



City of Keene Planning Board

AGENDA

Monday, November 22, 2021

6:30 PM

City Hall, 2nd Floor Council Chambers

Note: *The public may join the meeting online or at City Hall in the 2nd Floor Council Chambers. To access the meeting online, visit www.zoom.us/join or call (646) 558-8656 and enter the Meeting ID: 857 8338 6440.**

- I. **Call to Order** – Roll Call
- II. **Minutes of Previous Meeting** – October 25, 2021
- III. **Public Hearing**

SPR-16-14, Modification 7 – Site Plan Review – Mint Carwash Site Modifications, 433 & 435 Winchester St – Applicant and owner MOC76 Realty Co. LLC proposes to modify the Mint Carwash site located at 435 Winchester St (TMP #115-029-00), 433 Winchester St (TMP #115-030-000), and 0 Wetmore St (TMP #115-031-000) by reconfiguring the parking and reducing the width of the Wetmore St curb cut for the former Ocean Harvest Restaurant. A waiver is requested from Sec. 20.14.3.D of the Land Use Code to allow for parking with vacuum stations in front of the building. These combined parcels are 1.33 acres in size and are located in the Industrial, Commerce, and High Density Districts.

SPR-897, Modification 1 & SWP-CUP-04-21 – Site Plan Review & Surface Water Protection Conditional Use Permit – U-Haul of South Keene – Applicant Fieldstone Land Consultants, on behalf of owner Amerco Real Estate Company, proposes to renovate two existing buildings, construct a 12,175 sf building, and install 28,900 sf of prefabricated storage units on the properties located at 472 Winchester St (TMP #115-020-00) and 0 Krif Rd (115-019-000). A Conditional Use Permit is requested to install pavement within the Surface Water Protection buffer. An exception to the City's Street Access standards is requested to increase the width of an existing street access to be 45 ft at the property line and 66 ft at the curblin. These combined properties are 7.51 acres in size and are located in the Commerce Limited District.

SPR-972, Modification 7 – Site Plan Review – Colony Mill Landscaping, 210-222 West Street – Applicant and owner Brady Sullivan Keene Properties LLC proposes to modify the landscaping for the new retail building on the property located at 210-222 West St (TMP# 576-009-000), including the removal of existing trees along Island St. The site is 6.49 acres and is located in the Commerce District.

IV. **Community Development Director Report**

V. **New Business**

VI. **Upcoming Dates of Interest – December 2021**

- Joint Committee of the Planning Board and PLD – December 13, 6:30 PM
- Planning Board Steering Committee – December 7, 11:00 AM
- Planning Board Site Visit – December 15, 8:00 AM – To Be Confirmed
- **Planning Board Meeting – December 20, 6:30 PM**

**A Zoom link and call in information is being provided as a public service; however, the public body will be meeting in person with a quorum present at the location, date and time contained in this notice. If for some reason the Zoom link or call in does not work, the public meeting will continue in person.*

1 **City of Keene**
2 **New Hampshire**

3
4
5 **PLANNING BOARD**
6 **MEETING MINUTES**
7

8 **Monday, October 25, 2021**

6:30 PM

**Council Chambers,
City Hall**

Members Present:

David Orgaz, Vice Chair
Mayor George S. Hansel
Councilor Michael Remy
Andrew Weglinski
Harold Farrington, Alternate

Staff Present:

Rhett Lamb, Community Development
Director/Assistant City Manager
Tara Kessler, Senior Planner

Members Not Present:

Pamela Russell Slack, Chair
Roberta Mastrogiovanni
Emily Lavigne-Bernier
Gail Somers
9 Tammy Adams, Alternate

10
11 **I) Call to Order – Roll Call**

12
13 Vice-Chair Orgaz called the meeting to order at 6:30 PM and a roll call was taken.
14

15 **II) Minutes of Previous Meeting – September 27, 2021**

16
17 A motion was made by Mayor George Hansel to approve the September 27, 2021 meeting
18 minutes. The motion was seconded by Councilor Michael Remy and was unanimously approved.
19

20 **III) Public Hearing**

21 **SWP-CUP-03-21 – Surface Water Protection Conditional Use Permit – W110 Distribution**
22 **Line Pole Replacement**– Applicant GZA GeoEnvironmental, on behalf of owner Public Service
23 Company of New Hampshire (dba Eversource Energy), proposes to replace 2 existing wood
24 utility structures along the W110 Distribution Line with 2 new steel structures. This project will
25 temporarily impact 4,764 sf of the Surface Water Protection Buffer for work pad placement and
26 the creation/improvement of access roads.
27

28 **A. Board Determination of Completeness**

29 Senior Planner Tara Kessler recommended accepting this application as complete.
30

31 A motion was made by Mayor George Hansel to recommend that the Board accept application
32 SWP-CUP-03-21 as complete. The motion was seconded by Councilor Michael Remy and was
33 unanimously approved.
34

35 B. Public Hearing

36 Lindsey White of GZA GeoEnvironmental addressed the Board first. Ms. White stated that
37 Eversource was proposing to replace poles 178 and 179 on the W110 Distribution Line with two
38 new steel structures. The W110 Distribution Line is located off Chesterfield Road near the town
39 line between Keene and Chesterfield. During inspection, it was determined that these poles had
40 defects and needed to be replaced to maintain safety and reliability. Ms. White explained that
41 these changes will have an impact on the Surface Water Protection Overlay District; however,
42 there is the need to access the poles and to construct a work pad around the poles to stage
43 equipment.
44

45 Ms. White referred to the orange hatched area on the plan, which delineates areas of impact
46 within the surface water buffer, and the yellow hatched area, which shows areas of wetland
47 impacts from the proposed work. Timber matting will be placed in the impacted wetlands. Once
48 the work is completed, the area will be seeded and mulched. The upland buffer area will also be
49 restored to prior conditions by seeding and mulching the affected areas. This item was raised at
50 the Conservation Commission meeting, where it was suggested a pollinator friendly seed mix be
51 used. The applicant is in agreement with this suggestion. Ms. White stated that they have
52 submitted an NHDES Wetlands Application, which was approved. This concluded the
53 applicant's presentation.
54

55 Mr. Weglinski asked for the duration of the activity. Ashley Ruprecht from Eversource stated
56 that the work should last about three days and explained that the restoration of the site will
57 happen in the spring.
58

59 Mr. Farrington asked when Eversource thinks this area will need to be disturbed again in the
60 future to replace additional poles or lines. Ms. Ruprecht stated they don't have any future plans
61 at this time to revisit this area or to increase capacity.
62

63 Staff comments were next. Senior Planner Tara Kessler addressed the committee and noted that
64 the applicant has addressed the Conditional Use Permit criteria fairly well. She noted the
65 suggestion from the Conservation Commission regarding the use of a pollinator-friendly seed
66 mix for remediation and confirmed that they had received the list from Ms. White
67

68 The Chairman asked for public comment next. With no comments from the public, the Chairman
69 closed the public hearing.
70

71 C. Board Discussion and Action

72 A motion was made by Mayor George Hansel to approve Conditional Use Permit SWP-CUP-03-
73 21 as shown on the plan set identified as "W110 Distribution Line Structure Replacement
74 Project, Keene New Hampshire" prepared by GZA GeoEnvironmental Inc. at varying scales and
75 dated September 9, 2021, with the following conditions:
76

- 77 A. Applicant submits to the Community Development Department documentation of all
78 necessary permits and approvals from local, state and federal agencies, including but not
79 limited to:
- 80 i. An approved Wetlands Permit from the NH Department of Environmental
81 Services;
 - 82 ii. Temporary driveway permits for each access point from a public right of-way
83 from the City Public Works Department;
 - 84 iii. Excavation permits for any ground disturbance within the public right of-way;
85 and,
 - 86 iv. Encumbrance permits for any vehicles, equipment or materials to be staged or
87 stored within the public right-of-way.
- 88 B. Applicant shall remediate all disturbed areas with vegetation that includes pollinator
89 friendly plant species. A list of plant species used for remediation shall be submitted to
90 the Community Development Department in advance of planting.

91
92 The motion was seconded by Councilor Michael Remy and was unanimously approved.
93

94 **S-08-21 – 3-Lot Subdivision – 120 Stearns Rd** – Applicant Michael Ploof of Fieldstone Land
95 Consultants, PLLC, on behalf of owner Megan Burke Kidder, proposes to subdivide the existing
96 76-acre parcel at 120 Stearns Rd (TMP# 236-023-000) into three lots. The proposed subdivision
97 would result in a 41.9-acre lot, a 12-acre lot, and a 22.1-acre lot, all of which would share a
98 common driveway. The site is located in the Rural District. The applicant is requesting an
99 exception from Section 22.5.4.A.5 of the City’s Street Access Standards to permit 4 dwellings to
100 share a common driveway where no more 2 dwellings are permitted to share a common
101 driveway.

102
103 A. Board Determination of Completeness
104 Senior Planner Tara Kessler recommended accepting this application as complete.
105

106 A motion was made by Mayor George Hansel to recommend that the Board accept application S-
107 08-21 as complete. The motion was seconded by Councilor Michael Remy and was unanimously
108 approved.
109

110 B. Public Hearing
111 Mr. John Noonan of Fieldstone Land Consultants addressed the Board on behalf of the owner,
112 Megan Burke Kidder. Mr. Noonan stated this applicant is looking to subdivide a 76 acre lot into
113 three lots. All three lots will have frontage along Stearns Road. Following the subdivision, the
114 parent parcel will be 41.9 acres and will have 215 ft. of frontage along Stearns Road. Proposed
115 Lot #1 will be 12 acres in size with 305 feet of frontage on Stearns Road. Lot #2 will be 22.1
116 acres in size with 1,494 feet of frontage on Stearns Road. A total of 393 feet of this frontage will
117 be along the Class V section of Stearns Road. He explained that they are requesting an exception
118 from Section 22.5.4.A.5 of the City’s Street Access Standards, so that all 4 dwellings can utilize
119 the existing driveway that comes off Chesterfield Road to access their property. An easement
120 will be created to allow for access onto the lots.
121

122 The existing driveway is 24 feet in width and accesses the residential lot on 361 Chesterfield
123 Road and the parcel at 120 Stearns Road. The reason for requesting access off Chesterfield Road
124 is due to the fact that the other section of the road is quite steep, because Grimes Brook runs
125 parallel to Stearns Road, and because there are also jurisdictional wetlands that surround this
126 area. The Applicant explained that allowing the shared driveway would eliminate impacts to
127 wetlands and would allow the future homeowners to avoid steep slopes.
128

129 Mr. Noonan explained that a conceptual driveway plan and profile have been submitted. The
130 house for Lot #1 is in the process of being designed. There is no buyer for Lot #2 as of yet. The
131 proposed plans show a potential house and driveway location on Lot #2; however, the house
132 could be relocated. The plans for both homes show a 4,000 square foot area with test pit
133 information, along with a 75 foot well radius.
134

135 He further stated to provide access to all four proposed houses, there will be a hammer head
136 turnaround for fire and emergency access. The driveway will be built to City road standards. The
137 two extension driveways will be ten feet wide and will start at Chesterfield Road. Mr. Noonan
138 stated that these are all conceptual plans that show that any future driveways on these properties
139 can meet the City's width and slope standards. He went on to say that when plans and permits
140 are submitted for these lots, a driveway permit will also be submitted at that the same time. This
141 concluded the presentation from Mr. Noonan.
142

143 Mayor Hansel clarified that the ten foot wide sections of driveway are not serving multiple
144 houses. Mr. Noonan stated there is an area of overlap for Lot #1 and Lot #2.
145

146 Councilor Remy asked whether easements would be placed on each of these properties or just on
147 the Chesterfield Street property. Mr. Noonan stated Lot #2 would end up with an easement on
148 Lot #1, as well as on the lot located at 120 Stearns Road (TMP# 236-023-000). He referred to the
149 two adjoining lots owned by Megan Burke Kidder at 120 Stearns Road and 361 Chesterfield
150 Road, which don't have an easement; however, the two lots being created would need an
151 easement allowing them to access their residences through her lot.
152

153 Mr. Weglinski asked whether any portion of this land is maintained by the City. Mr. Noonan
154 answered in the negative. It will be a private driveway servicing four lots.
155

156 Councilor Remy asked if there was a way to access each property from Stearns Road, if this is
157 not approved for a driveway permit. Mr. Noonan stated it could be done with a wetland permit
158 for Lot #1 and a bridge construction.
159

160 Staff comments were next.
161

162 Ms. Kessler stated that the subdivision standards she would like to focus on are traffic and access
163 management. Ms. Kessler noted that the parcel proposed to be subdivided has a common
164 driveway that comes off Chesterfield Road and serves a single family home off Chesterfield
165 Road and the home at 120 Stearns Road. The applicant is requesting a waiver from the City's
166 Public Improvement Standards, specifically the Street Access Standards, which specify that only
167 two dwellings can share a common driveway. This proposal is to allow four dwellings to share a

168 common driveway. She explained that the existing driveway is long and does meet the long
169 driveway definition in that it exceeds 300 feet; however, it does not meet all other long driveway
170 requirements outlined in our current code.

171
172 Ms. Kessler explained that the City Engineer has reviewed the application and is not requiring
173 anything be done to the existing portion of the driveway to bring it up to current standards;
174 however, any new sections added to the existing driveway will need to meet the requirements of
175 the long driveway standard. She explained that the new sections of the driveway can be no more
176 than 10 feet wide and that there need to be turnarounds for emergency vehicles. The City
177 Engineer also noted that if the exception is granted, new Chesterfield Road addresses will need
178 to be issued for the two new lots. Ms. Kessler noted that a new driveway concept plan would also
179 need to be submitted and should show that the new sections of driveway are 10 feet wide or
180 smaller in width. She explained that when a driveway permit is submitted, an easement stating
181 that the four properties can utilize a common driveway will also need to be submitted.

182
183 Ms. Kessler called the Board's attention to the driveway exception criteria included in the
184 Board's packet, which are as follows:

185 *A. Issuance of the exception will not adversely affect the safety of pedestrians, bicyclists, and*
186 *vehicles using adjacent streets and intersections.*

187 *B. Issuance of the exception does not adversely affect the efficiency and capacity of the street or*
188 *intersection.*

189 *C. There are unique characteristics of the land or property which present a physical hardship to*
190 *the requestor.*

191 *D. In no case shall financial hardship be used to justify the granting of the exception.*

192
193 Ms. Kessler noted there are significant steep slopes close to the Stearns Road portion of the
194 property shown on the existing conditions plan, as well as some very large wetland complexes
195 that any new driveways would have to cross to access these two new lots. She added that this is
196 something the Board should take into consideration tonight. If the Board chooses not to grant the
197 exception, then they will want ensure that the applicant can demonstrate there is the possibility
198 for access to the properties from Stearns Road. Ms. Kessler explained that part of the subdivision
199 review process is to ensure that lots are developable and that they can be accessed for future
200 development. This concluded staff comments.

201
202 Mr. Farrington asked for the requirement for the construction of a City Street. Mr. Lamb stated
203 that there is no specific road standard, what exists here is a Class V street on Stearns Road. Even
204 though it is not going to be used for their actual driveway, the subdivision will create adequate
205 frontage on an existing street. In this case, two new lots are being created on an existing Class V
206 street. He explained that the applicant has to demonstrate access to the lots from Stearns Road;
207 however, they do not necessarily have to utilize that frontage to access the parcels and can use
208 another way to access the lots.

209
210 He went on to say this applicant wants to construct two homes on the hill near the existing
211 residence; however, if they decided not to construct the homes in this location, we would need to
212 know if they could build two homes closer to the Stearns Road frontage without the need to cross
213 wetlands and the stream. Mr. Noonan stated that there is room, but there is the issue with the

214 wetlands and setbacks. He explained that in that scenario, they would most likely utilize the
215 existing driveway as a shared driveway that would allow for access to Lot #1. Mr. Lamb stated
216 that, in his opinion, this demonstrates that there is adequate road frontage for the two new lots on
217 Stearns Road and that the applicant can choose to gain access to the properties from a different
218 access point.

219
220 The Vice-Chair then asked for public comment.

221
222 Ms. Megan Burke Kidder addressed the Board. Ms. Burke Kidder stated that what they refer to
223 as the “old driveway” can be treacherous at times and is not safe to drive on during the winter.
224 Ms. Burke Kidder noted she is not planning on selling any of these lots – they are going to be
225 kept in the family. She noted that the house that is going to be built on Lot #1 will be handicap
226 accessible. She further explained that a small cabin will be built on Lot #2 for use by her family.

227
228 Mr. Gary Johnson of 132 Arch Street was the next speaker. Mr. Johnson stated his father owns
229 the abutting property and felt that using the existing driveway would be better than using Stearns
230 Road to access this property. He explained that the existing driveway is wider and straighter than
231 Stearns Road. He felt that using the existing driveway would reduce traffic congestion.

232
233 Ms. Burke Kidder noted that there are several other properties with shared driveways that allow
234 access to more than two homes. She stated that there is a property at the beginning of Stearns
235 Road that utilizes a shared private driveway to access three properties. Mr. Lamb confirmed what
236 the applicant stated, but noted that this is not a common practice in the City, even though there
237 are some other instances where such driveways exist.

238
239 With no further public comment, the Vice-Chair closed the public hearing.

240
241 C. Board Discussion and Action

242 Mayor Hansel stated felt this subdivision seems straightforward and noted that they have the
243 street frontage as required by Code. He also stated that they meet the criteria to grant the
244 exception. Councilor Remy echoed the Mayor’s comments and added he is also happy there is
245 buildable space next to Stearns Road, in the circumstance that it is needed in the future.

246
247 A motion was made by Mayor George Hansel that the Planning Board approve the request for an
248 exemption from Section 22.5.4.A.5 of the Land Development Code to permit a common street
249 access to serve 4 dwellings, and to approve S-08-21 for a 3-lot subdivision of the parcel located
250 at 120 Stearns Rd (TMP# 236-023-000), as shown on the plans identified as “Subdivision Plan –
251 South” and “Subdivision Plan – North” prepared by Fieldstone Land Consultants PLLC at a
252 scale of 1” = 100’, dated September 16, 2021 and last revised on October 8, 2021 with the
253 following conditions:

- 254 1. Prior to signature by Planning Board Chair:
- 255 A. Owner’s signature appear on plan.
 - 256 B. Submission of a revised driveway concept plan showing the portion of new driveway
 - 257 to be no wider than 10-feet.
- 258 2. Prior to installation of new driveways:
- 259 A. Approval from the City Engineer for a street Access Permit.

260 B. Submission of a recorded driveway easement for the four properties sharing the
261 common street access, which should include a description of the responsibilities for
262 ongoing maintenance and dispute resolution of the driveway.
263

264 The motion was seconded by Councilor Remy and was unanimously approved.
265

266 **IV) Community Development Director Report**
267

268 Minor Project Review

269 Ms. Kessler addressed the Committee and noted that the Minor Project Review Committee is
270 an administrative item. She indicated in August, the Board appointed members to this
271 committee. However, with the upcoming staff changes, there is a new roster to make sure the
272 committee is properly staffed. Ms. Kessler stated this is her last meeting with the City of
273 Keene, as she is resigning in the next few weeks. At this time, there is no one assigned to
274 replace her on the committee, but someone should be appointed soon. Ms. Kessler noted that
275 with the Community Development Director's upcoming retirement, Mari Brunner is set to
276 replace him as Chair of the committee.
277

278 Mayor Hansel expressed his appreciation to both Ms. Kessler and Mr. Lamb for all they've
279 done for the City.
280

281 A motion was made Mayor George Hansel that the following individuals serve as regular and
282 alternate members of the Minor Project Review Committee.
283

284 John Rogers – Building Health Official; Mari Brunner, Planner; Don Lussier, City Engineer;
285 Lt. John Bates, Fire Prevention Officer; Med Kopczynski, Economic Development Director.
286 Alternates: Michael Hagan, Plans Examiner; Kurt Blomquist, Public Works Director.
287

288 The motion was seconded by Councilor Michael Remy and was unanimously approved.
289

290 Mr. Lamb expressed his appreciation working for the City for the past 26 years. Mr. Lamb
291 recognized Ms. Kessler for everything she has done for the city and the department.
292

293 **V) New Business**

294 None

295 **VI) Upcoming Dates of Interest – November 2021**

- 296 • Joint Committee of the Planning Board and PLD – November 8, 6:30 PM
 - 297 • Planning Board Steering Committee – November 9, 11:00 AM
 - 298 • Planning Board Site Visit – November 17, 8:00 AM – To Be Confirmed
 - 299 • Planning Board Meeting – November 22, 6:30 PM
- 300

301 There being no further business, the Vice-Chair adjourned the meeting at 7:20 PM.
302

303 Respectfully submitted by,

304 Krishni Pahl, Minute Taker
305
306 Reviewed and edited by,
307 Megan Fortson, Planning Technician

STAFF REPORT

SPR-16-14, MODIFICATION #7 — SITE PLAN REVIEW – MINT CARWASH – 433 & 435 WINCHESTER STREET

Request:

Applicant and owner MOC76 Realty Co. LLC proposes to modify the Mint Carwash site located at 435 Winchester St (TMP #115-029-00), 433 Winchester St (TMP #115-030-000), and 0 Wetmore St (TMP #115-031-000) by reconfiguring the parking and reducing the width of the Wetmore St curb cut for the former Ocean Harvest Restaurant. A waiver is requested from Sec. 20.14.3.D of the Land Use Code to allow for parking with vacuum stations in front of the building. These combined parcels are 1.33 acres in size and are located in the Industrial, Commerce, and High Density Districts.

Background:

The 435 Winchester Street property is the site of Mint Carwash, a two-bay drive-in carwash facility with street access on both Winchester Street and Wetmore Street. It is 0.99 acres and is located in the Industrial District. This property received a variance from the Zoning Board of Adjustment to allow for a carwash facility in the Industrial District in August 2014, and site plan approval from the Planning Board in October 2014. As part of the site plan approval, the Planning Board granted a waiver to allow for four parking spaces with vacuum stations in front of the building.



Figure 1. Image of the Mint Carwash building located at 435 Winchester Street.

The property located at 433 Winchester Street is the former location of the Ocean Harvest Restaurant. It is 0.22 acres in size and is located in the Commerce District. The property directly to the west, 0 Wetmore Street, is the location of a section of the parking lot for the former restaurant. This property is 0.12 acres in size and is located in the High Density District.



Figure 2. Image of the former Ocean Harvest Restaurant as viewed from Winchester Street.

The owner of the 435 Winchester Street property purchased the properties located at 433 Winchester Street and 0 Wetmore Street (former Ocean Harvest Restaurant site) with the intention of constructing a second carwash tunnel; however, this application was withdrawn after the ZBA denied a request for a variance to allow a carwash tunnel to be partially located within the High Density District.

All three of these properties are located within the 100-year floodplain. Surrounding land uses on Winchester Street are mostly commercial or industrial; however, there are adjacent residential

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uses along Wetmore Street directly to the west. Combined, these properties are 1.33 acres in size.

The Ocean Harvest Restaurant has been demolished. The Applicant proposes to use the former restaurant parking lot for employee parking for the Mint Carwash site, narrow the Wetmore Street access point for the former Ocean Harvest Restaurant from approximately 100 ft to 40 ft, move the existing vacuum stations and associated parking spaces from their current location directly in front of the carwash building to the new parking area on the corner of Winchester and Wetmore Street, and install one additional parking space along Winchester Street. Landscaping is proposed along Winchester Street to screen these parking spaces from view. A waiver is requested from Section 20.14.3, subsection D to allow for parking in front of the building.

Completeness:

The Applicant requests exemptions from providing a traffic report, drainage report, soils report, and architectural elevations. Staff recommend that the Board grant the requested exemptions and accept the application as “complete.”

Departmental Comments:

- **Code Enforcement:** Please be aware that a Floodplain Development Permit will be required.

Application Analysis: The following is a review of the Planning Board development standards relevant to this application.

- 20.2 **Drainage and Stormwater Management:** The Applicant has requested an exemption from providing a Drainage Report, noting that the total impervious cover on the site is being reduced from 28,609 sf to 26,599 sf. The Applicant proposes to remove the western section of the parking lot for the former Ocean Harvest Restaurant and create a compensatory storage area in this location to offset the filling of the former restaurant basement within the 100-year floodplain. In addition, the Applicant proposes to install an infiltration basin along Winchester Street on the east side of the site. No other changes to the existing drainage pattern are proposed. This standard appears to be met.
- 20.3 **Sediment & Erosion Control:** The Applicant proposes to install silt fencing along the perimeter of areas to be disturbed as shown on the Grading and Erosion Control Plan (Sheet C3). In addition, the Grading and Erosion Control Plan identifies various erosion and sediment control measures that will be followed during construction, including measures to stabilize any disturbed areas during winter construction (see “Erosion Control Sequence” and “Winter Construction Notes” on plan). This standard appears to be met.
- 20.4 **Snow Storage & Removal:** Proposed snow storage areas are shown on the site plan in several locations, including to the west and south of the carwash tunnel queuing area, the northwest side of the Wetmore Street driveway, and in the compensatory storage area located to the west of the parking lot. In the project narrative, the Applicant states that any excess snow will be hauled off site if necessary. Staff recommend that the removal of snow storage from the compensatory flood storage areas be a condition of approval, and that a note be added to the plan which states that the owner is responsible for hauling any excess snow off site.

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- 20.5 Landscaping: The applicant proposes to screen the parking and vacuum stations from Winchester Street with 15 shrubs, including 12 Casino gold juniper plants (mature height 3 ft) and three RP purple pavement rose plants (mature height 2.5 ft.). In addition, one Snow drift flowering crabapple tree is proposed near the corner of Winchester and Wetmore Street.



Figure 3. Google street view image of the former Ocean Harvest Restaurant, as viewed from Wetmore Street looking south. This area of the site has been paved over and the Applicant proposes to use it for parking spaces with vacuum stations.

Section 9.4.4 of the Land Development Code, Parking Lot Screening, states *“If an on-site parking lot is visible from the public right-of-way and/or is located adjacent to a residential zoning district, a perimeter landscape area shall be established along the full length of the edge(s) of the parking lot that is adjacent to the public right(s)-of-way and to parcels located in a residential zoning district, with the exception of areas designed for pedestrian and vehicular access into the parking lot.”* This section further states that *“A minimum of 1 shade tree shall be provided for every 30 lf of perimeter parking lot landscape area.”* The Planning Board may approve an alternative design for screening if the proposed design generally meets the intent of the parking lot screening standards.

The Board may wish to consider asking the Applicant to install shade trees or additional ornamental trees/shrubs to screen the new parking area from adjacent public rights-of-way, including Wetmore Street.

- 20.6 Screening: The proposed screening for the new section of the parking lot is described above under Landscaping. In addition, the Applicant proposes to relocate dumpster from the northwest corner of the 435 Winchester Street parcel to the northwest corner of the parking lot. This dumpster will be screened from view with a six foot tall vinyl fence, as shown in the detail provided on the Proposed Site Plan (Sheet C-2).
- 20.7 Lighting: The Applicant proposes to install two 15 ft pole-mounted lights to provide lighting for the parking lot. The proposed light fixtures are full cut-off LED lights, and the photometric plan shows that the light levels are consistent with the Planning Board’s standards for lighting. No other changes to site lighting are proposed. This standard appears to be met.
- 20.8 Sewer & Water: No changes to sewer or water are proposed as part of this project. This standard does not apply.
- 20.9 Traffic & Access Management: The current proposal to use the former Ocean Harvest Restaurant property for parking is not expected to result in an increase in traffic generation as compared to the previous use, which was a 50-seat restaurant.

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Once merged, this site will have three curb cuts, including one on Winchester Street and two on Wetmore Street. The Applicant proposes to narrow the Wetmore Street curb cut for the former Ocean Harvest Restaurant from approximately 100 ft to 40 ft, which would bring this street access closer to compliance with the City's standard for commercial driveway widths, which is 25 ft at the property line and 50 ft at the curblin. In addition, the Applicant proposes to move the vacuum stations and associated parking spaces from in front of the carwash building to the new parking lot, which would reduce conflicts between vehicles exiting the carwash and vehicles backing out of the vacuum station parking spaces. There are no sidewalks or bicycle lanes on this section of Winchester Street or Wetmore Street. This standard appears to be met.

- 20.10 Filling & Excavation: All three properties that are the subject of this proposal are located within the 100-year floodplain. The Applicant is aware that a Floodplain Development Permit will need to be obtained for the proposed work, and proposes to create a compensatory flood storage area in order to offset the filling of the former restaurant basement. This standard appears to be met.
- 20.11 Surface Waters & Wetlands: There are no surface waters or wetlands present on this site; this standard does not apply.
- 20.12 Hazardous or Toxic Materials: There are no known hazardous or toxic materials on the site. This standard does not apply.
- 20.13 Noise: The Applicant proposes to relocate the vacuum stations and associated parking spaces at the east end of the new parking area, which is the furthest point from the adjacent residential property. No other changes to site are proposed that would impact noise levels. This standard appears to be met.
- 20.14 Architecture & Visual Appearance: The Applicant requests a waiver from subsection 3.D of this standard to allow parking spaces to be located in front of the building. The waiver request is included as an attachment to this staff report.

In making a determination whether to grant the requested waiver, the Board should find that the waiver criteria listed in Section 25.12.14 of the LDC are met:

- “1. Strict conformity would pose an unnecessary hardship to the applicant and the waiver would not be contrary to the spirit and intent of the regulations; or,*
- 2. Specific circumstances relative to the site plan, or conditions of the land in such site plan, indicate that the waiver will properly carry out the spirit and intent of the regulations.*
- 3. In granting a waiver, the Planning Board may require any mitigation that is reasonable and necessary to ensure that the spirit and intent of the standard being waived will be preserved, and to ensure that no increase in adverse impacts associated with granting the waiver will occur.”*

STAFF REPORT

Recommended Motion:

If the Board is inclined to approve this request, the following language is recommended for a motion:

Grant a waiver from Section 20.14.3.D of the Land Development Code to allow for five parking spaces with vacuum stations in front of the building; and

Approve SPR-16-14 Modification #7 for modifications to the Wetmore Street curb cut and site, as presented in the plan set identified as "Proposed Two Bay Car Wash, 435 Winchester St. Keene, NH" prepared by Brickstone Land Use Consultants at a scale of 1 inch = 20 feet on September 19, 2014 and last revised on October 29, 2021 with the following conditions prior to signature by Planning Board Chair:

1. Submittal of revised plans to show the following:
 - A. Removal of snow storage from compensatory flood storage areas.
 - B. Addition of a note on the site plan which states that the owner is responsible for removing any excess snow from the site.
2. Owner's signature appears on the plan.

CITY OF KEENE | PLANNING BOARD

SITE PLAN REVIEW / MODIFICATION APPLICATION



This form must be filled out in its entirety. If a box is not checked, staff will assume that the information is not provided and the application is, therefore, not complete. Incomplete applications will not be accepted for review.

A Project Name MINT CAR WASH Mod. # 7		[REDACTED]	
Tax Map Parcel number(s) 1 1 5 - 0 2 9 - 0 0 0 - _____ 1 1 5 - 0 3 1 - 0 0 0 - _____ 1 1 5 - 0 3 0 - 0 0 0 - _____		[REDACTED]	
Project Address: 435 Winchester Street	O W N E R	PRINTED Name: MOC76 REALTY CO LLC	Signature:
Acreage/S.F. of Parcel: 1.33 AC / 58,124 SF		Address: 435 Winchester Street Keene NH 03431	Telephone\ Email:
Zoning District: Industrial / Commerce/ HD		PRINTED Name: MINT CAR WASH	Signature:
		Address: 435 Winchester Street Keene NH 03431	Telephone\ Email: 603-289-7553 - digger76@me.com
Modifications: Is this a modification to a previously-approved site plan: <input type="checkbox"/> No <input checked="" type="checkbox"/> Yes: SPR#: 16-14 Date: 10/20/14			
For those sections of the application that are not affected by the proposed modification to the previously approved site plan, you are encouraged to request exemptions in lieu of submitting required documents.			
B Descriptive Narrative Information			
<input checked="" type="checkbox"/> Type of development <input checked="" type="checkbox"/> Sedimentation Control <input checked="" type="checkbox"/> Scope/scale of development <input checked="" type="checkbox"/> Proposed uses <input checked="" type="checkbox"/> Vegetation <input checked="" type="checkbox"/> Parcel size <input checked="" type="checkbox"/> Location of access points <input checked="" type="checkbox"/> Debris management <input checked="" type="checkbox"/> Proposed stormwater, drainage & erosion plan <input checked="" type="checkbox"/> Any other descriptive information <input type="checkbox"/> Disposal proposals for boulders, stumps & debris			
C A complete application must include the following:			

**City of Keene
SITE PLAN REVIEW**

**Mint Carwash
433-435 Winchester Street
Keene, NH**

Property Owner:
MOC76 Realty Co. LLC
435 Winchester Street
Keene, NH 03431

November 08, 2021

Narrative

MOC76 Realty Co. LLC is the owner of Mint Carwash at 435 Winchester Street. They recently purchased the former Ocean Harvest Restaurant property at 433 Winchester Street and are merging the properties. The merged lots will produce a 1.33 acre lot which lies within three zoning districts, 0.12 acres are in the High Density District, 0.22 acres lies within the Commerce district, and 0.99 acres lies within the Industrial district.

The former restaurant building has been removed. The former restaurant parking area will be reconfigured to provide 4 additional parking spaces, a new dumpster pad, and 5 vacuum parking spaces as shown on the proposed plan. The western portion of the former parking area will be removed to provide flood storage to offset the filling of the restaurant basement area. The proposed plan meets the setback requirements in all three zoning districts, including the increased setback for a corner lot in the High density district. The proposed plan also complies with the permitted lot coverages for each district.

The entire property lies within the 100 year floodplain and will require a Flood Permit for construction. No net loss of flood storage is proposed.

On-site parking for 10 spaces is provided on the merged property. Access to the site will remain via two existing driveways, one on Winchester Street and one on Wetmore Street. Both driveways will remain. The existing driveway to the former restaurant from Wetmore Street will be reduced in width from approximately 100 feet to 40 feet.

An exemption is requested from providing a drainage report since there is no increase in lot coverage or stormwater runoff. With the addition of an infiltration basin at the east side of the site, there will be less stormwater runoff leaving the site.

A waiver is requested from section 20.14.3.D of the Land Use Code to allow the proposed vacuum parking spaces to be located along the Winchester Street frontage. The waiver criteria are as follows:

1. *Strict conformity would pose an unnecessary hardship to the applicant and waiver would not be contrary to the spirit and intent of the regulations;* The purpose for moving the existing vacuums from their existing location is to improve onsite traffic circulation. It will allow cars leaving the tunnels safer access to the exit at Winchester Street. It removes the possibility of a conflict with cars backing out of the vacuum spaces at the same time that cars are

exiting the tunnels. The spirit and intent of the regulations is to protect public safety by providing safe onsite traffic circulation. Denial of the waiver will result in the possibility of conflicts with cars trying to exit the site. This is an unnecessary hardship for the owner.

2. *Specific circumstances relative to the site plan, or conditions of the land in such site plan, indicate that the waiver will properly carry out the spirit and intent of the regulations;* The existing vacuum locations create a potential conflict for cars leaving the tunnels and cars backing out of the vacuum spaces. By relocating the vacuum parking spaces to the former restaurant site, that potential conflict is eliminated. Granting the waiver to allow the vacuum spaces to be along the Winchester Street frontage will place the vacuums as far from the single family residences on Wetmore Street as possible. The vacuum spaces will be screened from Winchester Street with Junipers and pavement rose shrubs to provide a year round screen.
3. *In granting a waiver, the Planning Board may require any mitigation that is reasonable and necessary to ensure that the spirit and intent of the standard being waived will be preserved, and to ensure that no increase in adverse impacts associated with granting the waiver will occur.* The use of screen plantings and the placement of the vacuum spaces away from the residential area ensures that no adverse impacts will result from granting the waiver.

DEVELOPMENT STANDARDS:

1. Drainage and Stormwater Management.

Total lot coverage is being reduced from 49.4% to 45.9%. This results in less stormwater runoff at the site. The existing onsite drainage pattern will be maintained with additional drainage measures provided at the east side of the site. Stormwater runoff will continue to sheet drain to a drainage swale and culvert at the northwest corner of the property and into an existing catch basin at Wetmore Street. Runoff will also be directed to an infiltration basin to be installed on the east side of the property next to Winchester Street. There will be no increase in runoff leaving the site. An exemption is requested from providing a formal drainage study.

2. Sediment and Erosion Control.

Sediment and erosion control measures as shown on the attached plans are designed to meet the State of New Hampshire RSA 485-A:17 for control of runoff and sedimentation. Silt fence will be installed along the limits of the areas to be disturbed and to provide inlet protection at the existing culvert on Wetmore Street. Please see attached plans for more details.

3. Hillside Protection.

Not applicable.

4. Snow Storage and Removal.

Snow will be stored on site as shown on the site plan. Excess snow will be removed from the site.

5. Landscaping.

New landscaping on the site is proposed. Along the Wetmore Street frontage, one flowering crab tree will be added along with 15 juniper and pavement rose shrubs to screen the proposed parking spaces from the public right of way. Areas outside the pavement will be loamed and seeded. The planting bed along Winchester Street will be mulched.

6. Screening.

The new dumpster and dumpster access pad will be screened with a 6 foot high solid wall enclosure. An existing 6' high solid fence along the north side of the driveway to Wetmore Street will be maintained.

7. Lighting.

On site lighting will be accomplished using high efficiency, full cutoff LED fixtures by Lumark. Two pole mounted lights on 15' poles will provide an average 1.07 footcandles throughout the paved area. Uniformity levels will be 2.68. Parking lot light fixtures will be placed on timers and photocells with light shut off after 9:00 PM. Please see attached plans and specifications for more detail. Fixture cut sheets have also been provided.

8. Sewer and Water.

No new sewer and water services are required.

9. Traffic and Access Management.

The previous use on the site was a 2000 sf (50 seat) seafood restaurant which has been removed. The site will be merged to the existing carwash site and used for parking and for the vacuum parking spaces. Relocating the existing vacuums to his site will improve onsite circulation and eliminate the existing conflicts that occur with cars leaving the tunnels at the same time as cars are backing out of the vacuum stations. No significant change to the traffic generated at the site is anticipated.

The existing curb cut to Wetmore Street from the former restaurant site is approximately 100 feet wide. This proposal will reduce the width of the curb cut to 40 feet. This will accommodate a rubbish truck into and out of the site. It will also create a safer traffic flow through the site.

10. Filling and Excavation.

The proposed grading plan calls for the excavation of approximately 534 CY of soil and the placement of approximately 417 CY of fill. Trucks will use Rt. 10 and Winchester Street as the haul route. No net loss of flood storage will result from this site plan.

11. Surface Waters and Wetlands.

There are no surface waters or wetlands on the site. No impacts to surface waters or wetlands are proposed.

12. Hazardous or Toxic Materials.

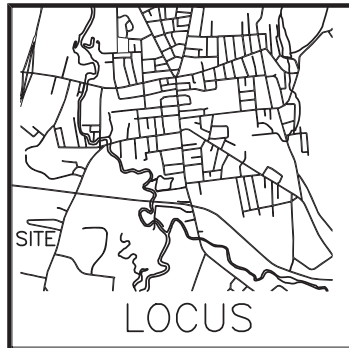
The owner has no knowledge of hazardous or toxic materials at the site.

13. Noise.

No excess noise is expected as a result of this proposal.

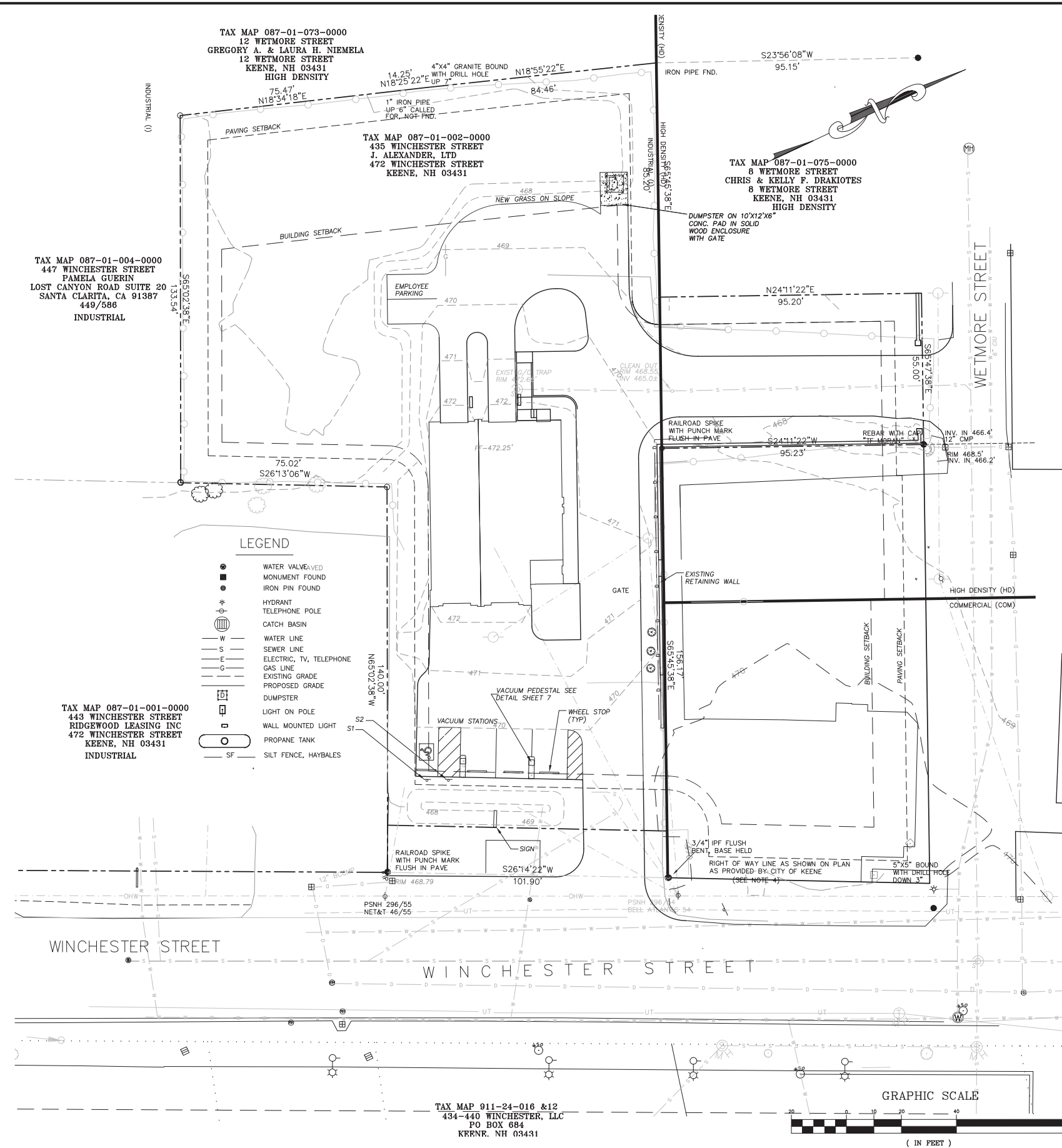
14. Architecture and Visual Appearance.

No new buildings are proposed. The proposed vacuum parking spaces will be screened from public view at Winchester Street with plantings as shown on the site plan. The new dumpster location will be screened with 6 foot high solid fencing.



GENERAL NOTES

1. REFERENCE PLAN: "TOPOGRAPHIC SURVEY PLAN PREPARED FOR JAY CLARK WINCHESTER STREET, KEENE" BY DAVID A. MANN, LLS
2. LOCATION OF UTILITIES IS FROM THE CITY OF KEENE ENGINEERING DEPARTMENT RECORDS. LOCATIONS ARE APPROXIMATE. CONTRACTOR TO VERIFY LOCATIONS OF ALL UTILITIES PRIOR TO EXCAVATION.
3. CONTRACTOR TO NOTIFY "DIG SAFE" AT 1-888-344-7233 A MIN. OF 72 HOURS PRIOR TO THE START OF EXCAVATION.
4. THE ENTIRE PROPERTY IS LOCATED WITHIN THE 100 YEAR FLOODPLAIN FOR THE CITY OF KEENE AND IS SUBJECT TO FLOODING TO ELEVATION 471.0' PER FEMA FIRM 33005C0266E DATED MAY 23, 2006.
5. DUE TO FLOODPLAIN COMPENSATION PERMIT REQUIREMENTS, GRADING AT EXTERIOR OF BUILDING WALLS IS LOWERED BELOW FINISH FLOOR IN SEVERAL AREAS. ARCHITECT IS RESPONSIBLE FOR COORDINATING FOUNDATION DESIGN WITH THE SITE GRADING TO ENSURE PERMIT REQUIREMENTS FOR FLOODPLAIN COMPENSATION AND FOOTING FROST PROTECTION ARE MET.
6. ALL FILL MATERIALS REQUIRED SHALL BE FREE OF HAZARDOUS OR TOXIC MATERIALS.
7. ALL UNDERGROUND TANKS SHALL BE DESIGNED AND ANCHORED TO PREVENT FLOTATION.



LEGEND

- WATER VALVE/VED
- MONUMENT FOUND
- IRON PIN FOUND
- * HYDRANT
- TELEPHONE POLE
- ⊞ CATCH BASIN
- W — WATER LINE
- S — SEWER LINE
- E — ELECTRIC, TV, TELEPHONE
- G — GAS LINE
- — — EXISTING GRADE
- — — PROPOSED GRADE
- ⊞ DUMPSTER
- ⊞ LIGHT ON POLE
- ⊞ WALL MOUNTED LIGHT
- PROPANE TANK
- SF — SILT FENCE, HAYBALES

TAX MAP 087-01-001-0000
443 WINCHESTER STREET
RIDGEWOOD LEASING INC
472 WINCHESTER STREET
KEENE, NH 03431
INDUSTRIAL

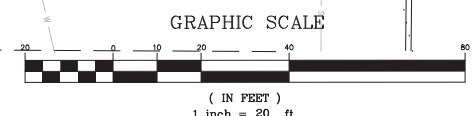
TAX MAP 087-01-004-0000
447 WINCHESTER STREET
PAMELA GUBRIN
LOST CANYON ROAD SUITE 20
SANTA CLARITA, CA 91387
449/588
INDUSTRIAL

TAX MAP 087-01-073-0000
12 WETMORE STREET
GREGORY A. & LAURA H. NIEMELA
12 WETMORE STREET
KEENE, NH 03431
HIGH DENSITY

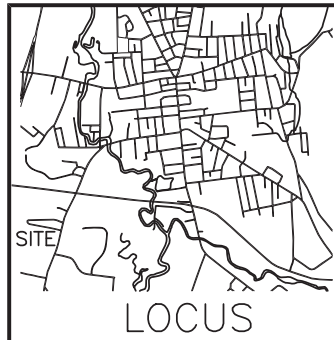
TAX MAP 087-01-002-0000
435 WINCHESTER STREET
J. ALEXANDER, LTD
472 WINCHESTER STREET
KEENE, NH 03431

TAX MAP 087-01-075-0000
8 WETMORE STREET
CHRIS & KELLY F. DRAKIOTES
8 WETMORE STREET
KEENE, NH 03431
HIGH DENSITY

TAX MAP 911-24-016 & 12
434-440 WINCHESTER, LLC
PO BOX 684
KRRNR, NH 03431



REVISIONS: DATE: JAN. 8, 2016 REVISE RECLAIM SYSTEM REVISE SEWER REVISE WATER ADD UTILITY SHEET REVISE SHEET NUMBERS SEPTEMBER 30, 2021 REISSUE TO ADD SECOND LOT
OWNER: MOC 76 REALTY COMPANY, LLC 435 WINCHESTER STREET KEENE, NH 03431 TAX MAP 087-01-002-0000
<p>Brickstone Land Use Consultants, LLC</p> <p>Site Planning, Permitting and Development Consulting 185 Winchester Street, Keene, NH 03431 Phone: (603) 357-0116</p>
PROPOSED TWO BAY CAR WASH 435 WINCHESTER ST. KEENE, NH
EXISTING PLAN
SCALE: 1"=20' DATE: 9/19/14
SHEET C 1



LOT DATA TABLE

TAX MAP #: MERGED LOTS 115-029-000, 115-030-000 & 115-031-000
 ZONES: INDUSTRIAL, COMMERCIAL & HIGH DENSITY DISTRICTS
 MERGED LOT SIZE: 57,945 SF OR 1.33 AC +/-
 LOT FRONTAGE:
 WINCHESTER STREET 196.91'
 WETMORE STREET 214.64'

	EXISTING	PROPOSED
BUILDINGS:	5,434 SF 9.4%	3,140 SF 5.4%
PAVEMENT:	23,175 SF 40.0%	23,459 SF 40.5%
TOTAL IMPERMEABLE:	28,609 SF 49.4%	26,599 SF 45.9%

PARKING: 10 SPACES PROVIDED

GENERAL NOTES

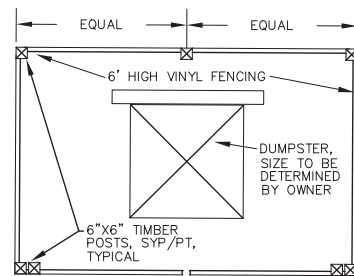
- REFERENCE PLAN: "TOPOGRAPHIC SURVEY PLAN PREPARED FOR JAY CLARK WINCHESTER STREET, KEENE" BY DAVID A. MANN, LLS
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- DUE TO FLOODPLAIN COMPENSATION PERMIT REQUIREMENTS, GRADING AT EXTERIOR OF BUILDING WALLS IS LOWERED BELOW FINISH FLOOR IN SEVERAL AREAS. ARCHITECT IS RESPONSIBLE FOR COORDINATING FOUNDATION DESIGN WITH THE SITE GRADING TO ENSURE PERMIT REQUIREMENTS FOR FLOODPLAIN COMPENSATION AND FOOTING FROST PROTECTION ARE MET.
- ALL FILL MATERIALS REQUIRED SHALL BE FREE OF HAZARDOUS OR TOXIC MATERIALS.
- ALL UNDERGROUND TANKS SHALL BE DESIGNED AND ANCHORED TO PREVENT FLOTATION.

LANDSCAPE LEGEND

TYPE	QUANTITY	PLANTING SIZE	MATURE HEIGHT
CG CASINO GOLD JUNIPER	12 EA	3 GAL - 3' O.C.	3 FEET
RP PURPLE PAVEMENT ROSE	3 EA	3 GAL - 3' O.C.	2.5 FEET
SNOW DRIFT FLOWERING CRAB	1 EA	1 - 1 1/2" CAL(6' HEIGHT 4' WIDTH)	15 FEET



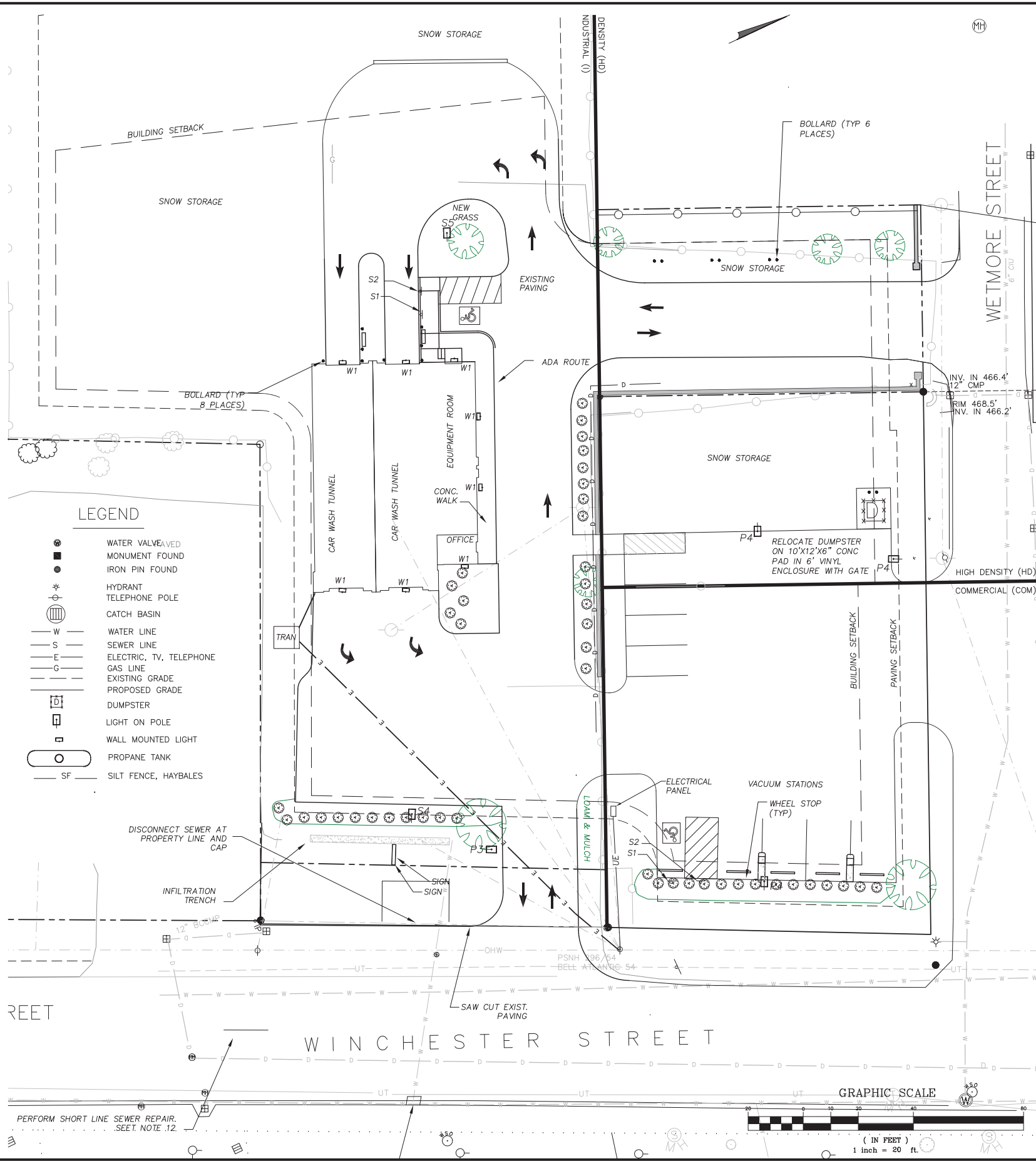
SNOW DRIFT FLOWERING CRAB 1 EA 1 - 1 1/2" CAL(6' HEIGHT 4' WIDTH) 15 FEET



NOTES:

- ALL WOOD POSTS TO BE PRESSURE TREATED, FREE FROM EXCESSIVE CRACKS, CHIPS, WARPS OR KNOTS.
- ALL FASTENERS TO BE HOT DIPPED GALVANIZED.
- VINYL SCREEN MEMBERS AND GATES TO CONFORM TO DETAIL ELEVATION AS NOTED.
- DIMENSIONS OF ENCLOSURE TO BE DETERMINED BY OWNER PRIOR TO FABRICATION OR CONSTRUCTION.
- PROVIDE 4' WIDE ACCESS OPENING ON SIDE, W/ A.D.A. ROUTE.

VINYL DUMPSTER ENCLOSURE
NO SCALE



LEGEND

●	WATER VALVE
■	MONUMENT FOUND
●	IRON PIN FOUND
*	HYDRANT
○	TELEPHONE POLE
⊖	CATCH BASIN
— W —	WATER LINE
— S —	SEWER LINE
— E —	ELECTRIC, TV, TELEPHONE
— G —	GAS LINE
---	EXISTING GRADE
---	PROPOSED GRADE
⊞	DUMPSTER
□	LIGHT ON POLE
□	WALL MOUNTED LIGHT
○	PROPANE TANK
— SF —	SILT FENCE, HAYBALES

- REVISIONS:**
- △ MERGED LOTS, REMOVE RESTAURANT BUILDING, ADD PARKING, MOVE VACUUMS OCTOBER 8, 2021
 - △ RELOCATED DUMPSTER, ADD GREEN SPACE OCTOBER 14, 2021
 - △ ADD LIGHTING PLAN OCTOBER 29, 2021

OWNER:
MOC 76 REALTY COMPANY, LLC
 435 WINCHESTER STREET
 KEENE, NH 03431
 TAX MAP 087-01-002-0000

Brickstone Land Use Consultants, LLC
 Site Planning, Permitting and Development Consulting
 185 Winchester Street, Keene, NH 03431
 Phone: (603) 357-0116

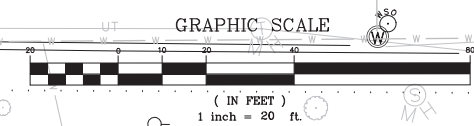
SITE MODIFICATIONS
TWO BAY CAR WASH
 435 WINCHESTER ST.
 KEENE, NH

PROPOSED PLAN

SCALE: 1"=20'

DATE: SEPT. 30, 2021

SHEET C 2



EROSION CONTROL SEQUENCE

THIS IS A SUGGESTED SEQUENCE FOR THE IMPLEMENTATION OF EROSION CONTROLS. IT SHALL NOT LIMIT THE CONTRACTOR IN MEANS, METHODS AND TECHNIQUES FOR THE CONTROLLING OF EROSION AND SEDIMENTATION NECESSARY TO FULFILL THE REQUIREMENTS OF NEW HAMPSHIRE STORM WATER MANAGEMENT AND EROSION CONTROL 485-A:17.

1. INSTALL SILT FENCE WHERE SHOWN AND WHERE REQUIRED TO CONTROL EROSION AND INSTALL STABILIZED CONSTRUCTION ENTRANCE.
2. REMOVE EXISTING STRUCTURES AND PAVING WHERE REQUIRED OR WHERE SHOWN ON THE PLANS.
3. CLEAR AND GRUB CONSTRUCTION AREAS AND COMPENSATORY FLOOD STORAGE AREAS.
4. CONSTRUCT RETENTION PONDS AND DRAINAGE CONTROL STRUCTURES.
5. CONSTRUCT FOUNDATION AND BRING TO SUBGRADE AND STABILIZE.
6. INSTALL UNDERGROUND UTILITIES AND STABILIZE.
7. CONSTRUCT FLOODPLAIN COMPENSATION AREA, FINE GRADE AND STABILIZE ALL AREAS WITHIN 72 HOURS OF ACHIEVING FINISH GRADE.
8. ALL CUT/FILL SLOPES TO BE LOAMED AND MULCHED OR PLANTED WITH GROUND COVER WITHIN 72 HOURS OF ACHIEVING FINISHED GRADE.
9. THE SMALLEST AREA PRACTICABLE SHALL BE DISTURBED DURING CONSTRUCTION. STABILIZATION SHALL BE DEFINED AS EITHER BASE COURSE PAVING, CONCRETE, RIP RAP, 3 INCHES OF BARK MULCH, OR A MIN. OF 85% OF VEGETATED GROWTH HAS BEEN ESTABLISHED.
10. ALL DISTURBED AREAS SHALL BE STABILIZED WITHIN 45 DAYS.
11. WHEN ALL WORK IS COMPLETED, SLOPES AND DITCHES ARE STABILIZED, REMOVE TEMPORARY EROSION CONTROL MEASURES AND ANY SEDIMENT THAT HAS COLLECTED IN DITCHES, CULVERTS OR DETENTION PONDS.
12. SHOULD THE CONTRACTOR WISH TO ALTER THIS SEQUENCE, HE SHALL SUBMIT THE PROPOSED CHANGES IN WRITING TO THE ENGINEER. THE ENGINEER WILL REVIEW THE PROPOSED CHANGES AND ADVISE THE CONTRACTOR.
13. CONTRACTOR IS RESPONSIBLE FOR IMPLEMENTING ANY AND ALL PROCEDURES TO CONTROL EROSION ON AND OFF THE CONSTRUCTION SITE.
14. CONTRACTOR SHALL MAINTAIN AN INSPECTION LOG FOR VIEWING BY THE CITY ENGINEER.

EROSION CONTROL INSPECTION:

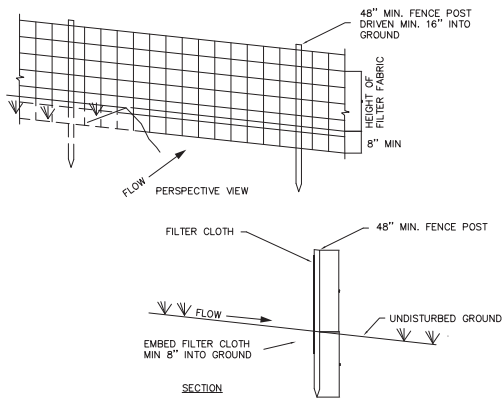
INSPECTIONS OF THE CONSTRUCTION SITE SHALL BE MADE BY PERSONNEL FAMILIAR WITH THE CONSTRUCTION ACTIVITY. EROSION AND SEDIMENT CONTROL MEASURES IDENTIFIED IN THE PLAN SHALL BE OBSERVED TO ENSURE THAT THEY ARE OPERATING CORRECTLY.

INSPECTIONS SHALL BE CONDUCTED AT LEAST ONCE EVERY WEEK AND WITHIN 48 HOURS OF THE END OF A STORM EVENT THAT IS 0.5 INCH OR GREATER. THESE INSPECTIONS SHALL COVER DISTURBED AREAS THAT HAVE NOT BEEN FINALLY STABILIZED, AREAS USED FOR STORAGE OF MATERIALS THAT ARE EXPOSED TO PRECIPITATION, STRUCTURAL CONTROL MEASURES, AND LOCATIONS WHERE VEHICLES ENTER OR EXIT THE SITE. WHERE AREAS HAVE BEEN FINALLY OR TEMPORARILY STABILIZED OR RUNOFF IS UNLIKELY DUE TO WINTER CONDITIONS (E.G., SITE IS COVERED WITH SNOW, ICE, OR FROZEN GROUND EXISTS) SUCH INSPECTIONS SHALL BE CONDUCTED AT LEAST ONCE EVERY MONTH.

THE CONTRACTOR SHALL KEEP WRITTEN REPORTS SUMMARIZING EACH INSPECTION ONSITE.

WINTER CONSTRUCTION NOTES:

1. ALL PROPOSED VEGETATED AREAS WHICH DO NOT EXHIBIT A MINIMUM OF 85% VEGETATIVE GROWTH BY OCTOBER 15TH, OR WHICH ARE DISTURBED AFTER OCTOBER 15TH, SHALL BE STABILIZED BY SEEDING AND INSTALLING EROSION CONTROL BLANKETS ON SLOPES GREATER THAN 3:1, AND SEEDING AND PLACING 3 TO 4 TONS OF MULCH PER ACRE, SECURED WITH ANCHORED NETTING, ELSEWHERE. THE INSTALLATION OF EROSION CONTROL BLANKETS OR MULCH AND NETTING SHALL NOT OCCUR OVER ACCUMULATED SNOW OR ON FROZEN GROUND AND SHALL BE COMPLETED IN ADVANCE OF THAW OR SPRING MELT EVENTS.
2. ALL DITCHES OR SWALES WHICH DO NOT EXHIBIT A MINIMUM OF 85% VEGETATIVE GROWTH BY OCTOBER 15TH OR WHICH ARE DISTURBED AFTER OCTOBER 15TH, SHALL BE STABILIZED TEMPORARILY WITH STONE OR EROSION CONTROL BLANKETS APPROPRIATE FOR THE DESIGN FLOW CONDITIONS.
3. AFTER NOVEMBER 15TH, INCOMPLETE ROAD OR PARKING SURFACES, WHERE WORK HAS STOPPED FOR THE WINTER SEASON, SHALL BE PROTECTED WITH A MINIMUM OF 3 INCHES OF CRUSHED GRAVEL PER NHDOT ITEM 304.3.

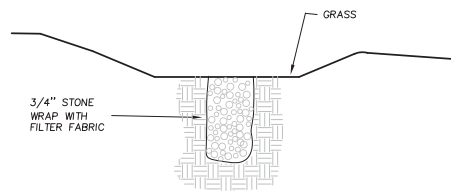


CONSTRUCTION SPECIFICATIONS

1. FABRIC TO BE FASTENED SECURELY TO FENCE POSTS WITH WIRE TIES OR STAPLES.
2. WHEN TWO SECTIONS OF FILTER CLOTH ADJOIN EACH OTHER, THEY SHALL BE OVERLAPPED AT A POST BY 6 INCHES, FOLDED AND STAPLED.
3. MAINTENANCE SHALL BE PERFORMED AS NEEDED AND MATERIAL REMOVED WHEN "BULGES" DEVELOP IN THE SILT FENCE.

SILT FENCE DETAIL

NTS



STONE TRENCH TO BE A MINIMUM OF 12" WIDE X 24" DEEP. WRAP SIDES AND BOTTOM OF STONE IN FILTER FABRIC.

INFILTRATION TRENCH IN RETENTION BASIN

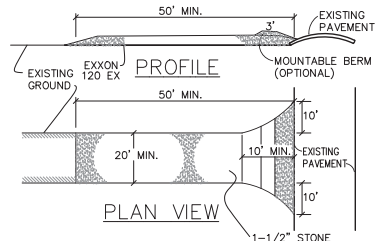
NOT TO SCALE

GENERAL NOTES

1. WATER SERVICE TO BE SIZE AND TYPE AS SHOWN ON THE SITE PLANS.
2. ALL UTILITY WORK SHALL BE IN ACCORDANCE WITH THE CITY OF KEENE STREET AND UTILITY STANDARDS AND INSPECTED BY DPW ENGINEERING OR WATER/SEWER DEPARTMENT PRIOR TO BURIAL OR BEING PLACED IN SERVICE.
3. MAINTAIN MIN. 10' HORIZONTAL SEPARATION OF WATER AND SEWER LINES.
4. PAVING SHALL CONSIST OF 2" BASE COURSE AND 1" TOP PAVING BASE SHALL BE OF 12" BANK RUN GRAVEL AND 6" COURSE CRUSHED GRAVEL. ALL FILL MATERIAL AND DISTURBED AREAS UNDER PAVING TO BE COMPACTED TO A MIN 95% DENSITY.
5. CONSTRUCTION WITHIN THE CITY ROW REQUIRES AN EXCAVATION PERMIT FROM THE PUBLIC WORKS DEPT. CONTRACTOR IS RESPONSIBLE FOR ALL EXCAVATION PERMITS REQUIRED.
6. EXCAVATION WITHIN THE CITY STREET SHALL BE DONE TO DPW STANDARDS AND INSPECTED BY DPW.
7. SEWER LINES WITH LESS THAN 4 FEET OF COVER SHALL BE INSULATED WITH 1 1/2" EPS.
8. WATER SERVICE SHALL REQUIRE A KEENE DPW APPROVED TESTABLE BACKFLOW DEVICE TO BE INSTALLED IN BUILDING.
9. INSTALL CORPORATIONS OR GATE VALVES AT ALL WATER LINE CONNECTIONS TO THE CITY MAIN LINE.
10. ABANDONED WATER SERVICE TO BE DISCONNECTED AT THE MAIN LINE IN WINCHESTER STREET AND CORP TO BE CLOSED. BUILDING FLOOR DRAINS SHALL HAVE A RAISED OUTLET TO ALLOW SAND SETTLEMENT IN THE FLOOR DRAIN TRENCH.
11. EXISTING SEWER SERVICE TO BE CLOSED USING THE "SHORT LINE" SEWER REPAIR METHOD PER CITY OF KEENE DIRECTION. SEWER SERVICE TO BE DISCONNECTED AT THE PROPERTY LINE AND CAPPED.

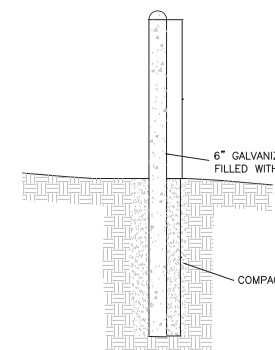
GENERAL NOTES

1. REFERENCE PLAN: "TOPOGRAPHIC SURVEY PLAN PREPARED FOR JAY CLARK WINCHESTER STREET, KEENE" BY DAVID A. MANN, LLS
2. LOCATION OF UTILITIES IS FROM THE CITY OF KEENE ENGINEERING DEPARTMENT RECORDS. LOCATIONS ARE APPROXIMATE. CONTRACTOR TO VERIFY LOCATIONS OF ALL UTILITIES PRIOR TO EXCAVATION.
3. CONTRACTOR TO NOTIFY "DIG SAFE" AT 1-888-344-7233 A MIN. OF 72 HOURS PRIOR TO THE START OF EXCAVATION.
4. THE ENTIRE PROPERTY IS LOCATED WITHIN THE 100 YEAR FLOODPLAIN FOR THE CITY OF KEENE AND IS SUBJECT TO FLOODING TO ELEVATION 471.0' PER FEMA FIRM 3300500266E DATED MAY 23, 2006. DUE TO FLOODPLAIN COMPENSATION PERMIT REQUIREMENTS, GRADING AT EXTERIOR OF BUILDING WALLS IS LOWERED BELOW FINISH FLOOR IN SEVERAL AREAS. ARCHITECT IS RESPONSIBLE FOR COORDINATING FOUNDATION DESIGN WITH THE SITE GRADING TO ENSURE PERMIT REQUIREMENTS FOR FLOODPLAIN COMPENSATION AND FOOTING FROST PROTECTION ARE MET.
5. ALL FILL MATERIALS REQUIRED SHALL BE FREE OF HAZARDOUS OR TOXIC MATERIALS.
6. ALL UNDERGROUND TANKS SHALL BE DESIGNED AND ANCHORED TO PREVENT FLOTATION.



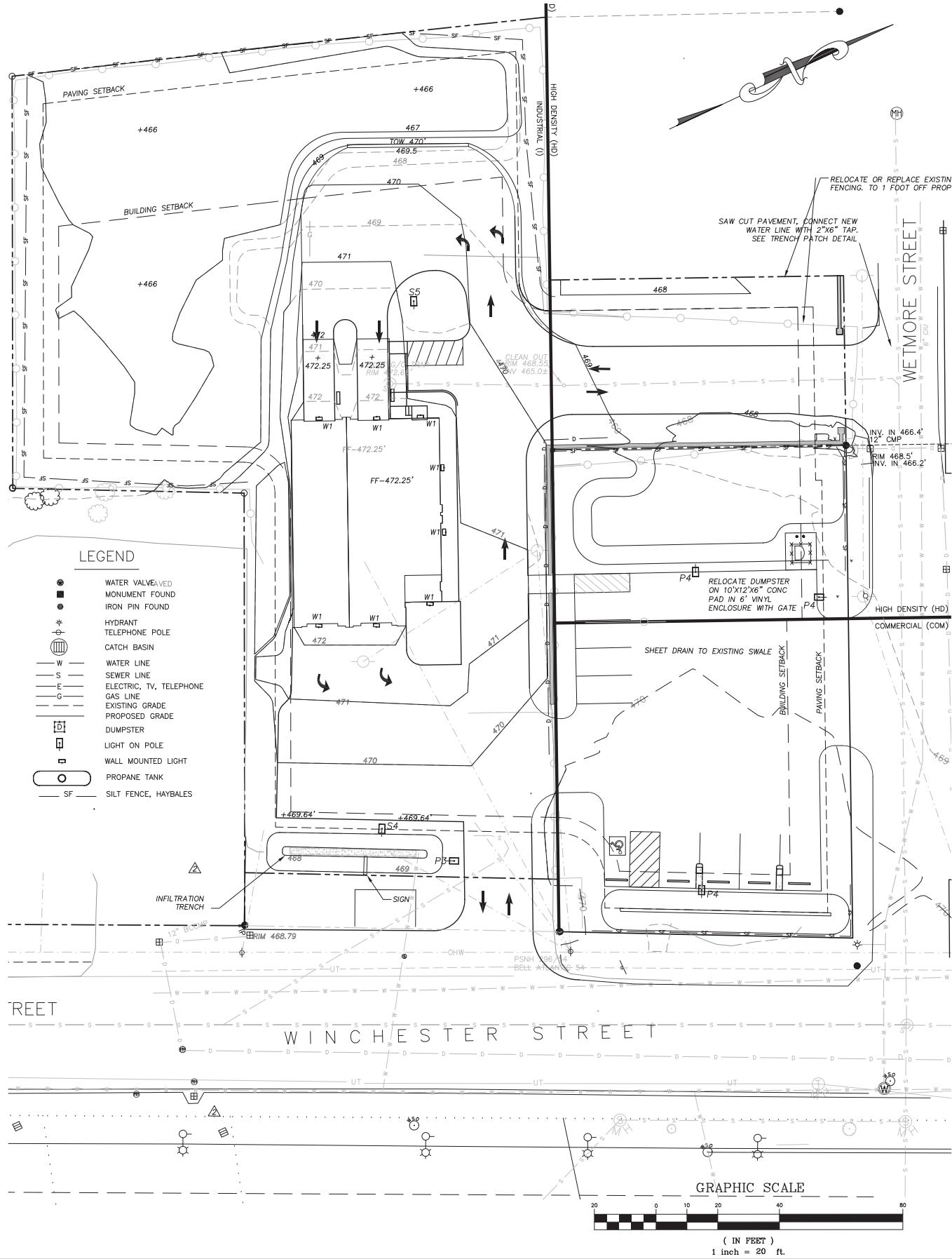
STABILIZED CONSTRUCTION ENTRANCE

NOT TO SCALE



BOLLARD DETAIL

OTHER SUITABLE MATERIAL ACCEPTED SUCH AS 3" STEEL BEAMS, RAILROAD RAILS, ETC. PENDING APPROVAL BY OWNER.



REVISIONS: DATE:

△ MERGED LOTS, REMOVE RESTAURANT BUILDING, ADD PARKING, MOVE VACUUMS OCTOBER 8, 2021

△ RELOCATED DUMPSTER, ADD GREEN SPACE OCTOBER 14, 2021

△ ADD LIGHTING PLAN OCTOBER 29, 2021

OWNER:
MOC 76 REALTY COMPANY, LLC
435 WINCHESTER STREET
KEENE, NH 03431
TAX MAP 087-01-002-0000

Brickstone
Land Use Consultants, LLC
Site Planning, Permitting and Development Consulting
185 Winchester Street, Keene, NH 03431
Phone: (603) 357-0116

PROPOSED
TWO BAY CAR WASH
435 WINCHESTER ST.
KEENE, NH

GRADING & EROSION CONTROL PLAN

SCALE: 1"=20'

DATE: SEPT. 30 2021

SHEET C 3

Project	Mint	Catalog #	prv-c15-d-unv-t4-bz-hss	Type	
Prepared by		Notes		Date	

Lumark
PRV / PRV-XL Preval LED
Area / Site Luminaire

Typical Applications
Outdoor • Parking Lots • Walkways • Roadways • Building Areas

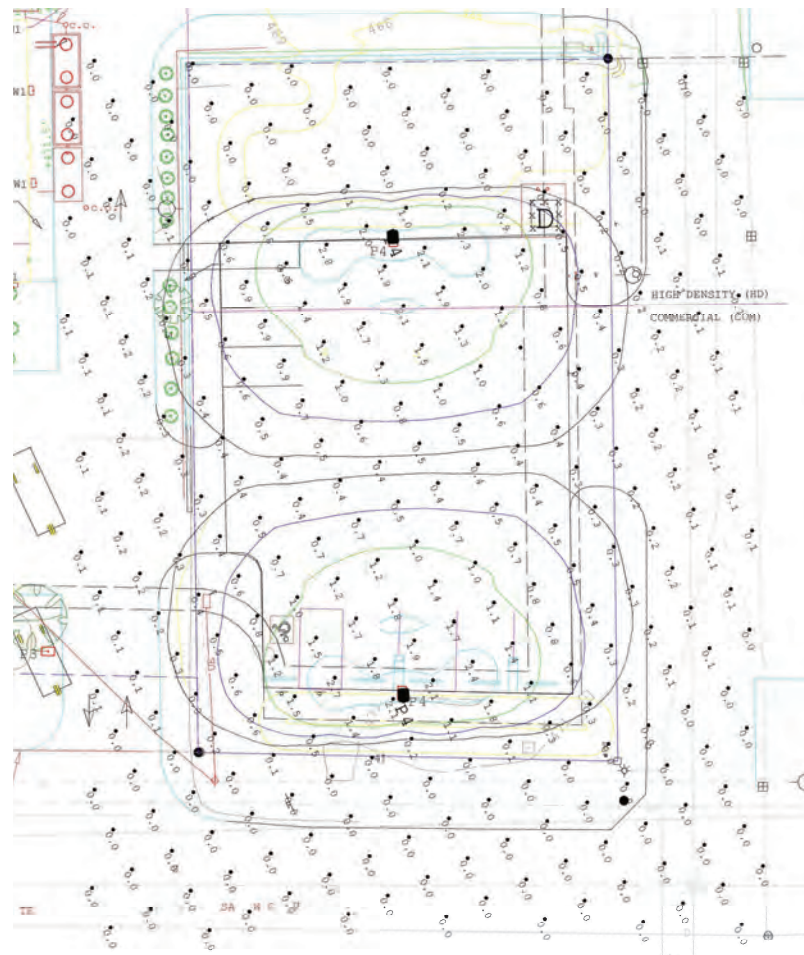
Product Certifications
ISU, UL, DLE, DLE, KC, ENEC, VDA, SAA, CE, RoHS

Product Features
• Energy and maintenance savings up to 85% versus HID solutions
• Standard universal quick mount arm with universal drill pattern

Quick Facts
• Lumen packages range from 7,100 - 48,600 lumens (50W - 350W)
• Replaces 70W up to 1,000W HID equivalents
• Efficacies up to 148 lumens per watt
• Energy and maintenance savings up to 85% versus HID solutions
• Standard universal quick mount arm with universal drill pattern

Dimensional Details
Preval: 13-15/16" (354mm) x 26-13/16" (681mm)
Preval XL: 11-7/8" (300mm) x 29-1/8" (743mm)

COOPER
Lighting Solutions



Volumes by Triangulation (Prisms) 10-14-2021.txt
Thu Oct 14 09:54:32 2021
Existing Surface: KEENE WINCHESTER ST MINT CAR WASH\EG 10-14-2021.tin
Final Surface: KEENE WINCHESTER ST MINT CAR WASH\FG 10-14-2021.tin
Difference Surface: KEENE WINCHESTER ST MINT CAR WASH\EG FG 10-14-2021.tin

Cut Volume (CY)	534.2
Fill Volume (CY)	417.4
Cut Volume (Acre-Ft)	0.33
Fill Volume (Acre-Ft)	0.26
Cut Area (SF)	26335.3
Cut Area (Acres)	0.60
FILL Area (SF)	13133.7
FILL Area (Acres)	0.30
Total Area (SF)	54484.5
Total Area (Acres)	1.25
Cut Volume (CF)	14424.0
Fill volume (CF)	11269.0

STAFF REPORT

SPR-897, MODIFICATION #1 — SITE PLAN REVIEW & SWP-CUP-2021-04 – SURFACE WATER PROTECTION CONDITIONAL USE PERMIT REVIEW – U-HAUL OF SOUTH KEENE – 472 WINCHESTER STREET & 0 KRIF ROAD

Request:

Applicant Fieldstone Land Consultants, on behalf of owner Amerco Real Estate Company, proposes to renovate two existing buildings, construct a 12,175 sf building, and install 28,900 sf of prefabricated storage units on the properties located at 472 Winchester St (TMP #115-020-00) and 0 Krif Rd (115-019-000). A Conditional Use Permit is requested to install pavement within the Surface Water Protection buffer. An exception to the City's Street Access standards is requested to increase the width of an existing curb cut to be 45 ft at the property line and 66 ft at the curblines. These combined properties are 7.51 acres in size and are located in the Commerce Limited District.

Background:

The 472 Winchester Street property is the site of the former Clark Distributors complex, located on the southeast corner of Winchester Street and Krif Road. This property is 4.03 acres in size and has four existing access points, including one on Winchester Street and three on Krif Road. The adjacent 0 Krif Road property is 3.48 acres and is located to the south and east of the 472 Winchester Street property. This parcel is currently undeveloped and has 50 feet of frontage on Krif Road. Both parcels abut Ash Swamp Brook and are located in the Commerce Limited District.



Figure 1. Street view imagery from November 2019 of the former Clark Distributors building located at 472 Winchester Street.

The Applicant proposes to merge these two parcels and develop the site for use as a U-Haul truck rental business and self-storage facility. A Conditional Use Permit is requested to allow the construction of a paved drive aisle within the 30-foot Surface Water Protection buffer. Per Section 11.6.3.A of the LDC, the CUP Application has been referred to the Conservation Commission and will be reviewed at their meeting on November 15. Staff will share any recommendations from the Conservation Commission at the Planning Board meeting.

In addition, the Applicant requests an Exception from Section 22.5.4.9 of the City's Street Access Standards in order to widen the easternmost curb cut on Krif Road to 45 ft at the property line and 66 ft at the curblines.

Completeness:

The Applicant requests an exemption from providing a Traffic Report. Staff recommend that the Board grant the requested exemption and accept the application as "Complete."

STAFF REPORT

Departmental Comments:

Included below are questions and comments from City staff on this application.

Engineering:

1. *Please provide traffic generation estimates based on the most recent version of the ITE Trip Generation Manual.

**Note: The Applicant has stated that traffic generation estimates will be provided at the Planning Board meeting.*

2. Excessive curb cuts and access management on Winchester St. is an ongoing concern. Per Section 20.9.2 of the Land Development Code, “*The use of common driveways and service roads is encouraged*”. The existing and proposed conditions include four separate curb cuts for this site. Engineering Division recommends that, at a minimum, the curb cut directly onto Winchester Street be closed as a condition of approval.

3. **The Property Owner should be informed that the City is planning a major reconstruction of Lower Winchester Street beginning in 2025. As part of that project, the City will be replacing the existing bridge over Ash Swamp Brook, widening the roadway, constructing sidewalks and other improvements. Although no design decisions have been made, it is possible that the City will require additional right-of-way width to accommodate the new bridge and project features. This could potentially include temporary access rights to install a temporary bridge on the applicant’s property during construction.

***Note: Engineering staff met with the Applicant on October 29 to discuss the lower Winchester Street reconstruction project, including potential future impacts to this property. The owner has been made aware of these potential impacts.*

Code Enforcement/Zoning:

1. Please be aware that a Floodplain Development Permit will be required for this project.
2. Please show where U-Haul trucks will be parked on the site.

Application Analysis: The following is a review of the Planning Board development standards relevant to this application.

- 20.2 **Drainage and Stormwater Management:** The proposed development would result in an increase in impervious surface on the site from 2.178 acres to 4.957 acres. In order to mitigate the increase in impervious area, the Applicant proposes to install a series of catch basins in the new paved areas that will be connected by a sub-surface perforated pipe and stone system. Half of the roof drains from the new warehouse building will tie into this system, while the other half will tie into the existing drainage system that connects to Krif Road, which will remain in place to handle runoff from the existing buildings and parking lot. The proposed catch basins will have hooded outlets to prevent contamination of discharge, and the 24-inch perforated pipe and stone system is designed to infiltrate water into the ground. Overflow from the new stormwater management system will be directed to a flood compensation basin at the south end of the site. This flood compensation basin is located partially within the floodway, and entirely within the 100-year floodplain.

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The Applicant submitted a Storm Water Management Report, which concludes that there will be no increase in the peak flow of runoff from the site as a result of the proposed development. A summary of the storm water report is attached to this staff report. This standard appears to be met.

- 20.3 Sediment & Erosion Control: Proposed sedimentation and erosion control measures during construction include silt fencing along the perimeter of the area to be disturbed, silt socks around all catch basins, stabilized construction entrances to the site to prevent tracking of sediment onto the public right-of-way, and stabilization fabric on slopes that are 2:1 or steeper. Post-construction, all disturbed non-paved areas will be stabilized, loamed, and seeded immediately following construction, and rip-rap stone aprons will be installed at the outfall of the overflow pipes from the subsurface drainage system. This standard appears to be met.
- 20.4 Snow Storage & Removal: Proposed snow storage areas are shown on the site plan at the southern end of the paved area. This snow storage area is about 5 ft wide, and is located at the edge of the compensatory storage basin. In the project narrative, the Applicant states that any excess snow will be hauled off site if necessary. Due to the limited snow storage area on the site (relative to the area that would need to be cleared), staff recommend that a condition of approval be added to require off-site snow removal for any excess snow.
- 20.5 Landscaping: The applicant proposes to plant five red maple trees in the rear and side of the site in order to meet the requirement to provide 1 tree for every 10 parking spaces, of which there are 42. The location of these trees is shown in Figure 2, below. No trees are proposed along Winchester Street where new pavement is proposed for the circulation driveway and display area. The Applicant noted during a conversation with staff that the owner would prefer not to plant trees along Winchester Street in order to keep a clear view of display area, which runs the full length of the Winchester Street frontage. The display area will be used to display U-Haul trucks and UBox storage units when they are not in use.

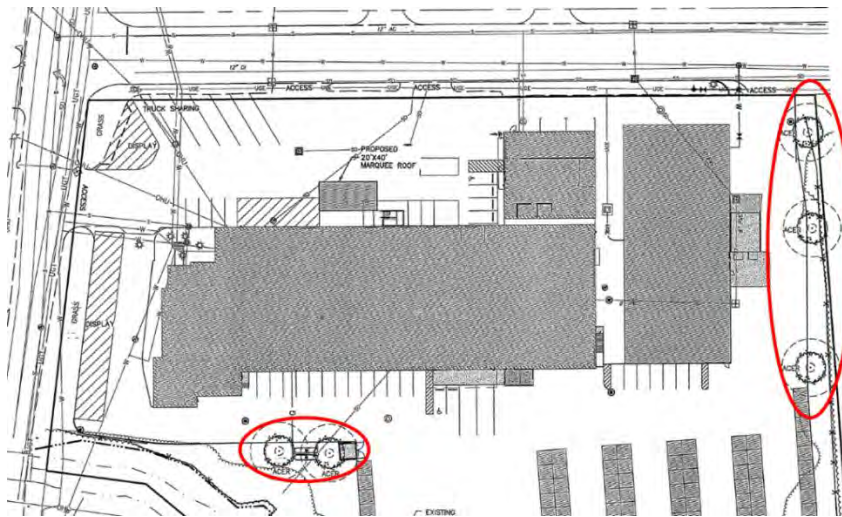


Figure 2. Image of the proposed Landscaping Plan with the location of the Red maple trees circled in red.

- 20.6 Screening: The Applicant proposes to locate the dumpster to the south of the existing building and screen it with a six foot tall vinyl fence (see detail on sheet DT-3 of the plan set). In response to staff comments, the Applicant stated that the HVAC

STAFF REPORT

systems for the new building have not been designed yet; however, any HVAC equipment will be located in the corridor between the existing buildings. Any new mechanical equipment for the existing buildings will be located either in this same corridor or on the roof, out of sight from the public right-of-way.

Sub-section E of this standard ("Parking Lots") states that "Parking lots shall be designed and screened in accordance with Article 9 of this LDC." Although the Applicant has stated that they would prefer not to screen the new paved area in front of the building from Winchester Street due to the proposed display area, the Board may wish to consider whether some amount of screening along Winchester Street would be appropriate for this site.

- 20.7 Lighting: The Applicant proposes to illuminate the new parking area and circulation lanes using building-mounted LED lights, as shown on the Lighting Plan (Sheet LT-1). There will be eight wall pack lights mounted on the south side of the existing building and the south and east sides of the proposed new building at a height of 15 ft. A different style of LED wall pack lights (29 total) will be mounted to the sides of the prefabricated self-storage buildings at a height of 8 ft, and will be put on motion sensors to turn on only when there is activity. The only changes proposed to the lighting for the existing parking area is the removal of one pole-mounted light in the northwest corner of the paved area and the removal of the flagpole and flag light. The existing wall pack lights on the north and west sides of the existing building will remain, along with the bollard lights at the northwest corner of the building near the entrance facing Winchester Street.

According to the photometric plan submitted with the application, the average light levels for the new parking area will be 1.20 footcandles (fc), the minimum illumination will be 0.3 fc, and the uniformity ratio will be 4:1. In addition, the "Design Notes" section of the Lighting Plan states that all proposed lighting will have a color rendering index of 70 or greater, and all lighting must be 3500 Kelvin color-temperature. This standard appears to be met.

- 20.8 Sewer & Water: No changes are proposed to the sewer and water access for the existing buildings. The new building, which is proposed to be used as a warehouse, will connect to the existing water line on Krif Road. A water hydrant is proposed to be moved at the northeast entrance to the site to accommodate a wider curb cut.

Other utilities on the site include two 1,000 gallon above ground propane tanks, which are proposed to be located to the rear of the existing building adjacent to the dumpster. This standard appears to be met.

- 20.9 Traffic & Access Management: With respect to traffic generation, staff have requested traffic generation estimates based on the most recent version of the ITE Trip Generation Manual to demonstrate that there will be no increase in traffic as compared to the previous use, which was a beverage distribution company (Clarke Distributors). At the time of this staff report, this information had not been provided; however, the Applicant indicated that this information would be brought to the Planning Board meeting.

Subsection 2.B of this standard states "*Wherever possible, the number of curbcuts or driveways on public streets shall be limited to one per lot.*" City Engineering staff

STAFF REPORT

standards for the Surface Water Protection district:

A. The proposed use and/or activity cannot be located in a manner to avoid encroachment into the Surface Water Protection Overlay District.

The Applicant proposes to install a 24-ft wide circulation lane and a portion of the paved display area within the 30-ft buffer, and states that the circulation lane is required to provide fire truck access to all four sides of the building. Without this circulation lane, there would be fire truck access to three sides of the building, and fire trucks could access the fourth side from Winchester Street if necessary. Staff from the Fire Department have been asked whether this circulation lane is required to meet the Fire Code. In addition, the Board may wish to ask the Applicant whether the display area cannot be located in a manner to avoid encroachment into the buffer, as that area is not required for fire truck access.

B. Encroachment into the buffer area has been minimized to the maximum extent possible, including reasonable modification of the scale or design of the proposed use.

Based on staff review, the encroachment into the buffer area has not been minimized to the maximum extent possible given the size and location of a display area within the buffer, which is not necessary for the functioning of the site. The Board may consider asking the Applicant to reduce the encroachment into the buffer to only what is required for fire truck access. If the circulation lane is not required to meet the Fire Code, the Board may consider asking the applicant to remove the circulation lane entirely,

C. The nature, design, siting, and scale of the proposed use and the characteristics of the site, including but not limited to topography, soils, vegetation, and habitat, are such that when taken as a whole, will avoid the potential for adverse impacts to the surface water resource.

The Applicant proposes to install curbing to prevent surface runoff from the paved area from entering the brook. This runoff will be directed into the perforated pipe and stone infiltration system. In addition, the catch basin will have an oil/debris separator hood and deep sump to collect sediment/debris from entering the drainage system.

D. The surface water buffer area shall be left in a natural state to the maximum extent possible. The Planning Board may establish conditions of approval regarding the preservation of the buffer, including the extent to which trees, saplings and ground cover shall be preserved.

- 1. Dead, diseased, unsafe, fallen or invasive trees, saplings, shrubs, or ground cover may be removed from the surface water buffer area.*
- 2. Tree stumps and their root systems shall be left intact in the ground, unless removal is specifically approved in conjunction with a surface water protection conditional use permit granted by the Planning Board. The stumps and root balls of exotic, invasive species may be removed by hand digging and/or hand cutting.*
- 3. Preservation of dead and living trees that provide dens and nesting places for wildlife is encouraged. Planting of native species of trees, shrubs, or ground cover that are beneficial to wildlife is encouraged.*

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4. *Where there has been disturbance or alteration of the surface water buffer during construction, revegetation with native species may be required by the Planning Board.*

The Applicant notes in the CUP application narrative that the only area where vegetation would be removed is the area proposed to be paved and graded. All other existing vegetation within the buffer will not be altered, and will be allowed to grow naturally.

E. The Planning Board may consider the following to determine whether allowing the proposed encroachment will result in an adverse impact on the surface water resource.

1. *The size, character, and quality of the surface water and the surface water buffer being encroached upon.*
2. *The location and connectivity of the surface water in relation to other surface waters in the surrounding watershed.*
3. *The nature of the ecological and hydrological functions served by the surface water.*
4. *The nature of the topography, slopes, soils, and vegetation in the surface water buffer.*
5. *The role of the surface water buffer in mitigating soil erosion, sediment and nutrient transport, groundwater recharge, flood storage, and flow dispersion.*
6. *The extent to which the surface water buffer serves as wildlife habitat or travel corridor.*
7. *The rate, timing and volume of stormwater runoff and its potential to influence water quality associated with the affected surface water or any associated downstream surface waters.*
8. *The sensitivity of the surface water and the surface water buffer to disruption from changes in the grade or plant and animal habitat in the buffer zone.*

The CUP application narrative notes that the Ash Swamp Brook was part of the City Tax Ditch system in the past and is bordered by a number of commercial properties. The proposed area of encroachment is about 0.8 miles (~4,200 ft) from the point where it joins the Ashuelot River. According to the wetland report, the wetlands on the site are classified as Riverine, Lower Perennial, unconsolidated bottom, sand (R2UB2) and are located along the bank of the brook. The underlying soils are very fine-fine sandy flood plain soils and are well drained; however, there may be some areas of the site that have been altered in the past which may have slower drainage rates.

The Applicant notes that the site has been designed to preserve the ecological and hydrological functions of the brook with the proposed drainage/infiltration system and compensatory flood storage area, which will be allowed to return to a natural state after it has been constructed.

Per Section 11.6.3.A of the LDC, the Surface Water Protection CUP Application has been referred to the Conservation Commission and will be reviewed at their meeting on November 15. Staff will share any recommendations from the Conservation Commission at the Planning Board meeting.

- 20.12 Hazardous or Toxic Materials: There are no known hazardous or toxic materials present on the site. This standard is not applicable.

STAFF REPORT

- 20.13 Noise: In the project narrative, the Applicant notes that the noise generated by the site is expected to be similar to the noise generated by the surrounding businesses, which are all commercial.
- 20.14 Architecture & Visual Appearance: The Applicant proposes to renovate the two existing buildings on the site for use as a climate-controlled self storage facility and a U-Haul truck rental business. In addition, a new building is proposed to the east of the existing buildings that would be used as a warehouse to store U-Box mobile storage units. This new building is proposed to be 12,175 square feet and approximately 32 feet tall, as shown in the renderings and elevations attached to this staff report. This is about 10 feet higher than the existing buildings on the site.

The design of the proposed climate controlled self-storage facility building appears to be based on the corporate aesthetic for U-Haul, including bright orange colors, metal siding, and faux architectural features such as fake storage unit doors facing Winchester Street and Krif Road which appear to be intended for the sole purpose of advertising the business (see Figure 4 for a rendering of the proposed Winchester Street elevation). The current elevation facing Winchester Street is a one-story brick façade with windows.

Section 2.C of this standard states “*Architectural features shall not serve primarily as an advertisement, commercial display, or identifying characteristics corresponding to corporate identity.*” In addition, section 2.G of this standard states that “*Exterior materials, textures, and colors shall minimize visual aggressiveness and shall harmonize with the City’s distinctive architectural identify and unique character.*” The Board should ask the Applicant how the proposed building design meets these standards.



Figure 4. Image of the proposed rendering for the Winchester Street facade. The Applicant proposes to replace the windows and brick exterior with metal siding with bright orange, faux self-storage doors that match the corporate U-Haul aesthetic.

STAFF REPORT

Recommended Motion:

Staff recommend that the Board continue the public hearing for SPR-897 Modification #1 to the December 20, 2021 Planning Board meeting.

CITY OF KEENE | PLANNING BOARD

SITE PLAN REVIEW / MODIFICATION APPLICATION



This form must be filled out in its entirety. If a box is not checked, staff will assume that the information is not provided and the application is, therefore, not complete. Incomplete applications will not be accepted for review.

A	Project Name U-Haul of South Keene		Date Received/Date of Submission:	
			Date of pre-application meeting:	
Tax Map Parcel number(s) 115 - 019 - 000 - 000 - 000 115 - 020 - 000 - 000 - 000		Date Application is Complete:		Community Development Dept File #: _____
Project Address: 472 Winchester Street Keene NH 03431	O W N E R	PRINTED Name: Amerco Real Estate Company		
		Signature: <i>A.H. Varin</i>		
Acreage/S.F.of Parcel: 4.03 Ac / 175,480 SF 3.48 Ac / 151,690 SF		Address: 499 Montgomery St, Chicopee, MA 01020		
Zoning District: CL - Commerce Limited		A P P L I C A N T	Telephone\ Email: 603-762-5535	
			PRINTED Name: Same as Owner	
			Signature: <i>A.H. Varin</i>	
		Address:		Telephone\ Email:

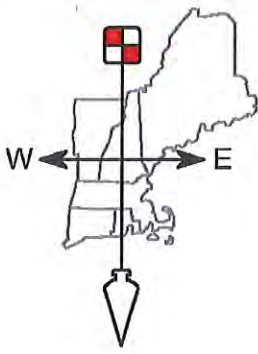
Modifications: Is this a modification to a previously-approved site plan: No Yes: SPR#: _____ Date: _____

For those sections of the application that are not affected by the proposed modification to the previously approved site plan, you are encouraged to request exemptions in lieu of submitting required documents.

B Descriptive Narrative Including

- | | | |
|--|---|---|
| <input type="checkbox"/> Type of development | <input type="checkbox"/> Sedimentation Control | <input type="checkbox"/> Scope/scale of development |
| <input type="checkbox"/> Proposed uses | <input type="checkbox"/> Vegetation | <input type="checkbox"/> Parcel size |
| <input type="checkbox"/> Location of access points | <input type="checkbox"/> Debris management | <input type="checkbox"/> Proposed stormwater, drainage & erosion plan |
| <input type="checkbox"/> Any other descriptive information | <input type="checkbox"/> Disposal proposals for boulders, stumps & debris | |

C A complete application must include the following



FIELDSTONE

Surveying ♦ Engineering
Land Planning ♦ Septic Designs

LAND CONSULTANTS, PLLC

206 Elm Street, Milford, NH 03055 - Phone: 603-672-5456 - Fax: 603-413-5456
www.FieldstoneLandConsultants.com

Site Plan Review Application Narrative

U-HAUL OF SOUTH KEENE
Tax Map Parcel 115-019 & 20
472 Winchester Street
Keene, New Hampshire

Revised November 8, 2021

Project Narrative:

Fieldstone Land Consultants, on behalf of Amerco Real Estate Company, is submitting this application for Planning Board approval. The proposal consists of renovating two existing buildings, constructing one new building, setting pre-fabricated self-storage units, and expanding parking to the southern side of the buildings. The site will be utilized as a U-Haul truck rental business, and a self-storage facility with both climate-controlled and dry storage units.

There are currently two lots owned by the applicant that would be joined into one lot to accommodate the proposed site development. The former Clark Distributors site is located on Tax Map Parcel 115-020 with the street address of 472 Winchester Street. The lot size is 4.03+/- acres (175,480 square feet) with 445.91 feet of frontage along Krif Road and 263.5 feet along Winchester Street. The abutting lot is Tax Map Parcel 115-019 with the street address of 0 Krif Road. This lot is 3.48+/- acres (151,690 square feet) with 50.0 feet of frontage on Krif Road. The combined parcel size will be 7.51 acres (327,170 square feet) after merging of the two lots.

The applicant is looking to redevelop the site as a multi-use development. The existing two buildings will be renovated for new business uses. The larger (30,493 square foot), main building will be repurposed as a climate-controlled storage facility. The smaller (3,810 square foot), detached building will be renovated as a U-Haul truck rental business with a showroom and shop to install towbars and hitches on vehicles.

To the east of the existing buildings, a new building is proposed at 12,175 square feet in size, to warehouse U-Box mobile storage units. The building will have a loading dock along the eastern façade to accommodate two tractor trailers.

To the south of the existing buildings there will be pre-fabricated storage units. These units will not be climate controlled, and just dry storage. The area will be paved to provide access to the storage units. Over 10 percent of these units will be ADA accessible with paved travel ways and compliant grades.

The southern border of the property abuts the Ash Swamp Brook. The majority of the site is located within the 100-year floodplain, with the 100-year flood elevation at 471.2 feet. The proposed building,

display area, rear parking and storage unit areas all fall within the Floodplain. The fill required to develop these areas will be offset by a flood compensatory storage area. This was done along the southern end of the property and provides a total volume of compensation to offset the volume of fill in the floodplain. The Floodway also crosses the south-southwest border of the property as depicted on the plans. There is no fill proposed within the floodway, although there is flood compensation cut proposed within the Floodway. A portion of the site is also in the City's Surface Water Overlay Protection District with a 30' buffer requirement. The work proposed within the buffer is for floodplain compensatory storage grading, as allowed under Section 11.5.L of the Land Development Code, and the paved driveway around the southwest corner of the existing building. A separate Floodplain Development Permit will be required for this work and a Conditional Use Permit for the driveway encroachment of the thirty (30') buffer.

Erosion and sediment controls are specified on the Grading Plan and Erosion Control Details. These will consist of silt fence along the work limit and silt socks installed in nearby catch basins. The slopes for the flood compensation area will have erosion control matting installed, as the slopes are designed at 2:1.

The current vegetation along the bank of the brook will be maintained and proposed trees will be planted to meet the parking lot requirements.

Construction debris will be managed by the general contractor with a temporary on-site dumpster. Along the southern side of the building a dumpster enclosure has been proposed. The enclosure will hide the dumpster so it is not visible from the City street.

Access to the site will be provided by the existing driveway curb cuts in four (4) locations; one curb cut on Winchester Street and three on Krif Road. A NHDOT Driveway Permit will not be required for this proposal.

Development Standards Narrative:

- 1. Drainage** – This project consists of existing and proposed buildings and parking lots. The existing condition has all the parking lots draining to catch basins tied into the Krif Road drainage system. The existing northwest parking lot will continue to utilize the existing drainage catch basins. The existing drainage in northeast corner will be modified to accommodate the proposed warehouse building. There will be a slight decrease in stormwater runoff to the Krif Road drainage system. The majority of the proposed pavement and building areas will drain to a subsurface pipe and stone system. The paved areas will drain to catch basins that have perforated pipe and stone between them. This will infiltrate water into the ground. There are overflow pipes that will allow water to outfall to the large basin created for floodplain compensatory storage. Treatment of the stormwater will be provided with deep sump catch basins, outlet hoods, and filtering through the native soil, above the seasonal high water table. Please refer to the Grading Plan and Stormwater Management Report for further details on drainage.
- 2. Sedimentation/Erosion Control** – Silt fence will be installed along the down gradient perimeter of the site. The existing catch basins will have silt socks installed, as well as all the proposed catch basins prior to final paving. The overflow pipes from the subsurface system will outfall to

rip-rap stone aprons to prevent erosion. The slopes of the flood compensation area that are 2:1 or steeper will have stabilization fabric installed as a preventative measure.

3. **Hillside Protection** – There is no hillside protection required for this project.
4. **Snow Storage and Removal** – Snow storage is shown on the plan outside the surface water buffer. If snow is excessive, it will be hauled off site.
5. **Flooding** – This property is located within the 100 year floodplain as it is adjacent to the Ash Swamp Brook, which ties into the Ashuelot River. Based on floodplain elevations, the majority of the site is within the 100 year floodplain with the exception of the buildings. The proposed building and storage units will all have finished floor elevations 1 foot or more above the floodplain elevation of 471.2'. These elevations force the new buildings and most of the pavement to be fill within the floodplain. This fill has been offset by a large compensatory storage area along the southern portion of the property. The floodplain compensation area will provide enough volume that there will not be an increase in floodwaters elsewhere in the floodplain.
6. **Landscaping** – The proposed landscaping will provide shade trees based on the total parking count. The paved display area is not to be used or striped for parking, therefore, no landscaping was proposed in this area.
7. **Noise** – The proposed use will not generate any noise that will impact surrounding properties or businesses. The most noise will come from the Showroom building that installs tow bars and hitches in the shop. This noise will be minimal and done inside of the garage portion of the building. Any HVAC mechanical equipment will be located to the rear of the buildings. All of the surrounding businesses are commercial facilities that have similar levels of noise generation. We do not foresee noise as being problematic with the proposed use.
8. **Screening** – The southern border will have existing trees along the brook that will remain. The storage units will mostly be screened by the existing building. The dumpster enclosure provides screening of the dumpster located at the rear of the site.
9. **Air Quality** – The proposed operation will have no impacts to air quality.
10. **Lighting** – Site lighting will be provided for the proposed improvements, as shown on the lighting plan. All fixtures are full cutoff LED light fixtures and wall mounted. The proposed lights are at the rear of the existing building and along the storage units. The light levels proposed do not go past the property lines. All proposed lights will have motion or occupancy sensors to reduce electrical use and act as security lighting. All existing, wall mounted lights will remain, and one, double-fixture pole light will be removed from the northwest parking lot.
11. **Sewer and Water** – The site currently has municipal sanitary sewer and water service, which will continue to be used. The proposed warehouse will have a water supply for fire sprinklers, which will be incorporated into the relocation of the fire hydrant at the northeast entrance. No water is required or proposed for the storage units. No new sewer services are proposed.
12. **Traffic** – The proposed use will not increase traffic to the site in comparison to the previous distribution center. Self-storage traffic tends to be very spread out throughout the day.
13. **Driveways** – There are four existing curb cuts, that will remain. The northeast entrance will be widened to accommodate truck turning to the loading dock. Currently there is access to only two sides of the existing buildings. The proposed driveways will provide access to all four sides of the buildings, improving fire truck access to the site.
14. **Hazardous and Toxic Materials** – There is no proposed hazardous or toxic materials associated with this project.
15. **Filling/Excavation** – There will be filling in the floodplain and excavation to offset that fill

volume. No materials are being excavated for resale.

- 16. Wetlands** – There are no wetlands within the proposed project limits. The Ash Swamp Brook abuts the property but no work is proposed to the edge of the brook. There will be a small berm remaining between the compensatory storage area and the edge of the brook as a buffer.
- 17. Surface Waters** – The Ash Swamp Brook abuts the property along the southern property line. The floodplain compensation area will cross into the buffer. We are requesting a Conditional Use permit for the driveway encroaching the buffer at the southwest corner of the existing building. The stormwater from this paved area will be collected by catch basins and flow into the subsurface drainage system.
- 18. Stump Dumps** – There will be no stump dumps on the property. All tree stumps are to be removed, and hauled off site. This is noted on the Landscaping Plan.

Architectural and Visual Appearance – The U-Haul Center of South Keene's proposed design is to provide a welcoming and beneficial business for the city of Keene and its residents. The revitalized building design encompasses elements of the existing buildings while adding modern touches that complement and provide a cohesive appearance to the location. The buildings maintain a modern aesthetic utilizing clean lines with varied parapets, architectural fenestration to enhance retail uniformity in the area, and LEED energy-compliant materials that reduce waste and are environmentally friendly. Enhanced design features include brick and wood grain paneling, adding warmth, texture, and articulation to the façade.

Material Uses:

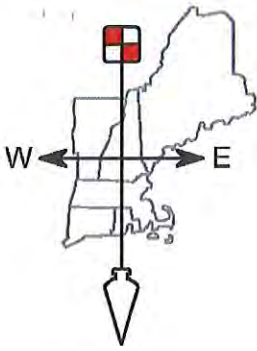
Atas Isoleren ML - Regal White & Sandstone: The use of the two different Atas Isoleren panels provides articulation along with superior insulation.

Atas Rigid Wall - Sandstone: Atas Rigid wall architectural panels add further articulation to the façade and provide horizontal modulation.

Atas Metafor - Ascot White: These vertical panels compliment the existing sheer paneling, tying the west façade into the other elevations of the property.

Atas Woodland Series - Walnut: These two-tone wood grain panels provide an authentic wood appearance along with exceptional durability.

Existing Brick: Enhances the appearance of the building by utilizing a change in material and providing visual separation to the façade.



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Land Planning ♦ Septic Designs

LAND CONSULTANTS, PLLC

206 Elm Street, Milford, NH 03055 - Phone: 603-672-5456 - Fax: 603-413-5456
www.FieldstoneLandConsultants.com

November 8, 2021

City of Keene Planning Board

Attn: Mari Brunner, Planner

City Hall - 4th Floor

3 Washington Street-

Keene, NH 0343

**RE: Site Plan Submittal – SPR-897
U-Haul of South Keene – Exception Request**

Dear Planning Board,

Fieldstone Land Consultants, on behalf of Amerco Real Estate, is submitting this exception request for Planning Board approval, based on the new Land Development Code (LDC) and the criteria outlined in Section 22.5.6.

§ 9.3.3.B.1 (and 22.5.4.9) Driveway Design Standards: Commercial Driveways: *The driveway shall be a minimum of 12-ft wide and maximum of 25-ft wide at the property line and 50-ft wide at the curblin, unless additional width or lanes are required as the result of a traffic study and/or geometric analysis prepared by a NH licensed engineer.*

As presented to the Board, the existing curb cut onto Krif Road is proposed to be widened to 45 feet at the property line, and 66 feet at the curblin. The width was determined by a geometric analysis by a licensed NH engineer, where a tractor trailer truck was modeled for turning movements in and out of the site. The location of the proposed loading dock and the turning radii of a tractor trailer truck, both entering and exiting, was unable to maintain the existing driveway width. The existing driveway width in this location is 31 feet at the property line, and 54 feet at the curblin. The proposed width would allow for easier truck movements in and out of the loading dock area on the new warehouse building. The widening of this driveway would require the relocation of a fire hydrant. This hydrant relocation would be incorporated into the work related to running a new water service to the proposed building.

We respectfully request that the Board grant an exception to this requirement for the following reasons, as outlined in the LDC exception criteria.

- A. *Issuance of the exception will not adversely affect the safety of pedestrians, bicyclists, and vehicles using adjacent streets and intersections.*
If the exception is granted the proposed driveway will not adversely impact the safety of the public, as it will allow for easier truck turning motions and trucks can exit the site at a perpendicular angle. This will provide a better sight distance for the truck driver and thus a safer traffic scenario. There are no existing sidewalks or bike lanes on this section of Krif Road.
- B. *Issuance of the exception does not adversely affect the efficiency and capacity of the street or intersection.*
By issuing an exception for this driveway widening, the efficiency and capacity of the street will remain unchanged. The wider driveway will reduce how long the truck blocks the city street to maneuver into the site. The trucks will enter the site, loop around storage units, and back into the loading dock without having to back into the site from the Krif Road.
- C. *There are unique characteristics of the land or property which present a physical hardship to the requestor:*
The unique characteristics of the property are the location of the existing buildings, access points, and the adjacent Ash Swamp Brook with a substantial floodplain. These characteristics hindered the applicant from obtaining an ideal site layout for the mixed-use business. The floodplain restricts the southern portion of the property and the location of the warehouse building. These reasons together present a physical hardship to the applicant.
- D. *In no case shall financial hardship be used to justify the granting of the exception:*
The financial implications of redeveloping the site are not a hardship of this request. The physical location of the driveway and location of the proposed building, require a wider entrance to accommodate the turning motions of large trucks. The loading dock is able to handle two trucks at a time, and having extra space for safe turning and egress of the site is an important function of the business.

If approved, the wider driveway will not have adverse impacts on the public or functionality of the Krif Road corridor. Thank you for your consideration in granting this exception.

Best Regards,
FIELDSTONE LAND CONSULTANTS, PLLC

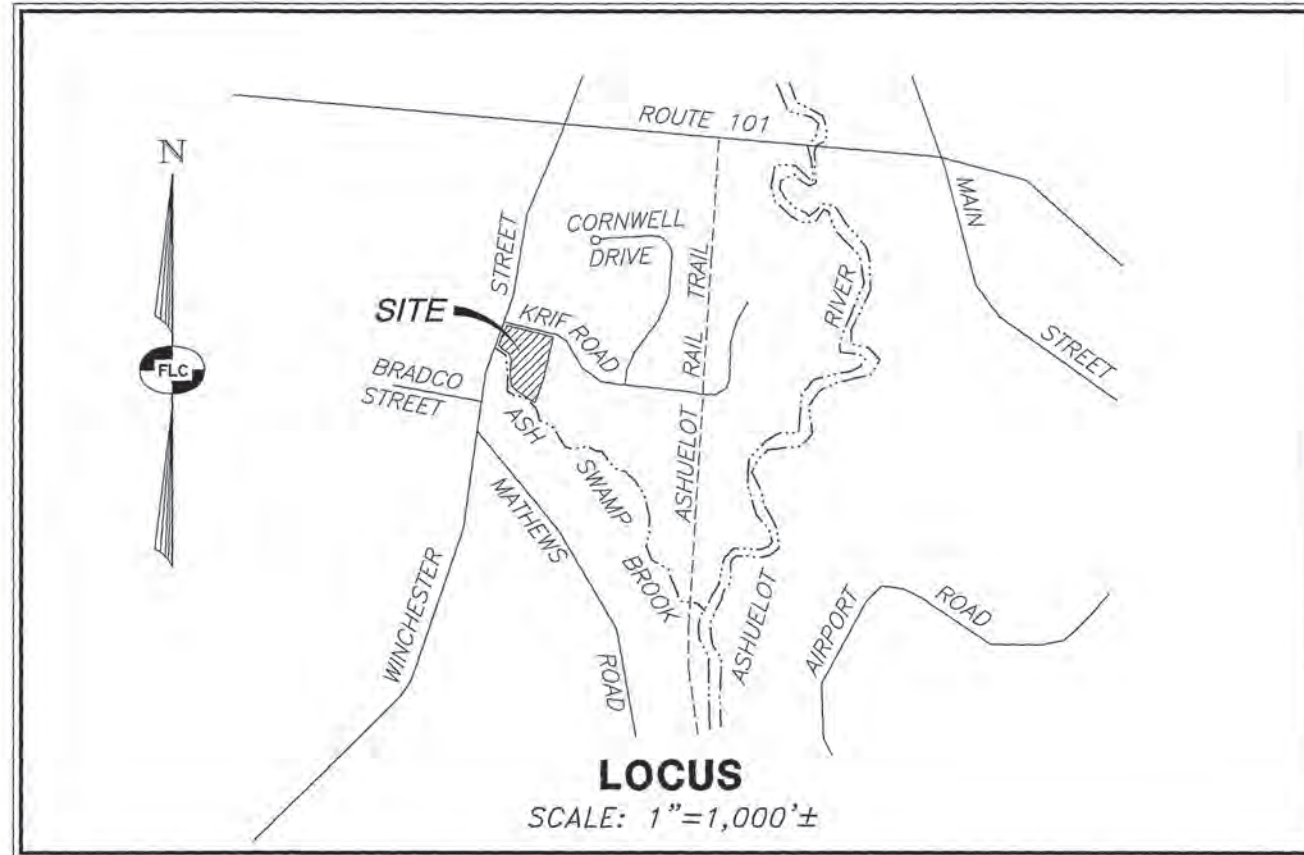

John Noonan
Project Engineer

UHAUL OF SOUTH KEENE

- TAX MAP 115, LOTS 19 & 20 -

472 WINCHESTER STREET
& O KRIF ROAD
KEENE, NH 03431

REVISED: NOVEMBER 8, 2021



SHEET INDEX		
PAGE	SHEET	TITLE
1	CV-1	COVER SHEET
2	EX-1	EXISTING CONDITIONS PLAN
3	SP-1	SITE PLAN
4	GR-1	GRADING & DRAINAGE PLAN
5	UT-1	UTILITY PLAN
6	LS-1	LANDSCAPING PLAN
7	LT-1	LIGHTING PLAN
8	DT-1	EROSION CONTROL DETAILS
9	DT-2	CONSTRUCTION DETAILS
10	DT-3	CONSTRUCTION DETAILS

APPLICANT & LAND OWNER:

AMERCO REAL ESTATE COMPANY

499 MONTGOMERY STREET, CHICOPEE, MA 01020

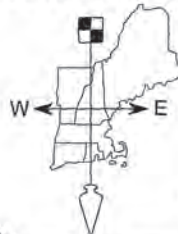


1. THE LOCATION OF THE UTILITIES SHOWN ARE APPROXIMATE. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO LOCATE AND PRESERVE ALL UTILITY SERVICES.
2. THE CONTRACTOR IS RESPONSIBLE FOR CONTACTING AND COORDINATING WITH ALL UTILITY COMPANIES AND JURISDICTIONAL AGENCIES PRIOR TO AND DURING CONSTRUCTION.
3. THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND PROPOSED WORK PRIOR TO CONSTRUCTION.

CONTACT DIG SAFE
72 HOURS PRIOR
TO CONSTRUCTION

DIGSAFE.COM
811

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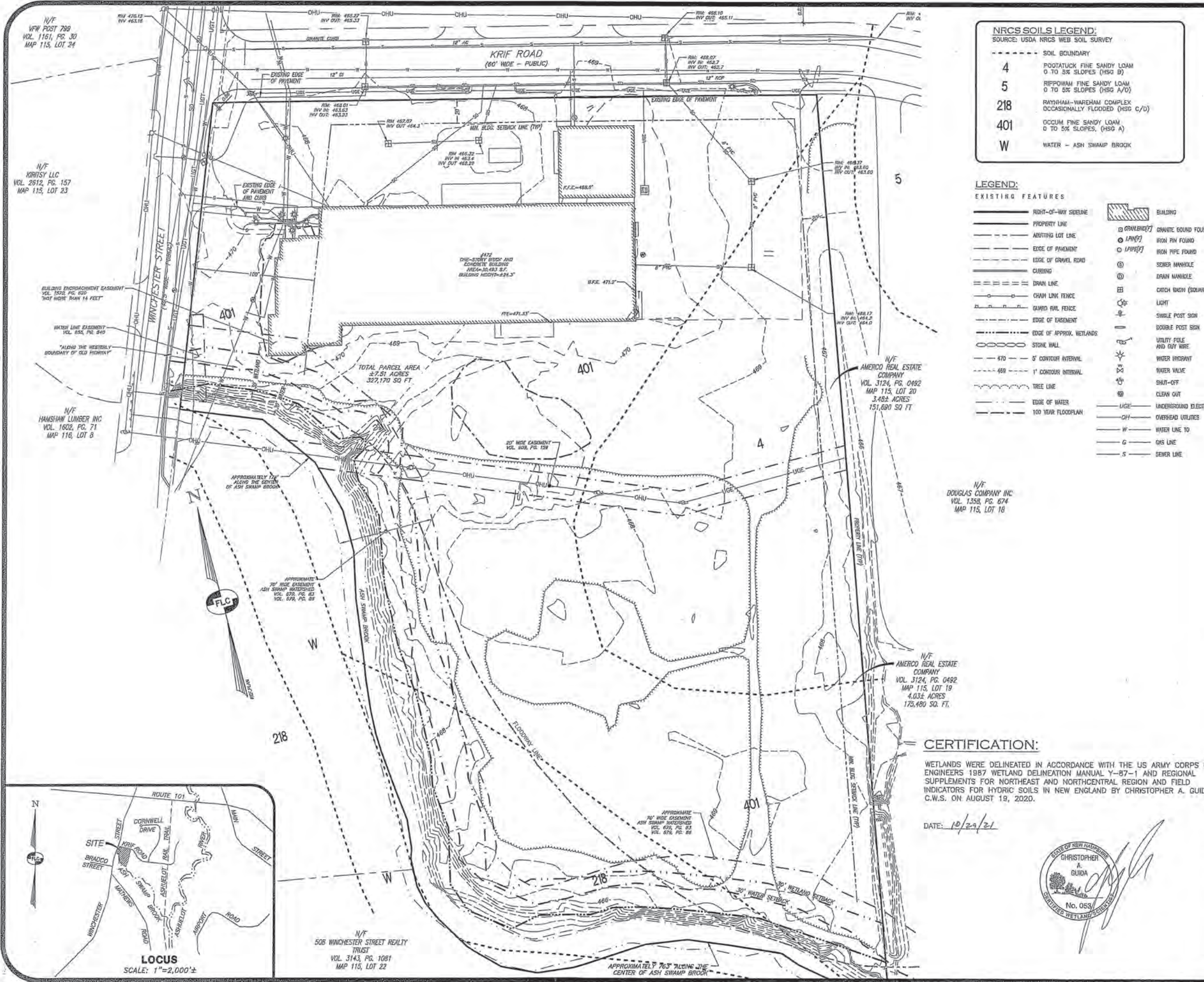


LAND-OWNER SIGNATURE
OWNER: _____ DATE: _____

APPROVED BY THE KEENE PLANNING BOARD
ON: _____ CERTIFIED BY _____ AND _____
CHAIRMAN: _____ SECRETARY: _____

REV.	DATE	DESCRIPTION	C/O	DR	CK
A	11/8/21	PER CITY STAFF REVIEW COMMENTS		JEN	CEB

FILE: 2250CV00A.dwg PROJ. NO. 2250.00 SHEET: 41 OF 80



NRCS SOILS LEGEND:
SOURCE: USDA NRCS WEB SOIL SURVEY

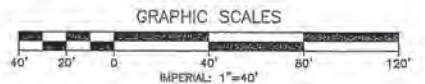
---	SOIL BOUNDARY
4	FOOTATUCK FINE SANDY LOAM 0 TO 5% SLOPES (H50 B)
5	RIPPOWAM FINE SANDY LOAM 0 TO 5% SLOPES (H5G A/D)
218	RAYHAM-WAREHAM COMPLEX OCCASIONALLY FLOODED (H5G C/D)
401	OCCUM FINE SANDY LOAM 0 TO 5% SLOPES, (H5G A)
W	WATER - ASH SWAMP BROOK

LEGEND:

EXISTING FEATURES	
---	RIGHT-OF-WAY SIDELINE
---	PROPERTY LINE
---	ABUTTING LOT LINE
---	EDGE OF PAVEMENT
---	EDGE OF GRAVEL ROAD
---	CURSING
---	DRAIN LINE
---	CHAIN LINK FENCE
---	GUARD RAIL FENCE
---	EDGE OF EASEMENT
---	EDGE OF APPROX. WETLANDS
---	STONE WALL
---	5' CONTOUR INTERVAL
---	1' CONTOUR INTERVAL
---	TREE LINE
---	EDGE OF WATER
---	100 YEAR FLOODPLAIN
[Symbol]	BUILDING
[Symbol]	GRANITE BOUND FOUND
[Symbol]	IRON PIN FOUND
[Symbol]	IRON PIPE FOUND
[Symbol]	SEWER MANHOLE
[Symbol]	DRAIN MANHOLE
[Symbol]	CATCH BASIN (SQUARE)
[Symbol]	LIGHT
[Symbol]	SINGLE POST SIGN
[Symbol]	DOUBLE POST SIGN
[Symbol]	UTILITY POLE AND OUT WIRE
[Symbol]	WATER HYDRANT
[Symbol]	WATER VALVE
[Symbol]	SHUT-OFF
[Symbol]	CLEAN OUT
[Symbol]	UNDERGROUND ELECTRIC
[Symbol]	OVERHEAD UTILITIES
[Symbol]	WATER LINE TO
[Symbol]	GAS LINE
[Symbol]	SEWER LINE

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 - EXISTING BUILDINGS = 34,303 SF, PROPOSED BUILDINGS = 41,075 = 75,378 SF (23%)
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 - (EXISTING IMP = 85,623 SF (26.6%), PROPOSED IMP = 210,947 SF (65.6%))
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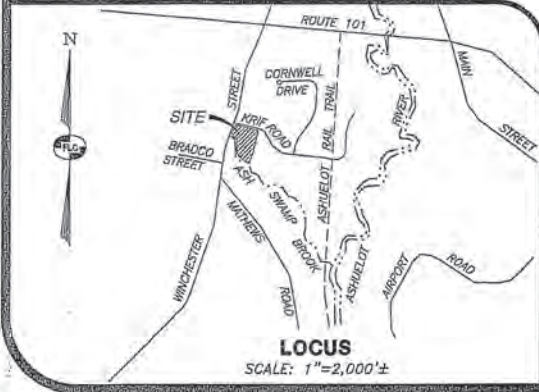
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 - "EXISTING PLAN", SCALE: 1"=50' AND DATED OCTOBER 20, 2017 BY BRICKSTONE LAND USE CONSULTANTS.
 - "ALTA/HSPS LAND TITLE SURVEY", SCALE 1" = 50', AND DATED AUGUST 27, 2020 BY CORNERSTONE ENERGY SERVICES, PROVIDED BY APPLICANT AND LAND OWNER.



CERTIFICATION:

WETLANDS WERE DELINEATED IN ACCORDANCE WITH THE US ARMY CORPS OF ENGINEERS 1987 WETLAND DELINEATION MANUAL Y-87-1 AND REGIONAL SUPPLEMENTS FOR NORTHEAST AND NORTHCENTRAL REGION AND FIELD INDICATORS FOR HYDRIC SOILS IN NEW ENGLAND BY CHRISTOPHER A. GUIDA, C.W.S. ON AUGUST 19, 2020.

DATE: 10/29/21



EXISTING CONDITIONS
U-HAUL OF SOUTH KEENE
TAX MAP 115 LOTS 19 & 20
472 WINCHESTER STREET
KEENE, NEW HAMPSHIRE

PREPARED FOR:
AMERCO REAL ESTATE COMPANY
499 MONTGOMERY STREET, CHICOPEE, MA 01020

SCALE: 1" = 40' OCTOBER 15, 2021

Surveying + Engineering + Land Planning + Permitting + Septic Designs

FIELDSTONE
LAND CONSULTANTS, PLLC

206 Elm Street, Milford, NH 03055
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FILE: 2250SP00_E.dwg PROJ. NO. 2250.00 SHEET: EX-1 PAGE NO. 2 OF 10
Page 42 of 89

N/F
VW POST 798
VOL. 1161, PG. 30
MAP 115, LOT 24

N/F
KRITSY LLC
VOL. 2612, PG. 157
MAP 115, LOT 23

BUILDING ENCROACHMENT EASEMENT
VOL. 1570, PG. 810
NOT MORE THAN 14 FEET

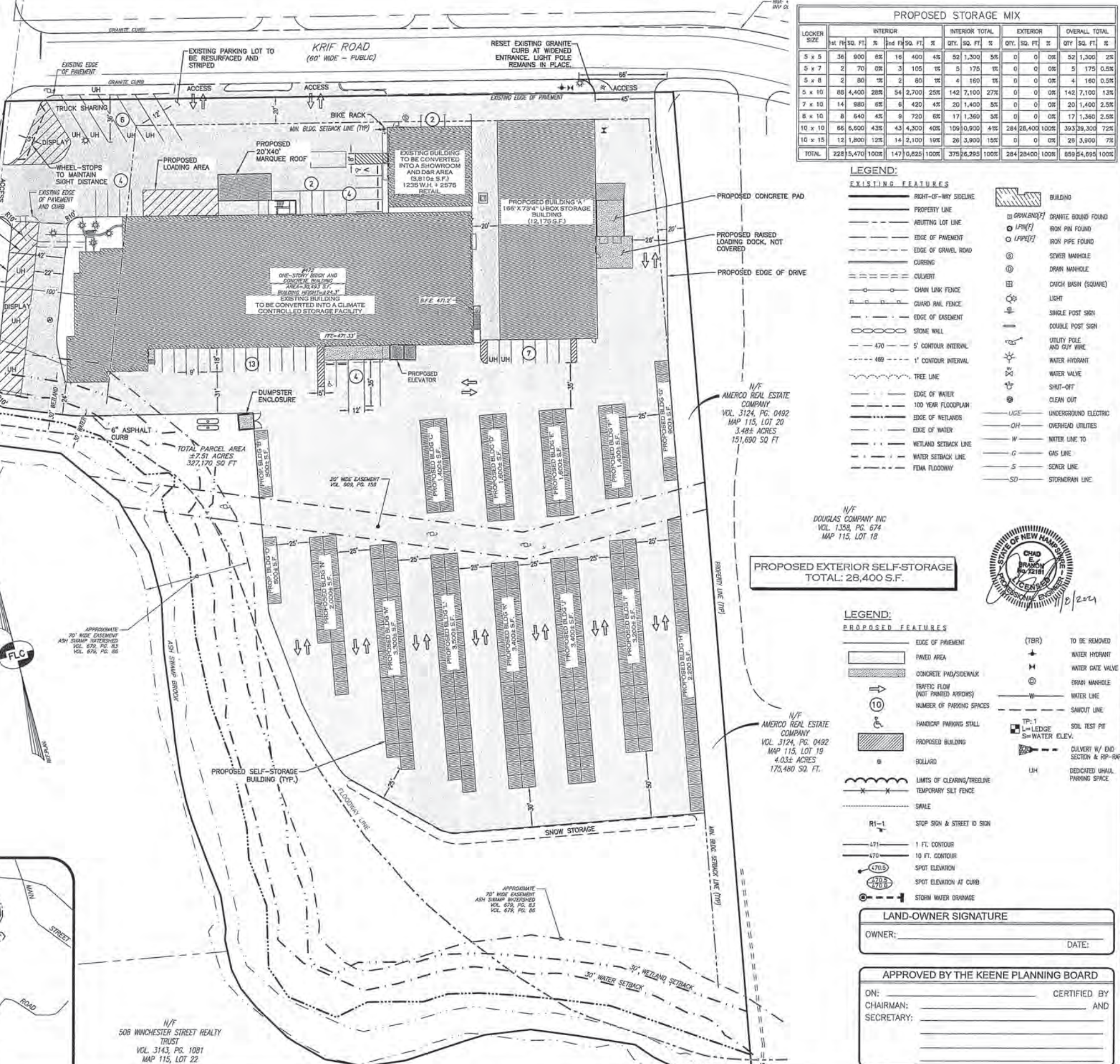
WATER LINE EASEMENT
VOL. 656, PG. 540

N/F
HAMSHAW LUMBER INC
VOL. 1602, PG. 71
MAP 115, LOT 8

APPROXIMATE
70' WIDE EASEMENT
ASH SWAMP INTERSECTED
VOL. 679, PG. 83
VOL. 679, PG. 86

N/F
508 WINCHESTER STREET REALTY
TRUST
VOL. 3143, PG. 1081
MAP 115, LOT 22

LOCUS
SCALE: 1"=2,000'



PROPOSED STORAGE MIX

LOCKER SIZE	INTERIOR			INTERIOR TOTAL			EXTERIOR			OVERALL TOTAL		
	QTY	SQ. FT.	%	QTY	SQ. FT.	%	QTY	SQ. FT.	%	QTY	SQ. FT.	%
5 x 5	36	900	6%	16	400	4%	52	1,300	5%	0	0	0%
5 x 7	2	70	0%	3	105	1%	5	175	1%	0	0	0%
5 x 8	2	80	1%	2	80	1%	4	160	1%	0	0	0%
5 x 10	88	4,400	28%	54	2,700	25%	142	7,100	27%	0	0	0%
7 x 10	14	980	6%	6	420	4%	20	1,400	5%	0	0	0%
8 x 10	8	640	4%	9	720	6%	17	1,360	5%	0	0	0%
10 x 10	86	6,600	43%	43	4,300	40%	109	9,900	41%	284	28,400	100%
10 x 15	12	1,800	12%	14	2,100	19%	26	3,900	15%	0	0	0%
TOTAL	228	15,470	100%	147	8,820	100%	375	26,290	100%	284	28,400	100%

- LEGEND:**
- EXISTING FEATURES**
- RIGHT-OF-WAY SIDEWALK
 - PROPERTY LINE
 - ABUTTING LOT LINE
 - EDGE OF PAVEMENT
 - EDGE OF GRAVEL ROAD
 - CURBING
 - CULVERT
 - CHAIN LINK FENCE
 - GUARD RAIL FENCE
 - EDGE OF EASEMENT
 - STONE WALL
 - 470 - 5' CONTOUR INTERVAL
 - 489 - 1' CONTOUR INTERVAL
 - TREE LINE
 - EDGE OF WATER
 - 100 YEAR FLOODPLAIN
 - EDGE OF WETLANDS
 - EDGE OF WATER
 - WETLAND SETBACK LINE
 - WATER SETBACK LINE
 - FEMA FLOODWAY
- EXISTING BUILDINGS**
- GRANITE BOUND FOUND
 - IRON PIN FOUND
 - IRON PIPE FOUND
 - SEWER MANHOLE
 - DRAIN MANHOLE
 - CATCH BASIN (SQUARE)
 - LIGHT
 - SINGLE POST SIGN
 - DOUBLE POST SIGN
 - UTILITY POLE AND GUY WIRE
 - WATER HYDRANT
 - WATER VALVE
 - SHUT-OFF
 - CLEAN OUT
 - UNDERGROUND ELECTRIC
 - OVERHEAD UTILITIES
 - WATER LINE TO
 - GAS LINE
 - SEWER LINE
 - STORMDRAIN LINE

- LEGEND:**
- PROPOSED FEATURES**
- EDGE OF PAVEMENT
 - PAVED AREA
 - CONCRETE PAD/SIDEWALK
 - TRAFFIC FLOW (NOT PAINTED ARROWS)
 - NUMBER OF PARKING SPACES
 - HANDICAP PARKING STALL
 - PROPOSED BUILDING
 - BOLLARD
 - LIMITS OF CLEARING/TREE LINE
 - TEMPORARY SILT FENCE
 - SWALE
 - R1-1 STOP SIGN & STREET ID SIGN
 - 1 FT. CONTOUR
 - 10 FT. CONTOUR
 - SPOT ELEVATION
 - SPOT ELEVATION AT CURB
 - STORM WATER DRAINAGE
- TO BE REMOVED**
- (TBR)
 - WATER HYDRANT
 - WATER GATE VALVE
 - DRAIN MANHOLE
 - WATER LINE
 - SAWTOOTH LINE
 - SOIL TEST PIT
 - WATER ELEV.
 - CULVERT W/ END SECTION & RP-RAP
 - UH DEDICATED UHAUL PARKING SPACE

LAND-OWNER SIGNATURE

OWNER: _____ DATE: _____

APPROVED BY THE KEENE PLANNING BOARD

ON: _____ CERTIFIED BY: _____ AND _____

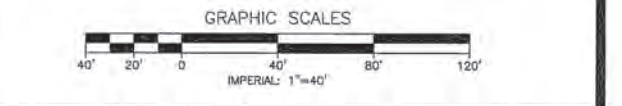
CHAIRMAN: _____ SECRETARY: _____

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- PARKING REQUIREMENTS:**
- PARKING BASED ON NEW I.D.C. SEPTEMBER 1, 2021 (TABLE 9-1)
- RETAIL = 1 SP / 250 SF GFA
2,575 SF / 250 SF = 10.3 SPACES
SELF-STORAGE = 1 SP / 3,000 SF
INTERIOR = 26,295 / 3,000 = 8.8 SPACES
EXTERIOR = 28,400 / 3,000 = 9.5 SPACES*
WAREHOUSE = 1 SP / 2,000 SF GFA
12,175 SF / 2,000 SF = 6.1 SPACES REQUIRED
1,235 SF / 2,000 SF = 1 SPACE
TOTAL REQUIRED: 36 SPACES
TOTAL PROVIDED: 42 SPACES
- A.D.A. SPACES REQ'D: 42 SPACES = 1 A.D.A. REG. + 1 A.D.A. VAN SPACE
A.D.A. PROVIDED: 2 SPACES, 1 REGULAR + 1 VAN SPACE
OVER 10% OF EXTERIOR STORAGE UNITS (31 UNITS) ARE A.D.A. COMPLIANT
- * EXTERIOR SELF-STORAGE IS ALLOWED TO PARK IN FRONT OF THEIR UNITS.

- PERMITS REQUIRED:**
- CITY OF KEENE PLANNING BOARD: SITE PLAN APPROVAL
 - CITY OF KEENE PLANNING BOARD: CONDITIONAL USE FOR PAVEMENT IN 30' WETLAND BUFFER
 - CITY OF KEENE ZONING BOARD: VARIANCE FOR MARQUEE ROOF (DENIED & RELOCATED).
 - CITY OF KEENE FLOODPLAIN DEVELOPMENT PERMIT.
 - CITY OF KEENE BUILDING PERMIT.
 - CITY OF KEENE VOLUNTARY LOT MERGER.
 - NHDES ALTERATION OF TERRAIN PERMIT.
 - FEMA LETTER OF MAP REVISION AND ELEVATION CERTIFICATE.

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 - "EXISTING PLAN", SCALE: 1"=50' AND DATED OCTOBER 20, 2017 BY BRICKSTONE LAND USE CONSULTANTS.



REV.	DATE	DESCRIPTION	C/D	DR	CK
C	11/8/21	REVISIONS PER CITY STAFF COMMENTS		JEN	CEB
B	10/28/21	REVISED CANOPY LOCATION, PARKING SPOTS		JEN	CEB
A	8/23/21	REVISED PER IN-HOUSE & CITY COMMENTS FOR ZBA		JEN	CEB

SITE PLAN CONCEPT
U-HAUL OF SOUTH KEENE
TAX MAP 115 LOTS 19 & 20
472 WINCHESTER STREET
KEENE, NEW HAMPSHIRE

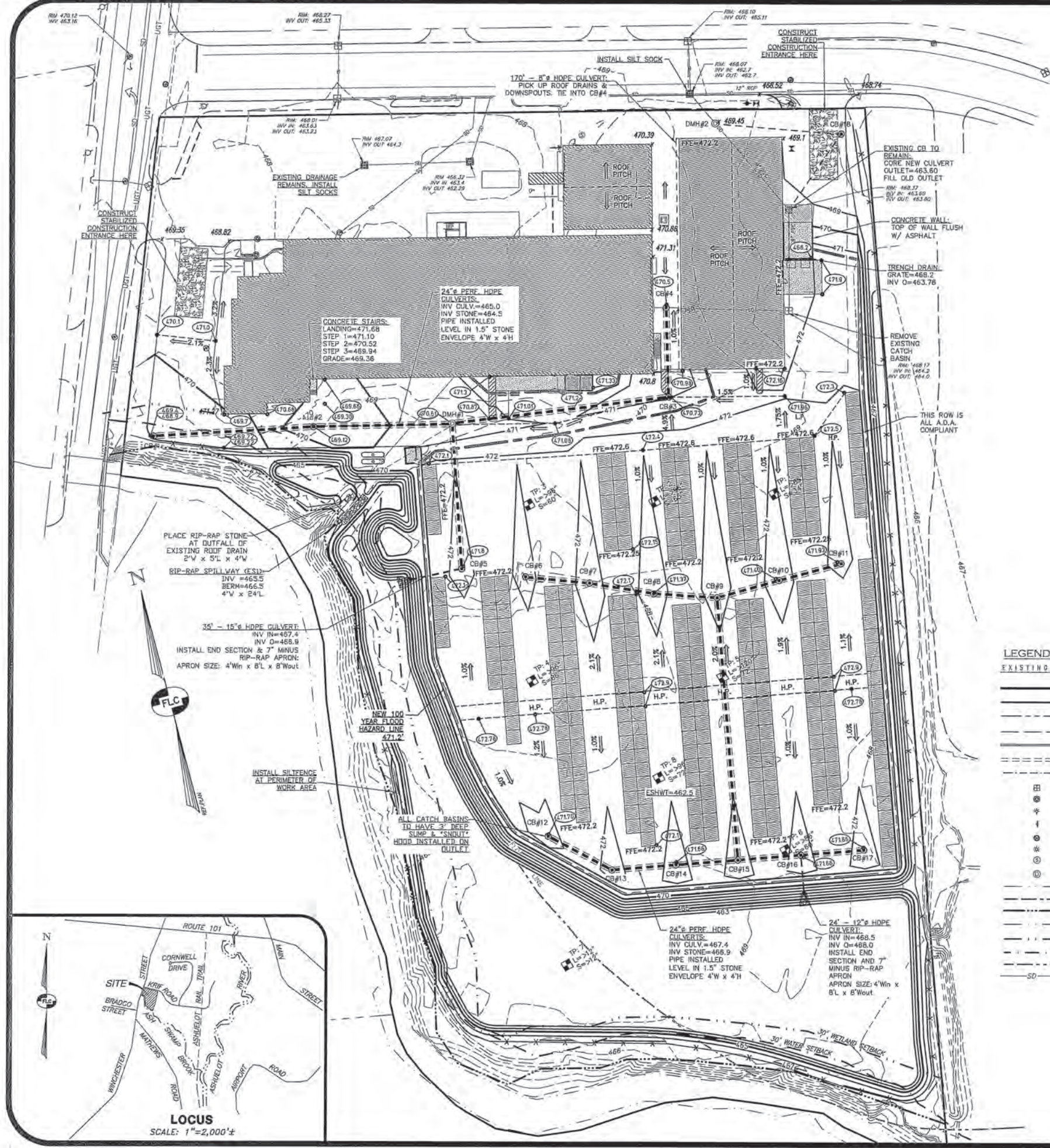
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499 MONTGOMERY STREET, CHICOPEE, MA 01020

SCALE: 1" = 40' OCTOBER 15, 2021

Surveying ♦ Engineering ♦ Land Planning ♦ Permitting ♦ Septic Designs

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GRADING NOTES:

1. ALL WORK SHALL CONFORM TO THE APPLICABLE REGULATIONS AND STANDARDS OF THE CITY OF KEENE AND SHALL BE BUILT IN ACCORDANCE WITH THE PLANS AND SPECIFICATIONS. THE CITY OF KEENE DEPARTMENT OF PUBLIC WORKS SPECIFICATIONS FOR ROAD CONSTRUCTION AND SEWERS AND DRAINS AND THE NHDOT STANDARDS FOR ROAD AND BRIDGE CONSTRUCTION APPROVED AND ADOPTED 2016 ARE HEREBY INCORPORATED BY REFERENCE.
2. ROAD AND DRAINAGE CONSTRUCTION SHALL CONFORM TO THE TYPICAL SECTIONS AND DETAILS SHOWN ON THE PLANS AND THE SPECIFICATIONS NOTED ABOVE. ANY ALTERATION OF THIS DESIGN OR CHANGE DURING CONSTRUCTION MAY REQUIRE APPROVAL OF VARIOUS CITY BOARDS OR AGENCIES AND SHALL BE DISCUSSED WITH THE OWNER AND ENGINEER PRIOR TO CONSTRUCTION.
3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING THE LOCATION, SIZE, AND ELEVATION OF ALL EXISTING UTILITIES SHOWN OR NOT SHOWN ON THESE PLANS AND SHALL VERIFY THAT ALL THE INFORMATION SHOWN HEREON IS CONSISTENT, COMPLETE, ACCURATE, AND CAN BE CONSTRUCTED PRIOR TO AND/OR DURING CONSTRUCTION. THE ENGINEER SHALL BE NOTIFIED IN WRITING OF ANY DISCREPANCIES, ERRORS, OMISSIONS, OR EXISTING UTILITIES FOUND INTERFERING WITH THE PROPOSED CONSTRUCTION SO THAT REMEDIAL ACTION MAY BE TAKEN BEFORE PROCEEDING WITH THE WORK.
4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING "DIGSAFE" AT LEAST 72 HOURS PRIOR TO THE START OF CONSTRUCTION (811).
5. THE CONTRACTOR IS RESPONSIBLE FOR CONTACTING THE APPROPRIATE CITY DEPARTMENTS PRIOR TO CONSTRUCTION TO ARRANGE FOR NECESSARY INSPECTIONS.
6. BLASTING, IF REQUIRED, SHALL BE PERFORMED IN ACCORDANCE WITH THE CITY OF KEENE FIRE DEPARTMENT REGULATIONS.
7. ALL DISTURBED NON-PAVED AREAS SHALL BE LOAMED AND SEEDDED IMMEDIATELY UPON BEING CONSTRUCTED.
8. ALL TRAFFIC SIGNS SHALL CONFORM TO THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES LATEST EDITION.
9. EXISTING PAVEMENT SHALL BE SAW-CUT AS NECESSARY. THE CONTRACTOR SHALL ENSURE A SMOOTH TRANSITION BETWEEN EXISTING AND NEW PAVEMENT.
10. ALL POWER WORK SHALL CONFORM WITH EVERSOURCE STANDARDS.
11. ALL TELEPHONE WORK SHALL CONFORM TO PROVIDERS' SPECIFICATIONS.
12. STREET RESTORATION, IF ANY, SHALL BE IN ACCORDANCE WITH NRO-265-13.
13. CONTRACTOR TO MAINTAIN A DOCUMENTATION LOG OF ALL INSPECTIONS.

PROPOSED DRAINAGE STRUCTURE SCHEDULE:

STRUCT.	FROM	TO	LENGTH FEET	DIA. INCHES	SLOPE FT/FT	STRUCT.	TO
CB #1	468.50	465.0	114	24	0.0	CB #2	465.0
CB #2	468.12	465.0	99	24	0.0	DMH #1	465.0
CB #3	470.73	465.0	155	24	0.0	DMH #1	465.0
CB #4	470.50	465.0	84	24	0.0	CB #3	465.0
DMH #1	470.61	465.0	104	24	0.0	CB #5	465.0
CB #5	471.80	467.40	35	15	0.014	OVERFLOW	466.90
CB #6	471.80	467.40	46	24	0.0	CB #7	467.40
CB #7	471.80	467.40	46	24	0.0	CB #8	467.40
CB #8	471.37	467.40	45	24	0.0	CB #9	467.40
CB #9	471.35	467.40	188	24	0.0	CB #15	467.40
CB #10	471.40	467.40	46	24	0.0	CB #9	467.40
CB #11	471.93	467.40	46	24	0.0	CB #10	467.40
CB #12	471.70	467.40	52	24	0.0	CB #13	467.40
CB #13	471.56	467.40	46	24	0.0	CB #14	467.40
CB #14	471.56	467.40	45	24	0.0	CB #15	467.40
CB #15	471.56	467.40	45	24	0.0	CB #16	467.40
CB #16	471.56	466.50	24	12	0.005	OVERFLOW	468.0
CB #17	471.56	467.40	45	24	0.0	CB #15	467.40
EX CB	468.37	463.60	65	8	0.04	DMH #2	462.95
CB #18	468.90	463.30	89	8	0.04	DMH #2	462.95
DMH #2	469.45	462.92	27.5	8	0.068	KRIF CB	462.7

LEGEND:

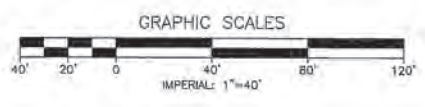
- | | |
|-----------------------|----------------------------------|
| RIGHT-OF-WAY SIDELINE | CONCRETE PAD/SIDEWALK |
| PROPERTY LINE | HANDICAP PARKING STALL |
| ABUTTING LOT LINE | PROPOSED BUILDING |
| EDGE OF PAVEMENT | TEMPORARY SILT FENCE |
| CURB | 1 FT. CONTOUR |
| DRAIN LINE | 10 FT. CONTOUR |
| EDGE OF EASEMENT | SPOT ELEVATION |
| CATCH BASIN | SPOT ELEVATION AT CURB |
| CLEAN OUT | STORM WATER DRAINAGE |
| HYDRANT | SIGN |
| WATER VALVE | WATER VALVE |
| LIGHT POLE | LIGHT POLE |
| SEWER MANHOLE | SEWER MANHOLE |
| DRAIN MANHOLE | DRAIN MANHOLE |
| EDGE OF WATER | SILT SOCK FOR CATCH BASINS |
| 100 YEAR FLOODPLAIN | SOIL TEST PIT |
| EDGE OF WETLANDS | WATER LEVEL |
| EDGE OF WATER | PROPOSED FLOODPLAIN |
| WETLAND SETBACK LINE | CULVERT W/ END SECTION & RIP-RAP |
| WATER SETBACK LINE | |
| FEMA FLOODWAY | |
| STORM DRAIN LINE | |

NRCS SOILS LEGEND:
SOURCE: USDA NRCS WEB SOIL SURVEY

SOIL BOUNDARY	
4	POOTLUCK FINE SANDY LOAM 0 TO 5% SLOPES (HSG B)
5	RIPPOWAM FINE SANDY LOAM 0 TO 5% SLOPES (HSG A/D)
218	RAYNHAM-WAREHAM COMPLEX OCCASIONALLY FLOODED (HSG C/D)
401	DODUM FINE SANDY LOAM 0 TO 5% SLOPES (HSG A)
W	WATER - ASH SWAMP BROOK

FLOODPLAIN - COMPENSATORY STORAGE:

1. THE LOTS LIE WITHIN THE BOUNDARY OF THE 100 YEAR FLOOD HAZARD PER FLOOD INSURANCE RATE MAP FOR THE CITY OF KEENE, GOSHEN COUNTY, COMMUNITY 330023, PANEL NUMBER 330050266E DATED MAY 23, 2006. BASED ON THIS PANEL, THE ESTIMATED ELEVATION OF THE 100 YEAR FLOOD AT THIS LOCATION IS 471.2 FT.
 2. THE PROPOSED SITE PLAN AND GRADING WILL REQUIRE A FLOODPLAIN DEVELOPMENT PERMIT.
 3. ESTIMATED SEASONAL HIGH WATER TABLE VARIES FROM 460.0 - 462.5'. THE INFILTRATION DESIGN AND FLOODPLAIN GRADING ARE BASED ON ESWT = 462.5'.
 4. ANY FILL AT OR BELOW ELEVATION 471.2 IS CONSIDERED FILL IN THE FLOODPLAIN. THESE VOLUMES HAVE BEEN OFFSET BY COMPENSATORY STORAGE FROM ELEVATIONS 463 - 471.2, AS LISTED BELOW:
- ELEVATION ZONE VOLUMES (SURFACE-SURFACE CALCULATION)
- ZONE: 463.0 TO 464.0
CUT VOLUME: 43,292.48 C.F., 1,603.43 C.Y.
FILL VOLUME: 9.69 C.F., 0.21 C.Y.
- ZONE: 464.0 TO 465.0
CUT VOLUME: 47,055.06 C.F., 1,742.78 C.Y.
FILL VOLUME: 14.62 C.F., 0.54 C.Y.
RUNNING TOTALS:
CUT VOLUME: 90,347.54 C.F., 3,346.21 C.Y.
FILL VOLUME: 20.31 C.F., 0.75 C.Y.
- ZONE: 465.0 TO 466.0
CUT VOLUME: 51,802.76 C.F., 1,911.21 C.Y.
FILL VOLUME: 11.24 C.F., 0.42 C.Y.
RUNNING TOTALS:
CUT VOLUME: 141,950.30 C.F., 5,257.42 C.Y.
FILL VOLUME: 31.55 C.F., 1.17 C.Y.
- ZONE: 466.0 TO 467.0
CUT VOLUME: 53,351.04 C.F., 2,072.33 C.Y.
FILL VOLUME: 264.55 C.F., 9.80 C.Y.
RUNNING TOTALS:
CUT VOLUME: 197,903.34 C.F., 7,329.75 C.Y.
FILL VOLUME: 296.10 C.F., 10.97 C.Y.
- ZONE: 467.0 TO 468.0
CUT VOLUME: 53,223.98 C.F., 1,971.25 C.Y.
FILL VOLUME: 2,469.50 C.F., 91.46 C.Y.
RUNNING TOTALS:
CUT VOLUME: 251,127.03 C.F., 9,301.00 C.Y.
FILL VOLUME: 2,765.60 C.F., 102.43 C.Y.
- ZONE: 468.0 TO 469.0
CUT VOLUME: 29,578.63 C.F., 1,099.21 C.Y.
FILL VOLUME: 56,171.67 C.F., 2,080.43 C.Y.
RUNNING TOTALS:
CUT VOLUME: 280,805.66 C.F., 10,400.21 C.Y.
FILL VOLUME: 58,937.27 C.F., 2,182.86 C.Y.
- ZONE: 469.0 TO 470.0
CUT VOLUME: 4,334.38 C.F., 160.55 C.Y.
FILL VOLUME: 102,978.26 C.F., 3,810.31 C.Y.
RUNNING TOTALS:
CUT VOLUME: 285,140.64 C.F., 10,560.76 C.Y.
FILL VOLUME: 161,815.52 C.F., 5,993.17 C.Y.
- ZONE: 470.0 TO 471.0
CUT VOLUME: 2,502.02 C.F., 92.67 C.Y.
FILL VOLUME: 122,277.32 C.F., 4,526.79 C.Y.
RUNNING TOTALS:
CUT VOLUME: 287,642.66 C.F., 10,653.43 C.Y.
FILL VOLUME: 284,092.85 C.F., 10,521.96 C.Y.
- ZONE: 471.0 TO 472.0 (FLOOD ELEV = 471.2')
CUT VOLUME: 0.00 C.F., 0.00 C.Y.
FILL VOLUME: 3,195.52 C.F., 118.35 C.Y.
RUNNING TOTALS:
CUT (COMP.) VOLUME: 287,642.66 C.F., 10,653.43 C.Y.
FILL VOLUME: 287,298.37 C.F., 10,640.31 C.Y.



B	11/8/21	REVISIONS PER CITY STAFF COMMENTS	JEN	CEB	
A	10/28/21	REVISED CANOPY LOCATION & WETLAND SETBACK	JEN	CEB	
REV.	DATE	DESCRIPTION	C/O	DR	CK

GRADING PLAN
U-HAUL OF SOUTH KEENE
TAX MAP 115 LOTS 19 & 20
472 WINCHESTER STREET
KEENE, NEW HAMPSHIRE

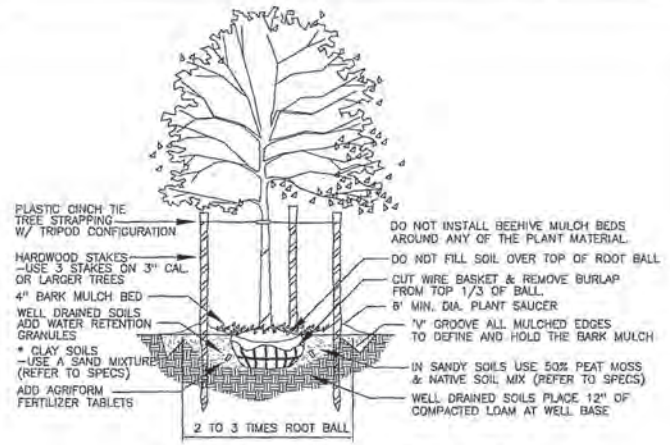
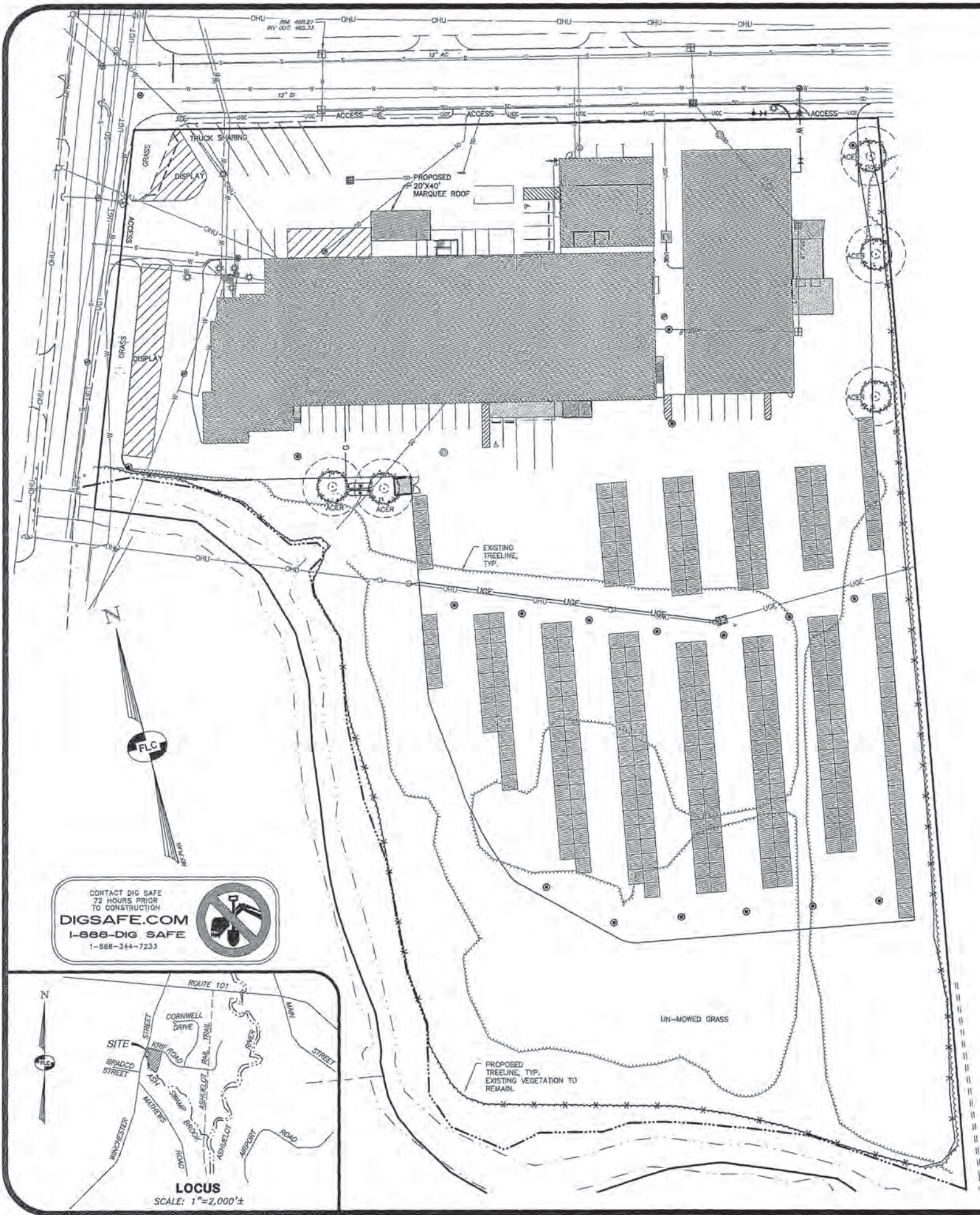
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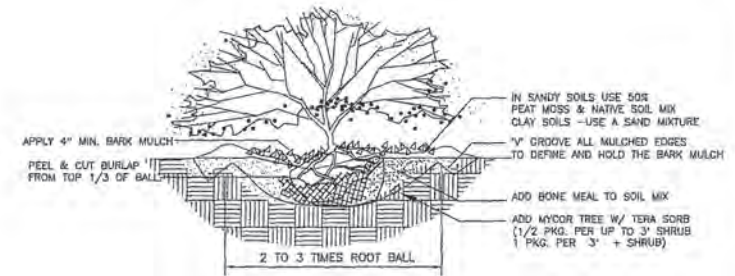
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DECIDUOUS TREE PLANTING DETAIL 1 LS-1



B & B SHRUB PLANTING DETAIL 2 LS-1

PLANT SYMBOLS	PLANT LIST & NAME	QTY.
	ACER RUBRUM (RED MAPLE TREE)	(5) 3" CAL., 12' TALL, 6' DRIP LINE 40' DRIP LINE AT MATURITY



DESIGN NOTES:

- LANDSCAPING REQUIREMENTS:

TREES:	REQUIRED:	PROPOSED:
1/10 PARKING SPACE (45)	5 TREES	5 TREES
PARKING PERIMETER TREES	1/30 (5)	5 TREES*
ORNAMENTAL TREES	2:1=10 TREES	X
- SHRUBS:

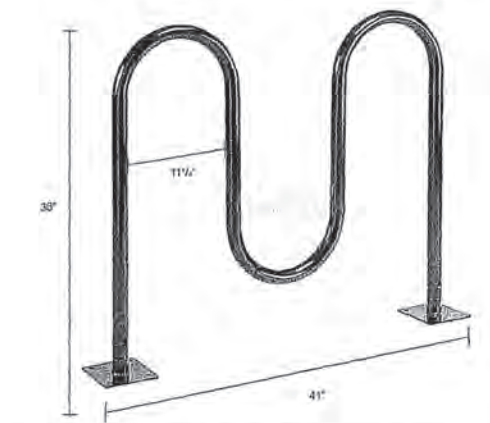
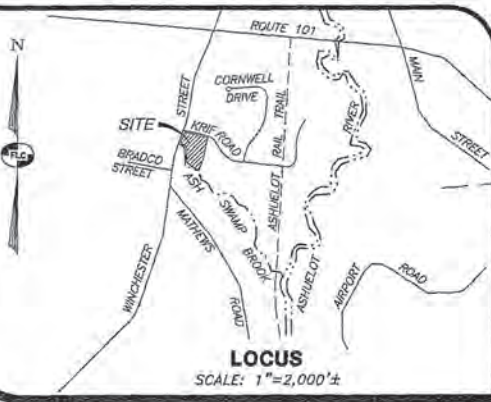
EVERGREENS:	REQUIRED:	PROPOSED:
NON-EVERGREEN	145/3=48	0*
	145/3=48	0*
- PERIMETER LANDSCAPING:

PARKING ON PERIMETER:	REQUIRED:	PROPOSED:
8' WIDTH	8' WIDTH	15' WINCHESTER DISPLAY AREA
8' WIDTH	8' WIDTH	5' ON KRIF (EXISTING)
- FLOWERS & PLANTS: 60% AREA OR

*THERE IS NO PROPOSED PERIMETER PARKING, THEREFORE NO SHRUBS OR TREES ARE PROPOSED BASED ON PERIMETER PARKING REQUIREMENTS.

- LANDSCAPING NOTES:**
- THE LANDSCAPE CONTRACTOR IS RESPONSIBLE FOR CONTACTING DIG-SAFE AND FOR VERIFICATION OF ALL UTILITIES AND SHALL NOTIFY THE OWNER'S REPRESENTATIVE OF ANY CONFLICTS PRIOR TO COMMENCING.
 - EXISTING TREES TO REMAIN SHALL BE PRESERVED AND PROTECTED DURING CONSTRUCTION. TEMPORARY FENCING SHALL BE INSTALLED PRIOR TO THE START OF SITE WORK TO PROTECT ROOT MASSES.
 - EXISTING TREES THAT ARE TO BE REMOVED, SHALL BE REMOVED ENTIRELY FROM THE SITE, INCLUDING STUMPS. NO STUMP-DUMPS ARE ALLOWED ON SITE.
 - UNTIL ALL GRADING AND CONSTRUCTION HAS BEEN COMPLETED WITHIN THE IMMEDIATE AREA NO PLANT MATERIAL SHALL BE INSTALLED.
 - UNLESS OTHERWISE NOTED OR APPROVED, ALL TREES MUST BE BALLED AND BURRLAPPED.
 - ALL PLANT MATERIALS INSTALLED SHALL MEET OR EXCEED THE SPECIFICATIONS OF "THE AMERICAN STANDARDS FOR NURSERY STOCK" BY THE AMERICAN ASSOCIATION OF NURSERYMEN.
 - ANY PROPOSED PLANT MATERIAL SUBSTITUTIONS MUST BE APPROVED BY THE OWNER'S REPRESENTATIVE.
 - ALL PLANT MATERIAL SHALL BE GUARANTEED BY THE INSTALLER FOR ONE YEAR FOLLOWING DATE OF ACCEPTANCE. ANY PLANT MATERIAL THAT IS SIGNIFICANTLY DAMAGED, MISSING, DISEASE RIDDEN, OR DEAD SHALL BE REPLACED WITHIN 1-YEAR OR BEFORE THE END OF THE FOLLOWING PLANTING SEASON, WHICHEVER OCCURS FIRST.
 - IN AREAS OF STONE MULCH LAY 6 MIL SHEETS OF "VISQUEEN" TYPE POLYETHYLENE ON COMPACTED SUBGRADE BEFORE PLACING STONE, MINIMUM 6" OVERLAP. PERFORATE SHEETING IN PLANTING BEDS BEFORE PLACING STONE.
 - UNLESS OTHERWISE NOTED LOAM AND SEED ALL DISTURBED AREAS WITH A MINIMUM 4" OF SUITABLE LOAM. SLOPES GREATER THAN 3:1 SHALL BE PROTECTED WITH AN EROSION CONTROL BLANKET. SEE SITE PLAN.
 - WHERE APPLICABLE, THE CONTRACTOR SHALL HAVE ALL FALL TRANSPLANTING HAZARD PLANTS DUG IN THE SPRING AND STORED FOR FALL PLANTING.
 - PLANTS SHALL BE INSTALLED WITHIN ONE YEAR OF COMMENCEMENT OF CONSTRUCTION.
 - ALL LANDSCAPING SHALL BE LOCATED AND MAINTAINED SO AS NOT TO IMPACT THE LINES OF SIGHT AT THE ENTRANCE AND INTERNAL INTERSECTIONS TO PROVIDE SAFE PASSAGE OF PEDESTRIANS, BICYCLISTS, AND MOTORISTS.
 - ALL LANDSCAPED AREAS WILL BE MAINTAINED TO HAVE A SUFFICIENT AMOUNT OF WATER TO MAINTAIN VIABILITY EITHER BY IRRIGATION OR BY OTHER MEANS.
 - PROPOSED PLANTINGS SHALL NOT CONFLICT WITH SNOW STORAGE AREAS, LIGHT FIXTURES OR UNDERGROUND UTILITIES.
 - ALL LANDSCAPING APPROVED AS PART OF THE SITE PLAN SHALL BE CONSIDERED AS ELEMENTS OF THE SITE, IN THE SAME MANNER AS PARKING, BUILDING MATERIALS, AND OTHER SITE DETAILS. ANY CHANGES WILL REQUIRE APPROVAL BY THE OWNER AND CITY COMMUNITY DEVELOPMENT DIRECTOR.

CONTACT DIG SAFE
72 HOURS PRIOR
TO CONSTRUCTION
DIGSAFE.COM
1-888-DIG SAFE
1-888-344-7233



- INSTALL 1/2" STAINLESS BOLTS INTO DROP-IN ANCHORS BY H.I.T.
- SET TWO 6" SONOTUBES FILLED WITH CONCRETE TO CONNECT BASE PLATES TO.

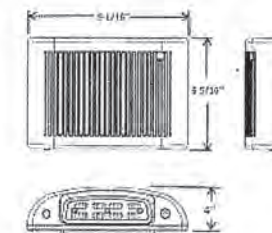
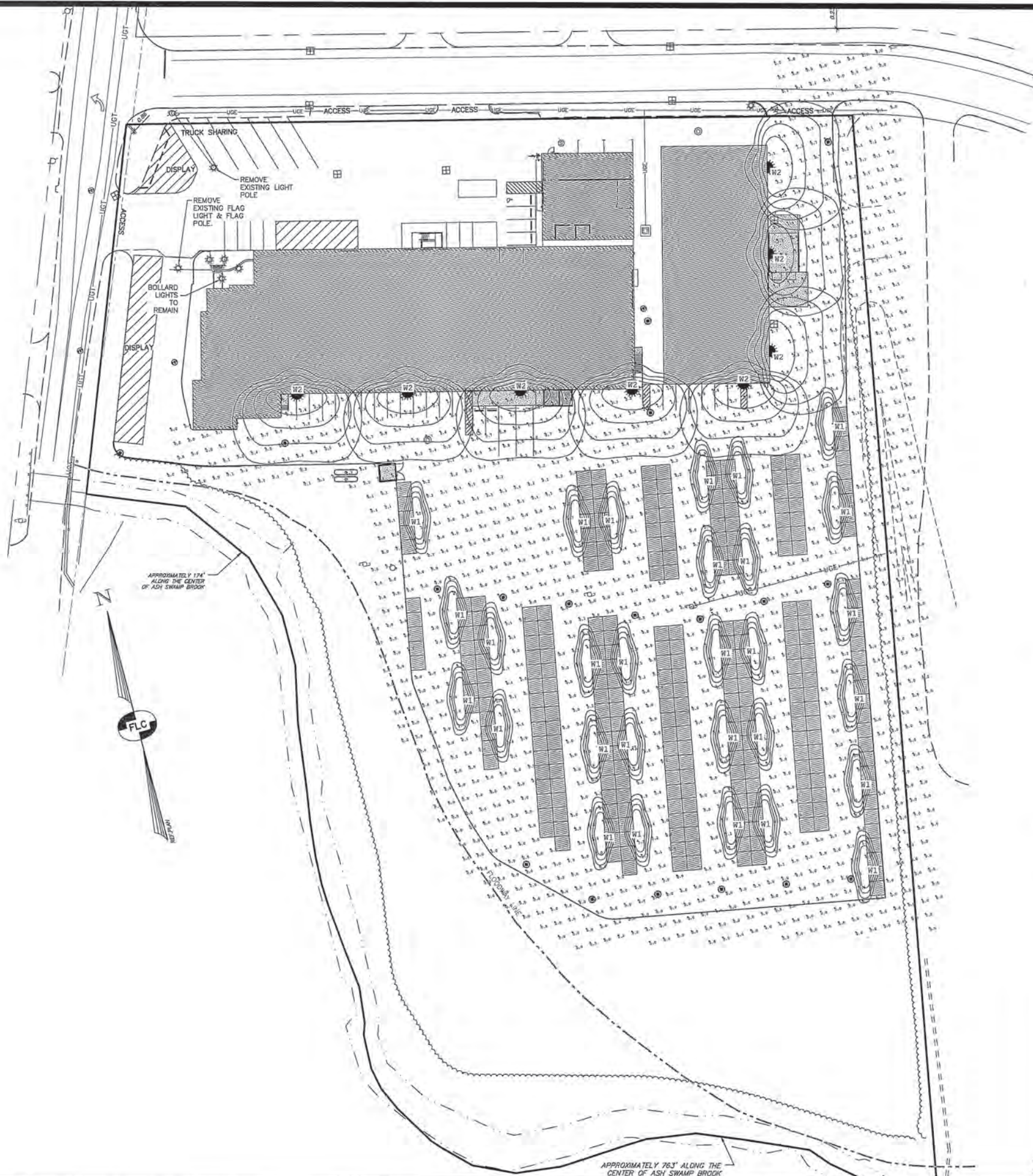
5-BIKE WAVE BIKE RACK

LANDSCAPING PLAN
U-HAUL OF SOUTH KEENE.
TAX MAP 115 LOTS 19 & 20
472 WINCHESTER STREET
KEENE, NEW HAMPSHIRE

PREPARED FOR:
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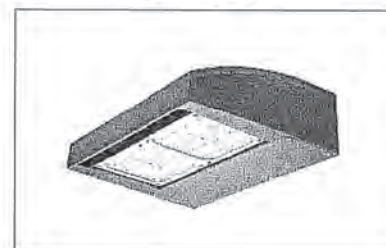
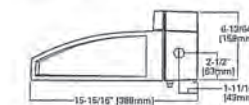
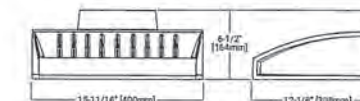
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W1 - WALL PACK ON STORAGE UNITS

McGraw-Edison
GWC Galleon Wall
Wall Mount Luminaire

Dimensional Details



W2 - WALL PACK ON BUILDINGS

1. THESE LIGHTS ARE TO BE INSTALLED ON THE SOUTH SIDE OF BUILDINGS.
2. EXISTING WALL PACKS ON NORTH AND WEST WALLS TO REMAIN.

DESIGN NOTES:

1. LIGHTING REQUIREMENTS:

PARKING LOTS	REQUIRED	PROPOSED
AVERAGE	3.5 Fc MAX	1.20 Fc
MINIMUM	0.33 Fc	0.3 Fc
U _r RATIO (AVG/MIN)	5:1	4:1
MAX AT PROP. LINE	0.1 Fc	0.1 Fc
MAX AT R.O.W.	1.0 Fc	0.5 Fc
2. ALL PROPOSED LIGHTING MUST BE 70 OR GREATER (CRI) COLOR RENDERING INDEX.
3. ALL PROPOSED LIGHTING MUST BE 3500 KELVIN COLOR-TEMPERATURE.

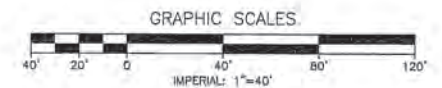
LIGHTING NOTES:

1. LIGHTING SHALL BE INSTALLED AND ARRANGED SO AS NOT TO REFLECT OR CAUSE GLARE UPON ADJUTING LAND OR HIGHWAYS AND ROADS.
2. ALL FIXTURES ARE FULL CUTOFF, LED FIXTURES, FLOOD LIGHTING AND UP-LIGHTING ARE PROHIBITED.
3. NO SIDEWALKS ARE PROPOSED. BOLLARD LIGHTS AT FRONT ENTRANCE TO REMAIN. FLAG LIGHTS WILL BE REMOVED, ALONG WITH FLAG POLE AT FRONT ENTRANCE.
4. MOUNTING HEIGHT OF ALL PROPOSED WALL MOUNT LIGHTING FIXTURES SHALL BE 15 FEET ABOVE FINISH GRADE ON THE BUILDINGS. FIXTURES TO BE MOUNTED 8 FEET A.F.G. ON SELF STORAGE UNITS.
5. MAXIMUM POLE HEIGHT ALLOWED IS 30 FEET IN COMMERCE LIMITED ZONE.
6. ALL LIGHTS ARE TO BE SETUP ON PHOTOCELLS TO AUTOMATICALLY TURN OFF DURING DAYLIGHT HOURS.
7. ALL NEW LIGHTS TO BE CONTROLLED BY MOTION SENSOR TO ONLY TURN ON AS NEEDED. THE OPERATION WILL BE 24 HOUR FOR ACCESS TO THE SELF STORAGE BUILDINGS.
8. ALL FIXTURES, POLES AND HARDWARE ARE TO BE BRONZE IN COLOR TO MATCH.
9. ANY EXISTING EXTERIOR FIXTURES ARE TO REMAIN, UNLESS NOTED OTHERWISE. EXISTING CONTROLS AND LIGHTING SCHEDULES ARE TO REMAIN UNCHANGED.
10. LIGHT FIXTURES ARE AVAILABLE THROUGH CHARON LIGHTING. ANY CHANGE IN FIXTURE MUST BE APPROVED BY THE OWNER AND DESIGN ENGINEER.

Luminaire Schedule	Symbol	Qty	Label	Description
	W2	8	W2	GWC-SAIB-740-U-SL4-MS-L20 / WALL MTD 15' AFG
	W1	29	W1	MERU-LED-AC-PIR / WALL MTD 8' AFG

StatArea 1
PARKING ASIDE BUILDING
Illuminance (Fc)
Average = 1.20
Maximum = 4.0
Minimum = 0.3
Avg/Min Ratio = 4.00
Max/Min Ratio = 13.33

StatArea 2
SELF STORAGE AREA
Illuminance (Fc)
Average = 0.44
Maximum = 14.1
Minimum = 0.0
Avg/Min Ratio = N.A.
Max/Min Ratio = N.A.



REV.	DATE	DESCRIPTION	C/O	DR	CK
A	10/28/21	REVISED CANOPY & PARKING		JEN	CEB

LIGHTING PLAN
U-HAUL OF SOUTH KEENE
TAX MAP 115 LOTS 19 & 20
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- PRIOR TO STARTING ANY WORK ON THE SITE THE CONTRACTOR SHALL NOTIFY APPROPRIATE AGENCIES.
- ALL SOIL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE IN ACCORDANCE WITH STANDARDS AND SPECIFICATIONS THEREOF IN NEW HAMPSHIRE DEPARTMENT OF ENVIRONMENTAL SERVICE STORM WATER MANUALS, VOLUME 1-3, LATEST EDITION.
- EROSION AND SEDIMENTATION CONTROL MEASURES SHALL BE INSTALLED PER PLANS AND DETAILS. PERIMETER CONTROLS SHALL BE IN PLACE PRIOR TO COMMENCEMENT OF EARTH DISTURBING ACTIVITIES.
- INSTALL INLET PROTECTION AROUND ALL STORM DRAIN STRUCTURES. INLET PROTECTION BMP'S SHALL REMAIN UNTIL THE SITE IS STABILIZED. CONSTRUCTION OF STORMWATER BASINS AND TREATMENT SWALES SHALL OCCUR PRIOR TO AND EARTH MOVING OPERATION THAT WILL INFLUENCE STORM WATER RUNOFF.
- THE WORK AREA SHALL BE GRADED, SHIPPED AND OTHERWISE DRAINAGE IN SUCH A MANNER AS TO MINIMIZE SOIL EROSION, SITUATION OF DRAINAGE CHANNELS, DAMAGE TO EXISTING VEGETATION, AND DAMAGE TO PROPERTY OUTSIDE THE LIMITS OF THE WORK AREA.
- EXISTING VEGETATION IS TO REMAIN UNDISTURBED WHEN POSSIBLE.
- EROSION AND SEDIMENTATION CONTROL MEASURES SHALL BE KEPT CLEAN DURING CONSTRUCTION. EROSION AND SEDIMENTATION CONTROL MEASURES SHALL BE INSPECTED AT LEAST ONCE A WEEK AND AFTER EVERY 0.25-INCH OR GREATER RAINFALL. SEDIMENTS SHALL BE DEPOSITED OF IN AN UPLAND AREA THAT WILL NOT CONTRIBUTE TO SEDIMENT OFF-SITE AND BE PERMANENTLY STABILIZED.
- THE SMALLEST PRACTICAL AREA SHALL BE DISTURBED DURING CONSTRUCTION. AT NO TIME SHALL THE TOTAL UNSTABILIZED DISTURBED AREA, INCLUDING LOT DISTURBANCES, BE GREATER THAN FIVE (5) ACRES. ALL AREAS SHALL BE STABILIZED WITHIN 45 DAYS (MAX) OF THE INITIAL DISTURBANCE.
- THE LAND AREA EXPOSED SHALL BE KEPT TO THE SHORTEST PRACTICAL PERIOD OF TIME. ALL NON-ACTIVE DISTURBED AREAS SHALL BE STABILIZED WITHIN 30 DAYS OF THE DISTURBANCE. ALL DISTURBED AREAS SHALL BE STABILIZED WITHIN 72 HOURS OF FINAL GRADING.
- DITCHES, SWALES AND DRAINAGE BASINS SHALL BE CONSTRUCTED DURING THE INITIAL PHASE OF CONSTRUCTION AND STABILIZED PRIOR TO DIRECTING RUNOFF TO THEM.
- AN AREA SHALL BE CONSIDERED STABILIZED IF ONE OF THE FOLLOWING HAS OCCURRED:
 - BASE COURSE GRAVELS HAVE BEEN INSTALLED IN AREAS TO BE PAVED;
 - A MINIMUM OF 85% VEGETATED GROWTH HAS BEEN ESTABLISHED;
 - A MINIMUM OF 3-INCHES OF NON-EROSIVE MATERIAL, SUCH AS STONE OR RIPRAP, HAS BEEN INSTALLED; OR
 - EROSION CONTROL BLANKETS HAVE BEEN PROPERLY INSTALLED.
- EROSION CONTROL BLANKETS SHALL BE INSTALLED ON ALL SLOPES THAT ARE STEEPER THAN 3:1 (HORIZONTAL / VERTICAL), UNLESS OTHERWISE SPECIFIED THE CONTRACTOR SHALL USE NORTH AMERICAN GREEN S2150, OR APPROVED EQUAL.
- ALL AREAS RECEIVING EROSION CONTROL STONE OR RIPRAP SHALL HAVE A GEOTEXTILE MATERIAL INSTALLED BELOW THE STONE (SEE APPROPRIATE DETAILS).
- ALL DISTURBED AREAS TO TURF FINISHED SHALL BE COVERED WITH A MINIMUM THICKNESS OF 6 INCHES OF COMPACTED LOAM. LOAM SHALL BE COVERED WITH THE APPROPRIATE SEED MIXTURE AS INDICATED BELOW.

PERMANENT SEED (LAWN AREAS) LBS / 1,000 SQ. FT.	PERMANENT SLOPE SEED MIX LBS / 1,000 SQ. FT.
CREeping RED FESCUE 0.92 LBS	CREeping RED FESCUE 0.80 LBS
PERENNIAL RYEGRASS 1.15 LBS	PERENNIAL RYEGRASS 0.69 LBS
KENTUCKY BLUEGRASS 0.58 LBS	RYEGRASS 0.12 LBS
RENOVET 0.12 LBS	ALISC CLOVER 0.12 LBS
	BIRDFOOT TREFOIL 0.12 LBS

**APPLICATION RATE TOTALS
2.8 LBS PER 1,000 SF**
- TEMPORARY STABILIZATION OF DISTURBED AREAS:
STRIPPED SOIL SHALL BE STOCKPILED UNPROTECTED, AND STABILIZED AGAINST EROSION AS OUTLINED BELOW:
SEED BED PREPARATION: 10-10-10 FERTILIZATION TO BE SPREAD AT THE RATE OF 7 LBS. PER 100 SF AND AGRICULTURAL LIMESTONE AT A RATE OF 90 LBS PER 1000 SF AND INCORPORATED INTO THE SOIL. THE SOIL, FERTILIZER AND LIMESTONE SHALL BE TILLED TO PREPARE FOR SEEDING.

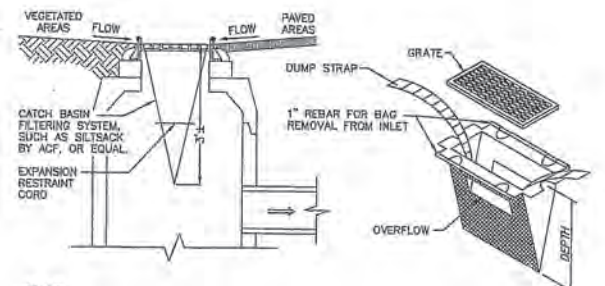
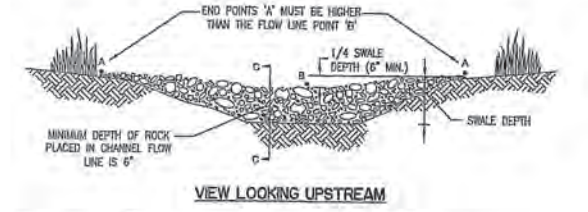
EROSION CONTROL NOTES 1 DT-1

- ALL PROPOSED VEGETATED AREAS WHICH DO NOT EXHIBIT A MINIMUM OF 85% VEGETATED GROWTH BY OCTOBER 15TH, OR WHICH ARE DISTURBED AFTER OCTOBER 15TH, SHALL BE STABILIZED. STABILIZATION METHODS SHALL INCLUDE SEEDING AND INSTALLING EROSION CONTROL BLANKETS ON SLOPES GREATER THAN 3:1, AND SEEDING AND PLACING 3 TO 4 TONS OF MULCH PER ACRE, SECURED WITH ANCHORED NETTING, ELSEWHERE. THE INSTALLATION OF EROSION CONTROL BLANKETS OR MULCH AND NETTING SHALL NOT OCCUR OVER ACCUMULATED SNOW OR FROZEN GROUND AND SHALL BE COMPLETED IN ADVANCE OF THAW OR SPRING MELT EVENTS.
- ALL DITCHES OR SWALES WHICH DO NOT EXHIBIT A MINIMUM OF 85% VEGETATED GROWTH BY OCTOBER 15TH, OR WHICH ARE DISTURBED AFTER OCTOBER 15TH, SHALL BE STABILIZED WITH STONE OR EROSION CONTROL BLANKETS APPROPRIATE FOR THE DESIGN FLOW CONDITIONS.
- AFTER NOVEMBER 15TH, INCOMPLETE ROAD OR PARKING SURFACES, WHERE WORK HAS STOPPED FOR THE WINTER SEASON SHALL BE PROTECTED WITH A MINIMUM OF 3 INCHES OF CRUSHED GRAVEL OR PROPERLY INSTALLED EROSION CONTROL BLANKETS COVERED WITH HAY. OTHER STABILIZATION OPTIONS ARE TO BE APPROVED BY THE APPROPRIATE AGENCIES AND THE DESIGN ENGINEER. IF CONSTRUCTION IS TO CONTINUE THROUGH THE WINTER MONTHS THEN THE ROAD SHOULD BE CLEARED OF ACCUMULATED SNOW AFTER EACH STORM EVENT.

WINTER CONSTRUCTION NOTES 2 DT-1

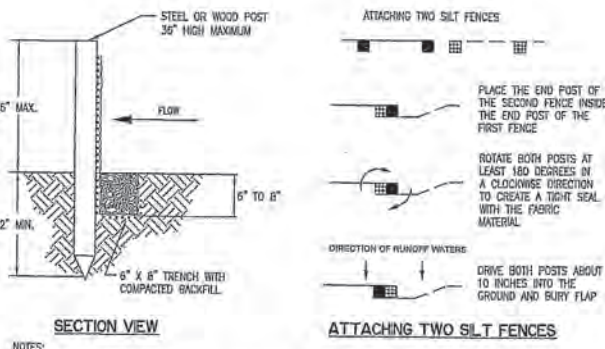
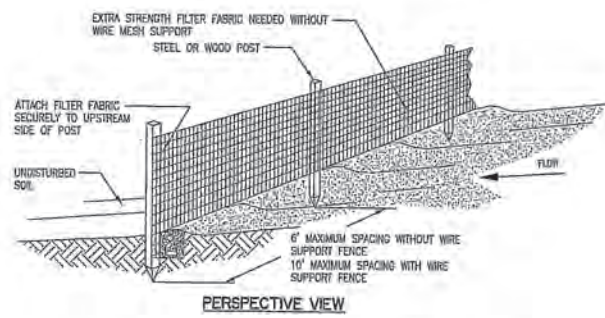
- THE PROJECT SHALL BE MANAGED IN A MANNER THAT MEETS THE REQUIREMENTS AND INTENT OF RSA 430:53 AND CHAPTER AGR 3800 TO CONTROL AGAINST INVASIVE SPECIES.
- SITUATION CONTROL FENCES IN LOCATIONS SHOWN HEREON. EROSION AND SEDIMENTATION CONTROL MEASURES SHALL BE INSTALLED PRIOR TO ANY EARTH MOVING OPERATION.
- INSTALL STABILIZED CONSTRUCTION EXITS.
- CUT AND CLEAR TREES; DISPOSE OF DEBRIS. STUMPS ARE TO BE REMOVED FROM THE SITE AND DISPOSED OF PROPERLY.
- REMOVE TOPSOIL AND STOCKPILE AWAY FROM ANY WETLAND. STABILIZE STOCKPILE IMMEDIATELY BY SEEDING. PLACE SILT FENCE AROUND THE DOWN SLOPE SIDE OF EARTH STOCKPILES.
- ROUGH GRADE SITE - CONSTRUCT DRAINAGE BASINS AND DRAINAGE SWALES DURING INITIAL PORTION OF CONSTRUCTION. STABILIZE IMMEDIATELY PER THE CONSTRUCTION AND EROSION CONTROL DETAILS. BUILD TEMPORARY WATER DIVERSION (SWALES, BASINS, ETC.) & USED AS NECESSARY UNTIL PERMANENT AREAS ARE STABILIZED. DO NOT DIRECT STORM WATER RUNOFF TO THESE STRUCTURES UNTIL A HEALTHY VEGETATIVE COVER IS ESTABLISHED.
- BEGIN BUILDING CONSTRUCTION.
- CONSTRUCT GRAVEL PARKING AREA (PAVEMENT OPTIONAL) AND BUILDING PAD. INSTALL UTILITIES AND STRUCTURES. ALL CUT AND FILL SLOPES SHALL BE STABILIZED UPON COMPLETION OF ROUGH GRADING PER THE EROSION CONTROL NOTES.
- INSPECT AND MAINTAIN EROSION CONTROL MEASURES ON A WEEKLY BASIS AND AFTER EVERY 0.25" OR GREATER RAINFALL.
- DAILY, OR AS REQUIRED, CONSTRUCT TEMPORARY BERMS, CULVERTS, DITCHES, SITUATION FENCES, SEDIMENT TRAPS, ETC. MULCH AND SEED AS REQUIRED.
- FINISH GRADING TO PREPARE FOR PAVING (IF ANY) AND LOAMING. ALL DISTURBED AREAS (ROADS, PARKING LOTS, AND CUT/FILL SLOPES) SHALL BE STABILIZED WITHIN 72 HOURS AFTER FINAL GRADING.
- FINISH PAVING (IF ANY). PERMANENT SEEDING SHALL BE PERFORMED UPON COMPLETION OF PARKING AREA (SEE EROSION CONTROL NOTES).
- COMPLETE PERMANENT SEEDING AND LANDSCAPING.
- TEMPORARY EROSION CONTROL MEASURES SHALL BE REMOVED WHEN ALL DISTURBED AREAS HAVE BEEN STABILIZED.
- ALL STRUCTURES SHALL BE CLEANED OF SEDIMENTS ONCE CONSTRUCTION IS COMPLETE.

CONSTRUCTION SEQUENCE 3 DT-1



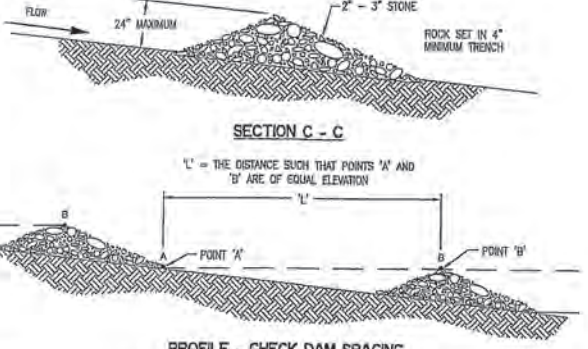
- NOTES:**
- INSTALL AND MAINTAIN SACKS IN ALL CATCH BASINS.
 - TO INSTALL SACK, REMOVE CATCH BASIN GRATE AND PLACE SACK IN OPENING. HOLD OUT APPROXIMATELY SIX INCHES OF THE SACK OUTSIDE THE FRAME FOR THE LIFTING STRAPS. REPLACE THE GRATE TO HOLD THE SACK IN PLACE.
 - THE SACK SHOULD BE INSPECTED AFTER EVERY STORM, OR ONCE EVERY TWO WEEKS, WHICHEVER OCCURS FIRST.
 - THE RESTRAINT CORD SHOULD BE VISIBLE AT ALL TIMES. IF THE CORD IS COVERED WITH SEDIMENT, THE SACK SHOULD BE EMPLOYED IMMEDIATELY PER THE CONSTRUCTION AND EROSION CONTROL DETAILS. BUILD TEMPORARY WATER DIVERSION (SWALES, BASINS, ETC.) & USED AS NECESSARY UNTIL PERMANENT AREAS ARE STABILIZED. DO NOT DIRECT STORM WATER RUNOFF TO THESE STRUCTURES UNTIL A HEALTHY VEGETATIVE COVER IS ESTABLISHED.
 - REPLACE THE SACK IN THE CATCH BASIN AFTER THE SACK HAS BEEN EMPLOYED. ONCE CONSTRUCTION IS COMPLETE AND ALL DISTURBED AREAS HAVE BEEN STABILIZED BY PAVING OR A HEALTHY VEGETATIVE COVER, REMOVE THE SACK FROM THE CATCH BASIN.

SILT SACK SEDIMENT FILTER 5 DT-1



- NOTES:**
- SILT FENCES SHOULD NOT BE USED ACROSS STREAMS, CHANNELS, SWALES, DITCHES OR OTHER DRAINAGE WAYS.
 - SILT FENCE SHOULD BE INSTALLED FOLLOWING THE CONTOUR OF THE LAND AS CLOSELY AS POSSIBLE AND THE ENDS OF THE SILT FENCE SHOULD BE FLARED UPSLOPE.
 - IF THE SITE CONDITIONS INCLUDE FROZEN GROUND, LEDGE OR THE PRESENCE OF HEAVY ROOTS THE BASE OF THE FABRIC SHOULD BE EMBEDDED WITH A MINIMUM THICKNESS OF 8 INCHES OF 3/4-INCH STONE.
 - SILT FENCES PLACED AT THE TOE OF SLOPES SHOULD BE INSTALLED AT LEAST 6 FEET FROM THE TOE TO ALLOW SPACE FOR SHALLOW PONDING AND ACCESS FOR MAINTENANCE.
 - THE MAXIMUM SLOPE ABOVE THE FENCE SHOULD BE 2:1 AND THE MAXIMUM LENGTH OF SLOPE ABOVE THE FENCE SHOULD BE 100 FEET.
 - REMOVED SEDIMENT SHALL BE DEPOSITED TO AN AREA THAT WILL NOT CONTRIBUTE TO SEDIMENT OFF-SITE AND CAN BE PERMANENTLY STABILIZED.
 - SILT FENCES SHOULD BE REMOVED WHEN THE UPSLOPE AREAS HAVE BEEN PERMANENTLY STABILIZED.

SILT FENCE 6 DT-1



- NOTES:**
- STONE CHECK DAMS SHOULD BE INSTALLED BEFORE RUNOFF IS DIRECTED TO THE SWALE OR DRAINAGE DITCH.
 - THE MAXIMUM CONTRIBUTING DRAINAGE AREA TO THE CHECK DAM SHOULD BE LESS THAN ONE ACRE.
 - STONE CHECK DAMS SHOULD NOT BE USED IN A FLOWING STREAM.
 - STONE CHECK DAMS SHOULD BE CONSTRUCTED OF WELL-DRAINED ANGULAR 2 TO 3 INCH STONE. THE INSTALLATION OF 3/4-INCH STONE ON THE UPSTREAM FACE IS RECOMMENDED FOR BETTER FILTERING.
 - WHEN INSTALLING STONE CHECK DAMS THE CONTRACTOR SHALL KEY THE STONE INTO THE CHANNEL BANKS AND EXTEND THE STONE BEYOND THE ABUTMENTS A MINIMUM OF 18-INCHES TO PREVENT FLOW AROUND THE DAM.
 - STONE CHECK DAMS SHOULD BE REMOVED ONCE THE SWALE OR DITCH HAS BEEN STABILIZED UNLESS OTHERWISE SPECIFIED.

STONE CHECK DAM 4 DT-1

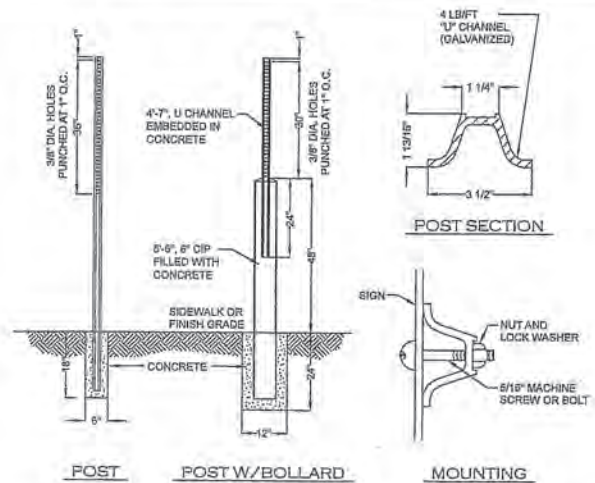
CONTACT DIG SAFE 72 HOURS PRIOR TO CONSTRUCTION
DIGSAFE.COM
 OR DIAL 811
 CALL 811 - KNOW WHAT'S BELOW

GRAVEL CONSTRUCTION EXIT 7 DT-1

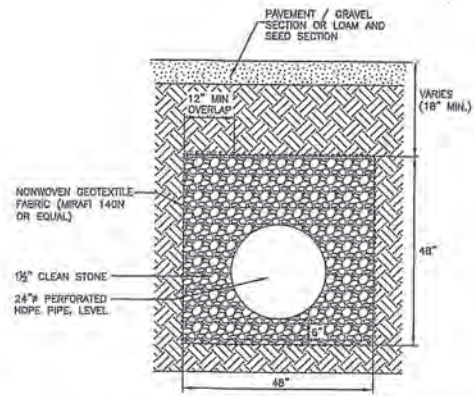


EROSION CONTROL DETAILS
 TAX MAP 115 LOTS 19 & 20
 472 WINCHESTER STREET
 KEENE, NEW HAMPSHIRE
 PREPARED FOR:
AMERCO REAL ESTATE COMPANY
 499 MONTGOMERY STREET, CHICOPEE, MA 01020
 SCALE: NOT TO SCALE OCTOBER 15, 2021
 Surveying + Engineering + Land Planning + Permitting + Septic Designs

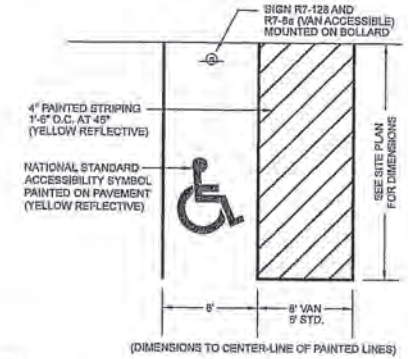
 206 Elm Street, Milford, NH 03055
 Phone: (603) 672-5456 Fax: (603) 413-5456
 www.FieldstoneLandConsultants.com
 FILE: 2250DT00.dwg PROJ. NO. 2250.00 SHEET: DT-1 PAGE NO. 8 OF 10
 Page 48 of 80



SIGN POST - STANDARD & W/BOLLARD 1 DT-3

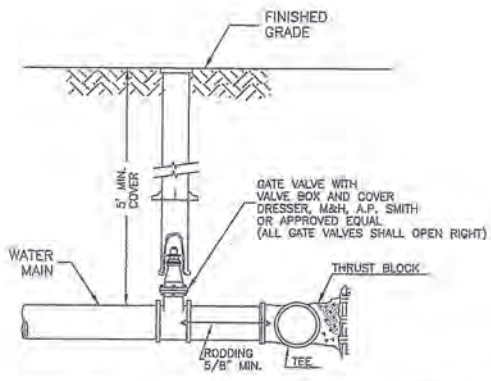


LEACHING TRENCH - PIPE AND STONE 4 DT-3

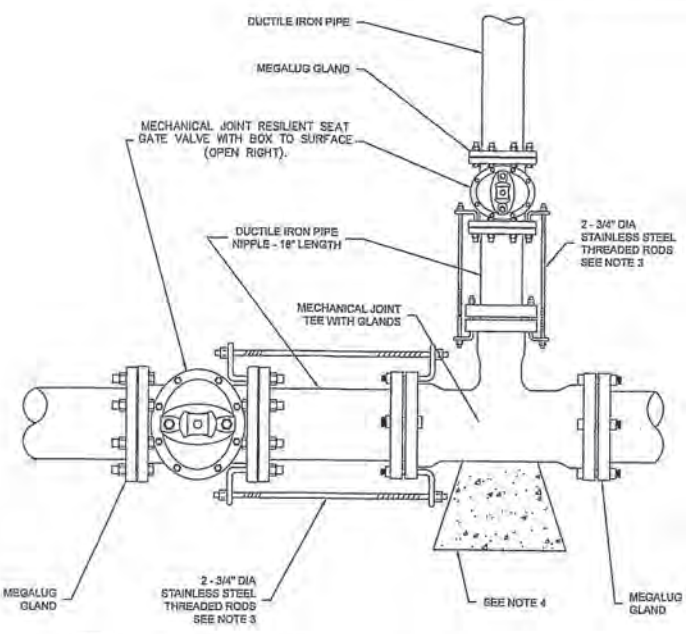


- NOTES:
1. MAXIMUM SLOPE IN ANY DIRECTION IS 2.0% FOR HANDICAP SPACES.
 2. SEE SITE PLAN FOR LOCATION OF HANDICAP SPACES AND TYPE OF HANDICAP RAMP TO BE USED.
 3. HANDICAP SYMBOL SHALL BE CENTERED ON WIDTH OF PARKING STALL AND LOCATED 66" FROM ACCESS DRIVE, INTO PARKING SPACE.
 4. THE HANDICAP SYMBOL SHALL HAVE A 5' X 5' CONTRASTING BACKGROUND, NORMALLY BLUE. A BACKGROUND IS NOT REQUIRED IF WHITE OR YELLOW SYMBOL IS ON BLACK ASPHALT.
 5. A MINIMUM OF ONE IN EVERY EIGHT (1:8) ACCESSIBLE SPACES, BUT NOT LESS THAN ONE, SHALL BE SERVED BY AN ACCESS AISLE 5 FT WIDE AND SHALL BE DESIGNATED VAN ACCESSIBLE.

HANDICAP PARKING STALL LAYOUT 7 DT-3

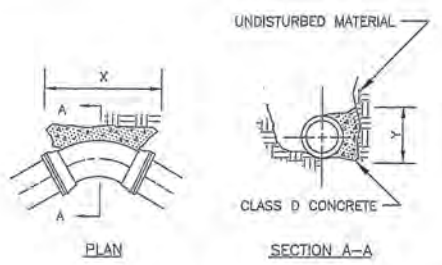


GATE VALVE WITH VALVE BOX 2 DT-3



- NOTES:
1. ALL MATERIAL AND INSTALLATION PROCEDURES WILL CONFORM TO KEENE PUBLIC WORKS TECHNICAL SPECIFICATIONS.
 2. ALL PIPE SHOULD HAVE A MINIMUM DEPTH OF 8" FROM TOP OF PIPE TO FINISH GRADE.
 3. ALL THREADED RODS AND NUTS MUST BE STAINLESS STEEL.
 4. MIN 2'x2'x4" PRECAST CONCRETE THRUST BLOCK MAY BE USED WITH K.P.W. APPROVAL OR CONCRETE THRUST BLOCK POURED AGAINST UNDISTURBED EARTH - SIZE TO BE BASED ON SIZE OF FITTING AND PRESSURE IN WATERMAIN.

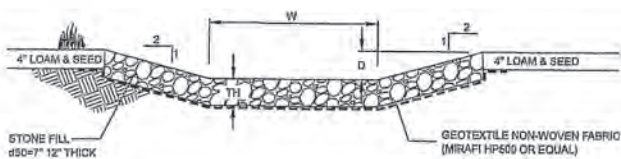
WATER MAIN TEE 5 DT-3



WATER LINE THRUST BLOCK 3 DT-3

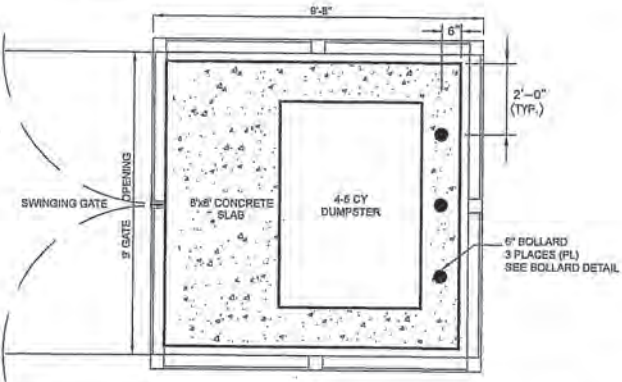
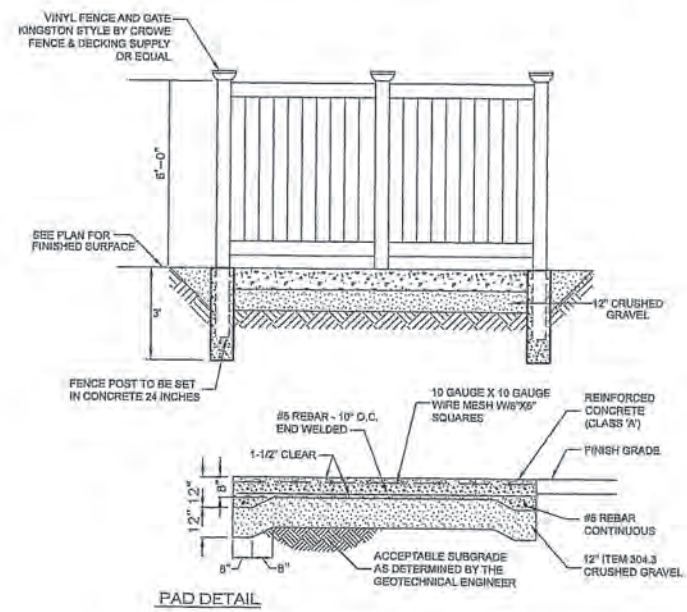
TABLE OF DIMENSIONS

PIPE SIZE	90° BEND		45° BEND		22 1/2° BEND		11 1/4° BEND		TEE & PLUG	
	X	Y	X	Y	X	Y	X	Y	X	Y
6"	33"	21"	18"	21"	12"	18"	9"	12"	21"	24"
8"	45"	27"	24"	27"	18"	21"	12"	15"	27"	33"
10"	60"	36"	36"	36"	24"	30"	18"	21"	36"	42"
12"	66"	39"	36"	42"	24"	30"	18"	21"	39"	45"
14"	72"	45"	42"	45"	27"	36"	18"	27"	45"	54"



STRUCTURE	W	D	TH	#50
BS-1	24 FT	12"	12"	7"

EMERGENCY SPILLWAY DETAIL 6 DT-3



DUMPSTER ENCLOSURE 8 DT-3



REV.	DATE	DESCRIPTION	C/O	DR	CK

CONSTRUCTION DETAILS
TAX MAP 115 LOTS 19 & 20
472 WINCHESTER STREET
KEENE, NEW HAMPSHIRE

PREPARED FOR:
AMERCO REAL ESTATE COMPANY
 499 MONTGOMERY STREET, CHICOPEE, MA 01020

SCALE: NOT TO SCALE OCTOBER 15, 2021

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MOVING & STORAGE OF SOUTH KEENE

472 Winchester St. Keene, NH 03431



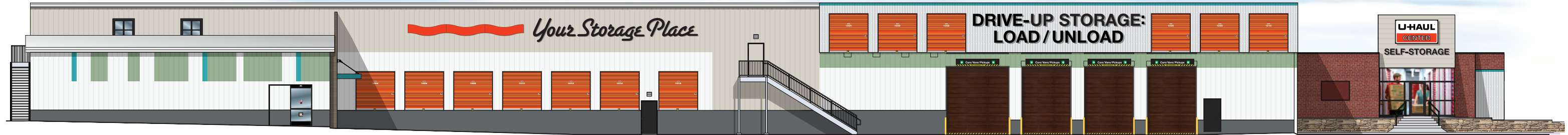
MOVING & STORAGE OF SOUTH KEENE

472 Winchester St. Keene, NH 03431



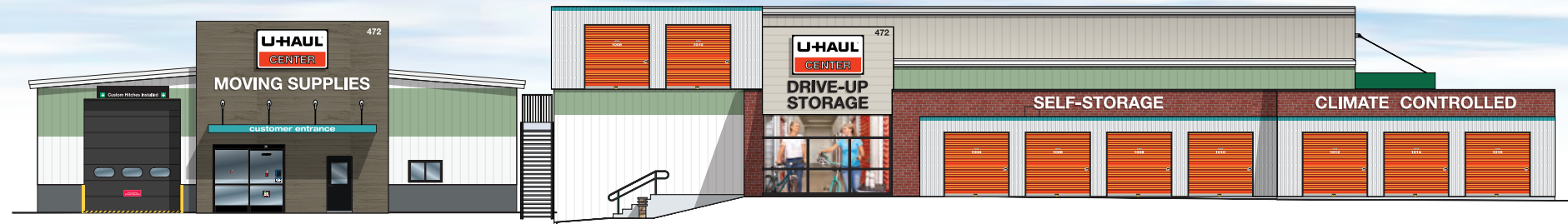
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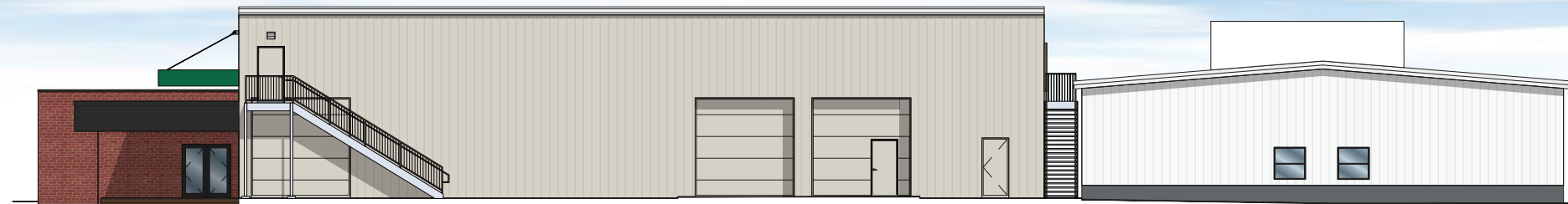
NORTH ELEVATION

Scale: 1" = 20'



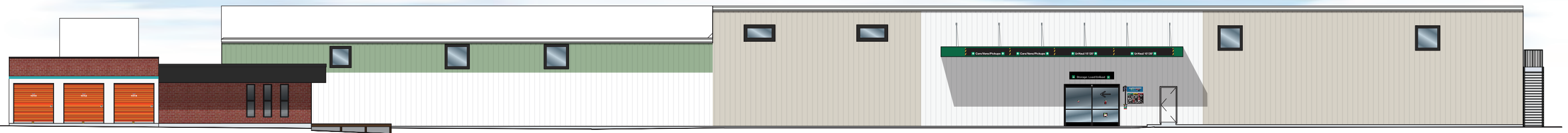
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EAST ELEVATION

Scale: 1" = 20'

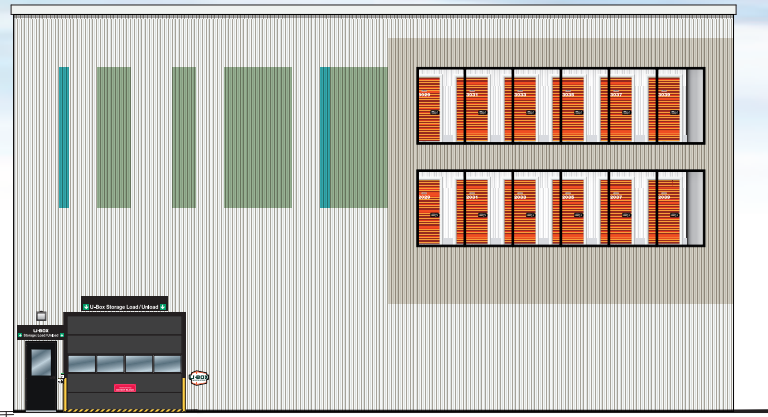


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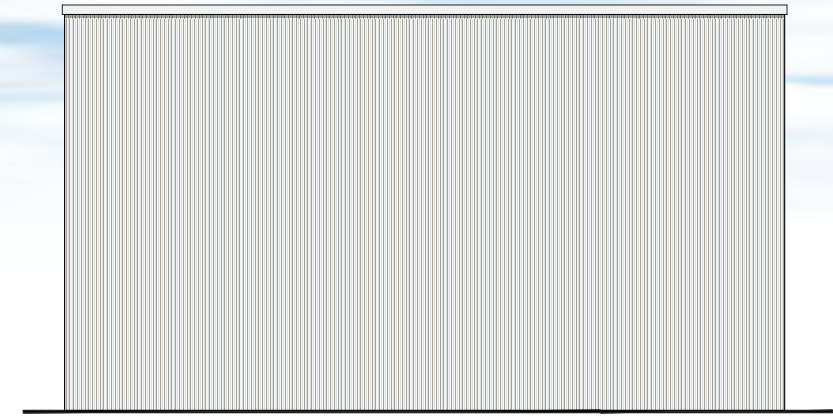
MOVING & STORAGE OF SOUTH KEENE

472 Winchester St. Keene, NH 03431



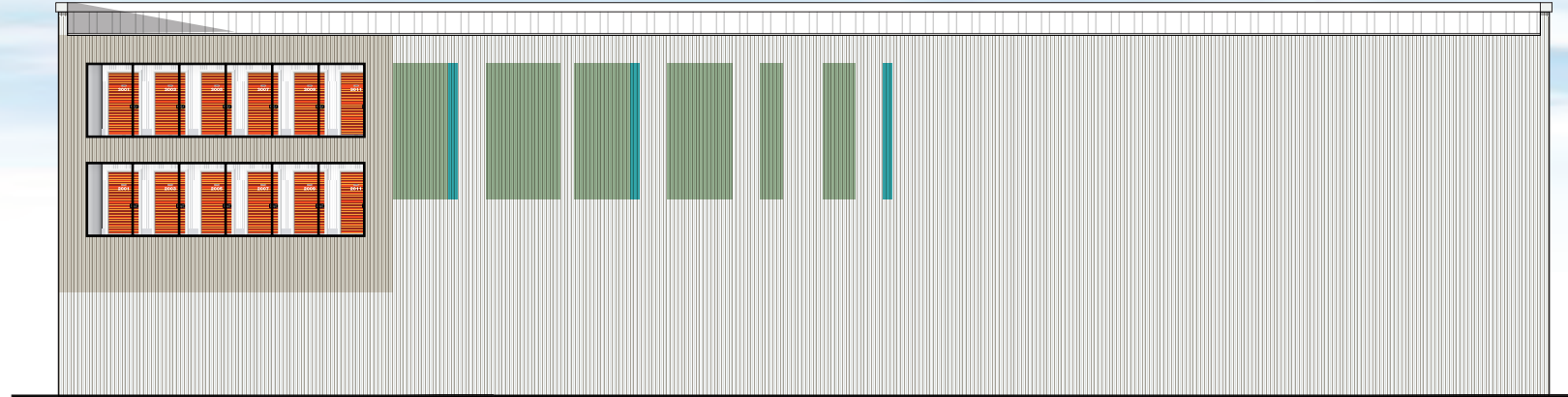
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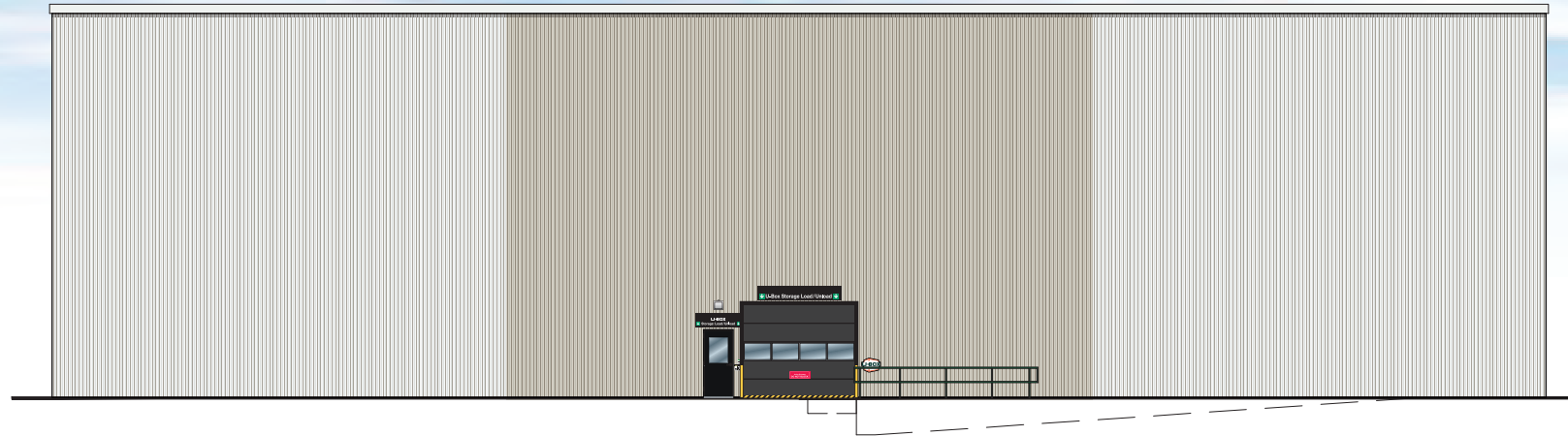
SOUTH ELEVATION

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WEST ELEVATION

Scale: 1" = 20'

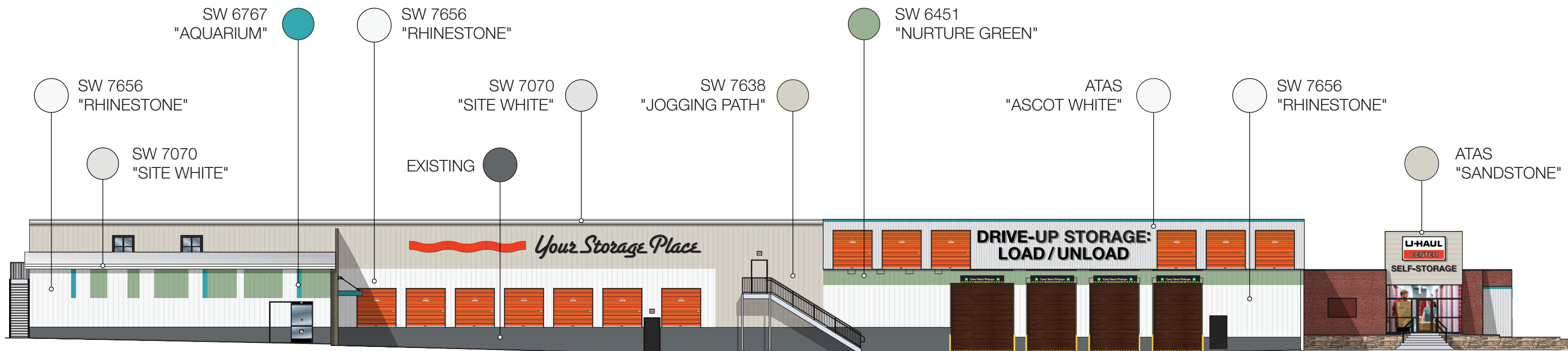


EAST ELEVATION

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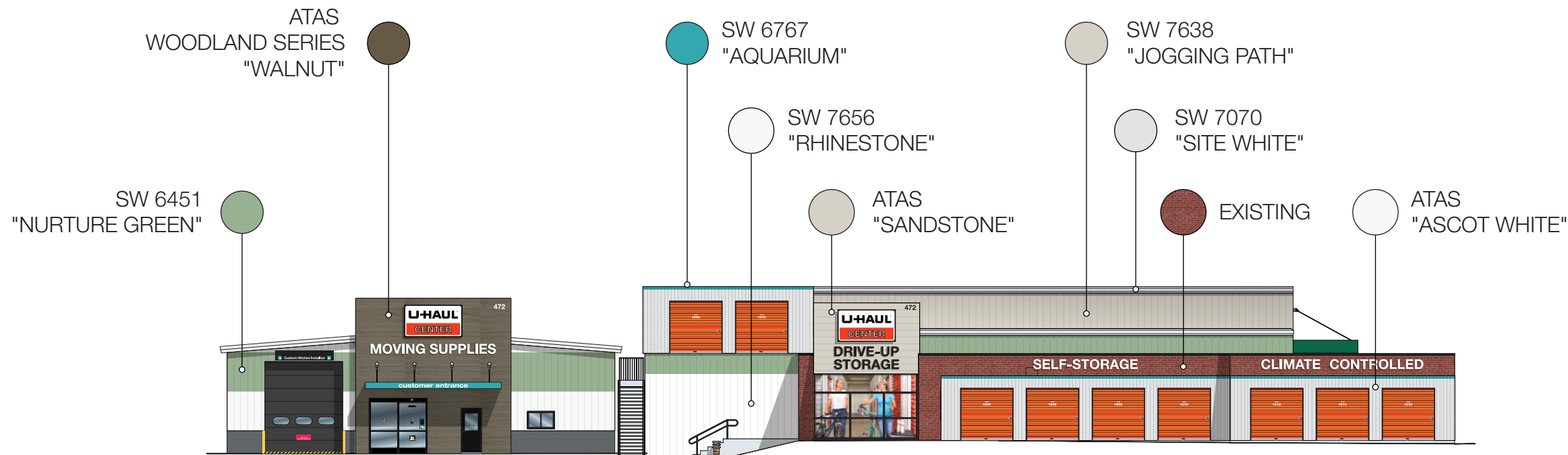
MOVING & STORAGE OF SOUTH KEENE

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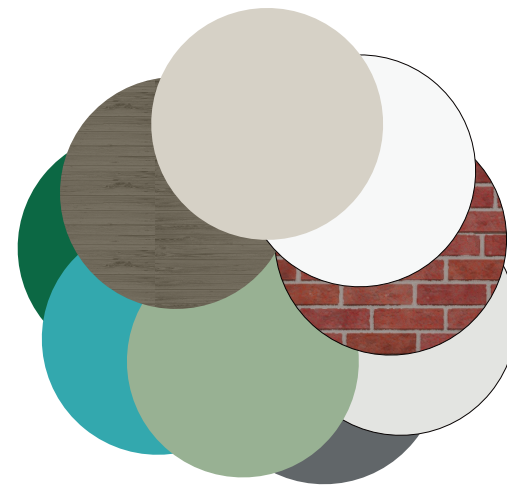
NORTH ELEVATION

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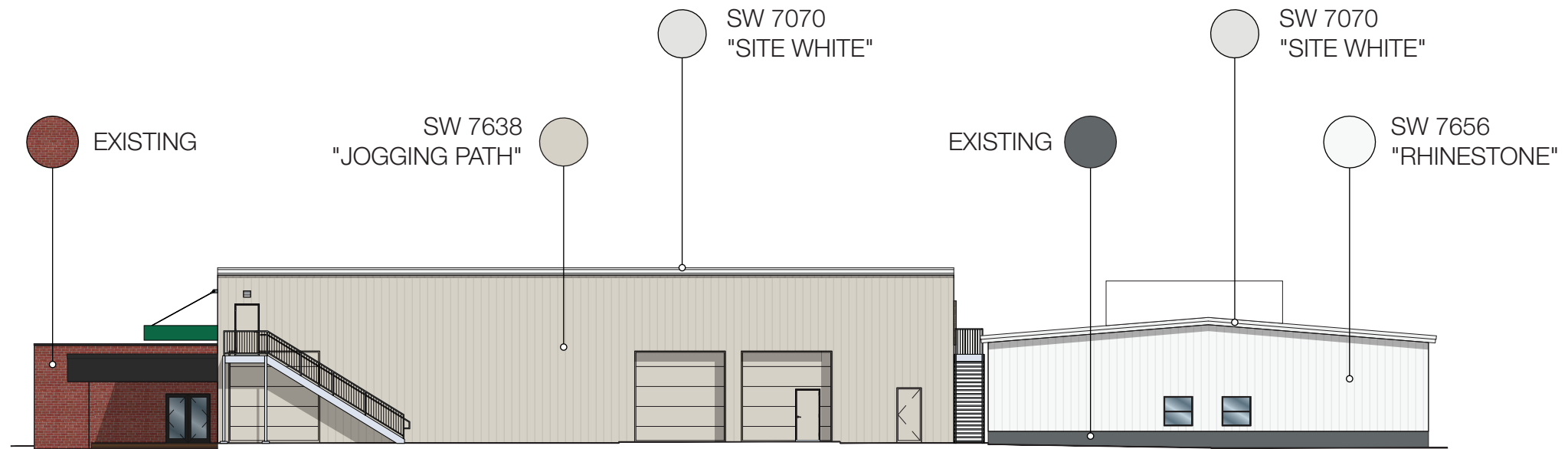
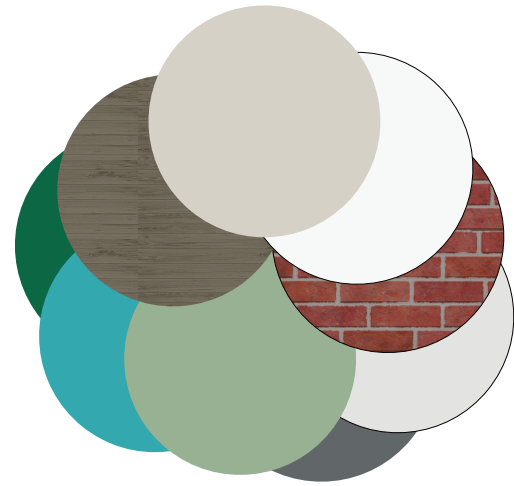
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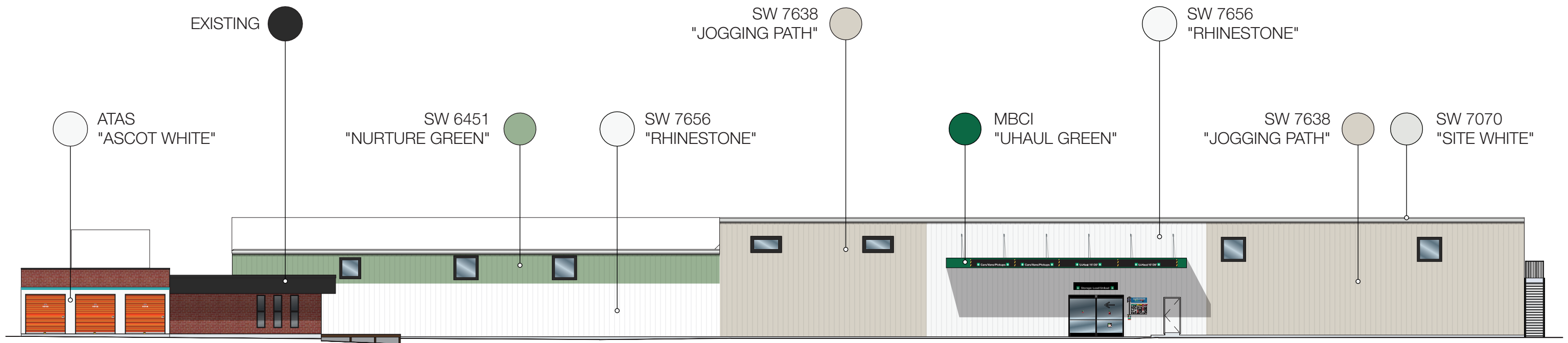
MOVING & STORAGE OF SOUTH KEENE

472 Winchester St. Keene, NH 03431



EAST ELEVATION

Scale: 1" = 20'



SOUTH ELEVATION

Scale: 1" = 20'

MOVING & STORAGE OF SOUTH KEENE

472 Winchester St. Keene, NH 03431

SW 6767
"AQUARIUM"



NORTH ELEVATION

Scale: 1" = 20'

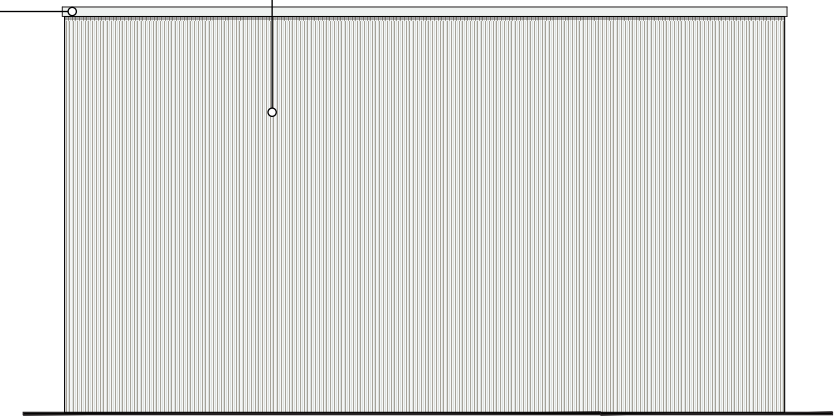
ATAS
"BONE WHITE"



SW 7638
"JOGGING PATH"



ATAS
"REGAL WHITE"



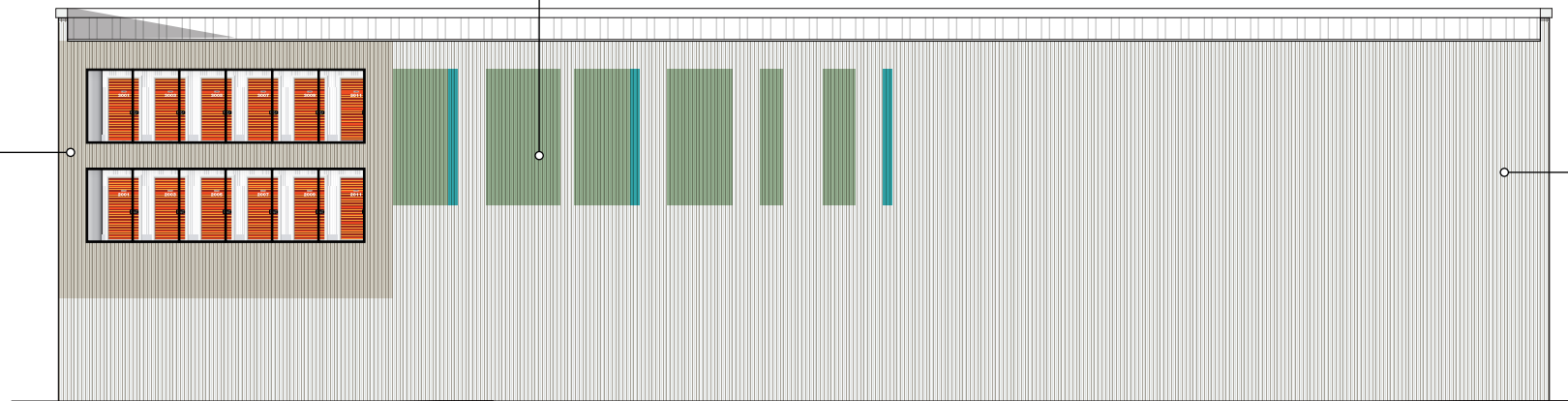
SOUTH ELEVATION

Scale: 1" = 20'

SW 6451
"NURTURE GREEN"



SW 7638
"JOGGING PATH"



ATAS
"REGAL WHITE"



WEST ELEVATION

Scale: 1" = 20'

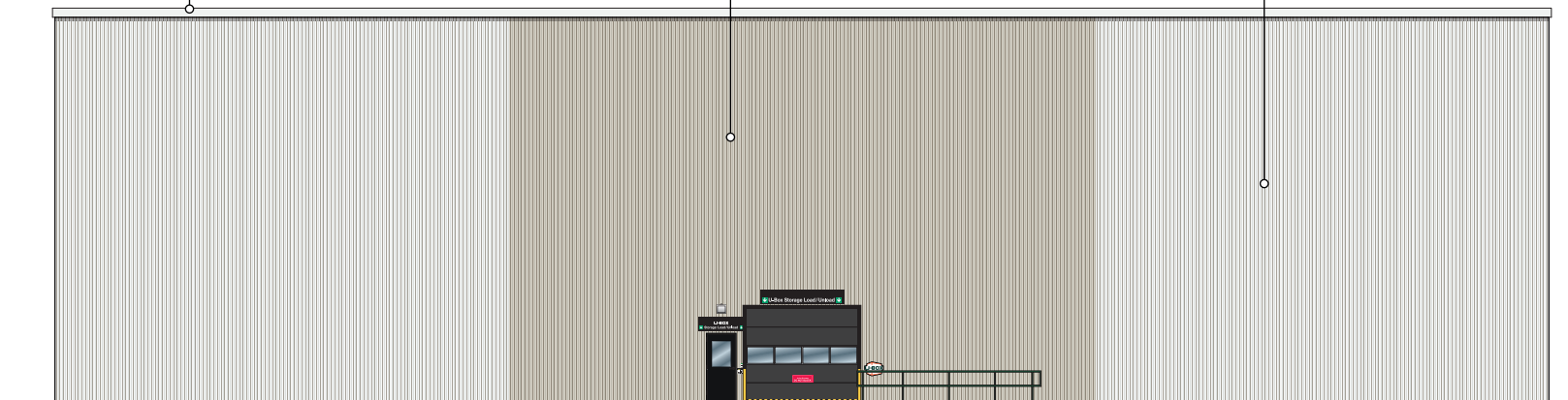
ATAS
"BONE WHITE"



ATAS
"SANDSTONE"

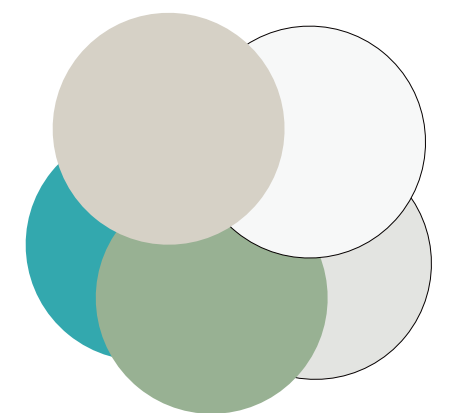


ATAS
"REGAL WHITE"



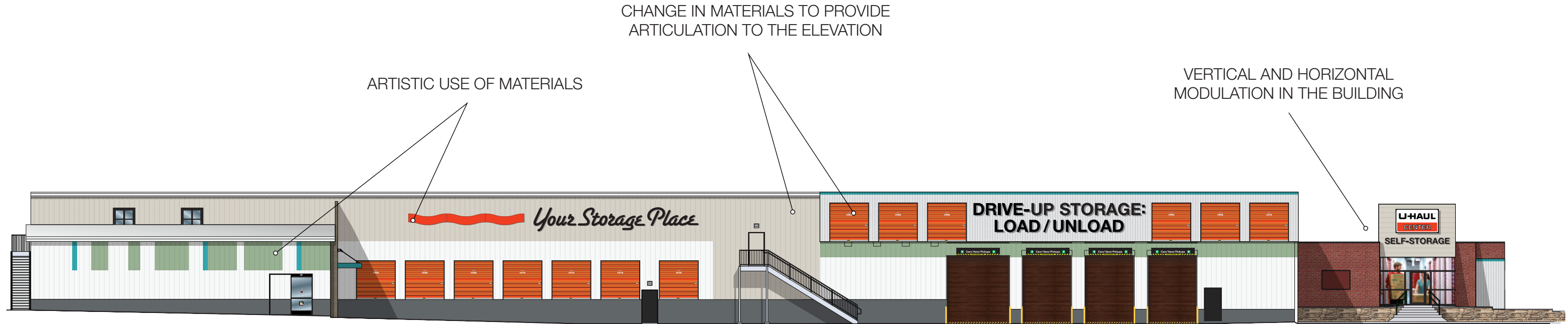
EAST ELEVATION

Scale: 1" = 20'



MOVING & STORAGE OF SOUTH KEENE

472 Winchester St. Keene, NH 03431



NORTH ELEVATION

Scale: 1" = 20'



WEST ELEVATION

Scale: 1" = 20'

MOVING & STORAGE OF SOUTH KEENE

472 Winchester St. Keene, NH 03431

CHANGE IN MATERIALS TO PROVIDE VISUAL SEPARATION TO THE FACADE

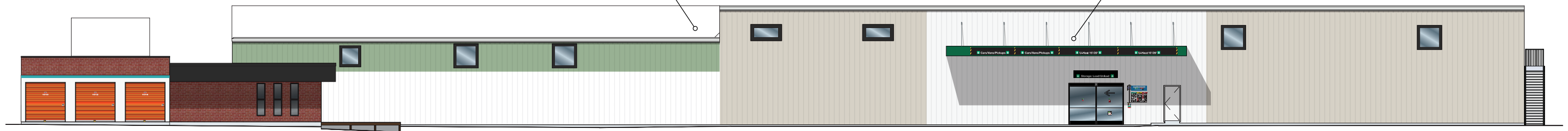


EAST ELEVATION

Scale: 1" = 20'

PROJECTION CREATES ARCHITECTURAL INTEREST ABOVE CUSTOMER STORAGE AREA AND PROVIDES PROTECTION FROM THE ELEMENTS.

VERTICAL AND HORIZONTAL MODULATION IN THE BUILDING



SOUTH ELEVATION

Scale: 1" = 20'

MOVING & STORAGE OF SOUTH KEENE

472 Winchester St. Keene, NH 03431

NORTH ELEVATION

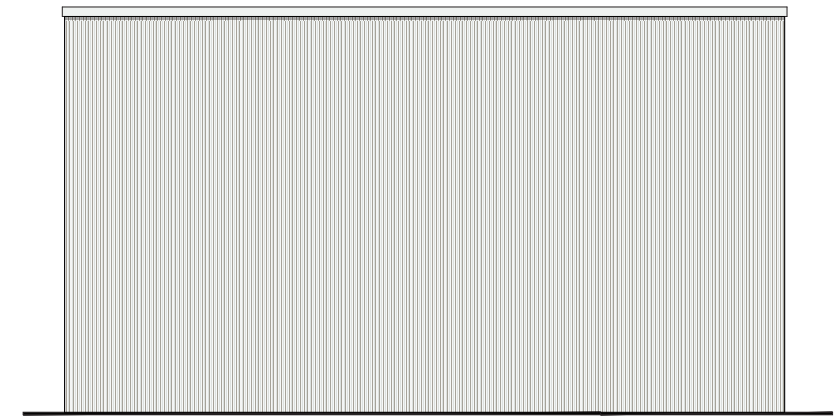
Scale: 1" = 20'



FAUX WINDOW DECALS ON ALUMINUM TO PROVIDE FENESTRATION & RETAIL APPEARANCE

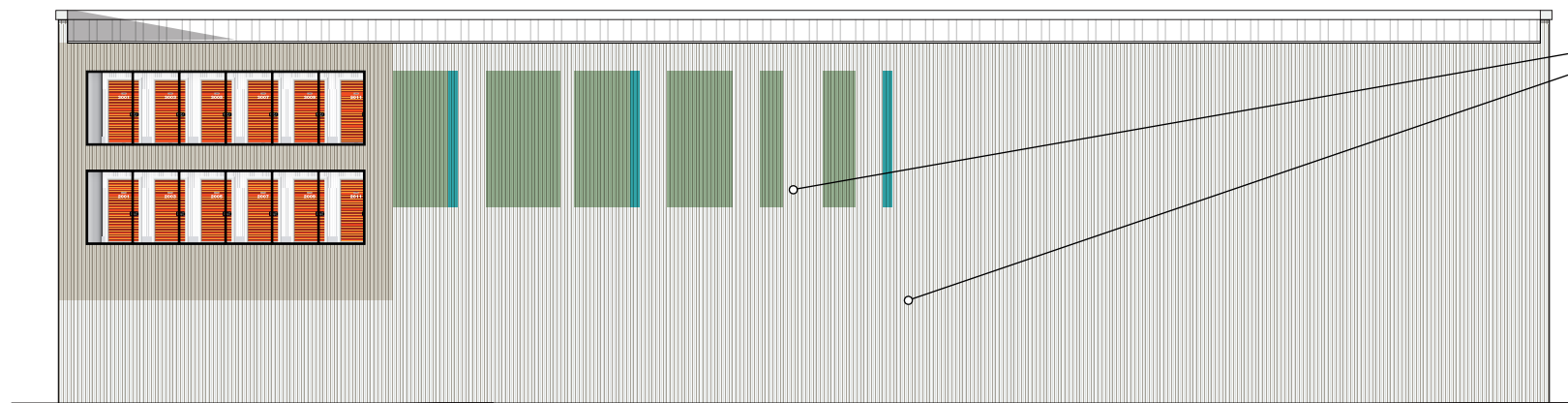
SOUTH ELEVATION

Scale: 1" = 20'



WEST ELEVATION

Scale: 1" = 20'

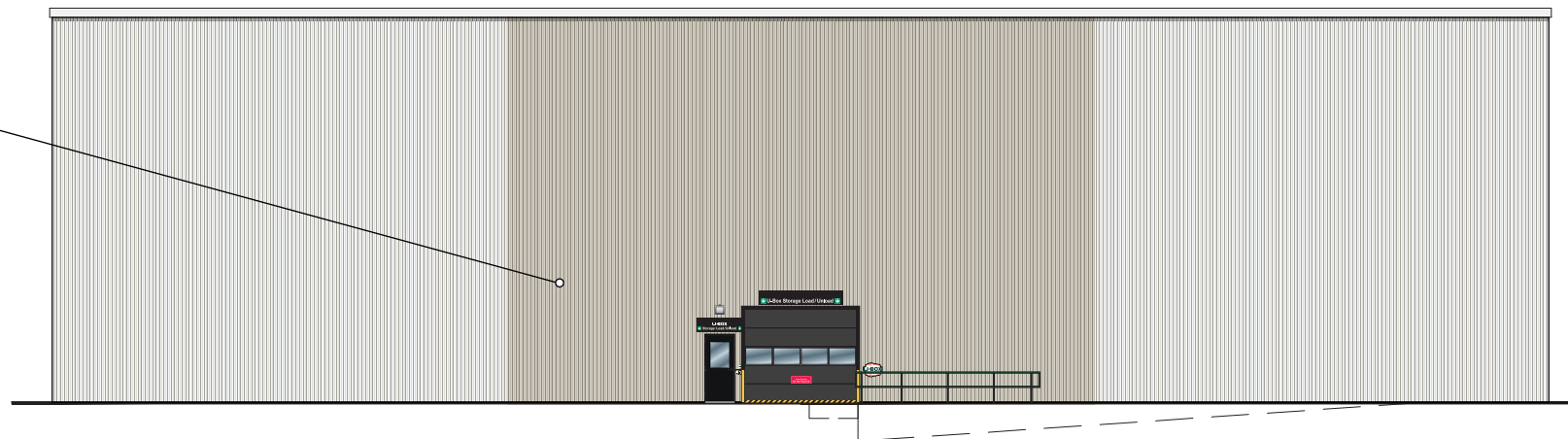


ARTISTIC USE OF MATERIALS

CHANGE IN MATERIALS TO PROVIDE ARTICULATION TO THE ELEVATION

EAST ELEVATION

Scale: 1" = 20'

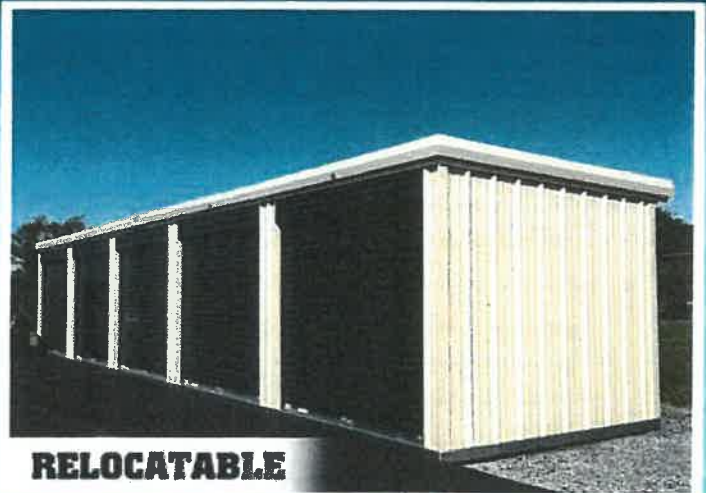


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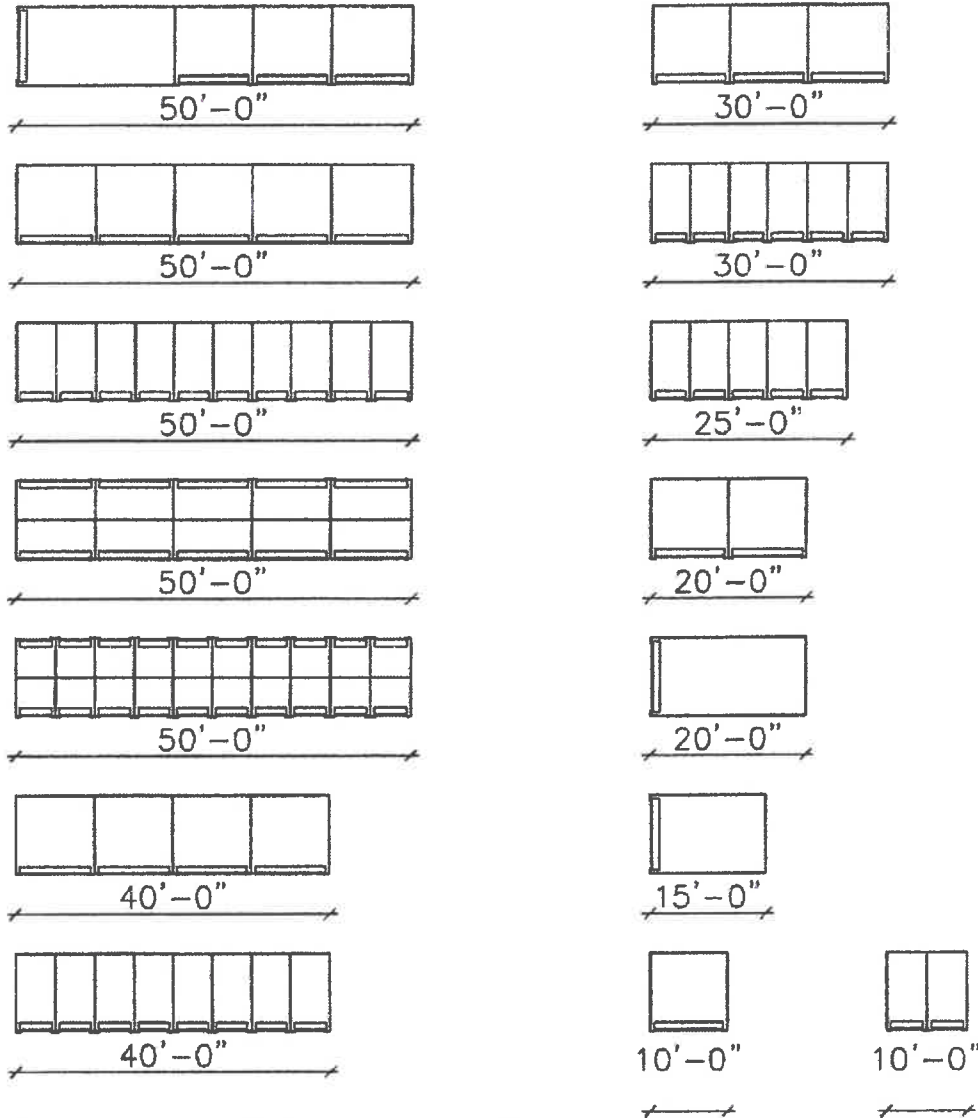
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Sample Layouts



Contemporary Relocatable specifications:

- 26 gauge standing seam roof ; 42.5 lb. snow load
- 26 gauge siliconized polyester exterior walls in 15 colors
- Roll up doors (20 colors) with tension control and ball bearings
- Insulated ply foil roof ; Architectural blue prints
- Delivered complete with a pressure treated wood floor system
- 23' crane required by buyer to lift off of Miller truck
- Sales tax is not included. Quantity discounts available
- All roofs above quoted as 1/4" x 12" shed slope
- No gutters and downspouts
- One truck load delivers 40 to 50 feet of relocatables
- Freight charge is \$3.00 per mile and \$70.00 per state from Harrisburg, PA

CITY OF KEENE PLANNING BOARD

Department File# _____

Received _____

Surface Water Protection Conditional Use Permit (SWP CUP) Application To be used when project also requires Site Plan Review

Please fill out entire application

Section One: Project

Tax Map Parcel # 115 - 019/20 000 - 000 - 000

Existing Zoning: CL - COM. LIMITED

Project:

Name U-HAUL OF SOUTH KEENE

Address 472 WINCHESTER ST., KEENE, NH 03431

Section Two: Contact Information

Owner:

Name AMERCO REAL ESTATE COMPANY

Signature

M. H. Vaino

Name _____

Signature _____

Address 2727 NORTH CENTRAL AVE, PHOENIX, AZ 85004

Phone (day) (603) 762 - 5535

(Fax) () _____

Date _____

Applicant/Agent:

Name SAME AS OWNER

Signature

M. H. Vaino

Address _____

Phone: (day) () _____

Fax: () _____

Date _____

Surveyor/Planner/Engineer:

Name FIELDSTONE LAND CONSULTANTS

Signature

John Roman

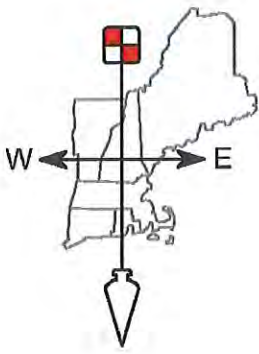
Address 206 ELM ST, MILFORD, NH 03055

Phone: (day) (603) 672 - 5456

Fax: () _____

Date:

10/29/21



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Land Planning ♦ Septic Designs

LAND CONSULTANTS, PLLC

206 Elm Street, Milford, NH 03055 - Phone: 603-672-5456 - Fax: 603-413-5456
www.FieldstoneLandConsultants.com

Conditional Use Application Narrative

U-HAUL OF SOUTH KEENE
Tax Map Parcel 115-019 & 20
472 Winchester Street
Keene, New Hampshire

October 28, 2021

Project Narrative:

Fieldstone Land Consultants, on behalf of Amerco Real Estate Company, is submitting this application in conjunction with the Site Plan approval, for the Conditional Use within a surface water buffer. The overall proposal consists of renovating two existing buildings, constructing one new building, setting pre-fabricated self-storage units, and expanding parking to the southern side of the buildings. The site will be utilized as a U-Haul truck rental business, and a self-storage facility with both climate-controlled and dry storage units. As part of the redevelopment, a driveway will be constructed to access all sides of the building. The proposed driveway around the southwestern corner of the building will encroach into the 30' surface water buffer, based on the Commerce Limited (CL) Zoning requirements.

The southern border of the property is defined by the Ash Swamp Brook. The majority of the site is located within the 100-year floodplain, with the 100-year flood elevation at 471.2 feet. The proposed building, display area, rear parking and storage unit areas all fall within the Floodplain. The fill required to develop these areas will be offset by a flood compensatory storage area. This was done along the southern end of the property and provides a total volume of compensation to offset the volume of fill in the floodplain. The Floodway also crosses the south-southwest border of the property as depicted on the plans. There is no fill proposed within the floodway, although there is flood compensation cut proposed within the Floodway. A portion of the site is also in the City's Surface Water Overlay Protection District with a 30' buffer requirement. The work proposed within the buffer is for floodplain compensatory storage grading, as allowed under Section 11.5.L of the Land Development Code, and the paved driveway around the southwest corner of the existing building. The proposed driveway is 24' in width and will encroach the 30' buffer by 25' at the greatest extent into the buffer. The buffer is based on 30' from the delineated wetland line, which is approximately the top of the stream bank.

The section of driveway adjacent to the surface water buffer will be curbed to prevent runoff and sediment from the pavement from entering the surface water resource. Erosion and sediment controls are specified on the Grading Plan and Erosion Control Details. These will consist of silt fence along the work limit and silt socks installed in nearby catch basins. All catch basins will have deep sumps and oil/debris separator hoods installed as a pretreatment method. The slopes for the flood compensation area will have erosion control matting installed, as the slopes are designed at 2:1.

The current vegetation along the bank of the brook will be maintained in a natural state. The floodplain compensation area will be seeded and allowed to grow into a natural state. Only woody vegetation and small trees will be trimmed at the drainage structures, such as the overflow spillway, and rip rap aprons. Ultimately this area will become a natural buffer and wildlife habitat adjacent to the Ash Swamp Brook.

Below is an outline of the LDC permit standards, followed by the permit criteria and how each item has been addressed.

LDC Section 11.6.2: Conditional Use Permit Standards:

- A. The proposed use cannot be located in a manner to avoid encroachment:** Due to the proximity of the existing building to the Ash Swamp Brook the 30' overlay buffer restricts the use around the southwest corner of the building. The driveway is being proposed for fire truck access to all sides of the building, along with providing access to the rear parking lot. If the driveway cannot be placed within the buffer, there would be a dead end to the display area and rear parking lot. The fire truck access would be eliminated, forcing emergency vehicles to back up and go around the entire site to reach the rear of the buildings.
- B. Encroachment into the buffer area has been minimized:** The driveway width starts at the building wall and is wide enough for fire truck access and vehicle passage.
- C. The nature, design, siting, and scale of the proposed use will avoid the potential for adverse impacts to the surface water resource:** The paved driveway will be curbed along the southern side to collect stormwater runoff and direct it away from the bank of the brook. The catch basins on site will have oil/debris hoods installed and deep sumps to collect sediment.
- D. The surface water buffer area shall be left in a natural state:** The encroachment area will be permanently altered; however, the remaining buffer area will have existing vegetation. The floodplain compensation area will be an upland buffer to the water resource, and natural vegetation will grow into this area.
- E. The Planning Board may consider the following:**
- 1. The size, character, and quality of the surface water being encroached upon:** The Ash Swamp Brook runs west to east in the southern portion of Keene. It was originally part of the City Tax Ditch system, used to drain water from Keene's lowlands. The brook winds through many of the commercial developments and flows into the Ashuelot River.
 - 2. The location and connectivity of the surface water in relation to other surface waters:** The area of encroachment is approximately 4,200 feet upstream of where Ash Swamp Brook intersects the Ashuelot River.
 - 3. The nature of the ecological and hydrological functions served by the surface water:** The proposed project will not have an adverse impact on the ecological or hydrological function of Ash Swamp Brook. The floodplain compensation will offset any fill within the floodplain and allow floodwaters to back up in a similar fashion as the existing condition, with no increase to flood elevation. The floodplain area will provide wildlife habitat to maintain the ecological balance around the site. The drainage systems have been designed to reduce the stormwater runoff volumes and velocities during the 2, 10, and 50 year storm events.
 - 4. The nature of the topography, slopes, soils and vegetation in the buffer:** The slopes, soils and vegetation within the buffer area will not change, only the where the pavement encroaches the buffer.

5. **The role of the surface water buffer in mitigating soil erosion, sediment and nutrient transport, groundwater recharge, flood storage and flow dispersion:** The drainage systems will support groundwater recharge and the flood storage will equal the volume of the current storage on site. The soil erosion, sediment and nutrient transport will be mitigated by using proper erosion control measures and the installation of the proposed stormwater drainage system.
6. **The extent to which the surface water buffer serves as wildlife habitat or travel corridor:** The buffer area is directly adjacent to NH Route 10 and the bridge. The travel corridor is the edge of the brook and the banks on either side of the brook. These areas will not be altered.
7. **The rate, timing and volume of stormwater runoff and its potential to influence water quality:** The rate, timing and volume of stormwater will be mitigated by the use of deep sump catch basins, oil/debris hoods, and interconnecting perforated pipe and stone drainage trenches. The outfall of the subsurface drainage will be into the floodplain compensation area, that also provides detention of stormwater in large storm events. There will be no adverse influence to the water quality from the site development.
8. **The sensitivity of the surface water and the surface water buffer to disruption from changes in grade, or plant and animal habitat in the buffer zone:** The grades for the floodplain compensation will allow for floodwaters to inflow and recede from the site in a more controlled manner than the existing condition. The floodplain compensation will provide habitat for native plants and animals.

SWP CUP Application Section Four: Conditional Use Permit Criteria (per application form):

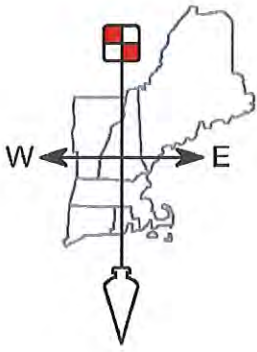
1. **Proposed mitigation measures** – The bank adjacent to the buffer encroachment will be left in a natural state, which consists of small trees and mixed brush. The floodplain compensation area will be allowed to return to a natural state, following grading and stabilization practices. This area will ultimately become a habitat for natural flora and fauna to propagate. The area will serve as an upland buffer to the Ash Swamp Brook.
2. **Disclosure of State & Federal Permits** – The overall impact of the project will require an Alteration of Terrain Permit from NHDES and a General Construction Permit with a Notice of Intent with the EPA. Upon completion of floodplain compensation grading, a Letter of Map Revision (LOMR) will be filed with FEMA.
3. **Why proposed use cannot be placed outside of the Overlay District** – Due to the proximity of the existing building to the Ash Swamp Brook the 30' overlay buffer restricts the use around the southwest corner of the building, as addressed above.
4. **How encroachment has been minimized** – The encroachment has been minimized by paving up to the building wall and providing a 24' wide drive aisle, enough to pass vehicles and allow for firetruck turning motions.
5. **How proposed use was designed to avoid adverse impacts to surface water resources** – The proposed driveway was designed with asphalt curbing to collect stormwater runoff and pavement sediment prior to reaching the water resource. The stormwater is collected in a catch basin, fitted with a deep sump and oil/debris separator hood. The stormwater is infiltrated back into the ground via a subsurface drainage system. The bank of the Ash Swamp Brook will remain in its natural state and will have silt fencing installed to protect the resource during construction.
6. **How the buffer will be maintained in a natural state** – The bank of the brook will be

maintained in its natural state. The existing buffer that is being impacted is currently a maintained grass lawn. The encroachment area of 25' will be permanently paved.

7. **Location and connectivity of surface water in relation to surrounding watershed** – The Ash Swamp Brook flows to the east and connects to the Ashuelot River.
8. **Extent to which buffer serves as wildlife habitat** – The existing buffer is not a high value wildlife habitat due to its proximity to NH Route 10 (Winchester Street) and the concrete abutment of the NH Route 10 bridge. There is no proposed change to the banks of the brook or the stream course, so those existing habitats would not be impacted by this project. The NHDOT is planning on replacing this bridge within the next year, and this area of brook will be impacted at that point. We do not know the extent of the NHDOT bridge impact, but it will likely impact the banks of the brook in this area.

Wetland Certification:

The wetlands shown on the plans were delineated in accordance with the US Army Corps of Engineers, 1987 Wetland Delineation Manual Y-87-1 and Regional Supplements for Northeast and Northcentral Region and field indicators for hydric soils in New England by Christopher A. Guida, Certified Wetland Scientist, on August 19, 2020.



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206 Elm Street, Milford, NH 03055 - Phone: 603-672-5456 - Fax: 603-413-5456

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October 29, 2021

Rhett Lamb
Community Development Director
City Hall - 4th Floor
3 Washington Street-
Keene, NH 03431.

**RE: Wetland Report
UHaul of South Keene
472 Winchester Street
Keene, NH**

Background:

In August 2020 field work was performed on the above referenced property located off Winchester Street at the former Clarke Distributors site. The project area consists of existing commercial buildings and associated paved parking lots and driveways. The existing commercial development is located along the northern portion of the property at the intersection of Winchester Street and Krif Road. The southern border of the property is defined by the Ash Swamp Brook. The areas directly adjacent to the brook are vegetated with small trees and brush in the southern portion of the lot, and grass lawn near the intersection of the brook and Winchester Street. The majority of the site falls within the 100 year Floodplain, as defined by FEMA mapping, with a flood elevation of 471.2 feet above sea level.

Wetlands Delineation:

Jurisdictional Wetlands within the project area were delineated by Certified Wetland Scientist Christopher A. Guida, CSS, CWS in August 2020. Wetlands on site were located along the bank of the Ash Swamp Brook with the delineation line being approximately the top of bank of the brook.

Under the US Fish and Wildlife Classification System (Cowardin System), the wetlands would be classified as Riverine, Lower Perennial, unconsolidated bottom, sand (R2UB2) associated with Ash Swamp Brook. The entire parcel appeared to have been previously cleared and altered throughout history with some re-grading including filling and ditching for drainage purposes due to the level topography. The underlying soils are typical very fine-fine sandy flood plain soils which are well drained; however due to level topography and previous alterations there are some areas that may have slower drainage rates than unaltered sloping sites. Previous reference plans have referenced "National Wetlands" which are typically generated using NRCS soil data based on the scale of 1:24000 which was prepared for county level planning purposes and is not suitable or accurate for site specific development. Field verification of the on-site wetlands was based on the US Army Corps of Engineers 1987 Wetland Delineation Manual Y-7-1 and Regional Supplements. Jurisdictional Wetlands on the site were limited to the area immediate to Ash Swamp Brook and banks of the brook. Upland areas adjacent to wetland areas are general level topography and overgrown with pioneering species such as Staghorn Sumac, Eastern White Pine with sapling and shrub understory dominated by same along with Quaking Aspen and Eastern Cottonwood.

Functions and Values:

Ash Swamp Brook flows west to east in the southern portion of Keene. The brook crosses many commercial developments and ultimately flows into the Ashuelot River. The brook intersects with the Ashuelot River approximately 4,000 feet southeasterly from the subject parcel. Functions and Values of the wetland area and adjacent upland areas would have to do to with stormwater treatment and flood attenuation as well as recreation associated with the riverine system.



Photo from the Winchester Street bridge along the Ash Swamp Brook

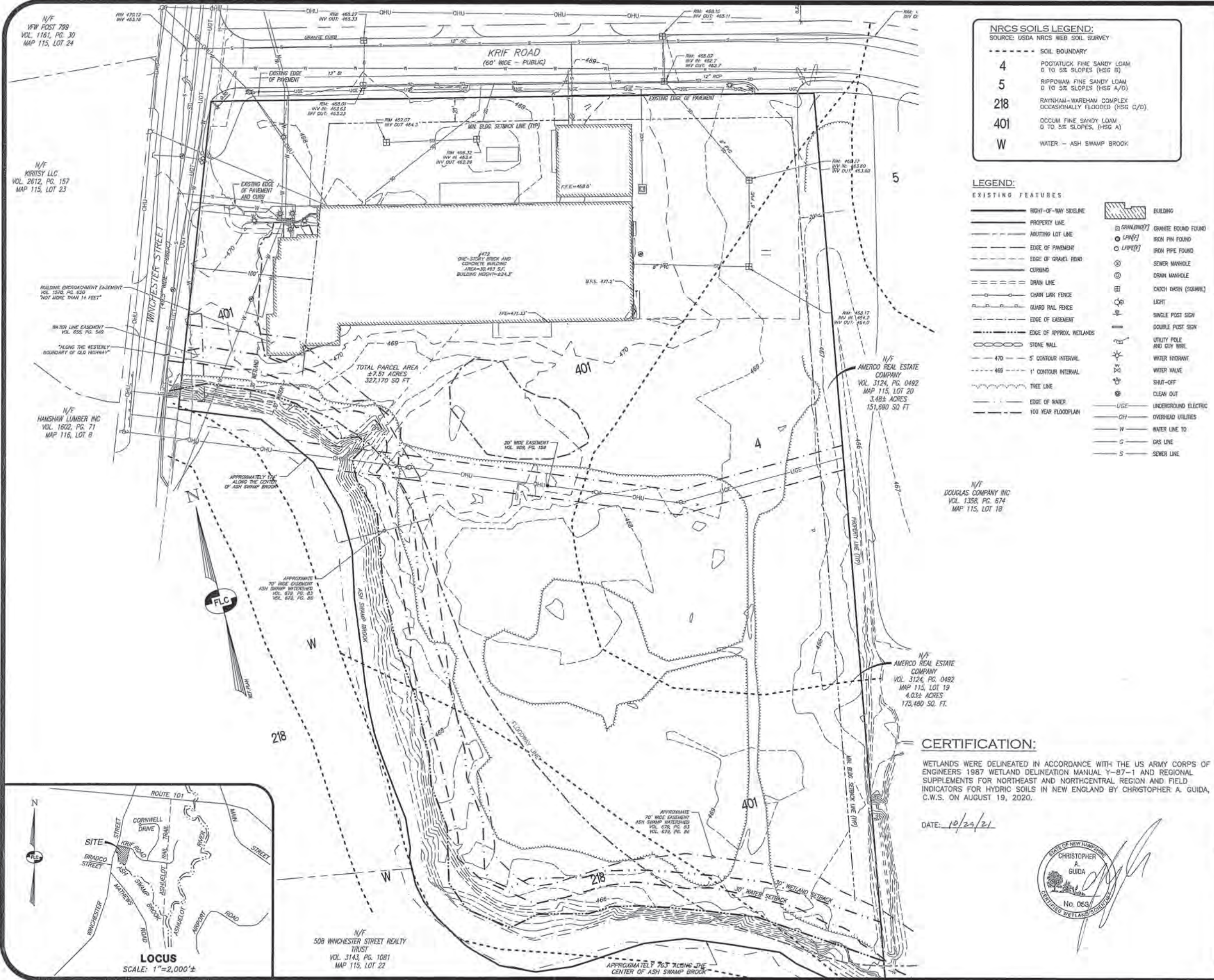


Typical Upland area towards rear of property

Sincerely,
Fieldstone Land Consultants, PLLC

Christopher A. Guida, CSS, CWS
Certified Soil & Wetland Scientist





NRCS SOILS LEGEND:
SOURCE: USDA NRCS WEB SOIL SURVEY

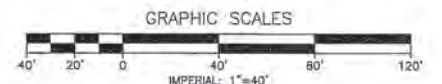
---	SOIL BOUNDARY
4	POQUITUCK FINE SANDY LOAM 0 TO 5% SLOPES (HSG B)
5	RIPPOWAM FINE SANDY LOAM 0 TO 5% SLOPES (HSG A/D)
218	RAYNHAM-WAREHAM COMPLEX OCCASIONALLY FLOODED (HSG C/D)
401	OCCUM FINE SANDY LOAM 0 TO 5% SLOPES (HSG A)
W	WATER - ASH SWAMP BROOK

LEGEND:

EXISTING FEATURES		BUILDING	
---	RIGHT-OF-WAY SIDELINE	[Hatched]	BUILDING
---	PROPERTY LINE	[Square]	GRANITE FOUND FOUND
---	ABUTTING LOT LINE	[Circle]	IRON PIN FOUND
---	EDGE OF PAVEMENT	[Circle]	IRON PIPE FOUND
---	EDGE OF GRAVEL ROAD	[Circle]	SEWER MANHOLE
---	CURBING	[Circle]	DRAIN MANHOLE
---	DRAIN LINE	[Square]	CATCH BASIN (SQUARE)
---	CHAIN LINK FENCE	[Circle]	LIGHT
---	GUARD RAIL FENCE	[Circle]	SINGLE POST SIGN
---	EDGE OF EASEMENT	[Circle]	DOUBLE POST SIGN
---	EDGE OF APPROX. WETLANDS	[Circle]	UTILITY POLE AND GUY WIRE
---	STONE WALL	[Circle]	WATER VALVE
---	470 5' CONTOUR INTERVAL	[Circle]	SHUT-OFF
---	469 1' CONTOUR INTERVAL	[Circle]	CLEAN OUT
---	TREE LINE	[Circle]	UNDERGROUND ELECTRIC
---	EDGE OF WATER	[Circle]	OVERHEAD UTILITIES
---	100 YEAR FLOODPLAIN	[Circle]	WATER LINE TO
		[Circle]	GAS LINE
		[Circle]	SEWER LINE

- NOTES:**
- THE OWNER OF RECORD FOR TAX MAP 115 LOT 19 AND LOT 20 IS AMERCO REAL ESTATE COMPANY. DEED REFERENCE IS VOL. 3124 PAGE 0492 RECORDED IN THE C.C.R.D. FOR BOTH LOTS.
 - THE PURPOSE OF THIS PLAN IS TO SHOW A PROPOSED U-HAUL MOVING AND STORAGE FACILITY WITH ASSOCIATED SITE IMPROVEMENTS.
 - THE TOTAL AREA OF TAX MAP PARCEL 115-19 IS 4.03± ACRES OR 175,480 SQ. FT. WITH 50.00 FT. AND THE TOTAL AREA OF TAX MAP PARCEL 115-20 IS 3.48± ACRES OR 151,690 SQ. FT. WITH 445.91 FT. OF FRONTAGE ALONG KRIF ROAD & 263.50 FT. OF FRONTAGE ALONG WINCHESTER STREET PER THE REFERENCE PLAN CITED HEREON.
 - THE COMBINED PARCEL SIZE IS 7.51± ACRES OR 327,170 SQ. FT.
 - ZONING FOR BOTH PARCELS IS COMMERCE LIMITED
 - FRONTAGE = 100 FT.
 - FRONT SETBACK = 100 FT.
 - REAR SETBACK = 20 FT.
 - SIDE SETBACK = 20 FT.
 - MAXIMUM BUILDING COVERAGE = 40%
 - EXISTING BUILDINGS = 34,303 SF, PROPOSED BUILDINGS = 41,075 = 75,378 SF (23%)
 - MAXIMUM IMPERVIOUS COVERAGE (INCLUDING STRUCTURES) = 70%
 - (EXISTING IMP = 65,623 SF (26.6%), PROPOSED IMP = 210,947 SF (65.8%))
 - MINIMUM GREEN/OPEN = 30% (EXISTING GREEN=73.4, PROPOSED GREEN = 34.4)
 - HEIGHT LIMIT = 35 FT. (2-STORIES). (PROPOSED BLDG=32', 35' FROM GRADE, ONE-STORY)
 - THE SURFACE FEATURES SHOWN WERE DEVELOPED ENTIRELY FROM THE REFERENCE PLANS CITED HEREON.
 - THE LOTS LIE WITHIN THE BOUNDARY OF THE 100 YEAR FLOOD HAZARD PER FLOOD INSURANCE RATE MAP FOR THE CITY OF KEENE, CHESHIRE COUNTY, COMMUNITY 330023, PANEL NUMBER 330050268E DATED MAY 23, 2006. BASED ON THIS PANEL, THE ESTIMATED ELEVATION OF THE 100 YEAR FLOOD AT THIS LOCATION IS 471.2 FT.

- REFERENCE PLANS:**
- "EXISTING CONDITIONS AND PERIMETER SURVEY PERFORMED FOR - CLARKE DISTRIBUTORS - 472 WINCHESTER STREET - KEENE, N.H. 03431 - OF LAND LOCATED IN - CITY OF KEENE-COUNTY OF CHESHIRE-STATE OF NEW HAMPSHIRE", SCALE: 1"=30' AND DATED APRIL 7, 1989 BY C.T. MALE ASSOCIATES, P.C.
 - "EXISTING PLAN", SCALE: 1"=50' AND DATED OCTOBER 20, 2017 BY BRICKSTONE LAND USE CONSULTANTS.
 - "ALTA/NEPS LAND TITLE SURVEY", SCALE 1" = 50', AND DATED AUGUST 27, 2020 BY CORNERSTONE ENERGY SERVICES. PROVIDED BY APPLICANT AND LAND OWNER.

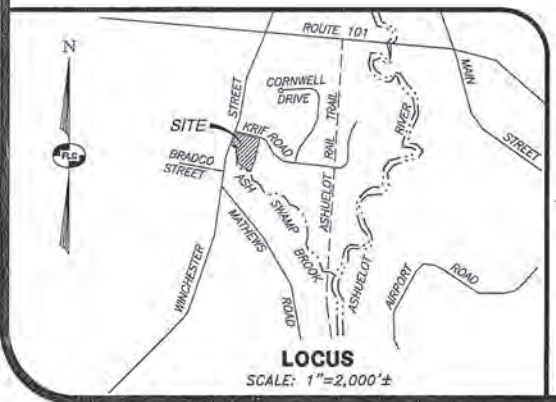


REV.	DATE	DESCRIPTION	C/O	DR	CK
A	8/23/21	REVISED PER IN-HOUSE & CITY COMMENTS FOR ZBA	JEN	CEB	

CERTIFICATION:

WETLANDS WERE DELINEATED IN ACCORDANCE WITH THE US ARMY CORPS OF ENGINEERS 1987 WETLAND DELINEATION MANUAL Y-87-1 AND REGIONAL SUPPLEMENTS FOR NORTHEAST AND NORTH-CENTRAL REGION AND FIELD INDICATORS FOR HYDRIC SOILS IN NEW ENGLAND BY CHRISTOPHER A. GUIDA, C.W.S. ON AUGUST 19, 2020.

DATE: 10/29/21



EXISTING CONDITIONS
U-HAUL OF SOUTH KEENE
TAX MAP 115 LOTS 19 & 20
472 WINCHESTER STREET
KEENE, NEW HAMPSHIRE

PREPARED FOR:
AMERCO REAL ESTATE COMPANY
499 MONTGOMERY STREET, CHICOPEE, MA 01020

SCALE: 1" = 40' OCTOBER 15, 2021

Surveying ♦ Engineering ♦ Land Planning ♦ Permitting ♦ Septic Designs

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www.FieldstoneLandConsultants.com

N/F
VFW POST 799
VOL. 1161, PG. 30
MAP 115, LOT 24

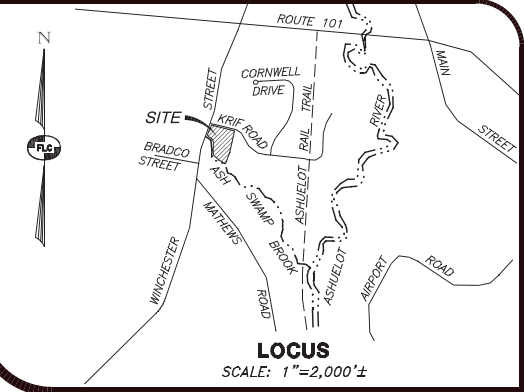
N/F
KIRITSY LLC
VOL. 2612, PG. 157
MAP 115, LOT 23

BUILDING ENCROACHMENT EASEMENT
VOL. 1570, PG. 620
"NOT MORE THAN 14 FEET"

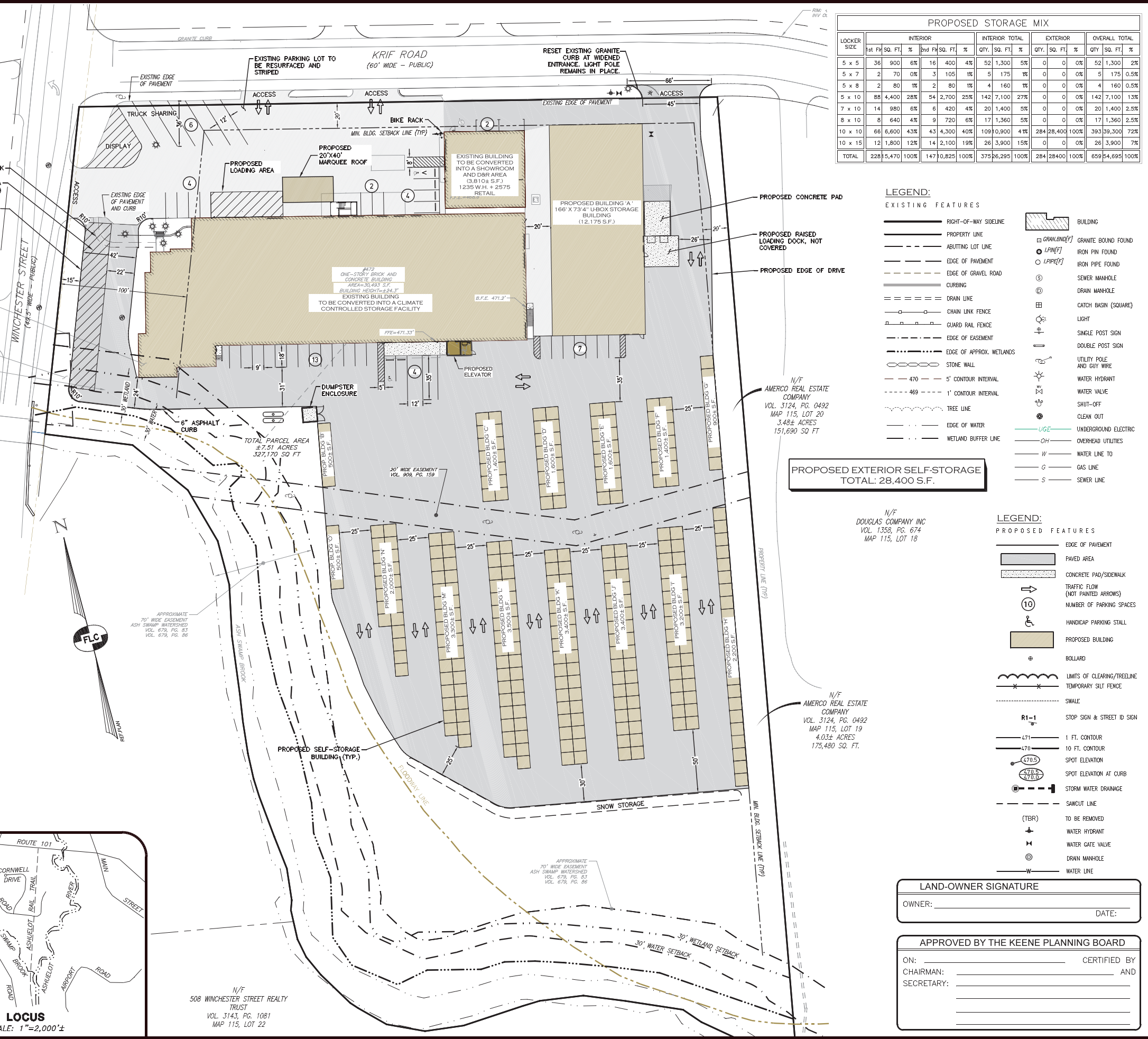
WATER LINE EASEMENT
VOL. 655, PG. 549

N/F
HAMSHAW LUMBER INC
VOL. 1602, PG. 71
MAP 116, LOT 8

APPROXIMATE
70' WIDE EASEMENT
ASH SWAMP INTERSECTED
VOL. 678, PG. 83
VOL. 679, PG. 86



N/F
508 WINCHESTER STREET REALTY
TRUST
VOL. 3143, PG. 1081
MAP 115, LOT 22



PROPOSED STORAGE MIX

LOCKER SIZE	INTERIOR				INTERIOR TOTAL				EXTERIOR				OVERALL TOTAL			
	Qty	Sq. Ft.	%	Cost	Qty	Sq. Ft.	%	Cost	Qty	Sq. Ft.	%	Cost	Qty	Sq. Ft.	%	Cost
5 x 5	36	900	6%	16,400	36	900	6%	16,400	0	0	0%	0	36	900	2%	16,400
5 x 7	2	70	0%	3,105	2	70	0%	3,105	0	0	0%	0	2	70	0%	3,105
5 x 8	2	80	1%	2,800	2	80	1%	2,800	0	0	0%	0	2	80	0%	2,800
5 x 10	88	4,400	28%	54,270	88	4,400	28%	54,270	0	0	0%	0	88	4,400	13%	54,270
7 x 10	14	980	6%	6,420	14	980	6%	6,420	0	0	0%	0	14	980	2.5%	6,420
8 x 10	8	640	4%	9,720	8	640	4%	9,720	0	0	0%	0	8	640	2.5%	9,720
10 x 10	66	6,600	43%	43,430	66	6,600	43%	43,430	284	28,400	100%	393,300	350	35,000	72%	393,300
10 x 15	12	1,800	12%	14,210	12	1,800	12%	14,210	0	0	0%	0	12	1,800	7%	14,210
TOTAL	228	5,470	100%	147,082.5	228	5,470	100%	147,082.5	284	28,400	100%	659,646.9	659	64,870	100%	659,646.9

LEGEND:

EXISTING FEATURES

- RIGHT-OF-WAY SIDELINE
- PROPERTY LINE
- ABUTTING LOT LINE
- EDGE OF PAVEMENT
- EDGE OF GRAVEL ROAD
- CURBING
- DRAIN LINE
- CHAIN LINK FENCE
- GUARD RAIL FENCE
- EDGE OF EASEMENT
- EDGE OF APPROX. WETLANDS
- STONE WALL
- 470 = 5' CONTOUR INTERVAL
- 469 = 1' CONTOUR INTERVAL
- TREE LINE
- EDGE OF WATER
- WETLAND BUFFER LINE

PROPOSED FEATURES

- BUILDING
- GRANITE FOUND
- IRON PIN FOUND
- IRON PIPE FOUND
- SEWER MANHOLE
- DRAIN MANHOLE
- CATCH BASIN (SQUARE)
- LIGHT
- SINGLE POST SIGN
- DOUBLE POST SIGN
- UTILITY POLE AND GUY WIRE
- WATER HYDRANT
- WATER VALVE
- SHUT-OFF
- CLEAN OUT
- UNDERGROUND ELECTRIC
- OVERHEAD UTILITIES
- WATER LINE TO
- GAS LINE
- SEWER LINE

PROPOSED EXTERIOR SELF-STORAGE
TOTAL: 28,400 S.F.

LEGEND:

PROPOSED FEATURES

- EDGE OF PAVEMENT
- PAVED AREA
- CONCRETE PAD/SIDEWALK
- TRAFFIC FLOW (NOT PAINTED ARROWS)
- NUMBER OF PARKING SPACES
- HANDICAP PARKING STALL
- PROPOSED BUILDING
- BOLLARD
- LIMITS OF CLEARING/TREELINE
- TEMPORARY SILT FENCE
- SWALE
- STOP SIGN & STREET ID SIGN
- 1 FT. CONTOUR
- 10 FT. CONTOUR
- SPOT ELEVATION
- SPOT ELEVATION AT CURB
- STORM WATER DRAINAGE
- SAWCUT LINE
- TO BE REMOVED
- WATER HYDRANT
- WATER GATE VALVE
- DRAIN MANHOLE
- WATER LINE

LAND-OWNER SIGNATURE

OWNER: _____ DATE: _____

APPROVED BY THE KEENE PLANNING BOARD

ON: _____ CERTIFIED BY: _____ AND _____
CHAIRMAN: _____ SECRETARY: _____

- NOTES:**
- THE OWNER OF RECORD FOR TAX MAP 115 LOT 19 AND LOT 20 IS AMERCO REAL ESTATE COMPANY. DEED REFERENCE IS VOL. 3124 PAGE 0492 RECORDED IN THE C.C.R.D. FOR BOTH LOTS.
 - THE PURPOSE OF THIS PLAN IS TO SHOW A PROPOSED U-HAUL MOVING AND STORAGE FACILITY WITH ASSOCIATED SITE IMPROVEMENTS.
 - THE TOTAL AREA OF TAX MAP PARCEL 115-19 IS 4.03± ACRES OR 175,480 SQ. FT. WITH 50.00 FT. AND THE TOTAL AREA OF TAX MAP PARCEL 115-20 IS 3.48± ACRES OR 151,690 SQ. FT. WITH 445.91 FT. OF FRONTAGE ALONG KRIF ROAD & 263.50 FT. OF FRONTAGE ALONG WINCHESTER STREET PER THE REFERENCE PLAN CITED HEREON.
 - THE COMBINED PARCEL SIZE IS 7.51± ACRES OR 327,170 SQ. FT.
 - ZONING FOR BOTH PARCELS IS COMMERCE LIMITED
 - FRONTAGE = 100 FT.
 - FRONT SETBACK = 100 FT.
 - REAR SETBACK = 20 FT.
 - SIDE SETBACK = 20 FT.
 - MAXIMUM BUILDING COVERAGE = 40%
 - EXISTING BUILDINGS = 34,303 SF, PROPOSED BUILDINGS = 41,075 = 75,378 SF (23%)
 - MAXIMUM IMPERVIOUS COVERAGE (EXISTING STRUCTURES) = 70%
 - EXISTING IMP = 85,623 SF (26.6%), PROPOSED IMP = 210,947 SF (65.6%)
 - MINIMUM GREEN/OPEN = 30%. (EXISTING GREEN=73.4, PROPOSED GREEN = 34.4)
 - HEIGHT LIMIT = 35 FT. (2-STORIES). (PROPOSED BLDG=32', 35' FROM GRADE, ONE-STORY)
 - THE SURFACE FEATURES SHOWN WERE DEVELOPED ENTIRELY FROM THE REFERENCE PLANS CITED HEREON.
 - THE LOTS LIE WITHIN THE BOUNDARY OF THE 100 YEAR FLOOD HAZARD PER FLOOD INSURANCE RATE MAP FOR THE CITY OF KEENE, CHESHIRE COUNTY, COMMUNITY 330023, PANEL NUMBER 330050266 DATED MAY 23, 2006. BASED ON THIS PANEL, THE ESTIMATED ELEVATION OF THE 100 YEAR FLOOD AT THIS LOCATION IS 471.2 FT.

PARKING REQUIREMENTS:
PARKING: BASED ON NEW I.D.C. SEPTEMBER 1, 2021 (TABLE 9-1)

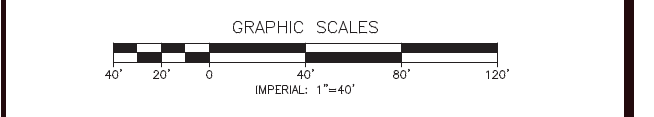
RETAIL = 1 SP / 250 SF GFA
2,575 SF / 250 SF = 10.3 SPACES
SELF-STORAGE = 1 SP / 3,000 SF
INTERIOR = 26,295 / 3000 = 8.8 SPACES
EXTERIOR = 28,400 / 3000 = 9.5 SPACES*
WAREHOUSE = 1 SP / 2,000 SF GFA
12,175 SF / 2,000 SF = 6.1 SPACES REQUIRED
1,235 SF / 2,000 SF = 1 SPACE
TOTAL REQUIRED: 36 SPACES
TOTAL PROVIDED: 42 SPACES

A.D.A. SPACES REQ'D: 42 SPACES = 1 A.D.A. REG. + 1 A.D.A. VAN SPACE
A.D.A. PROVIDED: 2 SPACES, 1 REGULAR + 1 VAN SPACE
OVER 10% OF EXTERIOR STORAGE UNITS (31 UNITS) ARE A.D.A. COMPLIANT

* EXTERIOR SELF-STORAGE IS ALLOWED TO PARK IN FRONT OF THEIR UNITS.

- PERMITS REQUIRED:**
- CITY OF KEENE PLANNING BOARD: SITE PLAN APPROVAL
 - CITY OF KEENE PLANNING BOARD: CONDITIONAL USE FOR PAVEMENT IN 30' WETLAND BUFFER
 - CITY OF KEENE ZONING BOARD: VARIANCE FOR MARQUEE ROOF.
 - CITY OF KEENE FLOODPLAIN DEVELOPMENT PERMIT.
 - CITY OF KEENE BUILDING PERMIT.
 - CITY OF KEENE VOLUNTARY LOT MERGER.
 - NHDES ALTERATION OF TERRAIN PERMIT.
 - FEMA LETTER OF MAP REVISION AND ELEVATION CERTIFICATE.

- REFERENCE PLANS:**
- "EXISTING CONDITIONS AND PERMETER SURVEY PERFORMED FOR - CLARKE DISTRIBUTORS - 472 WINCHESTER STREET - KEENE, N.H. 03431 - OF LAND LOCATED IN - CITY OF KEENE-COUNTY OF CHESHIRE-STATE OF NEW HAMPSHIRE", SCALE: 1"=30' AND DATED APRIL 7, 1989 BY C.T. MALE ASSOCIATES, P.C.
 - "EXISTING PLAN", SCALE: 1"=50' AND DATED OCTOBER 20, 2017 BY BRICKSTONE LAND USE CONSULTANTS.



REV.	DATE	DESCRIPTION	C/O	DR	CK
B	10/28/21	REVISED CANOPY LOCATION, PARKING SPOTS	JEN	CEB	
A	8/23/21	REVISED PER IN-HOUSE & CITY COMMENTS FOR ZBA	JEN	CEB	

SITE PLAN CONCEPT
U-HAUL OF SOUTH KEENE
TAX MAP 115 LOTS 19 & 20
472 WINCHESTER STREET
KEENE, NEW HAMPSHIRE

PREPARED FOR:
AMERCO REAL ESTATE COMPANY
499 MONTGOMERY STREET, CHICOPEE, MA 01020

SCALE: 1" = 40' OCTOBER 15, 2021

Surveying + Engineering + Land Planning + Permitting + Septic Designs

FIELDSTONE
LAND CONSULTANTS, PLLC

206 Elm Street, Millford, NH 03055
Phone: (603) 672-5456 Fax: (603) 413-5456
www.FieldstoneLandConsultants.com

Storm Water Management Report

U-HAUL OF SOUTH KEENE

Project Location:

Tax Map 115, Lots 19 & 20
472 Winchester Street
Keene, NH 03431

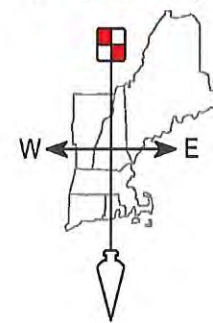
Prepared for:

Amerco Real Estate Company
499 Montgomery Street
Chicopee, MA 01020

Date: October 15, 2021



Surveying ♦ Engineering ♦ Land Planning ♦ Permitting ♦ Septic Designs



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LAND CONSULTANTS, PLLC

206 Elm Street, Milford NH 03055
Phone: (603)-672-5456 Fax: (603)-413-5456
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Narrative with Summary Tables

USGS Locus Map

Web Soil Survey

Aerial Photograph

Drainage Analysis / Storm Water Management Report:

Section 1.1 Existing Conditions – 2, 50 Year Storm Node List

Section 1.2 Existing Conditions – 10 Year Storm Full Summary

Section 2.1 Proposed Conditions – 2, 50 Year Storm Node List

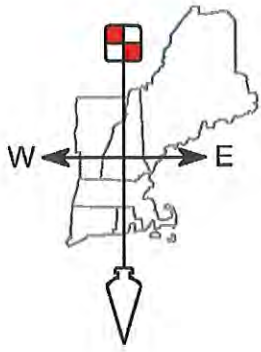
Section 2.2 Proposed Conditions – 10 Year Storm Full Summary

Supplemental Data & Reports:

Section 3.1 Inspection & Maintenance Manual

Section 3.2 Test Pit Data

Section 3.3 Drainage Area Plans



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www.FieldstoneLandConsultants.com

STORM WATER MANAGEMENT REPORT

U-HAUL OF SOUTH KEENE

KEENE, NEW HAMPSHIRE

Prepared for:

AMERCO REAL ESTATE COMPANY

October 15, 2021

I) INTRODUCTION

This storm-water management report was conducted for a proposed site re-development known as U-Haul Moving and Storage of South Keene in Keene, NH. The property is located at 472 Winchester Street (NH Route 10), specifically on City of Keene Assessor's Map 115, Lots 19 and 20. The site was formerly used as a beverage distribution facility, referred to as Clarke Distributors. The applicant is proposing to redevelop the site by utilizing the existing buildings and construct a self-storage facility. The two existing buildings would be repurposed, a new warehouse building constructed, and pre-fabricated, mobile storage units placed to the rear of the site. The main building will be converted to a climate-controlled storage facility that is 30,493 square feet in size. The smaller existing building, 3,810 square feet in size, will serve as the U-Haul showroom and towing accessory installation shop. The new building will be a warehouse for U-Box storage and rentals, proposed to be 11,250 square feet in size. The existing parking lots will be re-stripped and the four existing curb cuts are to remain. There will be a new paved display area along Winchester Street, and new paved parking to south side of the buildings. The areas around the self-storage units will be compacted gravel, providing access to each side of the buildings. The total lot size will be 7.51 Acres, after the proposed merger of Lots 115-19 and 115-20. The proposed development would disturb approximately 222,865 square feet (5.12 Acres) of the land.

The purpose of this report is to analyze the qualitative and quantitative impacts of the proposed development on stormwater runoff. The objective of the proposed stormwater management system for this project is to mitigate any increases resulting from the proposed development and to meet the drainage guidelines set forth in the City of Keene Land Development Code, along with the NHDES Alteration of Terrain Regulations.

II) SITE DESCRIPTION (EXISTING)

The subject property consists of 7.51 acres of land that contains two buildings and paved parking areas. The project is bordered by commercial properties, in the Commerce Limited district, and has frontage along Winchester Street (NH Route 10) and Krif Road. The property was formerly used by a commercial beverage distribution company with a mix of warehouse and office space. The southern border of the property is the centerline of the Ash Swamp Brook, which is listed with FEMA as having a base-flood elevation of 471.2 feet. This flood elevation places the majority of the

site in the 100-Year Flood Plain, with the existing buildings being at or just above the floodplain elevation.

The area surrounding the existing buildings is comprised of pavement surfaces and grass lawn. The southern portion of the building, Lot 155-19, is wooded and heavily vegetated along the bank of the Ash Swamp Brook.

NRCS soil survey maps indicate that the dominant soils present on the site consist of Pootatuck Fine Sandy Loam, Occum Fine Sandy Loam, and Raynham-Wareham complex (occasionally flooded). These are Hydrologic Group (HSG) "A" and "C/D" soils. Field examination was conducted with test pit excavations, which confirmed fine sandy loam, loamy fine-medium sand, and granular soils. No ledge and/or boulders were encountered in the excavations. The estimated seasonal high-water table varied from 32" to 72" across the test pits, and percolation rates were consistent between 12 -14 min/inch.

III) METHODOLOGY

The quantity of runoff and the conveyance of that flow through the site are determined using the software package HydroCAD r 10.10.6a by HydroCAD Software Solutions, LLC. HydroCAD is a computer aided design program for modeling storm water hydrology based on the Soil Conservation Service (SCS) TR-20 method, combined with standard hydraulics calculations used to model detention basins and culverts.

Stormwater management systems are designed in accordance with the methodology for the "Best Management Practices" (BMP's), as outlined in the New Hampshire Storm Water Manual, Volume 2.

IV) DRAINAGE DESIGN

In accordance with the City of Keene and NHDES standards, there will be no increase in the peak flow of surface runoff. In order to demonstrate this the two (2), ten (10), and fifty (50) year frequency storm events have been evaluated using a Type III, 24-hour storm distribution. These design storms have been analyzed to compare the Pre and Post-development peak flow rates for the site (see attached comparison tables).

The site was designed to meet the City of Keene Floodplain Regulations, as outlined in the new Land Development Code adopted on September 1, 2021. The proposed building and all the proposed storage units will have finish floor elevations at least 1 foot above the flood elevation of 471.2'. Based on this elevation requirement, the site grades had to be raised up, thus resulting in fill within the floodplain. The fill in the floodplain is offset by providing flood compensatory storage on site, with total volume of storage equal to or greater than the fill volume. There is no fill proposed in the Floodway or any utilities (including drainage) proposed in the Floodway.

Pre-Development Drainage Conditions:

The Pre-Development Drainage Area Plan outlines the two areas where water flows off the property. The first observation point is the Krif Road stormwater drainage system. The current site drainage all ties into the stormwater system on Krif Road. The intent of the model and design is to ensure that the volume and flow rate does not increase load on the municipal drainage system. The second observation point is to the southern border as water is directed to the Ash Swamp Brook. These two areas are modeled as Observation Points, OP 1 and OP 2, in the drainage report. A third observation point is modeled as a Summary of all off-site stormwater, shown as SUM.

Post-Development Drainage Conditions:

The proposed drainage systems were designed to capture runoff from the buildings and paved areas, and direct the flow to stormwater management systems. There are 8 Subcatchments modeled in the Post-Development condition.

The design intent was to leave the existing drainage systems in place, as much as was feasible. The proposed location of the warehouse building forced a change in the existing drainage layout. The existing roof drains off the main building have been redirected to the proposed subsurface drainage system. Half of the new warehouse roof drains will also tie into this system, while the eastern half will tie into the existing catch basin system and ultimately flow to the Krif Road drainage. There will be a slight reduction in pavement and roof impervious areas flowing to the Krif Road storm-drain (OP 1). The building roof drains and parking lot catch basins located on the northern side of the site will remain in place and unaltered (Subcat 1S).

The rear or southern side of the site directs all the water from the parking, paved storage area, and storage units to a series of catch basins. The catch basins tie together with perforated culverts surrounded by drainage stone, that are designed to infiltrate the water into the ground. There are two separate series of catch basins, modeled as Ponds 2P and 3P. The catch basins are all deep sump basins with hoods on the outlet to provide initial separation of debris, oils, and sediment particles from entering the subsurface system. The drainage system is sized to allow the water to fill the perforated pipes and stone, then slowly soak into the native subsoil, thus recharging the groundwater volumes in a similar fashion as the pre-development condition. The pipe and stone systems are large enough to store the water quality volume, as calculated for the Alteration of Terrain BMP worksheets.

Each subsurface system will have an overflow pipe, installed at a higher elevation, to pass water in larger storm events, while detaining smaller storm events. These overflow culverts outlet to the flood compensation area, which is essentially a large detention basin. The flood compensation area will be seeded and left as un-mowed grass, which will also provide treatment of the overflowed stormwater. The flood storage area will have a rip-rap stone overflow spillway constructed at the northwest end. The purpose of the spillway is to control incoming and receding floodwaters from the flood compensation area.

The default saturated conductivity rate (Ksat) for Pootatuck and Occum fine sandy loam is 8.8 – 8.9 in/hr. An exfiltration rate of 4.4 in/hr was used for the infiltration systems, which provides a factor of safety of 2.0. The test pits in this area had a field measured percolation rate of 12 min/in or 5.0 in/hr.

The drainage system has been designed to collect the stormwater runoff from most of the new impervious surfaces on-site and direct it to the proposed stormwater management systems. The existing grades for the pavement adjacent to Winchester Street and Krif Road will be maintained, so water will runoff in the same manner as in the existing condition. Stormwater will be treated via the deep sump catch basins, catch basin outlet hoods, and infiltration through native soils above the seasonal high-water table. The net result is that virtually all of the new impervious areas will receive qualitative treatment and there will be a reduction of peak rates of runoff as well as volume of runoff leaving this site for all storm events.

V) SUMMARY

The intent of the stormwater management system for this project is to address the qualitative and quantitative aspects of the stormwater runoff so that there are no downstream adverse impacts created by the project. To mitigate the resulting increases in runoff volume and peak rates due to the redevelopment of Lots 115-19 & 20, this project proposes that a stormwater management system consisting of twenty (18) catch basins, two (2) drain manholes, one-thousand, one-hundred and fifteen (1,115) lineal feet of perforated pipe and stone, and one (1) flood compensation basin to be constructed. The net result is that the new buildings and paved areas will receive qualitative treatment and there will be no increase in the peak rates of runoff leaving the site.

The stormwater management design for this project therefore complies with the stormwater standards set forth in the City of Keene’s Land Development Code.

The following tables are a summary of the attached calculations and show a comparison of the peak flow rates at the outlet point for the site. The values presented are based on pre- and post-development conditions.

Table 1.1: Peak Flow Rates (CFS)/Volume (AF) to SUMMARY (SUM) – with Post-Development Infiltration

STORM FREQUENCY	PRE-DEV. RUNOFF (CFS/AF)	POST-DEV. RUNOFF (CFS/AF)	CHANGE (CFS/AF)
2-YEAR	5.29/0.426	5.25/0.364	-0.04/-0.062
10-YEAR	8.10/0.743	8.10/0.603	0.00/-0.140
50-YEAR	13.11/1.367	12.83/0.996	-0.28 /-0.371



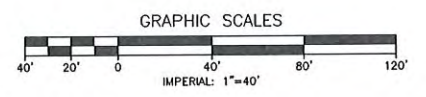
DRAINAGE SYMBOLS:

- 1P POND (PIPE OR BASIN)
- 1S SUBCATCHMENT
- 1R REACH
- OP1 OBSERVATION POINT

- WATERSHED BOUNDARY
- TIME OF CONCENTRATION
- SURFACE WATER FLOW

NRCS SOILS LEGEND:
SOURCE: USDA NRCS WEB SOIL SURVEY

- SOIL BOUNDARY
- 4 POOTATUCK FINE SANDY LOAM 0 TO 5% SLOPES (HSG B)
- 5 PIPPOWAM FINE SANDY LOAM 0 TO 5% SLOPES (HSG A/D)
- 218 PANTHAM-JAREHAM COMPLEX OCCASION-LLY FLOODED (HSG C/D)
- 401 OCCUM FINE SANDY LOAM 0 TO 5% SLOPES, (HSG A)
- W WATER - ASH SWAMP BROOK



REV.	DATE	DESCRIPTION	C/O	DR	CK
A	X/XX/XX	XXXX		XXX	XXX

PRE-DEVELOPMENT DRAINAGE PLAN
U-HAUL OF SOUTH KEENE
TAX MAP 115 LOTS 19 & 20
472 WINCHESTER STREET
KEENE, NEW HAMPSHIRE

PREPARED FOR:
AMERCO REAL ESTATE COMPANY
499 MONTGOMERY STREET, CHICOPEE, MA 01020

SCALE: 1" = 40' OCTOBER 15, 2021

Surveying + Engineering + Land Planning + Permitting + Septic Designs

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 www.FieldstoneLandConsultants.com

LEGEND:

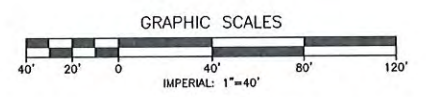
- CATCH BASIN
- CLEAN OUT
- WET LAND FLAG
- GUY ANCHOR
- HYDRANT
- IRON PIN FOUND
- MONUMENT FOUND
- NAIL FOUND
- IRON PIN SET
- SIGN
- NAIL SET
- TEST PIT
- UTILITY POLE
- WATER VALVE
- LIGHT POLE
- EDGE OF WATER



- DRAINAGE SYMBOLS:**
- 1P POND (PIPE OR BASIN)
 - 1S SUBCATCHMENT
 - 1R REACH
 - OP1 OBSERVATION POINT
 - WATERSHED BOUNDARY
 - TIME OF CONCENTRATION
 - SURFACE WATER FLOW

NRCS SOILS LEGEND:
SOURCE: USDA NRCS WEB SOIL SURVEY

SOIL BOUNDARY	
4	POOTATUCK FINE SANDY LOAM 0 TO 5% SLOPES (HSG B)
5	PIPPONHAM FINE SANDY LO-M 0 TO 5% SLOPES (HSG A/D)
218	PA-1114-1-1-PEHAM COMPLEX OCCASIONALLY FLOODED (HSG C/D)
401	OCCUM FINE SANDY LOAM 0 TO 5% SLOPES, (HSG A)
W	WATER - ASH SA-MP BROOK



- LEGEND:**
- CATCH BASIN
 - CLEAN OUT
 - WET LAND FLAG
 - GUY ANCHOR
 - HYDRANT
 - IRON PIN FOUND
 - MONUMENT FOUND
 - NAIL FOUND
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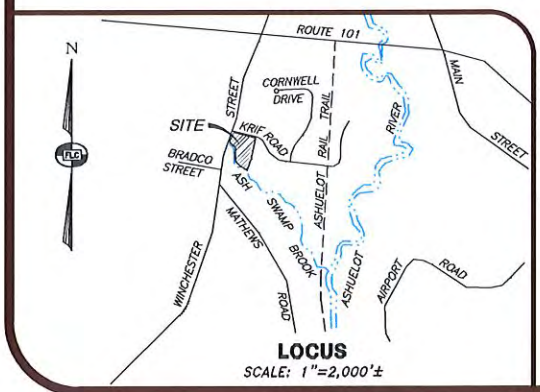
REV.	DATE	DESCRIPTION	C/O	DR	CK
A	X/XX/XX	XXXX		XXX	XXX

POST-DEVELOPMENT DRAINAGE PLAN
U-HAUL OF SOUTH KEENE
TAX MAP 115 LOTS 19 & 20
472 WINCHESTER STREET
KEENE, NEW HAMPSHIRE

PREPARED FOR:
AMERCO REAL ESTATE COMPANY
 499 MONTGOMERY STREET, CHICOPEE, MA 01020

SCALE: 1" = 40' OCTOBER 15, 2021
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STAFF REPORT

SPR-972 MODIFICATION #7 – SITE PLAN REVIEW – COLONY MILL LANDSCAPING 210-222 WEST STREET

Request:

Applicant and owner Brady Sullivan Keene Properties LLC proposes to modify the landscaping for the new retail building on the property located at 210-222 West St (TMP# 576-009-000), including the removal of existing trees along Island St. The site is 6.49 acres and is located in the Commerce District.

Background:

The 6.49-acre parcel at 210-222 West Street is situated on the southeast corner of West Street and Island Street and is located in the Commerce District. There are three existing historic buildings on the site, including the Colony Mill Apartment Building/Elm City Brewery Restaurant, the Casino, and NBT Bank.

In August 2020, the Planning Board granted approval for the construction of a 4,875 square foot multi-tenant retail and office building in the southeast portion of the site with two drive through windows and associated site improvements (SPR-972 Modification #6). At the time, staff requested that the Applicant place the building closer to Island Street in order to meet the Board's standard which requires all parking to be to the side or rear of the building. However, the Applicant chose to locate the building further back from the road to allow for two drive-throughs and parking in front of the building. Therefore, when the Planning Board originally approved this plan, the Applicant proposed to maintain the existing buffer of trees along Island Street and extend that buffer further to the south by installing seven new trees. This buffer was intended to screen the parking and drive-through lane located between the building and Island Street. Construction on the new retail building began in the Spring of 2021.



Figure 1. Google street view image of the existing trees on Island Street that are proposed to be removed (circled in yellow).

The current request is to modify the landscaping for the new retail building as follows:

- Along Island Street, remove 8 existing crabapple trees and 4 proposed crabapple trees (12 trees total) and replace with three Armstrong red maple trees and a mixture of 62 shrubs and 88 perennials.
- Within the parking lot island directly to the north of the new retail building, replace four crabapple trees that were removed during construction with three Little Leaf Linden trees, 6 shrubs, and 26 perennials.
- Within the landscape island located to the northeast of the new building, remove 1 existing crabapple tree and replace in-kind with a new crabapple tree.

STAFF REPORT

Completeness:

The Applicant has submitted existing and proposed landscaping plans. Staff recommend that the Board accept the application as “complete.”

Departmental Comments:

There were no departmental comments on this application.

Application Analysis: The following is a review of the Planning Board development standards relevant to this application.

- 20.5 **Landscaping:** The applicant proposes to modify the existing landscape plan for the new retail building as described in the “Background” section of this staff report, resulting in a decrease in the overall number of trees by 11 and an increase in the number of shrubs and perennials by 68 and 104, respectively. The proposed new plant species include Armstrong red maple, Little Leaf Linden, wintercreeper, sweet summer hydrangea, dwarf globe blue spruce, “PJM” Rhododendron, spreading yew, dwarf fountain grass, and reblooming daylily. None of these plants appear on the NH list of invasive species, and they are all hardy to regional climate conditions per the U.S. Department of Agriculture’s Plant Hardiness Zone Map.
- 20.6 **Screening:** Sub-section 2.E of this standard states “*Parking lots shall be designed and screened in accordance with Article 9 of this LDC.*” Article 9 includes parking lot screening standards for both perimeter and interior parking lot landscaping. The intent of these standards is to screen parking areas from view from the public right-of-way and any adjacent residential zoning districts. Specifically, the perimeter screening standards require one shade tree for every 30 feet of perimeter parking lot landscape area and one evergreen shrub for every 3 feet of perimeter area length, among other requirements. However, the Planning Board may approve an alternate design for screening of parking lots from public rights-of-way as part of a site plan review, if they determine the proposed design generally meets the intent of the parking lot screening standards listed in Article 9.

The perimeter landscape area for the parking associated with the new retail building is approximately 230 feet long (see Figure 2). As proposed, the trees within the perimeter landscape area are spaced between 60 ft to 30 ft apart. A mix of 63 shrubs is proposed along the perimeter landscape area, or about one shrub for every 3 linear feet of perimeter landscape area. These shrubs vary from 2 ft-8 ft in height at maturity.

STAFF REPORT

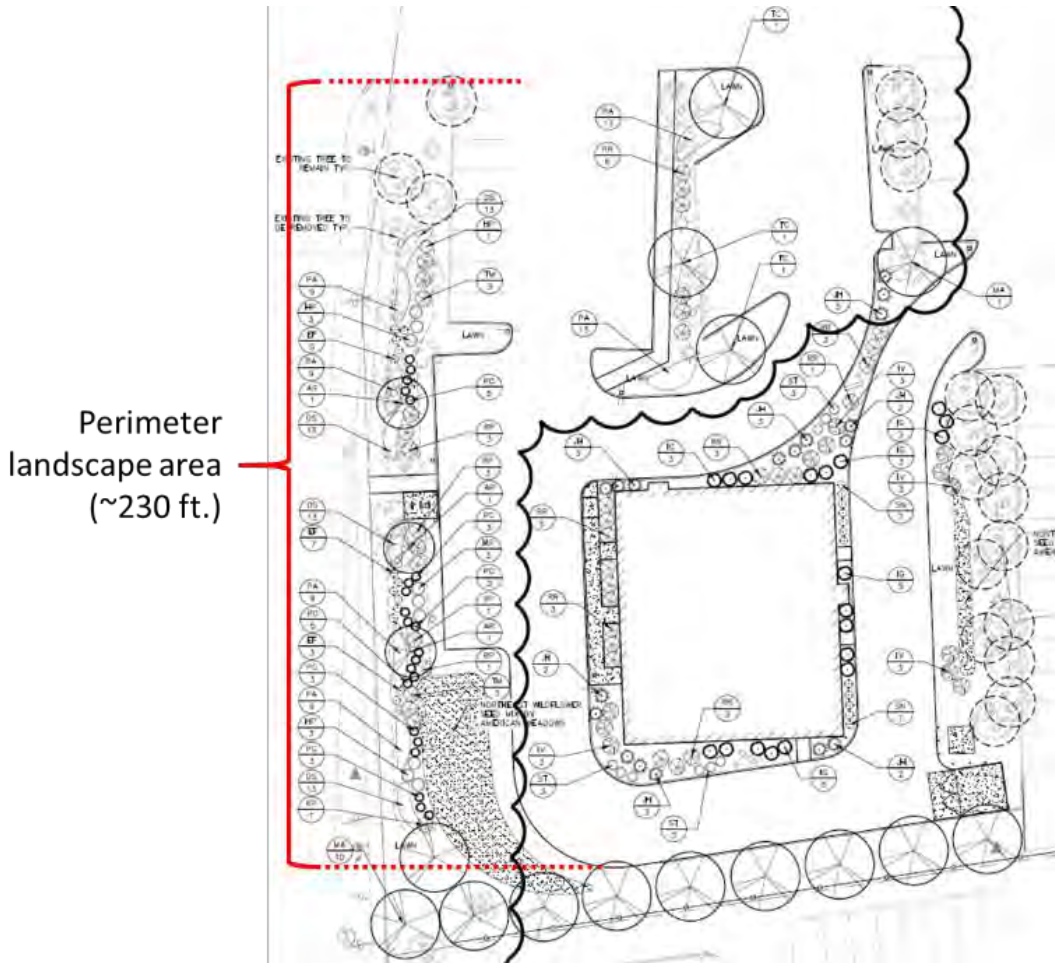


Figure 2. Image of the proposed Landscaping Plan with the perimeter landscape area indicated in red.

Recommended Motion:

If the Board is inclined to approve this request, the following language is recommended for a motion:

Approve SPR-972, Modification #7 for modifications to landscaping, as presented in the plan identified as "Landscape Plan, Multi-Tenant Use, 0 Island Street" prepared by Fuss & O'Neill at a scale of 1 inch = 20 feet, dated June 17, 2021 and last revised November 2, 2021 with the following conditions prior to signature by Planning Board Chair:

1. Landscape Architect's stamp appears on plan.
2. Owner's signature appears on plan.

CITY OF KEENE | PLANNING BOARD

SITE PLAN REVIEW / MODIFICATION APPLICATION



This form must be filled out in its entirety. If a box is not checked, staff will assume that the information is not provided and the application is, therefore, not complete. Incomplete applications will not be accepted for review.

A	Project Name 0 Island Street	Date Received/Date of Submission:
	Tax Map Parcel number(s) 5 8 3 - 0 1 1 - _____ - 5 8 3 - 0 1 2 5 8 3 - 0 1 4 - _____ - 5 8 3 - 0 1 5 _____ - _____ - _____ - _____	Date of pre-application meeting:
Project Address: 0 Island St	O w n e r	Date Application is Complete:
Acreage/S.F. of Parcel: .946 / 41,200		Community Development Dept File #: _____
Zoning District: Commerical	A p p l i c a n t	PRINTED Name: Brady Sullivan Keene Properties LLC
		Signature:
		Address: 670 N. Commercial St, Suite 303, Manchester, NH 03101
		Telephone\ Email: 603-231-1240 \ bkelley@bradysullivan.com
		PRINTED Name: Brady Sullivan Keene Properties LLC
		Signature:
		Address: 670 N. Commercial St, Suite 303, Manchester, NH 03101
		Telephone\ Email: 603-231-1240 \ bkelley@bradysullivan.com

Modifications: Is this a modification to a previously-approved site plan: No Yes: SPR#: 972 Date: 8/26/20

For those sections of the application that are not affected by the proposed modification to the previously approved site plan, you are encouraged to request exemptions in lieu of submitting required documents.

B Descriptive Narrative Including

- | | | |
|--|---|---|
| <input type="checkbox"/> Type of development | <input type="checkbox"/> Sedimentation Control | <input type="checkbox"/> Scope/scale of development |
| <input type="checkbox"/> Proposed uses | <input type="checkbox"/> Vegetation | <input type="checkbox"/> Parcel size |
| <input type="checkbox"/> Location of access points | <input type="checkbox"/> Debris management | <input type="checkbox"/> Proposed stormwater, drainage & erosion plan |
| <input type="checkbox"/> Any other descriptive information | <input type="checkbox"/> Disposal proposals for boulders, stumps & debris | |

C A complete application must include the following

--



October 13, 2021

Tara Kessler
Senior Planner
Keene Community Development Department
3 Washington Street
Keene, New Hampshire 03431

Re: Brady Sullivan Properties
0 Island Street, Keene, NH
Fuss & O'Neill Reference No. 20191323.A10

Dear Tara:

The purpose of this Site Plan application is to modify the approved landscape plan for the Brady Sullivan Properties development at 0 Island Street in Keene, New Hampshire. The following narrative describes the proposed changes to the originally Approved Landscape Plan and the Revised Landscape Plan.

Approved Landscape Plan

The originally approved landscape plan intended to retain 9 existing trees along Island Street, 4 existing trees within the landscaped island to the north of the proposed building and 4 existing trees within the island to the east of the island north of the proposed building. These existing trees are highlighted in green on the attached Figure 1 plan. Additionally, the approved landscape plan proposed 5 new trees along Island Street, highlighted in blue on the same Figure 1 plan.

This application requests that all trees highlighted in green and blue, be removed or eliminated from the approved design and replaced with new trees or plantings as shown on the revised Landscape Plan attached and described below.

Revised Landscape Plan

The proposed landscape plan replaces the 9 existing and 5 new trees along Island Street with one Prairie Fire Crabapple tree, in the southwest corner of the property, and a mix of 150 plantings along Island Street. The mix of plantings include,

- 15 Dwarf Globe Blue Spruce
- 15 Winter Creeper
- 10 Sweet Summer Hydrangea
- 52 Reblooming Daylily
- 9 PJM Rhododendron
- 6 Spreading Yew
- 36 Dwarf Fountain Grass

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New Hampshire
Rhode Island
Vermont

Tara Kessler
October 13, 2021
Page 2 of 2

The revised landscape plan also replaces the four existing trees within the landscape island to the north of the proposed building with,

- 2 Little Leaf Linden trees,
- 26 Dwarf Fountain Grass and
- 9 Coral Knock Out Rose.

For the island to the east of the island north of the proposed building, one Prairie Fire Crabapple was added to replace the existing tree that was removed.

The general intent of the landscape revisions is to provide visibility along Island Street of the new commercial building and eliminate the trip hazard from dropped apples along the new sidewalk. The revised landscape plan provides robust plantings with additional trees for shade within the landscape island.

The applicant, Brady Sullivan Properties is happy to answer any question you may have.

Very truly yours,



Amy R. Sanders
Senior Project Engineer

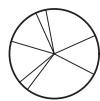
ARS:ars

Enclosures

cc w/ enclosures via email: Brady Sullivan Properties - Benjamin Kelley and Marissa Urquhart



LEGEND



FLOWERING TREE



SHRUBS



EVERGREEN SHRUBS

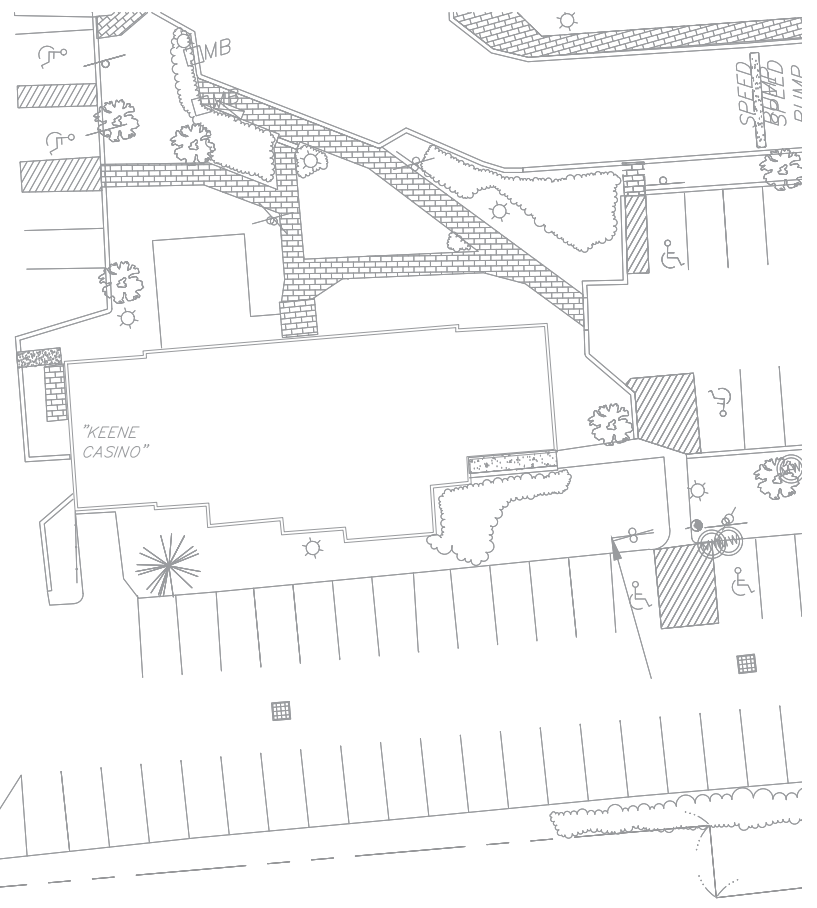
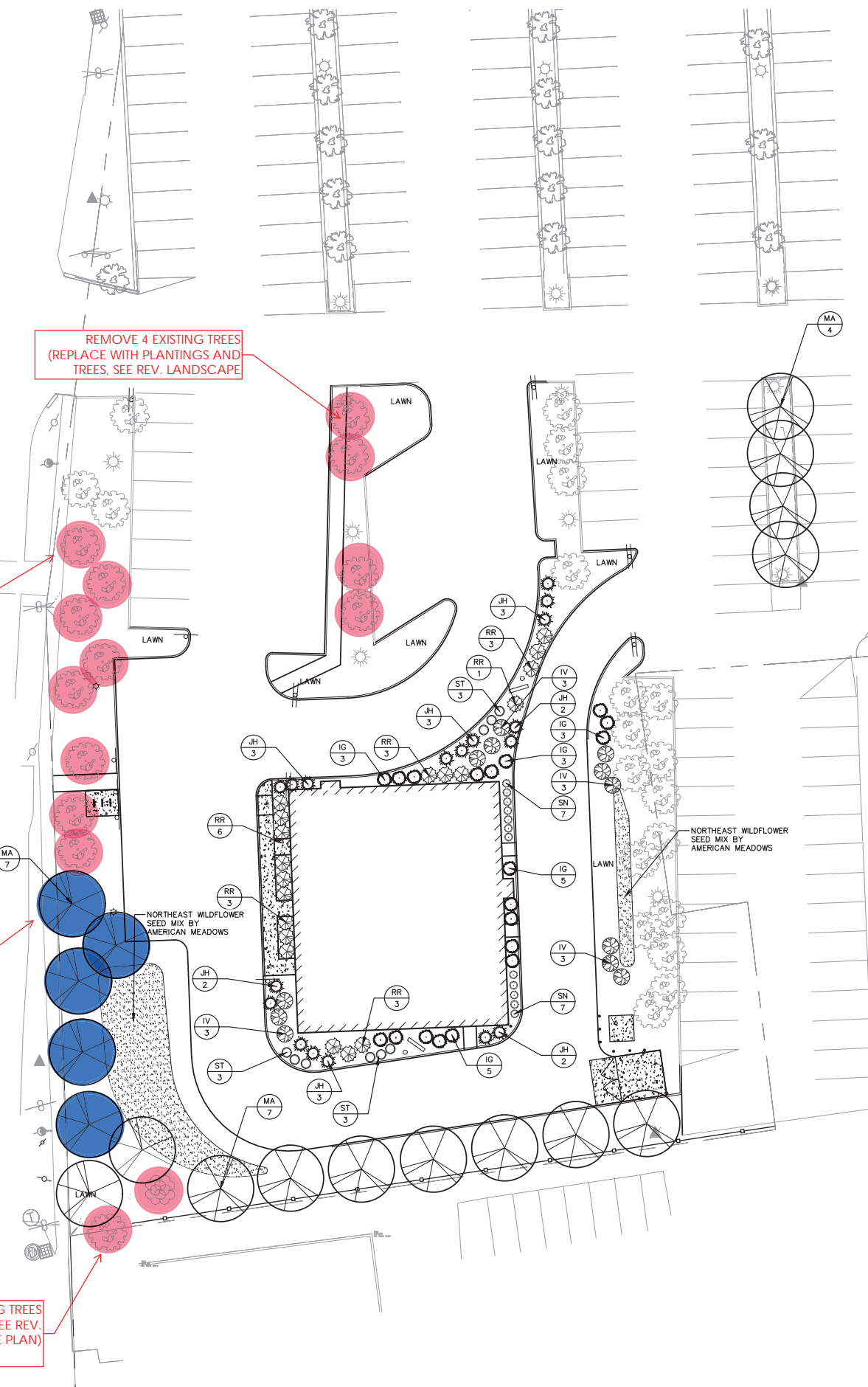
ORIGINALLY APPROVED LANDSCAPE PLAN

REMOVE 8 EXISTING TREES (REPLACE WITH PLANTINGS SEE REV. LANDSCAPE PLAN)

REMOVE 5 PROPOSED TREES (REPLACE WITH PLANTINGS SEE REV. LANDSCAPE PLAN)

REMOVE 2 EXISTING TREES (REPLACE WITH TREES, SEE REV. LANDSCAPE PLAN)

REMOVE 4 EXISTING TREES (REPLACE WITH PLANTINGS AND TREES, SEE REV. LANDSCAPE PLAN)



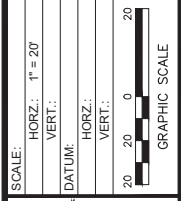
PLANTING NOTES:

1. ALL PLANTING MATERIAL TO BE NURSERY GROWN STOCK SUBJECT TO A.A.N. STANDARDS
2. THE CONTRACTOR SHALL SUPPLY ALL PLANTS IN QUANTITIES SUFFICIENT TO COMPLETE THE WORK SHOWN ON THE DRAWINGS AND LISTED IN THE PLANT LIST. IN THE EVENT OF A DISCREPANCY BETWEEN QUANTITIES SHOWN IN THE PLANT LIST AND THOSE REQUIRED BY THE DRAWINGS, THE LARGER NUMBER SHALL APPLY.
3. ALL PLANTS SHALL BE APPROVED PRIOR TO INSTALLATION AND SHALL BE LOCATED ON SITE BY THE CONTRACTOR FOR THE APPROVAL OF THE LANDSCAPE ARCHITECT. ANY INSTALLATIONS WHICH WERE NOT APPROVED BY THE LANDSCAPE ARCHITECT AND WHICH ARE SUBSEQUENTLY REQUESTED TO BE MOVED WILL BE DONE AT THE CONTRACTOR'S EXPENSE.
4. PRECISE LOCATION OF ITEMS NOT DIMENSIONED ON THE PLAN ARE TO BE FIELD STAKED BY THE CONTRACTOR AND SHALL BE SUBJECT TO THE REQUIREMENTS SPECIFIED IN THE PREVIOUS NOTE.
5. ALL SHRUB MASSINGS AND TREE PITS SHALL BE MULCHED TO A DEPTH OF 3" WITH SHREDDED CEDAR BARK MULCH.
6. TREES SHALL NOT BE STAKED UNLESS OTHERWISE NOTED.
7. THE CONTRACTOR IS RESPONSIBLE FOR ANY DAMAGED VEGETATION AND SHALL REPLACE OR REPAIR ANY DAMAGED MATERIAL, AT HIS OWN EXPENSE. THE CONTRACTOR SHALL CONTACT "DIG SAFE" AT 811 PRIOR TO CONSTRUCTION.
8. ALL SHRUB AND GROUND COVER PLANTING AREAS SHALL HAVE CONTINUOUS BEDS OF TOPSOIL 12" DEEP. ALL SOO AND HYDROSEED AREAS SHALL HAVE A MINIMUM TOPSOIL BED OF 6".
9. THE CONTRACTOR IS RESPONSIBLE FOR LOCATING ALL UTILITIES IN THE FIELD, WHERE PLANT MATERIAL MAY INTERFERE WITH UTILITIES, THE CONTRACTOR SHALL NOTIFY THE LANDSCAPE ARCHITECT TO COORDINATE THEIR INSTALLATION.
10. FOR PLANTING SOIL MIX, SEE SPECIFICATIONS OR PLANTING DETAILS.
11. ALL EXISTING RILL, GULLY OR CHANNEL EROSION SHALL BE FILLED WITH APPROPRIATE BACKFILL MATERIAL, FINE RAKED, SCARIFIED AND STABILIZED WITH APPROPRIATE VEGETATIVE MATERIAL AND / OR APPROPRIATE SEDIMENTATION AND EROSION CONTROL MEASURES.
12. ADJUSTMENTS IN THE LOCATION OF THE PROPOSED PLANT MATERIAL AS A RESULT OF EXISTING VEGETATION TO REMAIN SHALL BE APPROVED BY THE LANDSCAPE ARCHITECT PRIOR TO INSTALLATION.
13. THE CONTRACTOR IS RESPONSIBLE FOR ALL MAINTENANCE REPAIR AND REPLACEMENT OF PLANT MATERIAL, AS REQUIRED, FOR THE DURATION OF THE PROJECT AND SUBSEQUENT WARRANTY PERIOD.
14. PLANTINGS INSTALLED IN THE DRY SUMMER MONTHS AND / OR LAWN SEEDED OUT OF SPRING OR FALL PERIODS, IF ALLOWED BY THE OWNER, WILL REQUIRE AGGRESSIVE IRRIGATION PROGRAMS AT THE CONTRACTOR'S EXPENSE, UNLESS OTHERWISE DIRECTED BY THE OWNER.
15. UPON COMPLETION OF PLANTING, REMOVE FROM SITE ALL EXCESS SOIL, MULCH, AND MATERIALS AND DEBRIS RESULTING FROM WORK OPERATIONS. CLEAN UP SHOULD BE COMPLETED AT THE END OF EACH WORKING DAY. RESTORE TO ORIGINAL CONDITIONS ALL DAMAGED PAVEMENTS, PLANTING AREAS, STRUCTURES AND LAWN AREAS RESULTING FROM LANDSCAPING OPERATIONS.
16. CONTRACTOR SHALL SURVEY, LOCATE, AND PROTECT ALL TREES WITHIN AREAS SHOWN AS "EXISTING VEGETATION TO REMAIN" WITHIN THE DEVELOPMENT ENVELOPE FOR REVIEW BY L.A. PRIOR TO CLEARING OPERATIONS.
17. CONTRACTOR TO RESEED ALL DISTURBED AREAS.
18. ONE ADDITIONAL MALUS 'PRAIRIE FIRE' TREE SHALL BE ADDED NEAR THE INTERSECTION OF GILBO AVENUE TO REPLACE AN EXISTING DEAD TREE. REFER TO DEMOLITION PLAN FOR LOCATION.

PLANT LIST

KEY	BOTANICAL NAME	COMMON NAME	QTY.	SIZE
TREES				
MA	MALUS 'PRAIRIE FIRE'	PRAIRIE FIRE CRABAPPLE	19	2.5"-3" CAL.
SHRUBS				
JH	JUNIPERUS HORIZONTALIS 'BAR HARBOR'	BAR HARBOR CREEPING JUNIPER	18	3 GAL.
IV	ILEX VERTICILLATA 'BERRY HEAVY'	BERRY HEAVY WINTERBERRY HOLLY	12	3 GAL.
IG	ILEX GLABRA 'SHAMROCK'	SHAMROCK INKBERRY	19	3 GAL.
RR	ROSA 'RADRAL' 'CORAL KNOCK OUT'	CORAL KNOCK OUT ROSE	19	3 GAL.
ST	SPHRAEA X 'TRACY' 'DOUBLE PLAY BIG BANG'	DOUBLE PLAY BIG BANG SPIRAEA	9	3 GAL.
PERENNIALS/GRASSES/GROUNDCOVERS				
SN	SORGHASTRUM NUTANS	INDIAN GRASS	12	1 GAL.

No.	DATE	REV. PER STAFF REPORT	DESCRIPTION	DESIGNER REVIEWER
1.	8/20/2020			ARS



FUSS & O'NEILL
 50 COMMERCIAL STREET
 MANCHESTER, NEW HAMPSHIRE 03101
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BRADY SULLIVAN PROPERTIES
 LANDSCAPE PLAN
 MULTI-TENANT USE
 0 ISLAND STREET
 NEW HAMPSHIRE
 KEENE

PROJ. No.: 20191323.A10
 DATE: 6/17/2020Y

LP-101

