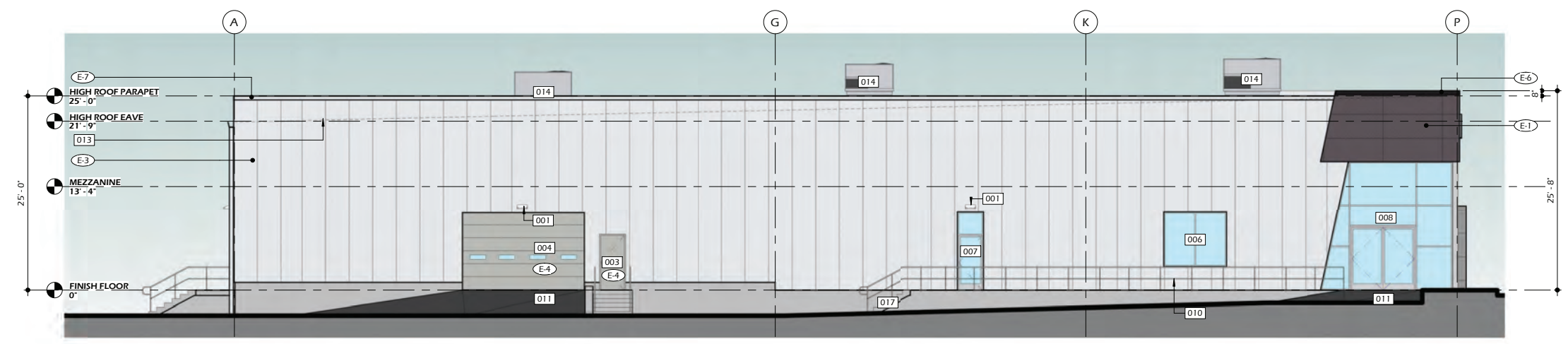
EXTERIOR MATERIAL LEGEND	
	E-1 ACM
	E-2 ACM
	E-3 METAL WALL PANEL

MARK	DESCRIPTION	MFGR	MODEL & COLOR
E-1	ALUMINUM COMPOSITE MATERIAL	ALPOLIC	ALPOLIC FR, COLOR: TOB BLACK
E-2	ALUMINUM COMPOSITE MATERIAL	ALPOLIC	ALPOLIC FR, COLOR: BSX SILVER METALLIC
E-3	INSULATED METAL PANEL	CHIEF BUILDINGS	COLOR: WHITE
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E-7	METAL	CHIEF BUILDINGS	PRE-FINISHED ALUMINUM, WHITE TO MATCH WALL PANELS

#	DESCRIPTION
001	WALL MOUNTED LIGHTING, GARDCO PURE FORM, REF: SITE LIGHTING PLAN
003	HOLLOW METAL DOOR, PAINTED
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010	GALV. METAL GUARD RAIL, PAINTED
011	ASPHALT RAMP
013	SLOPE OF ROOF BEYOND
014	ROOF TOP UNIT ON INSULATED CURB, REF: MECHANICAL, VERIFY SIZE AND LOCATION
015	DOWNSPOUT / LEADER, CONNECT TO UNDERGROUND DRAINAGE SYSTEM, COLOR TO MATCH WALL PANEL
016	GUTTER, COLOR TO MATCH WALL PANEL
017	CONCRETE STAIRS W/ RETAINING WALLS AND GALV. METAL GUARD RAIL, PAINTED

**ARCHITECT:**  
**MPA**architect PLLC  
 p: 603-769-1826  
 a: P.O. Box 305  
 New Ipswich, NH 03071  
 e: Marcus@MP-Architect.com

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**Fairfield Kia**  
 PROJECT ADDRESS: 440 Winchester St, Keene, NH 03431  
 DESIGN BUILDER: Hutter Construction Corporation  
 ADDRESS: PO Box 257, 810 Turnpike Road New Ipswich, NH 03071  
 CLIENT: Fairfield Kia  
 CLIENT ADDRESS: 440 Winchester St, Keene, NH 03431



**A3 NORTH ELEVATION**  
 SCALE: 1/8" = 1'-0"



**A1 EAST ELEVATION**  
 SCALE: 1/8" = 1'-0"

EXTERIOR MATERIAL LEGEND	
	E-1 ACM
	E-2 ACM
	E-3 METAL WALL PANEL

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**DATE:**  
10.01.24

**PHASE:**  
PROGRESS

**TITLE:**  
ARCHITECTURAL  
EXTERIOR  
ELEVATIONS

**NUMBER:**

**A-202**  
8 of 92

# Storm Water Management Report

## FAIRFIELD'S KIA REDEVELOPMENT

### Project Location:

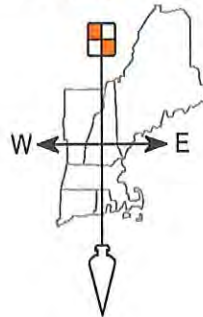
Tax Map 115, Lot 004  
440 Winchester Street  
Keene, NH 03431

### Prepared for:

434-440 WINCHESTER, LLC  
PO BOX 684  
Keene, NH 03431  
Project #3762.01

Date: October 18, 2024

Surveying ♦ Engineering ♦ Land Planning ♦ Permitting ♦ Septic Designs

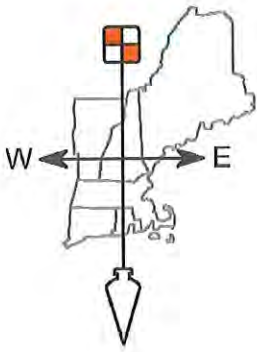


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## STORM WATER MANAGEMENT REPORT FAIRFIELD'S KIA REDEVELOPMENT KEENE, NEW HAMPSHIRE

Prepared for:  
434-440 Winchester, LLC

October 18, 2024

### I) INTRODUCTION

This stormwater management report was conducted for a proposed site development known as Fairfield's KIA in Keene NH. The property is located on Tax Map Lot 115-004 with frontage on Winchester Street/NH Route 10. The proposal is to redevelop the existing car dealership by razing the existing building and constructing a new, more modern facility. The new building will be constructed behind the existing building, so that the dealership can remain open during construction. The property is located within 100 Year Floodplain, Zone AE, per FEMA mapping, which drove the design of the project. The elevation of the new building will be higher than the existing building due to the floodplain and the site is designed to provide flood compensation to offset the proposed fill in the floodplain.

The purpose of this report is to analyze the qualitative and quantitative impacts of the proposed development on stormwater runoff. The objective of the proposed stormwater management system is to mitigate any increases resulting from the proposed development and to meet the drainage guidelines set forth in the City of Keene Land Development Code. The project does not include any wetland impacts and land disturbance will be under the NHDES Alteration of Terrain threshold of 100,000 square feet. No Wetland Permits or Alteration of Terrain permit is required.

### II) SITE DESCRIPTION (EXISTING)

The existing Tax Map Lot 115-004 has 97,139 square feet or 2.23+/- Acres with 133.91 feet of frontage along Winchester Street (aka NH Route 10). The parcel is currently developed and operates as a car dealership. The property abuts the Fairfield's Buick/GMC/Cadillac and Fairfield's Mitsubishi car dealerships to the north and south respectively. These dealerships and properties are owned by the same entity, 434-440 Winchester, LLC. There are crossing easements on the three properties to allow for use of the two curb cuts onto the state highway (NH Route 10).

NRCS soil survey maps indicate that the dominant soils present on the site consist of Raynham-Wareham complex, occasionally flooded (HSG C), and Occum fine sandy loam (HSG B). The development area is located mostly within the HSG C soils, with a small portion of HSG B soils.

which are mapped under the paved parking lot. This HSG B soil was omitted from the model. The infiltration rate is listed with Ksat value of 0.6 in/hour. These Hydrologic Group (HSG) are based on the Ksat Values for New Hampshire Soils, as used by NHDES. Field examination was conducted as part of the wetland delineation; test pits were not performed as the land disturbance will occur in the same footprint of the existing dealership. There is a wetland at the eastern (rear) end of the property along with manmade drainage ditches.

### III) METHODOLOGY

The quantity of runoff and the conveyance of that flow through the site are determined using the software package HydroCAD r 10.10.6a by HydroCAD Software Solutions, LLC. HydroCAD is a computer program for modeling storm water hydrology based on the Soil Conservation Service (SCS) TR-20 method, combined with standard hydraulics calculations used to model detention basins and culverts.

Stormwater management systems are designed in accordance with the methodology for the "Best Management Practices" (BMP's), as outlined in the New Hampshire Storm Water Manual, Volume 2.

### IV) DRAINAGE DESIGN

In accordance with the City of Keene and NHDES standards, there will be no increase in the peak flow rate of surface runoff. In order to demonstrate this, the two (2), ten (10), and fifty (50) year frequency storm events have been evaluated using a Type III, 24-hour storm distribution. These design storms have been analyzed to compare the Pre and Post-Development peak flow rates for the site (see comparison table).

#### Pre-Development Drainage Conditions:

The Pre-Development Drainage Area Plan outlines the three areas where water flows off the property. The first observation point (OP1) is the wetland complex to the rear of the lot, which was delineated by our office. The second observation point (OP2) is the catch basin in the front landscaped island near Winchester Street.

#### Post-Development Drainage Conditions:

The proposed drainage systems were designed to capture runoff from the building and paved areas, and direct the flow to stormwater management systems. There are four (4) Subcatchments modeled in the post-development condition with water flowing to the same locations as the Pre-Development scenario.

The proposed site layout ends up with a slight increase in impervious surfaces from the existing condition. The rear portion of the parking lot will be porous asphalt to reduce impervious areas and

provide a stormwater detention volume on site. The net result is there will be a reduction of peak rates of runoff leaving this site for all storm events.

**V) SUMMARY**

The intent of the stormwater management system for this project is to address the qualitative and quantitative aspects of the stormwater runoff so that there are no offsite adverse impacts created by the project. To mitigate the resulting increases in runoff volume and peak rates due to the redevelopment of Lot 115-004, this project proposes that a stormwater management system consisting of one (1) catch basin, one (1) porous asphalt basin, and under drains from the porous asphalt to daylight into the manmade drainage ditch at the rear of the property. The net result is that the new buildings and paved areas will receive qualitative treatment and there will be no increase in the peak rates of runoff leaving the site.

The stormwater management design for this project therefore complies with the stormwater standards set forth in the City of Keene's Land Development Code.

The following table is a summary of the attached calculations and show a comparison of the peak flow rates at the outlet point for the site. The values presented are based on pre- and post-development conditions.

**Table 1.1: Peak Flow Rates (CFS)/Volume (AF) to OBSERVATION POINT 1 (OP1)**

STORM FREQUENCY	PRE-DEV. RUNOFF (CFS/AF)	POST-DEV. RUNOFF (CFS/AF)	CHANGE (CFS/AF)
2-YEAR	8.08/0.567	6.55/0.519	-1.53/-0.048
10-YEAR	12.21/0.877	10.15/0.835	-2.06/-0.042
50-YEAR	18.18/1.377	15.22/1.305	-2.96/-0.072

**Table 1.2: Peak Flow Rates (CFS)/Volume (AF) to OBSERVATION POINT 2 (OP2)**

STORM FREQUENCY	PRE-DEV. RUNOFF (CFS/AF)	POST-DEV. RUNOFF (CFS/AF)	CHANGE (CFS/AF)
2-YEAR	0.66/0.044	0.37/0.024	-0.29/-0.02
10-YEAR	1.01/0.069	0.60/0.040	-0.41/-0.029
50-YEAR	1.53/0.107	0.93/0.064	-0.6 /0.043

FAIRFIELD'S KIA  
440 Winchester St. – Keene NH  
Storm Water Management System  
Inspection and Maintenance Manual

---

**Introduction**

The operation and maintenance of a storm water management system and its individual components is as critical to system performance as the design. Without proper maintenance, best management practices (BMPs) are likely to become functionally impaired or to fail, providing reduced or no treatment of storm water. Proper operation and maintenance will ensure that the storm water system and individual BMPs will remain effective at removing pollutants as designed and meeting New Hampshire's water quality objectives. Proper maintenance will:

- Maintain the volume of storm water treated over the long term;
- Sustain the pollutant removal efficiency of the BMP;
- Reduce the risk of re-suspending sediment and other pollutants captured by the BMP;
- Prevent structural deterioration of the BMP and minimize the need for expensive repairs;
- Decrease the potential for failure of the BMP.

**Responsible Maintenance Party:**

Owner/Applicant:      Fairfield's Kia  
                                 440 Winchester Street  
                                 Keene, NH 03431

**Report Information:**

- Christian McCauley will be the entity responsible for implementing the required reporting, inspection, and maintenance activities identified in the I & M manual.
- Inspection and maintenance reports shall be completed after each inspection. Copies of the report forms to be completed by the inspector are attached at the end of this manual, including:
  - Inspection checklist to be used during each inspection;
  - Inspection and maintenance logs to document each inspection and maintenance activity;
- A plan showing the locations of all the storm water practices described in the I & M manual is attached at the end of this manual.

## **Maintenance Recommendations for Best Management Practices:**

The following recommendations are to be used as a guide for the inspection and maintenance of the permanent erosion and sediment control measures.

We recommend that inspections be performed every couple of weeks and after larger storm events within the first year following construction to ensure that the site remains stabilized (site and slopes).

### **Sediment Forebays & Culverts**

- Remove debris and leaf litter at least once a year. Ensure outlet areas can drain freely.
- Remove any wooded vegetation growth within the forebay and basin areas.
- Inspect outlets of culverts and ensure the culvert is not blocked.

### **Drainage Catch Basins**

- Inspect basins at least semi-annually.
- Vacuum the sediment basins when the sediment reaches one-half the depth from the bottom of the catch basin to the invert of the outlet pipe.
- Repair damaged basin grates immediately after the inspection.
- Repair pavement damage around the basins immediately after the inspection to prevent further damage to the structure or paved area.
- Dispose of sediments and other wastes in conformance with applicable local, state and federal regulations.

### **Outlet Protection - Riprap Aprons**

- Inspect the outlet protection annually for damage and deterioration. Repair damages immediately.
- Remove debris from apron area.

### **Porous Asphalt**

- Inspect the clean outs to ensure there is no debris or sediment build up.
- Vacuum the asphalt once a year; periodically leaf blow off the surface (No salting).
- Refer to checklist at end of report for more instruction.
- Also refer to attached sheets following this report.



## **Inspection Checklist /Maintenance Logs**

The inspection checklist and maintenance logs following this report shall be used as a guide for the inspection reporting for this project.

## **Inspection Checklist**

- Sediment Forebays and culverts
- Treatment & Conveyance Swales
- Drainage Catch Basins
- Riprap Aprons at Headwall Outlets and Spillways
- Headwall Inlets and Outlets
- Porous Pavement

Fairfield's Kia  
 Winchester Street - Keene, NH  
 Storm Water Management System: Inspection and Maintenance Manual

Inspection and Maintenance Log					
	BMP	Inspection Date	Inspected By	Maintenance Required?	Maintenance Performed
1				<input type="checkbox"/> Yes <input type="checkbox"/> No	
2				<input type="checkbox"/> Yes <input type="checkbox"/> No	
3				<input type="checkbox"/> Yes <input type="checkbox"/> No	
4				<input type="checkbox"/> Yes <input type="checkbox"/> No	
5				<input type="checkbox"/> Yes <input type="checkbox"/> No	
6				<input type="checkbox"/> Yes <input type="checkbox"/> No	
7				<input type="checkbox"/> Yes <input type="checkbox"/> No	
8				<input type="checkbox"/> Yes <input type="checkbox"/> No	
9				<input type="checkbox"/> Yes <input type="checkbox"/> No	

## CHECKLIST FOR INSPECTION OF POROUS PAVEMENTS

Location:

Inspector:

Date:

Time:

Site Conditions:

Date Since Last Rain Event:

Inspection Items	Satisfactory (S) or Unsatisfactory (U)		Comments/Corrective Action
<b>1. Salt / Deicing *Note complete winter maintenance guidance is available at UNHSC</b>			
Use salt only for ice management	S	U	
Piles of accumulated salt removed in spring	S	U	
<b>2. Debris Cleanup (2-4 times a year minimum, Spring &amp; Fall)</b>			
Clean porous pavement to remove sediment and organic debris on the pavement surface via vacuum street sweeper.	S	U	
Adjacent non porous pavement vacuumed	S	U	
Clean catch basins (if available)	S	U	
<b>3. Controlling Run-On (2-4 times a year)</b>			
Adjacent vegetated areas show no signs of erosion and run-on to porous pavement	S	U	
<b>4. Outlet / Catch Basin Inspection (if available) (2 times a year, After large storm events)</b>			
No evidence of blockage	S	U	
Good condition, no need for cleaning/repair	S	U	
<b>5. Poorly Drained Pavement (2-4 times a year)</b>			
Pavement has been pressure washed and vacuumed	S	U	
<b>6. Pavement Condition (2-4 times a year minimum, Spring &amp; Fall)</b>			
No evidence of deterioration	S	U	
No cuts from utilities visible	S	U	
No evidence of improper design load applied	S	U	
<b>7. Signage / Stockpiling (As Needed)</b>			
Proper signage posted indicating usage for traffic load	S	U	
No stockpiling of materials and no seal coating	S	U	

Corrective Action Needed	Due Date
1.	
2.	
3.	

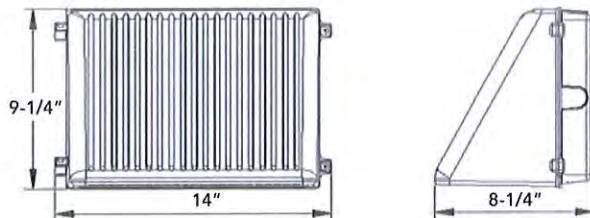


# LWP2 SERIES Outdoor Lighting

LWP2-50WW  
LWP2-50NW  
LWP2-50CW



Customer Name: \_\_\_\_\_  
Project Name: \_\_\_\_\_  
Note: \_\_\_\_\_ | Type: \_\_\_\_\_



Selection:



14" (W) x 9 1/4" (H) x 8 1/4" (D)

Ideal for general site lighting, alleys, loading docks, doorway, pathway, and parking areas.

## Features

- High Light Transmittance, Anti-UV and Fire Resistant PC Lens
- LED Type/Brand: SMD 3030/ Philips Lumileds LEDs
- Four 1/2" Knockouts: 1 Top, 1 on Each Side, 1 Back
- Die-Cast Aluminum With Powder Coat Finish (Dark Bronze)
- 7-Year Warranty

## Technical Specifications

### Electrical:

- Voltage: 120-277VAC 60Hz
- Wattage: 50W
- Power Factor: >0.95
- Efficacy: 100 / 110 LPW

### Mechanical:

- Die-Cast Aluminum With Powder Coat Finish (Dark Bronze)
- High Light Transmittance, Anti-UV and Fire Resistant Shatterproof PC Lens
- Rated life: 70,000 Hrs
- Operating Temperature: -4°F -104°F
- Four 1/2" Knockouts: 1 Top, 1 on Each Side, 1 Back
- Wet Location
- 7-Year Warranty
- Full Cutoff, Dark sky Compliant
- IP65

### Lighting:

- LED Type/Brand: SMD 3030/ Lumileds LEDs
- The Type II Distribution Is Used for Wide Walkways, on Ramps and Entrance Roadways, As Well As Other Long, Narrow Lighting
- Total Lumens: 5000LM/ 5500LM
- Color Temperatures: 3000K/ 4000K/ 5000K
- Color Rendering Index: CRI ≥ 80
- Beam angle: 80° x 100°

### Applications:

- General site Lighting
- Alleys
- Loading docks
- Doorways
- Pathways
- Parking areas

### Other Models:

- 80W | 8000LM | LWP2-80WW
- 80W | 9000LM | LWP2-80NW
- 80W | 9000LM | LWP2-80CW







# V-Locity Small (VALS)

## Outdoor LED Area Light



### OVERVIEW

Lumen Package	6,000 - 27,000
Wattage Range	36 - 178
Efficacy Range (LPW)	142 - 171
Weight lbs(kg)	20 (9.1)
Control Options	IMSBT, ALB, ALS, 7-Pin, PCI

### QUICK LINKS

## FEATURES & SPECIFICATIONS

### Construction

- Rugged die-cast aluminum housing contains factory prewired driver and optical units. Cast aluminum wiring access door located underneath.
- Self-contained optic, board and heat sink assembly can be rotated or replaced in the field.
- Fixtures are finished with LSI's DuraGrip® polyester powder coat finishing process. The DuraGrip finish withstands extreme weather changes without cracking or peeling. Other standard LSI finishes available. Consult factory.
- Shipping weight: TBD lbs in carton.

### Optical System

- State-of-the-Art acrylic optics delivers industry leading optical control with an integrated gasket to provide IP66 rated seal.
- Proprietary refractor optics provide exceptional coverage and uniformity in distribution types 2, 3M, 3W, 4M, 4W, 4F, FTM, 5QN, 5QM, 5Q, 5QW, AM, WF and LC/RC.
- Available in 5000K, 4000K, 3500K, 3000K and 2700K color temperatures per ANSI C78.377 as well as phosphor converted amber.
- Minimum CRI of 80 (optional 70 CRI for 5000K and 4000K).
- Factory or field installable integral shielding available for enhanced spill light control.
- Zero Uplight (excludes adjustable arms).

### Electrical

- High-performance driver features over-voltage, under voltage, short-circuit and over temperature protection.
- 0-10 volt dimming (10% - 100%) standard, must specify EXT option for dimming leads to be extended to fixture exterior.
- Standard Universal Voltage (120-277 VAC) Input 50/60 Hz or optional High Voltage (347-480 VAC).
- L70 Calculated Life: >60k Hours
- Total harmonic distortion: <20%
- Operating temperature: -40°C to +50°C (-40°F to +122°F).
- Power factor: >.90
- Input power stays constant over life.
- Field replaceable 10kV surge protection device meets a minimum Category C Low operation (per ANSI/IEEE C62.41.2).
- High-efficacy LEDs mounted to metal-core circuit board to maximize heat dissipation
- Driver is fully encased in potting material for moisture resistance and complies with FCC standards. Driver and key electronic components can easily be accessed.

### Controls

- Optional integral passive infrared Bluetooth™ motion and photocell sensor. Fixtures operate independently and can be commissioned via iOS or Android configuration app.
- LSI's AirLink™ wireless control system options reduce energy and maintenance costs while optimizing light quality 24/7.

### Installation

- REDiMount pole mount arm allows for a true one person installation.
- Side arm pole mount designed to mount to square or round poles.
- Pole mount arms can accommodate pole drill patterns from 2.4 to 5" on center and utilize LSI's reduced B3 or traditional B5 drill patterns.
- Additional mounting options are available including a mast arm or adjustable slip fitter that allow for luminaire attachment to a 2 3/8" tenon or mast arm.
- Adjustable arms allow for 65° of tilt (-5° below horizontal to +60° above horizontal) in 5° increments.

### Warranty

- LSI luminaires carry a 5-year limited warranty. Refer to <https://www.lsicorp.com/resources/terms-conditions-warranty/> for more information.

### Listings

- Listed to UL 1598 and UL 8750.
- Meets Buy American Act requirements.
- DarkSky approved with 3000K or warmer color temperature selection (side arm pole and mast arm mounting only).
- Title 24 Compliant; see local ordinance for qualification information.
- Suitable for wet locations.
- IP66 rated Luminaire per IEC 60598-1.
- 3G rated for ANSI C136.31 high vibration applications are qualified.

