



City of Keene Planning Board

AGENDA

Monday, March 28, 2022

6:30 PM

City Hall, 2nd Floor Council Chambers

- I. **Call to Order** – Roll Call
- II. **Minutes of Previous Meeting** – February 28, 2022
- III. **Boundary Line Adjustment**

S-01-22 – Boundary Line Adjustment – 0 Gilbo Ave & 0 Commercial St – Applicant Fieldstone Land Consultants PLLC, on behalf of owner City of Keene, proposes a boundary line adjustment between the properties located at 0 Commercial St (TMP# 575-010-000-000-000) and 0 Gilbo Ave (TMP# 575-015-000-000-000). This adjustment would result in a transfer of 0.32-ac from the 0.56-ac parcel at 0 Gilbo Ave to the 1.6-ac parcel at 0 Commercial St. A waiver is requested from Section 25.10.8.B.2 of the Planning Board Regulations regarding the requirement to submit an updated survey showing all metes and bounds of the revised parcels. Both properties are located in the Downtown Growth and Downtown Core Districts.

S-02-22 – Boundary Line Adjustment – 50 & 62 Rule St – Applicant Cardinal Surveying & Land Planning, on behalf of owners Elfriede Wagner & Pilot Realty LLC, proposes a boundary line adjustment between the properties located at 50 Rule St (TMP# 518-047-000-000-000) and 62 Rule St (TMP# 518-051-000-000-000). This adjustment would result in a transfer of 0.03-ac from the 6-ac parcel at 50 Rule St to the 0.21-ac parcel at 62 Rule St. A waiver is requested from Section 25.10.8.B.2 of the Planning Board Regulations regarding the requirement to submit an updated survey showing all metes and bounds of the revised parcels. Both properties are located in the Low Density District.

IV. **Public Hearings**

SPR-927, Modification #11B – Site Plan – 2 Ash Brook Rd #12 – Applicant KPR Centers LLC, on behalf of owner Keene MZL LLC, proposes to renovate the north tenant space in the building located at 2 Ash Brook Rd #12 (TMP# 109-027-000-010-012) for use as a Panera Bread Restaurant with a drive-through window. This site is 1.64-ac in size and is part of the larger Monadnock Marketplace plaza located in the Commerce District.

Change of Governmental Land Use – RSA 674:54 regarding a proposed salt shed on the former City landfill property located 0 Main St (TMP# 113-007-000-000-000). The 23.1-acre parcel is owned by the City of Keene and is located in the Industrial District.

V. **Minor Project Review Committee Membership**

VI. **Staff Updates**

VII. **New Business**

- Update to the Planning Board Rules of Procedure

VIII. **Upcoming Dates of Interest – April 2022**

- Joint Committee of the Planning Board and PLD – April 11, 6:30 PM
- Planning Board Steering Committee – April 12, 11:00 AM
- Planning Board Site Visit – April 20, 8:00 AM – To Be Confirmed
- Planning Board Meeting – April 25, 6:30 PM

1 **City of Keene**
2 **New Hampshire**

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5 **PLANNING BOARD**
6 **MEETING MINUTES**
7

8 **Monday, February 28, 2022**

6:30 PM

**Council Chambers,
City Hall**

Members Present:

Pamela Russell Slack, Chair
David Orgaz, Vice Chair
Mayor George S. Hansel
Harold Farrington
Armando Rangel, Alternate

Staff Present:

John Rogers, Acting Community
Development Director
Mari Brunner, Senior Planner

Members Not Present:

Councilor Michael Remy
Emily Lavigne-Bernier
Roberta Mastrogiovanni
Gail Somers
Tammy Adams, Alternate

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10 **I) Call to Order – Roll Call**

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12 Chair Russell Slack called the meeting to order at 6:30 PM and a roll call was taken.
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14 **II) Minutes of Previous Meeting – January 24, 2022**

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16 A motion was made by Mayor George Hansel that the Planning Board approve the January 24,
17 2022 meeting minutes. The motion was seconded by Harold Farrington and was unanimously
18 approved.
19

20 **III) Capital Improvement Program FY 2023-2029 – Presentation by Merri Howe, City**
21 **of Keene Finance Director and Kurt Blomquist, Public Works Director/Assistant**
22 **City Manager**
23

24 Finance Director Merri Howe and Public Works Director/Assistant City Manager Kurt
25 Blomquist addressed the Board. Ms. Howe indicated the CIP document can be found on the
26 City's website. Ms. Howe stated the Capital Improvement Program (CIP) is a comprehensive
27 program that covers a seven-year period. It covers projects and equipment that are estimated at
28 \$35,000 or more with a useful life of over five years. The CIP is presented bi-annually and
29 reviewed annually during operating budget process.
30

31 Ms. Howe stated the CIP is a tool for planning the City’s future that is guided by fiscal policy for
32 which goals are set by the City Council and are outlined in the City’s Comprehensive Master
33 Plan. The City’s master plan is the planning tool that guides the City’s vision and the CIP
34 projects presented in this book. Each project presented in this book, if applicable, is tied to a
35 master plan focus area. For example, the Transportation Heritage Trail Program focuses on a
36 healthy community, one of the vision focus areas. The Parking Structure Maintenance Program
37 is tied to a vision of a vibrant downtown. There is a lot of thought and planning that goes into
38 these projects. With that she turned the presentation over to Senior Planner Mari Brunner.

39 Ms. Brunner stated the Master Plan update is scheduled for fiscal years 24 and 25. This would be
40 a two-phase project. The first phase would be updating the community vision and updating data
41 and trends in the plan. There will also be a housing analysis done as part of the first phase. The
42 second phase would take the information received and input it into the document.

43 Ms. Brunner added the Planning Board does have a significant role in the Master Plan update
44 and historically the City has looked to the Board to contribute heavily in the process.

45 The Chair asked for clarification on what Ms. Brunner had stated about housing analysis. Ms.
46 Brunner explained housing has been identified as significant item not only for Keene, the County
47 and the State but for the entire country. Hence, the plan is to do an in-depth housing analysis;
48 what is available and what is needed. Chair Russell Slack asked whether there is conversation
49 being undertaken for instance if a large housing project is being planned, a certain percentage has
50 to be dedicated to workforce housing. Ms. Brunner felt there was mention of that in the last
51 master plan but there has not been anything put in place. The Chair felt the price of housing these
52 days is something that would need to be looked at very closely.

53 Public Works Director/Assistant City Manager Kürt Blomquist addressed the Board next. He
54 indicated this coming Thursday the City Council will be conducting a public hearing on the CIP.
55 He noted the theme of the CIP this year is “Strategic Governance: the Path to Our Future.” This
56 is important because as the departments started reviewing projects it was determined that there
57 are two levels of projects: one-time projects and maintenance projects.

58 One time projects means that something new is being built. The next level of projects are capital
59 maintenance projects to provide the various services for the community. He added one of the
60 items that has been emphasized over the past few years is the development of asset management
61 plans. He explained an asset management plan is how the City would take care of what it already
62 owns. The first component of developing an asset management plan is to conduct an inventory to
63 see what the City owns, the second step is to determine what condition these assets are in, and
64 the third step is to answer the question – what does the City want for services out of those assets?

65 Mr. Blomquist stated one of the new programs this year is sidewalks. In the past, sidewalks were
66 grouped with larger projects and completed at that time. He noted the City’s asphalt sidewalks
67 are the ones in the worst shape and those would be focused on first.

68 Mr. Blomquist then referred to the large “one and done” projects and gave a few examples:

69 The downtown infrastructure project which starts in fiscal year (FY) 23 which would be a
70 complete rework of all infrastructure in the downtown area. The present infrastructure ranges
71 from the 1890’s to 1930’s. This is a 7.4 million project spread out over three years.

72 The next is the Transportation Heritage Trail which is a four million dollar project to reconstruct
73 three bridges over the multi-use trail.

74 The third project is the lower Winchester Street project, which had the last work done in the 70's
75 – this work would include adding pedestrian and bicycle facilities and bringing the corridor up to
76 date.

77 The West Street corridor is scheduled just outside the CIP starting at the bike path at Route
78 9/10/12 and moving east into the City.

79 In the Parking Fund, the City is looking at electric vehicle infrastructure for City operations and
80 the general public along with the construction of parking structure scheduled for FY27.

81 Marlboro Street reconstruction to include complete streets improvements for Marlboro Street to
82 encourage redevelopment in the area.

83 Chair Russell Slack asked for clarification on the sidewalk project. Mr. Blomquist referred to
84 page 89-90: Approximately 17 miles of sidewalks have been deemed to be in substandard
85 condition. Sidewalks are part of the general fund and one of the Council goals is to minimize the
86 impact on the Keene tax rate. The City Engineer has been able to identify about \$270,000 in
87 sidewalk work over the next five years. Most of the work would be on the asphalt sidewalks
88 located in the southeastern section of the City.

89 In FY23 work will start at lower Main Street near the Route 101 corridor through the cemetery,
90 also some sidewalks on School Street and North Lincoln Street.

91 FY24 will be Belmont Avenue, Jennison Street and River Street. Mr. Blomquist stated this is a
92 focus on sidewalks not part of a larger project.

93 Chair Russell Slack noted there is a lot of federal monies available for different projects and
94 asked for Mr. Blomquist's opinion on it. Mr. Blomquist agreed and added the monies at the
95 present time are going to be funneled through existing programs and staff is definitely looking
96 into those programs. However, most programs have not published their guidelines yet but as
97 soon as the Appropriation Bill passes through Congress, staff will start focusing on those funds.

98 Mr. Farrington commended the Plan and how it was presented. Mr. Farrington asked about
99 triggers – changes in population and how that would affect the plan and items included in the
100 plan. Mr. Blomquist stated in his department a review is done annually of their equipment. For
101 instance in the transfer station if an equipment comes up for replacement but it is still in working
102 condition and can be pushed out for another year, that is what will be done. However, there could
103 be items like the Drummer Hill water tank that failed earlier than it was planned for replacement.

104 A motion was made by Mayor George Hansel that the Capital Improvement Program for 2023 –
105 2029 is consistent with the Comprehensive Master Plan. The motion was seconded by David
106 Orgaz and was unanimously approved.

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108 **IV) NHMA Presentation – Steve Buckley, Legal Counsel for the New Hampshire**
109 **Municipal Association (NHMA), will provide a virtual presentation titled “Planning**
110 **Board Role and Responsibilities.” This presentation is offered as part of the NHMA**
111 **on-demand training series**

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Ms. Brunner introduced Steve Buckley, Legal Counsel for NH Municipal Association (NHMA). Mr. Buckley addressed the Board and noted that he joined NHMA in 2014 and stated he is a member of the Planning Board and Zoning Board of Adjustment in his home town.

Mr. Buckley began with the Statutory Duties of the Planning Board – he addressed the role of the Board in its current role with updating the Master Plan. He indicated with the City’s current process he would recommend developing future land use opportunities. He felt master planning is an important role of the Board followed by what was discussed tonight, the Capital Improvement Program (CIP) – he said the CIP is part of the Master Plan process.

Zoning Amendment Process: In Keene, zoning amendments are proposed by the Planning Board which are ultimately considered by the City Council after a public hearing. The fundamental duties of the Planning Board related to the zoning ordinance is approval of subdivisions and site plans. The Planning Board also has the authority to regulate excavations and driveways.

Subdivision and Site Plan Approval: Mr. Buckley stated the Board can always require that all projects have preliminary review which benefits the City and the developer to help shape the final plan. Under the subdivision process the Board will approve plats, streets and will also approve layout of any utilities.

The Board not only has the ability to approve new development, but also has the ability to regulate changes to an existing use. Mr. Buckley stated it has always been his opinion if a development has a certain number of elements that need to be modified, as long as those changes don’t exceed a certain number of building square feet or impervious surface (significant change) those would be considered permissible but not required to come before the Board.

Mr. Buckley then addressed site plan review as it pertains to co-location of personal wireless facilities. RSA 12-K exempts from site plan review any time a project is planning to co-locate another antennae or telecommunication device on an existing tower.

Mr. Buckley then referred to the Master Plan process and indicated the goals and objectives will be an important aspect for the Master Plan update. With respect to adopting a zoning ordinance (which the City already has) vision and land use are two important pre-requisites. There are also many other subject areas that could be deemed important for a master plan update. For Keene, it could be the trail system, also planning for climate change.

Workforce Housing Statute: Mr. Buckley stated this could also be considered a form of statewide zoning and should be taken into consideration for the master plan process and that the zoning ordinance complies with workforce housing statute. As written, the current workforce housing statute requires workforce housing to be allowed in any land area zoned residential and the challenge for any community is whether it has its share of workforce housing, which is typically done through an analysis. Mr. Buckley noted the Regional Planning Commission is working on a statewide housing needs assessment which is required to be completed every five years.

Mr. Buckley reviewed what is considered workforce housing:

159 • Housing for Sale –affordable to a household with an income of no more than 100% of the
160 median income for a 4 person housing for the county in which the housing is located.

161 • Rental Housing - affordable to a household with an income of no more than 160% of the
162 median income for a 3 person housing for the county in which the housing is located.

163 Mr. Buckley asked that the community keep this in mind when updating the master plan.
164

165 Mr. Farrington asked who comes up with the actual numbers for affordable housing for sale
166 versus rental. Mr. Buckley stated the definition comes from the Workforce Housing Statute but it
167 is also available on the New Hampshire Housing. This table is modified every year based on
168 housing prices.
169

170 Mr. Buckley went on to say that the CIP helps a community identify what its priorities are as it
171 relates to capital improvement, what comes first, how are resources allocated and whether the
172 expenditure process fits in with a fair and reasonable impact on local tax rate. He also noted the
173 CIP is not a regulatory document. A subdivision cannot be denied because a CIP does not speak
174 to the needs of a particular project has. However, it is a prerequisite if the Keene Zoning
175 Ordinance has an impact fee requirement as it is necessary for growth management.
176

177 Mr. Buckley referred to Keene Charter Section 2-1024 which makes it clear that the Planning
178 Board recommends to the Council amendment to the zoning ordinance that are eventually
179 proposed and recommended to be consistent with the Board recommendations. The City Council
180 then determines if those amendments will be adopted. However, under RSA 675:2 there is a
181 requirement to hold at least one public hearing on those amendments.
182

183 Innovative Land Use Controls: Mr. Buckley stated this is another important tool which can
184 involve the Planning Board performing some of the business that is usually reserved for the
185 Zoning Board of Adjustment. If the Zoning Board is not designated regulator, the Planning
186 Board has to be consulted before any amendment is adopted. Mr. Buckley stated his experience
187 has been Innovative Land Use Controls is the most effective way to centralize a land use
188 regulatory process in one Board.
189

190 Planning Board and Public Streets – Mr. Buckley stated as previously noted the Planning Board
191 has an important role as it pertains to public streets. The Board has the power to say when it
192 approves a subdivision or site plan there has to be a minimum amount of completion before a
193 Certificate of Occupancy could be issued; this is an important item to include in the Board
194 approval as this provides for an added incentive so that a road or home will be built for
195 appropriate occupancy.
196

197 Driveways: The Planning Board is authorized under RSA 236:13 to approve driveway permits
198 on City roads, but most Boards don't carry out the day-to-day process of issuing driveway
199 permits and is usually done by the Public Works Department. The Board or the City has
200 continuing authority over any driveway even if no permit is issued and a property owner can be
201 ordered to fix a driveway that is hazard to the traveling public or threat to a road.
202

203 Merged Lots: RSA 674:39-a is a statute that has been in existence for about 15 years. Anyone
204 who has two or more contiguous pre-existing approved or subdivided lots can merge them
205 together without the need for a public hearing. It does not have to come before the Board for

206 approval and can be approved by its designee. No new survey plan needs to be recorded. The
207 notice has to be endorsed by the Board or its designee and recorded at the Registry of Deeds. Mr.
208 Buckley noted there was a requirement implemented about three years ago, if any of the lots
209 carries a mortgage, the underlying mortgagee has to consent to the merger and this consent has to
210 be recorded with the Registry of Deeds. You cannot merge lots on either side of a public road as
211 they are not considered to be contiguous.

212
213 Off Site Excavations: Mr. Buckley stated he was not sure if the City had an impact fee system
214 through its zoning ordinance, even if it didn't have that system as a supplement to the impact fee
215 system - RSA 674:21, V(j), makes it clear when the Planning Board approves a project there is a
216 statutory authority to exact impact fees. However, this is limited to improvement for highways,
217 drainage and sewer and water upgrades pertinent to a development.

218
219 Preliminary Review Conceptual Consultation – Design Review: Mr. Buckley advised if the
220 Board hasn't already, it would be a good idea to request the City Council to adopt under
221 RSA 674:35, I - to require preliminary review of all projects coming to the Board. He indicated
222 preliminary consultation is non-binding, no abutter notification is required – it is a general
223 discussion of the type of development and the relationship of that development to the master plan
224 and the issues that will arise under local regulations. Mr. Buckley felt this was a good
225 communication to have with a developer prior to when drawings are formulated.

226
227 There is then the halfway step prior to the formal application process, which is Design Review.
228 This is still non-binding but additional items such as abutter notification and other studies would
229 need to be provided.

230
231 Mr. Buckley then went over Formal Application for the Planning Board:

- 232 • Submit a completed application – regulations specify what is required – costs, internal
233 review and external review.
- 234 • Application is voted on at a public hearing where abutters are notified and are present.

235
236 Timeline for Decision Making: Defined under RSA 674:4, I.

- 237 • Preliminary consultation has no time limit
- 238 • Application needs to be submitted 21 days before acceptance to meeting.
- 239 • Notice to abutters and public needs to happen 10 days before a meeting for plan acceptance.
- 240 • There needs to be at least one public hearing conducted and a decision needs to be made
241 within 65 days of plan acceptance.
- 242 • A written decision from the Planning Board needs to be issued within five business days.

243
244 Mr. Buckley then went over some Alternative Site Plan Approval and Review Procedures:
245 The City or Council can authorize the Board to delegate its site review powers for minor site
246 plans to a committee of technically qualified administrators.

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248 The Board can also establish a Technical Review Group to provide advice to applicants.

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250 The Board or Council can establish thresholds based on the size of a project where a site plan
251 review would not be required.

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Acceptance of Vesting: This is an important aspect of decision making. At times there are projects that linger before a Planning Board for a long time and Acceptance of Vesting can be important for those instances. RSA 676:12, VI is clear when an application is deemed complete, the rules in existence at that time is what the application has to comply with; it vests the Board from changes to the regulatory structure.

Mr. Buckley stated there is a new statute that gives the Board time for plan approval for Developments of Regional Impact (DRI). This was designed for a project the Board might consider could have a regional impact – Statute Amended – RSA 676:4,I (c) (1). This will give the affected community along with the Regional Planning Commission a chance to be heard.

There is also another new Statute which addresses rules for 3rd party inspectors for better inspection process prepared by the Board. It also allows a municipality to have a certification process instead of having supervision of construction by a third party. SB 86 also has a new requirement prohibiting a municipality from adopting any regulation or practice that prohibits a person from installing a safe and commercially available heating or other energy system of that person’s choice. Mr. Buckley stated he was not sure what this requirement is aiming at but something Keene should keep in mind when updating its master plan or zoning ordinance.

Public Hearing Process: It is very clear in RSA 676:4,1 - that there are a certain population of people who are required to be heard from; abutters, public, anyone who has a direct interest, and others permitted by the Planning Board. Mr. Buckley stressed the importance of following the proper public input process and impartially following that process. He added the importance of site visits which are also considered to be public meetings. He added there could be an instance where an applicant might not want the public to attend a site visit, this is not an experience Mr. Buckley has had to deal with. Mr. Buckley referred to Riggins Rules which gives advice on how to be a good Planning Board member.

Deliberation and Weighing the Evidence: Obtaining all the necessary information prior to closing the public hearing; Make sure the Board members receive documentation it needs to review well in advance of a meeting; Board can deliberate and vote at a later meeting but avoid ex parte contacts with parties or deliberation among members outside meeting; Board is permitted to reply on personal knowledge of the area and are not bound to accept conclusions of experts, but when rejecting expert conclusions, the Board member needs to have sound reason for doing so; Board cannot ignore contradicted expert testimony unless Board can adequately explain in written decision – *Condos East Corp v. Conway 132 NH 431 (1989)*.

Mr. Buckley referred to the *Dartmouth V. Hanover, Supreme Court Case (2018)* – where a Board always has to keep in mind what it can and cannot do. This case is in reference to whether or not a very large athletic complex being constructed by Dartmouth College in the City of Hanover was going to impact adjacent property owners. It was indicated by the Board that this large structure was going to cast a shadow depriving the neighbors of sunshine, especially during the winter. It was proven that this very large structure met all the regulations and Dartmouth College produced a professional Light and Casting Study demonstrating that there would not be an impact to sunshine on abutting properties. The Board nonetheless denied approval but the

298 Supreme Court concluded the Board made a decision based on their feelings and personal
299 judgment.

300
301 Written Notice of Decision: Important points to remember include: Decisions need to be in
302 writing in the event of disapproval; Project cannot be disapproved without a written reason;
303 Decisions and meeting minutes need to be on file within five business days; and, A tie vote is not
304 a decision.

305
306 Conditional Approval: Mr. Buckley said that any representations made by an applicant are not
307 binding unless clearly made a condition of approval. In addition, conditions need to relate to the
308 Board regulations. He noted that many communities have standard conditions that are applicable
309 to all projects. Conditions Precedent – conditions that need to be satisfied before the Board signs
310 the plan (eg. obtaining a driveway permit or a wetlands permit); Conditions subsequent –
311 conditions used to control the use of the property (e.g. lights being turned off at a commercial
312 site after a certain time. A compliance hearing could also be requested – this is a public hearing
313 to determine whether certain conditions have been met.

314
315 Grandfather Rights: Two elements of a statute that are important to be considered – RSA
316 674:39,III – Applicants are protected from changes in regulatory structure after final approval as
317 long as the applicant has begun active and substantial development within 24 months – there is a
318 five year exemption under such condition. Once an applicant has completed active and
319 substantial improvement they are completely vested from any future zoning and subdivision
320 changes forever. It is important to be clear to the applicant what amount of work needs to be
321 completed within 24 months.

322
323 Housing Appeals Board: Went into effect in July 2020. Consists of three members appointed by
324 the Supreme Court and a venue to appeal a decision by the Planning Board. It is an
325 administrative body, not the Superior Court. This is for decisions regarding housing and housing
326 Development. The hearing is similar to that of the Superior Court. The Board will need to hold a
327 hearing on the merits within 90 days of receipt of appeal. Decision are made within 60 days.
328 Housing Appeals Board decisions ultimately go before the Supreme Court.

329
330 Conflict of Interest: RSA 674:14 defines how a land use member or a Planning Board member
331 could have a conflict of interest. A Planning Board member cannot sit on a case if that member
332 has a direct pecuniary (financial or personal) interest in the outcome. A Planning Board member
333 could also be recused if that member would be deemed unable to hear the same kind of case at
334 the Superior Court – are you related to anyone on the case, have you advised anyone, have you
335 formed an opinion, etc.

336
337 Mr. Buckley referred to a few examples. *W. Robert Foley, Trustee v. Enfield (2017)* – A ZBA
338 Chair, while the case was pending, made an inquiry through an email listserv referred to as “Plan
339 Link,” which is managed by the NH Office of Planning and Development (a site used often to
340 trade ideas). The chair received some feedback and a day later the ZBA denied the applicant’s
341 motion. The applicant learned of this dialog and felt he could not have a fair hearing because of
342 such sharing of information. The Supreme Court did not agree with the applicant that sufficient
343 information was shared which would impair the fairness of the process.

344

345 However, *Z-1 Express v. Manchester (2019)* – Conditional Use Permit Application before the
346 Planning Board for the construction of a gas station in a controversial area in the City of
347 Manchester. After the public hearing, but before voting on the application, two members of the
348 Board voiced opposition to the project on the opposition’s social media site. At the public
349 hearing prior to the vote, one of the individuals who spoke on social media was asked to recuse
350 himself and he refused and voted against this application. The Superior Court remanded the vote
351 after finding that the member’s failure to enter into and participate in deliberations with an open
352 mind threatened the integrity of the deliberative process.

353
354 Mr. Buckley advised Planning Board members to avoid social media sites put in place by those
355 supporting or opposing an application. He added if there is a conflict of interest, a member has
356 the right to ask for an advisory vote if he/she has a concern. He also felt the Board may want to
357 have social media rules of procedure. When dealing with such issues they should err on the side
358 of caution and recuse themselves. This concluded Mr. Buckley’s presentation.

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360 The Chair asked if available this presentation be emailed to the Board and to also include
361 Riggins Rules.

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363 **V. Staff Updates**

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365 Ms. Brunner stated the department has a new planner, Evan Clements, who will be starting next
366 week.

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368 **VI. New Business**

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370 Chair Russell Slack stated she would like to see the Work Force Housing Statute included as an
371 item for discussion – what NH Housing Finance Authority data is; affordability of housing in
372 this area. She stated this item is of great concern to her.

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374 **VII.Upcoming Dates of Interest** – March 2022

- 375 • Joint Committee of the Planning Board and PLD – March 14, 6:30 PM
- 376 • Planning Board Steering Committee – March 15, 11:00 AM
- 377 • Planning Board Site Visit – March 23, 8:00 AM – To Be Confirmed
- 378 • Planning Board Meeting – March 28, 6:30 PM

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380 There being no further business, Chair Russell Slack adjourned the meeting at 8:19 PM.

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382 Respectfully submitted by,
383 Krishni Pahl, Minute Taker

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385 Reviewed and edited by,
386 Mari Brunner, Senior Planner

STAFF REPORT

S-01-22 – BOUNDARY LINE ADJUSTMENT – 0 Gilbo Avenue and 0 Commercial Street

Request:

Applicant Fieldstone Land Consultants PLLC, on behalf of owner City of Keene, proposes a boundary line adjustment between the properties located at 0 Commercial St (TMP# 575-010-000-000-000) and 0 Gilbo Ave (TMP# 575-015-000-000-000). This adjustment would result in a transfer of 0.32-ac from the 0.56-ac parcel at 0 Gilbo Ave to the 1.6-ac parcel at 0 Commercial St. A waiver is requested from Section 25.10.8.B.2 of the Land Development Code regarding the requirement to submit an updated survey showing all metes and bounds of the revised parcels. Both properties are located in the Downtown Growth and Downtown Core Districts.

Background:

The property at 0 Gilbo Ave includes the current City of Keene Skate Park as well as a portion of the Cheshire Rail Trail extending between the Skate Park and Main Street. The parcel is owned by the City of Keene and is 0.564-acres. The 1.6±acre parcel at 0 Commercial St is an asphalt parking lot likewise owned by the City of Keene.



Figure 1. Aerial view of the properties located at 0 Commercial St. and 0 Gilbo Ave. highlighted in yellow.

The request is to swap land and adjust boundaries between the two properties. The result would net an increase of slightly more than 0.3-acres of the property at 0 Commercial Street, and a corresponding decrease in size of the parcel at 0 Gilbo Ave.

The bulk of the land swap would result from a transfer of the Cheshire Rail Trail portion of 0 Gilbo Ave to the property at 0 Commercial Street. This would increase the parcel size of 0 Commercial Street by slightly more than 0.3 acres. The property at 0 Gilbo Ave would in turn gain footage through a transfer of 770 sq. ft. of land from 0 Commercial Street to the southeast corner of the 0 Gilbo Ave property. This would result in the parcel at 0 Gilbo Ave becoming nearly rectangular in shape.

Currently, both properties are located predominately in the Downtown Growth (DT-G) district, with just the narrow extensions connecting to Main Street falling in the Downtown Core (DT-C) district. Approval of this boundary line adjustment would lead the parcel at 0 Gilbo Ave to lie entirely within the Downtown Growth (DT-G) district. The parcel at 0 Commercial Street would continue to be split between DT-G and DT-C districts, with the preponderance of the parcel situated in the DT-G district.

Table 1 provides the area of each lot prior to the proposed adjustment and after. There is no minimum lot size in either the DT-G district or the DT-C district.

<i>Table 1. Area of Land Affected by Proposed Boundary Line Adjustment</i>		
	0 Commercial Street (TMP# 575-010-000-000-000)	0 Gilbo Avenue (TMP# 575-015-000-000-000)
Prior to Adjustment	69,000± sf (1.6± ac)	24,552 sf (0.564 ac)
After Adjustment	83,000± sf (1.9± ac)	10,655 sf (0.245 ac)

STAFF REPORT

Completeness:

The Applicant has requested exemptions from providing a separate grading plan, landscaping plan, lighting plan, and technical reports. After reviewing this request, Staff have determined that exempting the Applicant from submitting this information would have no bearing on the merits of the application.

The Applicant requests a waiver from providing an updated survey showing all metes and bounds of the revised parcels. The submitted plan includes all the metes and bounds for 0 Gilbo Ave (TMP# 575-015-000-000-000), but does not include the metes and bounds for all parcel boundaries of the property located at 0 Commercial Street (TMP# 575-010-000-000-000). The Applicant submitted a written waiver request, which is included as an attachment to this staff report.

In making a decision to accept this application as “complete,” the Board may wish to consider whether the missing information (i.e., metes and bounds for all boundaries of the parcel located at 0 Commercial Street) is necessary for the Board to complete its review.

Departmental Comments: There were no departmental comments on this application.

Application Analysis: As no new development is proposed, the analysis provided below is focused on the Planning Board’s standards most relevant to this application.

- **Traffic & Access Management:** No changes are proposed to the existing access to either property as part of this proposal.
- **Surface Waters & Wetlands:** No surface waters or wetlands are present on either site.

Recommended Motion:

If the Board is inclined to approve this request, the following motion is recommended:

Approve S-01-22 as shown on the plan entitled “Lot Line Adjustment Prepared for City of Keene, 3 Washington St, City of Keene, County of Cheshire, State of New Hampshire” prepared by Fieldstone Land Consultants, PLLC at a scale of 1 inch = 30 feet on February 16, 2022 and last revised on March 9, 2022, with the following condition prior to signature by Planning Board Chair:

- 1. Owners’ signatures appear on the plan.**

CITY OF KEENE | PLANNING BOARD

BOUNDARY LINE ADJUSTMENT APPLICATION



This form must be filled out in its entirety. If a box is not checked, staff will assume that the information is not provided and the application is, therefore, not complete. Incomplete applications will not be accepted for review.

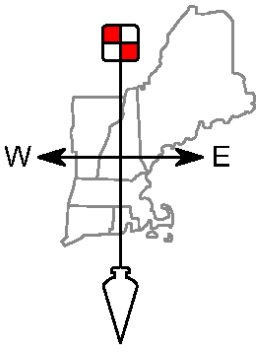
A	Project Name City of Keene Skate Park Boundary Line Adj.		Date Received/Date of Submission:	
	Tax Map Parcel number(s) 575 - 010 - 000 - 000 - 000 575 - 015 - 000 - 000 - 000 _____ _____		Community Development Dept File #:	
Project Address: Gilbo Avenue, Main Street, Wilson Street & Commercial Street			A p p i c a n t	Contact Name/Company: <u>PLEASE PRINT:</u> City Manager - Elizabeth A. Dragon
Acreage/S.F. of Parcel: 0.564 / 24,552 sq.ft. (Lot 575-15) 1.6+/- / 69,000+/- sq.ft. (Lot 575-10)				Address: 3 Washington Street, Keene, NH 03431
Zoning District: Downtown Growth & Downtown Core				Telephone: 603-357-9804 E-mail: EDragon@KeeneNH.gov
				Signature: Printed Name:
O w n e r # 1	Name/Company: <u>PLEASE PRINT:</u> Same as Applicant		O w n e r # 2	Name/Company: <u>PLEASE PRINT:</u> Same as Applicant
	Address:			Address:
	Telephone: E-mail:			Telephone: E-mail:
	Signature: Printed Name:			Signature: Printed Name:

B Descriptive Narrative Including

- | | | |
|---|--|--|
| <input checked="" type="checkbox"/> Type of development | <input checked="" type="checkbox"/> Sedimentation Control | <input checked="" type="checkbox"/> Scope/scale of development |
| <input checked="" type="checkbox"/> Proposed uses | <input checked="" type="checkbox"/> Vegetation | <input checked="" type="checkbox"/> Parcel size |
| <input checked="" type="checkbox"/> Location of access points | <input checked="" type="checkbox"/> Debris management | <input checked="" type="checkbox"/> Proposed stormwater, drainage & erosion plan |
| <input checked="" type="checkbox"/> Any other descriptive information | <input checked="" type="checkbox"/> Disposal proposals for boulders, stumps & debris | |

C A complete application must include the following

- | | |
|--|--|
| <input checked="" type="checkbox"/> Two (2) copies of completed application forms signed and dated | <input checked="" type="checkbox"/> Three (3) copies of "D" size architectural elevations (24" x 36") |
| <input checked="" type="checkbox"/> Two (2) copies of descriptive narrative | <input checked="" type="checkbox"/> Plans stamped/signed by reg. professional |
| <input checked="" type="checkbox"/> Notarized list of all owners of property within 200' - include owner and applicant | <input checked="" type="checkbox"/> Two (2) copies on |
| <input checked="" type="checkbox"/> Two (2) sets of mailing labels, per abutter | <input checked="" type="checkbox"/> Three (3) copies of all technical reports |
| <input checked="" type="checkbox"/> Seven (7) copies on "D" size paper of plans (24" x 36") | <input checked="" type="checkbox"/> Two (2) color architectural elevations on 11" x 17" |
| | <input checked="" type="checkbox"/> A check to cover the costs of legal notice to advertise the public hearing and mailing notices out to abutters |



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LAND CONSULTANTS, PLLC

206 Elm Street, Milford, NH 03055 - Phone: 603-672-5456 - Fax: 603-413-5456
www.FieldstoneLandConsultants.com

Boundary Line Adjustment Application Narratives

The City of Keene, New Hampshire
Tax Map Parcels 575-010 & 575-015
Gilbo Avenue, Main Street, & Wilson Street
Keene, New Hampshire

February 16, 2022

Project Narrative:

Fieldstone Land Consultants, on behalf of The Manager of the City of Keene, is submitting this application for Planning Board approval. The proposal consists of adjusting the lot lines between existing Tax Map Lots 575-010 & 575-015.

The proposed lot line adjustment will be between parcels 575-010 and 575-015. Parcel 575-010 is 1.6+/- acres. It is currently a public parking lot with no buildings which was laid out as a public way in 1969. Lot 575-015 is 0.564 acres with frontage on Wilson Street and Commercial Street and is currently a Skate Park. The lot line adjustment proposes to revise the common lines of lots 575-015 and lot 575-010, resulting in a net decrease in area of lot 575-015 and a net increase in area of lot 575-010.

After the Lot Line Adjustment, revised Lot 575-010 will contain 1.9+/- acres and will continue to be a laid out public way. The revised lot 575-015 will contain 0.245 acres with frontage on Commercial Street.

Presently both lots are and will continue to be serviced by municipal water and sewer. Electrical power and communication is run overhead to lot 575-015 from the north end of Wilson Street, then continues underground.

There are no improvements proposed at this time.

Development Standards Narrative:

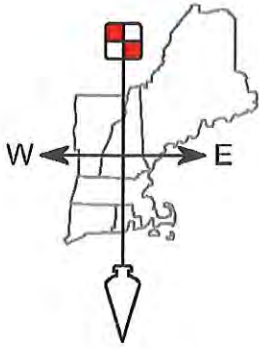
1. **Drainage** – No changes are proposed and drainage is to remain existing.
2. **Sedimentation/Erosion Control** – No construction or excavation is proposed thus sedimentation and erosion controls are not necessary.
3. **Hillside Protection** – There is no hillside protection required for this project.
4. **Snow Storage and Removal** – Existing snow storage will not be affected by the Lot Line Adjustment. All driveways and snow storage areas are to remain.
5. **Flooding** – An inspection of the Flood Insurance Rate Map (FIRM) for the area, Map Number 33005C0267E, indicates that the lot is not located within any flood hazard zone.
6. **Landscaping** – Landscaping is not anticipated with this application as no construction or improvements are proposed.

City of Keene

Tax Map Parcel 575-010 & 575-015

Gilbo Ave, Main Street, & Wilson Street, Keene, New Hampshire

7. **Noise** – No additional noise is anticipated with this application as the only change will be the common lot lines as depicted on the proposed Lot Line Adjustment Plan.
8. **Screening** – No improvements are anticipated and any existing screening is to remain.
9. **Air Quality** – The existing commercial and residential uses will have no impacts on air quality.
10. **Lighting** – No new lighting is proposed. Lighting is to remain unchanged.
11. **Sewer and Water** – The existing lots are presently serviced by municipal sewer and water. No change is anticipated.
12. **Traffic** – The proposed application will have no additional traffic since the uses are to remain unchanged.
13. **Driveways** – No change in driveways are planned.
14. **Hazardous and Toxic Materials** – No hazardous or toxic material currently exists on the site.
15. **Filling/Excavation** – No filling or Excavation is planned for this project.
16. **Wetlands** – There are no jurisdictional wetlands on site.
17. **Surface Waters** – No change to the existing surface runoff is proposed.
18. **Stump Dumps** – This project does not require removal of stumps or placement of stump dumps.
19. **Architectural and Visual Appearance** – No new improvements or structures are proposed for this site. Architectural and visual appearances will remain unchanged.



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www.FieldstoneLandConsultants.com

March 8, 2022

City of Keene Planning Board

Attn: Mari Brunner, Senior Planner
City Hall - 4th Floor
3 Washington Street-
Keene, NH 0343

RE: Boundary Line Adjustment
Skate Park & Commercial Street Parking Lot – **Waiver Request**

Dear Planning Board,

Fieldstone Land Consultants, on behalf of the City of Keene, is submitting this waiver request for Planning Board approval. The proposed project consists of adjusting the lot line of existing Tax Map 575 Lots 010 & 015 respectively. Based on Staff comments, received March 7, 2022, we are hereby seeking a waiver for following standard in the new Land Development Code (LDC).

§ 25.10.8.B.2 Boundary Line Adjustment Procedure: States an updated survey showing the boundary line adjustment, and all metes and bounds of the revised parcels shall be prepared by the applicant following approval from the Planning Board, and shall be filed with the Community Development Department for recording in the County Registry of Deeds.

The Commercial Street Parking Lot parcel (575/010) is a large property with a long and complicated history. The parcel is an assemblage of various lots acquired over the years. In 1969 the City Council laid out the parcel as a public way. The public way has been modified from time to time to accommodate development needs.

Researching the history of these various acquisitions, layouts, modifications, cross-easements, etc. will require substantial time and effort, and would unnecessarily delay the redevelopment of the Keene Skate Park. The results of that effort will also have no bearing on the adjustments between Parcels 575/010 and 575/015.

We believe this constitutes an unnecessary hardship on the City and the non-profit entity supporting the skate park project.

As part of the upcoming Downtown Infrastructure Repair & Replacement project, the City will be evaluating the future of Gilbo Avenue, including the potential extension of Wilson Street to meet Gilbo Avenue. At that time the City expects to modify the Commercial Street and Wilson Street layouts and a full property survey will be completed. Therefore, we respectfully request that the Planning Board grant this waiver and exempt metes and bounds on Tax Map Parcel 575/010. Thank you for your consideration in granting this waiver.

Best Regards,

FIELDSTONE LAND CONSULTANTS, PLLC

Michael D. Ploof, L.L.S.

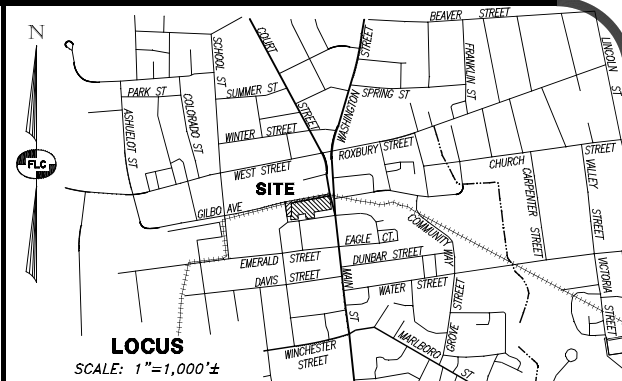
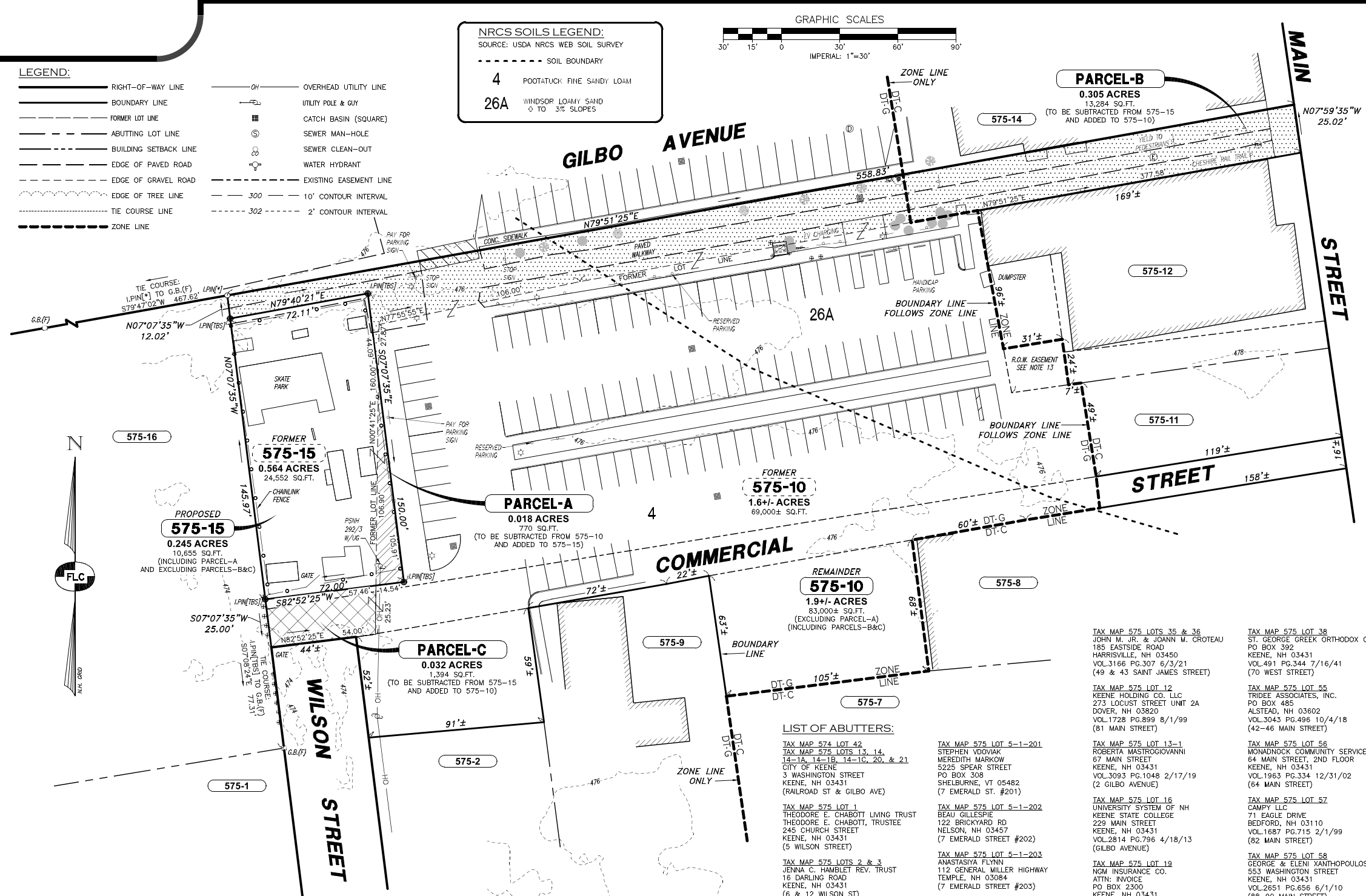
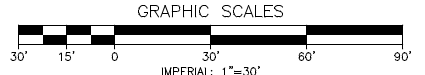
Project Manager

LEGEND:

—	RIGHT-OF-WAY LINE	—OH—	OVERHEAD UTILITY LINE
—	BOUNDARY LINE	—U—	UTILITY POLE & GUY
---	FORMER LOT LINE	—	CATCH BASIN (SQUARE)
---	ABUTTING LOT LINE	—S—	SEWER MAN-HOLE
---	BUILDING SETBACK LINE	—	SEWER CLEAN-OUT
---	EDGE OF PAVED ROAD	—H—	WATER HYDRANT
---	EDGE OF GRAVEL ROAD	---	EXISTING EASEMENT LINE
---	EDGE OF TREE LINE	---	10' CONTOUR INTERVAL
---	TIE COURSE LINE	---	2' CONTOUR INTERVAL
---	ZONE LINE		

NRCS SOILS LEGEND:
SOURCE: USDA NRCS WEB SOIL SURVEY

---	SOIL BOUNDARY
4	FOOTSTOCK FINE SANDY LOAM
26A	WINDSOR LOAMY SAND 0 TO 3% SLOPES



NOTES:

- THE APPLICANT AND OWNER OF RECORD FOR TAX MAP 575 LOTS 10 & 15 IS THE CITY OF KEENE - 3 WASHINGTON STREET, KEENE, NH 03431. THE DEED REFERENCES FOR LOT 575-10 ARE VOL.1003 PG.33 DATED OCTOBER 1, 1970, VOL.832 PG.592 DATED SEPTEMBER 17, 1971, & VOL.839 PG.348 DATED JANUARY 27, 1972 IN THE C.C.R.D. THE DEED REFERENCE FOR LOT 575-15 IS VOL.1396 PG.497 DATED FEBRUARY 13, 1992 IN THE C.C.R.D.
- THE TOTAL AREA OF EXISTING TAX MAP LOT 575-10 IS 1.6± ACRES OR 69,000± SQ.FT. THE TOTAL AREA OF EXISTING TAX MAP LOT 575-15 IS 0.564 ACRES OR 24,552 SQ.FT.
- THE PURPOSE OF THIS PLAN IS TO REVISE THE COMMON LOT LINES FOR EXISTING TAX MAP LOTS 575-10 AND 575-15 BY SUBTRACTING PARCEL-A FROM LOT 575-10 & ADDING PARCEL-A TO LOT 575-15, SUBTRACTING PARCELS-B & C FROM LOT 575-15 AND ADDING PARCELS-B & C TO LOT 575-10.
- LOT NUMBERS REFER TO THE CITY OF KEENE ASSESSORS MAP 575.
- ZONING FOR THE LOTS ARE THE "DOWNTOWN-CORE" DISTRICT (DT-C) & THE "DOWNTOWN-GROWTH" DISTRICT (DT-G).

DT-C ZONE:	REQUIRED	EXISTING 575-10	EXISTING 575-15	PROPOSED 575-10	PROPOSED 575-15
MIN LOT AREA	NONE	69,000± SF	24,552 SF	83,000± SF	10,655 SF
MIN LOT FRONTAGE	NONE	52 FT	44 FT	96 FT	96 FT
MIN FRONT SETBACK	0 FT	NONE	NONE	NONE	NONE
MIN REAR SETBACK	0 FT	NONE	NONE	NONE	NONE
MIN SIDE SETBACK	0 FT	NONE	NONE	NONE	NONE
MAX STORIES	7 STORY	NONE	NONE	NONE	NONE

DT-G ZONE:	REQUIRED	EXISTING 575-10	EXISTING 575-15	PROPOSED 575-10	PROPOSED 575-15
MIN LOT AREA	NONE	69,000± SF	24,552 SF	83,000± SF	10,655 SF
MIN LOT FRONTAGE	NONE	52 FT	44 FT	96 FT	96 FT
MIN FRONT SETBACK	0 FT	NONE	NONE	NONE	NONE
MIN REAR SETBACK	0 FT	NONE	NONE	NONE	NONE
MIN SIDE SETBACK	0 FT	NONE	NONE	NONE	NONE
MAX STORIES	7 STORY	NONE	NONE	NONE	NONE

THE LOTS ARE ALSO LOCATED IN THE DOWNTOWN HISTORIC OVERLAY DISTRICT.

- THE TOPOGRAPHIC SURFACE SHOWN IS A COMPILATION OF A FIELD SURVEY BY THIS OFFICE TOGETHER WITH NEW HAMPSHIRE GRANIT LIDAR (LIGHT DETECTION RANGING).
- THE BOUNDARY INFORMATION SHOWN WAS DEVELOPED FROM THE REFERENCE PLANS CITED TOGETHER WITH A FIELD SURVEY BY THIS OFFICE DURING THE MONTH OF FEBRUARY, 2022.
- HORIZONTAL ORIENTATION IS THE NEW HAMPSHIRE STATE PLANE COORDINATE SYSTEM, NAD83, BASED ON FIELD GPS OBSERVATIONS THAT WERE UPLOADED TO AND CALCULATED BY THE NOAA ONLINE POSITIONING USER SERVICE (OPUS). THE VERTICAL DATUM IS NAVD88.
- THE SITE DOES NOT LIE WITHIN THE "SURFACE WATER PROTECTION" OVERLAY DISTRICT.
- THE EXISTING LOTS ARE SERVICED BY UNDERGROUND UTILITIES, MUNICIPAL SEWER, WATER AND GAS.
- THE SUBJECT PARCEL IS NOT LOCATED IN A FLOOD HAZARD AREA AS DETERMINED FROM THE FLOOD INSURANCE STUDY (FIRM), CHESHIRE COUNTY, CITY OF KEENE, NEW HAMPSHIRE, COMMUNITY NO. 330023, PREPARED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY, MAP NUMBER: 33005C0267E, DATED: MAY 23, 2006.
- PERMANENT MARKERS ARE TO BE SET AT THE LOT CORNERS OF TAX MAP 575 LOT 15 AS SHOWN HEREON.
- LOT 575-10 IS SUBJECT TO ACCESS EASEMENTS, RIGHTS OF WAYS, AND DRAINAGE EASEMENTS BENEFITING LOT 575-12 & 575-11. SEE C.C.R.D. VOL.832 PG.592 DATED SEPTEMBER 17, 1971. LOT 575-15 IS SUBJECT TO A TELECOMMUNICATIONS EASEMENT RECORDED IN VOL.1287 PG.328, AND MODIFIED IN VOL.1321 PG.600 DATED APRIL 20, 1989 IN THE C.C.R.D. LOT 575-15 IS ALSO SUBJECT TO A SIDEWALK EASEMENT AND ACCESS EASEMENT TO THE CITY OF KEENE - SEE VOL.1325 PG.262 DATED APRIL 25, 1989 AND VOL.1321 PG.598 DATED APRIL 20, 1989 IN THE C.C.R.D.

REV.	DATE	REVISION	DESCRIPTION	C/O	DR	MDP	CK
A	3/09/22		REVISE ZONE LINE LABELING				

LOT LINE ADJUSTMENT PLAN
TAX MAP 575 LOTS 10 & 15
(GILBO AVENUE & MAIN STREET)
KEENE, NEW HAMPSHIRE
PREPARED FOR & LAND OF:
CITY OF KEENE
3 WASHINGTON STREET, KEENE, NH 03431

SCALE: 1" = 30' FEBRUARY 16, 2022

Surveying ♦ Engineering ♦ Land Planning ♦ Permitting ♦ Septic Designs

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206 Elm Street, Millford, NH 03055
Phone: (603) 672-5456 Fax: (603) 413-5456
www.FieldstoneLandConsultants.com

- REFERENCE PLANS:**
- "SUBDIVISION - LAND OF - MAIN ST. AMERICA CAPITAL CORPORATION - GILBO AVE - KEENE, N.H.", SCALE: 1"=50', DATED: SEPTEMBER, 1990, PREPARED BY: THE CITY OF KEENE ENGINEERING DEPARTMENT & RECORDED IN PLAN CABINET 11, DRAWER 6, NUMBER 606 IN THE C.C.R.D.
 - "LAND IN - KEENE, NEW HAMPSHIRE - MAIN ST. TO SCHOOL ST. - BOSTON AND MAINE CORPORATION - TO - MAIN ST. AMERICA CAPITAL CORP.", SCALE: 1"=50', DATED: MAY, 1987 & REVISED THROUGH MAY 25, 1988 PREPARED BY: THE CITY OF KEENE ENGINEERING DEPARTMENT AND RECORDED IN PLAN CABINET 11, DRAWER 00, NUMBER 118. IN THE C.C.R.D.
 - "COMMERCIAL STREET PROPERTY", SCALE: 1"=20', DATED: SEPTEMBER 2, 1970 & REVISED THROUGH APRIL, 1975, PREPARED BY & ON FILE AT: THE CITY OF KEENE ENGINEERING DEPARTMENT.

LOT REVISION AREA CHART

LOT NO.	EXISTING AREA	PROPOSED CHANGE	REVISED AREA
575-10	1.6± ACRES (69,000± SF)	- PARCEL-A (770 SQ.FT.) + PARCEL-B (13,284 SQ.FT.) + PARCEL-C (1,394 SQ.FT.)	1.9± ACRES (83,000± SF)
575-15	0.564 ACRES (24,552 SF)	+ PARCEL-A (770 SQ.FT.) - PARCEL-B (13,284 SQ.FT.) - PARCEL-C (1,394 SQ.FT.)	0.245 ACRES (10,655 SF)

CERTIFICATION:

"I, HEREBY CERTIFY THAT LOT 575-15 IS THE RESULT OF AN ACTUAL FIELD SURVEY MADE ON THE GROUND PER THE PRECISION AND ACCURACY STANDARDS FOR AN URBAN CLASSIFICATION SURVEY AS SPECIFIED IN THE NEW HAMPSHIRE LAND SURVEYOR'S ADMINISTRATIVE RULES (LAN 503.04) AND HAS A MAXIMUM ERROR OF CLOSURE OF ONE PART IN TEN THOUSAND (1:10,000) ON ALL PROPERTY LINES WITHIN AND BORDERING THE SUBJECT PROPERTY."

DRAFT

APPROVED BY KEENE PLANNING BOARD

CHAIRMAN: _____ DATE: _____

SECRETARY: _____ DATE: _____

LIST OF ABUTTERS:

- | | | | | | |
|---|--|---|--|--|--|
| TAX MAP 574 LOT 42
TAX MAP 575 LOTS 13, 14,
14-1A, 14-1B, 14-1C, 20, & 21
CITY OF KEENE
3 WASHINGTON STREET
KEENE, NH 03431
(RAILROAD ST & GILBO AVE) | TAX MAP 575 LOT 5-1-201
STEPHEN VDOVIK
MEREDITH MARKOW
5225 SPEAR STREET
PO BOX 308
SHELburne, VT 05482
(7 EMERALD ST. #201) | TAX MAP 575 LOT 13-1
ROBERTA MASTROGIOVANNI
67 MAIN STREET
KEENE, NH 03431
VOL.3093 PG.1048 2/17/19
(2 GILBO AVENUE) | TAX MAP 575 LOT 36
JOHN M. JR. & JOANN M. CROTEAU
185 EASTSIDE ROAD
HARRISVILLE, NH 03450
VOL.3166 PG.307 6/3/21
(49 & 43 SAINT JAMES STREET) | TAX MAP 575 LOT 38
ST. GEORGE GREEK ORTHODOX CHURCH
PO BOX 392
KEENE, NH 03431
VOL.491 PG.344 7/16/41
(70 WEST STREET) | TAX MAP 575 LOT 55
TRIDEE ASSOCIATES, INC.
PO BOX 485
ALSTEAD, NH 03602
VOL.3043 PG.496 10/4/18
(42-46 MAIN STREET) |
| TAX MAP 575 LOT 1
THEODORE E. CHABOTT LIVING TRUST
THEODORE E. CHABOTT, TRUSTEE
245 CHURCH STREET
KEENE, NH 03431
(5 WILSON STREET) | TAX MAP 575 LOT 5-1-202
BEAU GILLESPIE
122 BRICKYARD RD
NELSON, NH 03457
VOL.2814 PG.796 4/18/13
(GILBO AVENUE) | TAX MAP 575 LOT 16
UNIVERSITY SYSTEM OF NH
KEENE STATE COLLEGE
229 MAIN STREET
KEENE, NH 03431
VOL.2814 PG.796 4/18/13
(GILBO AVENUE) | TAX MAP 575 LOT 12
KEENE HOLDING CO. LLC
273 LOCUST STREET UNIT 2A
DOVER, NH 03820
VOL.1728 PG.899 8/1/99
(81 MAIN STREET) | TAX MAP 575 LOT 56
MONADNOCK COMMUNITY SERVICE CENTER INC.
3RD FLOOR
KEENE, NH 03431
VOL.1963 PG.334 12/31/02
(64 MAIN STREET) | TAX MAP 575 LOT 57
CAMPY LLC
71 EAGLE DRIVE
BEDFORD, NH 03110
VOL.1687 PG.715 2/1/99
(82 MAIN STREET) |
| TAX MAP 575 LOTS 2 & 3
JENNA C. HANBLETT REV. TRUST
16 DARLING ROAD
KEENE, NH 03431
(6 & 12 WILSON ST) | TAX MAP 575 LOT 5-1-203
ANASTASIYA FLYNN
112 GENERAL MILLER HIGHWAY
TEMPLE, NH 03084
(7 EMERALD STREET #203) | TAX MAP 575 LOT 19
NGM INSURANCE CO.
ATTN: INVOICE
PO BOX 2300
KEENE, NH 03431
VOL.1396 PG.495 2/13/92
(GILBO AVENUE) | TAX MAP 575 LOTS 35 & 36
JOHN M. JR. & JOANN M. CROTEAU
185 EASTSIDE ROAD
HARRISVILLE, NH 03450
VOL.3166 PG.307 6/3/21
(49 & 43 SAINT JAMES STREET) | TAX MAP 575 LOT 58
GEORGE & ELENI XANTHOPOULOS LIVING TRUST
553 WASHINGTON STREET
KEENE, NH 03431
VOL.2851 PG.656 6/1/10
(88-90 MAIN STREET) | TAX MAP 575 LOT 59
CHOW DOWN INVESTMENTS LLC
PO BOX 143
KEENE, NH 03431
VOL.3052 PG.619 1/8/19
(100 MAIN STREET) |
| TAX MAP 575 LOTS 4, 8,
8-1-3, 8-1-5, & 9
COLONIAL THEATRE GROUP INC.
PO BOX 77
KEENE, NH 03431
(7 EMERALD ST)
(95 MAIN ST)
(20 COMMERCIAL ST) | TAX MAP 575 LOT 5-1-204
MARK TORSSELLI
1 WOODLAND TERRACE
PROSPECT, CT 06712
(7 EMERALD ST #204) | TAX MAP 575 LOT 23, 24, 25, & 26
MITCHELL H. GREENWALD REV. TRUST
ERIKA GREENWALD REV. TRUST
PO BOX 361
KEENE, NH 03431
VOL.1792 PG.24 3/1/01
VOL.1583 PG.478 11/1/85
VOL.1792 PG.27 3/1/01
VOL.2391 PG.166 10/26/06
(GILBO AVENUE)
(49-55, 45-47, & 35-43 MAIN STREET) | TAX MAP 575 LOT 10
ANASTASIYA FLYNN
112 GENERAL MILLER HIGHWAY
TEMPLE, NH 03084
(7 EMERALD STREET #203) | TAX MAP 575 LOT 60
102 MAIN STREET ASSOCIATES LLC
PO BOX 3
WALPOLE, NH 03608
VOL.1692 PG.157 3/1/99
(102 MAIN STREET) | TAX MAP 575 LOT 61
RONALD A. FARINA REV. TRUST
17 MCKINLEY STREET
KEENE, NH 03431
VOL.2961 PG.124 9/22/16
(106 MAIN STREET) |
| TAX MAP 575 LOT 5-1-101
JOHN G. & DANA R. BURKE
179 OLD CRAIGVILLE RD
HYANNIS, MA 02601
(7 EMERALD ST #101) | TAX MAP 575 LOT 5-1-205
COURTNEY & AGUSTA DUTEAU
6 SUMMIT ROAD, UNIT 12
KEENE, NH 03431
(7 EMERALD ST #205) | TAX MAP 575 LOT 27
COUNTY OF CHESHIRE
33 WEST STREET
KEENE, NH 03431
VOL.1863 PG.299 12/31/01
(LAWSON STREET) | TAX MAP 575 LOT 5-1-102
DEBORAH J. CORY
PO BOX 372
JACKSONVILLE, VT 05342
(7 EMERALD ST #102) | TAX MAP 575 LOT 73
BARRY JOEL FOX
ONE HUNDRED ELEVEN REALTY ASSOCIATION
52 OAKHURST ROAD
CAPE ELIZABETH, ME 04107
VOL.1724 PG.625 10/1/99
(59-79 EMERALD STREET) | TAX MAP 575 LOT 62
VOL.2961 PG.124 9/22/16
(106 MAIN STREET) |
| TAX MAP 575 LOT 5-1-103
JOANN S. ALBERTINI
101 BREAD ROAD
PO BOX 82
NEW BOSTON, NH 03070
(7 EMERALD ST #103) | TAX MAP 575 LOT 7
WICHLAND BROTHERS REALTY
105 MAIN STREET
KEENE, NH 03431
VOL.1377 PG.359 7/1/91
(101 MAIN STREET) | TAX MAP 575 LOT 11
PCT REAL ESTATE LLC
87 MAIN STREET
KEENE, NH 03431
VOL.2982 PG.345 4/18/17
(87 MAIN STREET) | TAX MAP 575 LOT 5-1-104
JENNA C. HANBLETT REV. TRUST
16 DARLING ROAD
KEENE, NH 03431
(6 & 12 WILSON ST) | TAX MAP 575 LOT 74 & 75
BARRY JOEL FOX
50 EASTVIEW ROAD
KEENE, NH 03431
VOL.1377 PG.359 7/1/91
(43 & 37 EMERALD STREET) | |

STAFF REPORT

S-02-22 – BOUNDARY LINE ADJUSTMENT – 50 & 62 Rule Street

Request:

Applicant Cardinal Surveying & Land Planning, on behalf of owners Elfriede Wagner & Pilot Realty LLC, proposes a boundary line adjustment between the properties located at 50 Rule St (TMP# 518-047-000-000-000) and 62 Rule St (TMP# 518-051-000-000-000). This adjustment would result in a transfer of 0.03 ac from the 6-ac parcel at 50 Rule St to the 0.21-ac parcel at 62 Rule St. A waiver is requested from Section 25.10.8.B.2 of the Planning Board Regulations regarding the requirement to submit an updated survey showing all metes and bounds of the revised parcels. Both properties are located in the Low Density District.

Background:

The two properties that are the focus of this proposal are single-family lots in the Low Density District with frontage on Rule Street. The property at 50 Rule Street (TMP# 518-047-000-000-000) is 6 acres in size and is owned by Elfriede E. Wagner, and the property at 62 Rule Street (TMP# 518-051-000-000-000) is 0.21 acres in size and is owned by Pilot Realty LLC.

The request is to transfer 0.03 acres from the 50 Rule Street property to the west end of the 62 Rule Street property. The house at 62 Rule Street was constructed in 1920 and crosses the western property boundary. The proposed boundary line adjustment would make it so that the house on the 62 Rule Street parcel no longer crosses the property line and would make the lot more conforming with the dimensional requirements for the Low Density District, which specify that there should be a 20-ft rear setback for all properties in this district. This 0.03-acre transfer of land will add approximately 21 feet onto the western portion of the 62 Rule Street parcel. There is no development proposed to either lot as part of this application.

Table 1 provides the area of each lot prior to the proposed adjustment and after. The minimum lot size in the Low Density District is 10,000-sf.



Figure 1. Aerial view of the properties located at 50 & 62 Rule St. highlighted in yellow.

<i>Table 1. Area of Land Affected by Proposed Boundary Line Adjustment</i>		
	50 Rule Street (TMP# 518-047-000-000-000)	62 Rule Street (TMP# 518-051-000-000-000)
Prior to Adjustment	6.0 ac (261,360.0 sf)	0.21 ac (9,061.0 sf)
After Adjustment	5.97 ac (259,881.9 sf)	0.24 ac (10,539.1 sf)
Amount of Land Transferred	-0.03 ac (1,478.1 sf)	+0.03 ac (1,478.1 sf)

STAFF REPORT

Completeness:

The Applicant requests exemptions from submitting separate Existing and Proposed Conditions Plans that show the boundaries and acreage of the existing and proposed lots subject to review, a Grading Plan, a Landscaping Plan, a Lighting Plan, and Technical Reports. After reviewing this application, Staff have determined that exempting the Applicant from submitting this information would have no bearing on the merits of the application.

The Applicant has requested a waiver from Article 25.10.8.B.2 of the Land Development Code, which states that, “An updated survey showing the boundary line adjustment, and all metes and bounds of the revised parcels shall be prepared by the applicant.” The submitted plan includes all metes and bounds for the property at 62 Rule Street (TMP# 518-051-000-000-000), but does not include the metes and bounds for the property at 50 Rule Street (TMP# 518-047-000-000-000). The Applicant has submitted a written waiver request, which is included as an attachment to this staff report.

In making a decision to accept this application as “complete,” the Board may wish to consider whether the missing information (i.e., metes and bounds for all boundaries of the parcel located at 50 Rule Street) is necessary for the Board to complete its review.

Departmental Comments:

- **Zoning:** Is the shed being moved onto the property?
 - *Note: The Applicant submitted a revised plan with a note stating that the shed will be moved so that is located entirely on the 62 Rule Street property.*
- **Code Enforcement, Engineering, Fire, and Police:** No comments.

Application Analysis: As no new development is proposed, the analysis provided below is focused on the Planning Board’s standards most relevant to this application.

- **Article 20.9 - Traffic & Access Management:** There are no changes proposed to the existing access to either property as part of this proposal. This standard does not apply.
- **Article 20.11 – Surface Waters & Wetlands:** There are no known wetlands or surface waters present on either of the subject parcels, and there is no new development proposed as part of this application. This standard does not apply.

Recommended Motion:

If the Board is inclined to approve this request, the following motion is recommended:

Approve S-02-22 as shown on the plan entitled “Boundary Line Adjustment, Map 518-051-000 & Map 518-047-000, 50 & 62 Rule Street, Keene, NH 03431” prepared by Cardinal Surveying & Land Planning at a scale of 1 inch = 20 feet on February 15, 2022 and last revised on March 14, 2022, with the following condition prior to signature by Planning Board Chair:

1. **Owners’ signatures appear on the plan.**

8-02-22

CITY OF KEENE | PLANNING BOARD BOUNDARY LINE ADJUSTMENT APPLICATION



This form must be filled out in its entirety. If a box is not checked, staff will assume that the information is not provided and the application is, therefore, not complete. Incomplete applications will not be accepted for review.

A	Project Name BOUNDARY LINE ADJUSTMENT	Date Received/Date of Submission: RECEIVED FEB 18 2022
	Tax Map Parcel number(s) 51B-051-000-000-000 51B-047-000-000-000	Community Development Dept File #:

Project Address: 50 & 62 RILEY ST	Applicant	Contact Name/Company: PLEASE PRINT: WENDY PELLETIER CARDINAL SURVEYING & LAND PLANNING
Acreage/S.F. 9001 / of Parcel: 0.21 / 6 AC		Address: PO BOX 160 SULLIVAN, NH 03445
Zoning District: LOW DENSITY		Telephone: 603 847-3473 E-mail: WENDY@CARDINALSURVEYING.NET

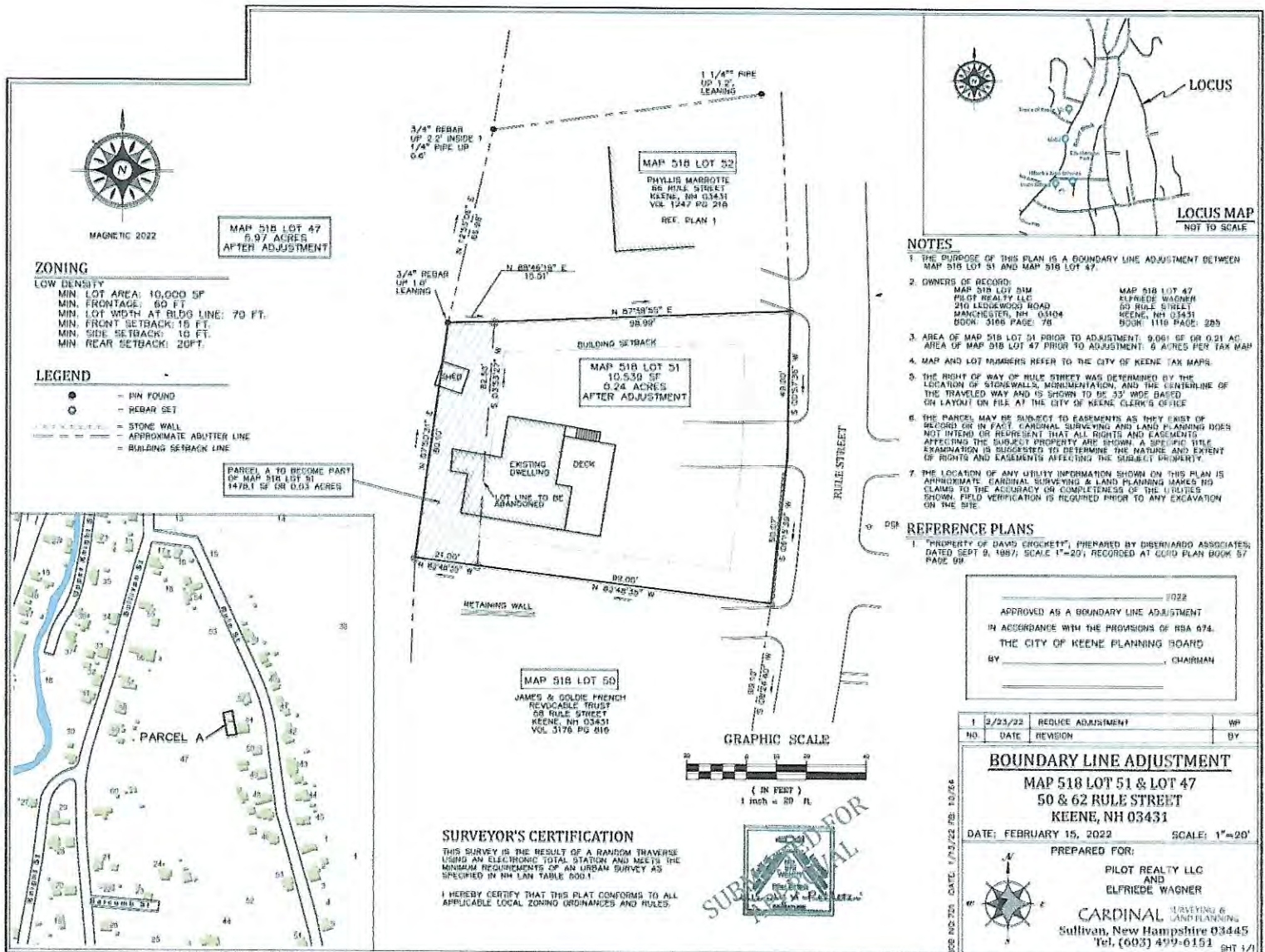
Owner # 1	Name/Company: PLEASE PRINT: PILOT REALTY LLC	Owner # 2	Name/Company: PLEASE PRINT: ELFRIEDI WAGNER KEVIN DEJESUS
	Address: 210 LEDGENOOD RD MANCHESTER, NH 03104		Address:
	Telephone: 603-566-2000 E-mail: PATRICK.J.HOUGHTON@YANCO.COM		Telephone: 603 852-8831 E-mail: FARAEDON@SIGNEC.YANCO.COM
	Signature: <i>[Signature]</i> Printed Name: PAT HOUGHTON		Signature: FORTA Printed Name: COMING

B Descriptive Narrative Including

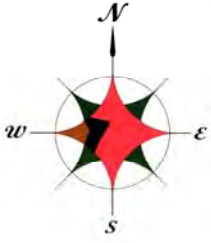
<input checked="" type="checkbox"/> Type of development	<input checked="" type="checkbox"/> Sedimentation Control	<input checked="" type="checkbox"/> Scope/scale of development
<input checked="" type="checkbox"/> Proposed uses	<input checked="" type="checkbox"/> Vegetation	<input checked="" type="checkbox"/> Parcel size
<input type="checkbox"/> Location of access points	<input checked="" type="checkbox"/> Debris management	<input checked="" type="checkbox"/> Proposed stormwater, drainage & erosion plan
<input checked="" type="checkbox"/> Any other descriptive information	<input checked="" type="checkbox"/> Disposal proposals for boulders, stumps & debris	

C A complete application must include the following

<input checked="" type="checkbox"/> Two (2) copies of completed application forms signed and dated	<input checked="" type="checkbox"/> Three (3) copies of "D" size architectural elevations (24" x 36")
<input checked="" type="checkbox"/> Two (2) copies of descriptive narrative	<input checked="" type="checkbox"/> Plans stamped/signed by reg. professional
<input checked="" type="checkbox"/> Notarized list of all owners of property within 200' - include owner and applicant	<input checked="" type="checkbox"/> Two (2) copies on
<input type="checkbox"/> Two (2) sets of mailing labels, per abutter	<input checked="" type="checkbox"/> Three (3) copies of all technical reports
<input checked="" type="checkbox"/> Seven (7) copies on "D" size paper of plans (24" x 36")	<input checked="" type="checkbox"/> Two (2) color architectural elevations on 11" x 17"
	<input type="checkbox"/> A check to cover the costs of legal notice to advertise the public hearing and mailing notices out to abutters



I approve this boundary line adjustment.
 Elfride Considine
 3/14/2022 10:35pm



CARDINAL SURVEYING AND LAND PLANNING, LLC

PO Box 160
Sullivan, NH 03445
(603) 209-1989

www.cardinalsurveying.net

"Know Your Boundaries"

February 18, 2022

62 Rule Street
Boundary Line Adjustment
TM 518-051-000 & TM 518-047-000

Project Narrative

This is a boundary line adjustment proposed between 62 Rule Street and 50 Rule Street. The existing dwelling at 62 Rule, built in 1920, is over the lot line. This adjustment will correct this issue and create a more conforming lot.

62 Rule Street is a .21-acre lot and 50 Rule Street is a 6 acre lot with back land spanning between the lots on Rule Street and Sullivan Street.

The proposed adjustment area is 1478.1 sf or .03 ac.

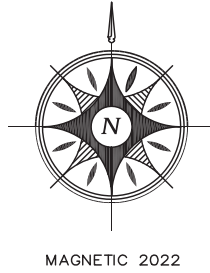
There is no building or improvements planned on either lot at this time, therefore we request waivers from a Proposed Conditions plan, Grading Plan, Landscaping Plan and Lighting Plan, also Development Standards 1-19.

Waiver Request

Article 25.10.8.B.2-All metes and bounds of revised parcels

Request a waiver of a full survey of Map 518 -047-000.

- A. Map 518-047-000 is a 6-acre lot of which .03 ac will be affected by the boundary line adjustment. The area of adjustment will square off Map 518-051-000 with the abutting lots. The remainder of the lot spans along the backside of the lots on Rule Street and Sullivan Street. Lot lines are shown based on tax maps, plans and deeds of record.
- B. The area of the boundary line adjustment will not affect abutters, lot frontage or conformity of this lot. The rear lot corners of the abutting lots were surveyed and are being used as the location of the proposed line
- C. A survey of the entire 6 acres would not be cost effective for the small area of adjustment being proposed.



MAP 518-047-000
5.97 ACRES
AFTER ADJUSTMENT

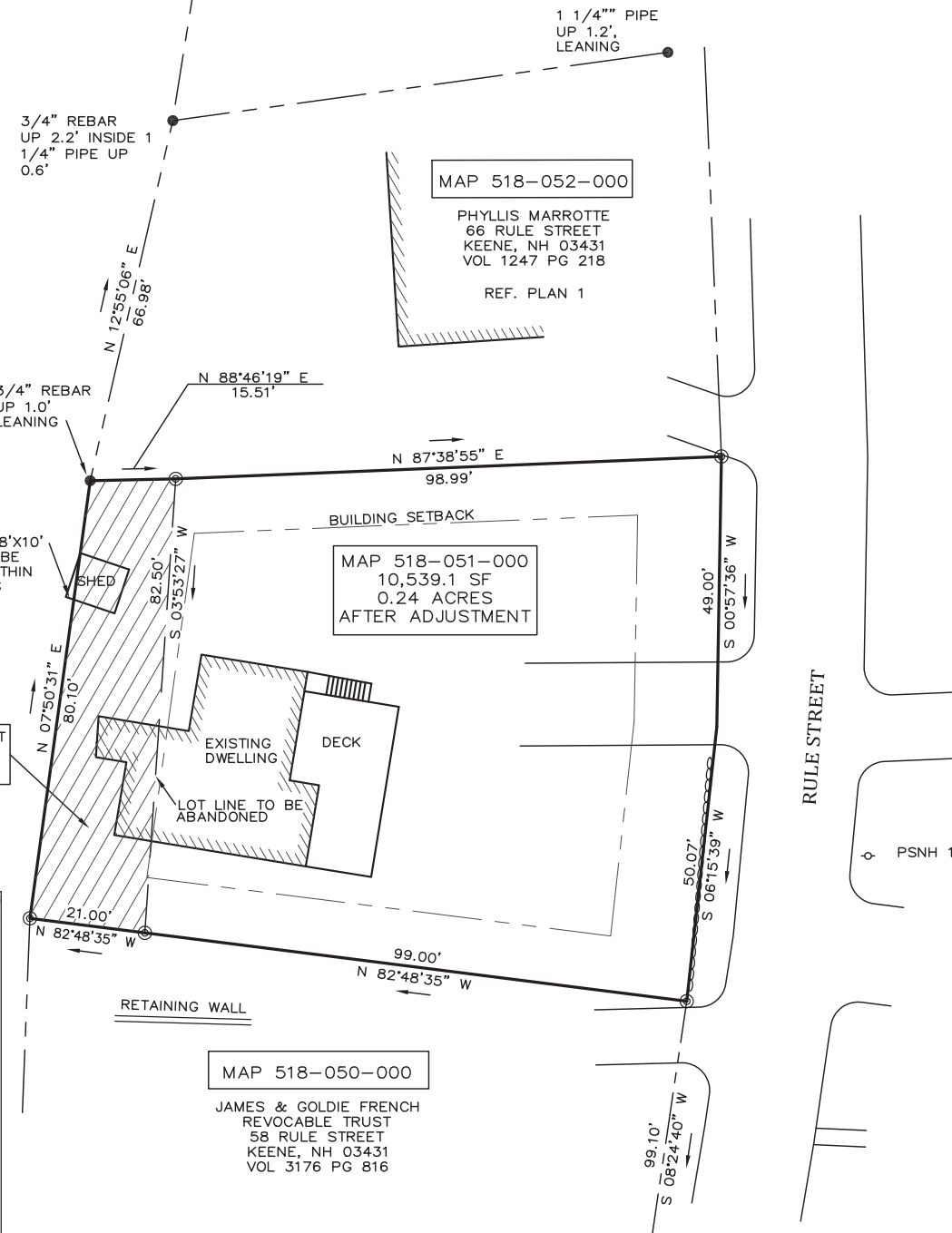
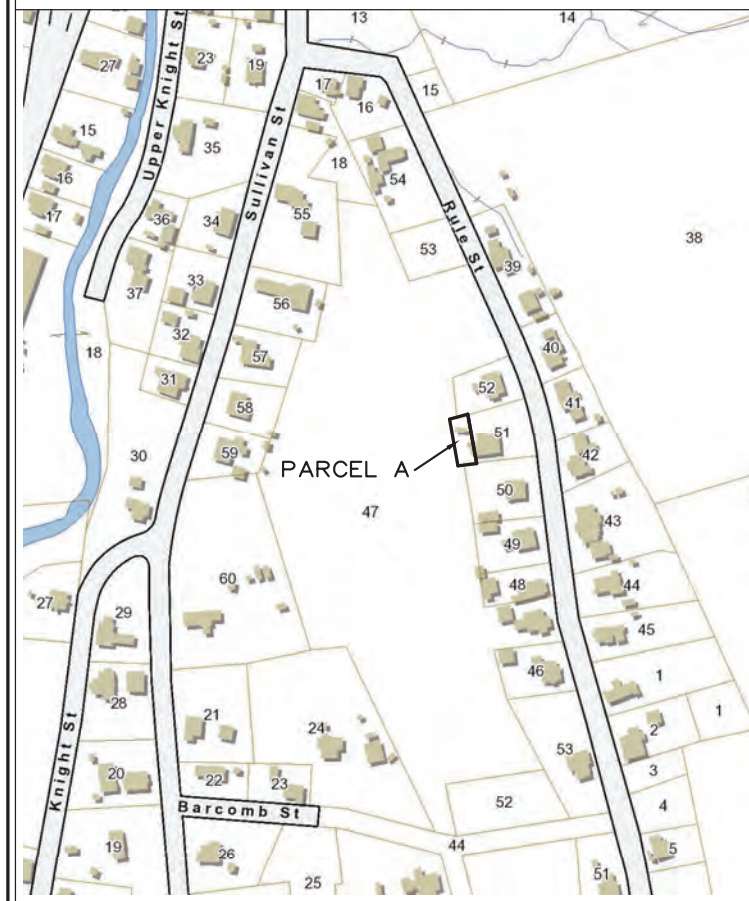
ZONING

LOW DENSITY
MIN. LOT AREA: 10,000 SF
MIN. FRONTAGE: 60 FT
MIN. LOT WIDTH AT BLDG LINE: 70 FT.
MIN. FRONT SETBACK: 15 FT.
MIN. SIDE SETBACK: 10 FT.
MIN. REAR SETBACK: 20FT.

LEGEND

- - PIN FOUND
- - REBAR SET
- — — — — STONE WALL
- - - - - APPROXIMATE ABUTTER LINE
- - - - - BUILDING SETBACK LINE

PARCEL A TO BECOME PART
OF MAP 518 LOT 51
1478.1 SF OR 0.03 ACRES

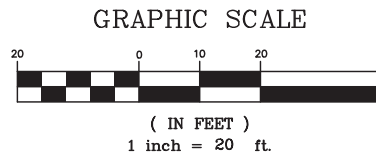


SURVEYOR'S CERTIFICATION

THIS SURVEY IS THE RESULT OF A RANDOM TRAVERSE USING AN ELECTRONIC TOTAL STATION AND MEETS THE MINIMUM REQUIREMENTS OF AN URBAN SURVEY AS SPECIFIED IN NH LAN TABLE 500.1.

I HEREBY CERTIFY THAT THIS PLAT CONFORMS TO ALL APPLICABLE LOCAL ZONING ORDINANCES AND RULES.

APPROVED AS A BOUNDARY LINE ADJUSTMENT
IN ACCORDANCE WITH THE PROVISIONS OF RSA 674.
THE CITY OF KEENE PLANNING BOARD
BY _____, CHAIRMAN



NOTES

- THE PURPOSE OF THIS PLAN IS A BOUNDARY LINE ADJUSTMENT BETWEEN MAP 518-051-000 AND MAP 518-047-000.
- OWNERS OF RECORD:
MAP 518-051-000: PILOT REALTY LLC, 210 LEDGEWOOD ROAD, MANCHESTER, NH 03104, BOOK: 3166 PAGE: 78
MAP 518-047-000: ELFRIEDE WAGNER, 50 RULE STREET, KEENE, NH 03431, BOOK: 1119 PAGE: 285
- AREAS:
MAP 518-051-000: EXISTING: 9,061 SF OR 0.21 AC. PARCEL A: +1,478.1 SF OR 0.03 AC. ADJUSTED: 10,539.1 SF OR 0.24 AC
MAP 518-047-000: EXISTING: 6 ACRES PER TAX MAP PARCEL A: -0.03 AC ADJUSTED: 5.97 AC
- MAP AND LOT NUMBERS REFER TO THE CITY OF KEENE TAX MAPS.
- THE RIGHT OF WAY OF RULE STREET WAS DETERMINED BY THE LOCATION OF STONEWALLS, MONUMENTATION, AND THE CENTERLINE OF THE TRAVELED WAY AND IS SHOWN TO BE 33' WIDE BASED ON LAYOUT ON FILE AT THE CITY OF KEENE CLERK'S OFFICE.
- THE PARCEL MAY BE SUBJECT TO EASEMENTS AS THEY EXIST OF RECORD OR IN FACT. CARDINAL SURVEYING AND LAND PLANNING DOES NOT INTEND OR REPRESENT THAT ALL RIGHTS AND EASEMENTS AFFECTING THE SUBJECT PROPERTY ARE SHOWN. A SPECIFIC TITLE EXAMINATION IS SUGGESTED TO DETERMINE THE NATURE AND EXTENT OF RIGHTS AND EASEMENTS AFFECTING THE SUBJECT PROPERTY.
- THE LOCATION OF ANY UTILITY INFORMATION SHOWN ON THIS PLAN IS APPROXIMATE. CARDINAL SURVEYING & LAND PLANNING MAKES NO CLAIMS TO THE ACCURACY OR COMPLETENESS OF THE UTILITIES SHOWN. FIELD VERIFICATION IS REQUIRED PRIOR TO ANY EXCAVATION ON THE SITE.

REFERENCE PLANS

- "PROPERTY OF DAVID CROCKETT"; PREPARED BY DIBERNARDO ASSOCIATES; DATED SEPT 9, 1987; SCALE 1"=20'; RECORDED AT CCRD PLAN BOOK 57 PAGE 99.

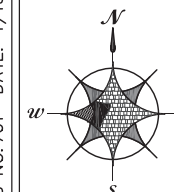
1	3/14/22	ADDRESS STAFF COMMENTS	WP
1	2/23/22	REDUCE ADJUSTMENT	WP
NO.	DATE	REVISION	BY

BOUNDARY LINE ADJUSTMENT
MAP 518-051-000 & MAP 518-047-000
50 & 62 RULE STREET
KEENE, NH 03431

DATE: FEBRUARY 15, 2022 SCALE: 1"=20'

PREPARED FOR:

PILOT REALTY LLC
AND
ELFRIEDE WAGNER



CARDINAL SURVEYING & LAND PLANNING
Sullivan, New Hampshire 03445
Tel. (603) 499-6151 SHT 1/1

JOB NO: 701 DATE: 1/13/22 FB: 10/64

STAFF REPORT

SPR-927, Modification #11B — SITE PLAN REVIEW – Panera Bread – 2 Ash Brook Rd #12

Request:

Applicant KPR Centers LLC, on behalf of owner Keene MZL LLC, proposes to renovate the north tenant space in the building located at 2 Ash Brook Rd #12 (TMP# 109-027-000-010-012) for use as a Panera Bread Restaurant with a drive-through window. This site is 1.64 ac in size and is part of the larger Monadnock Marketplace plaza located in the Commerce District.

Background:

12 Ash Brook Road #12 is located on the northwest corner of the NH Route 9 and Ash Brook Road intersection. It is part of the larger, 70.5-acre Monadnock Marketplace development, in the former location of the Olive Garden Restaurant.

In November 2015, the Planning Board approved SPR-927 Modification #11 for the construction of a 10,664 sf mixed-use building with four tenant spaces on the site of the former Olive Garden Restaurant, commonly referred to as the “Small Shops Building.” At this time, the Applicant noted that the intention was to fill three tenant spaces with retail uses and the fourth space with a restaurant with an outdoor patio.

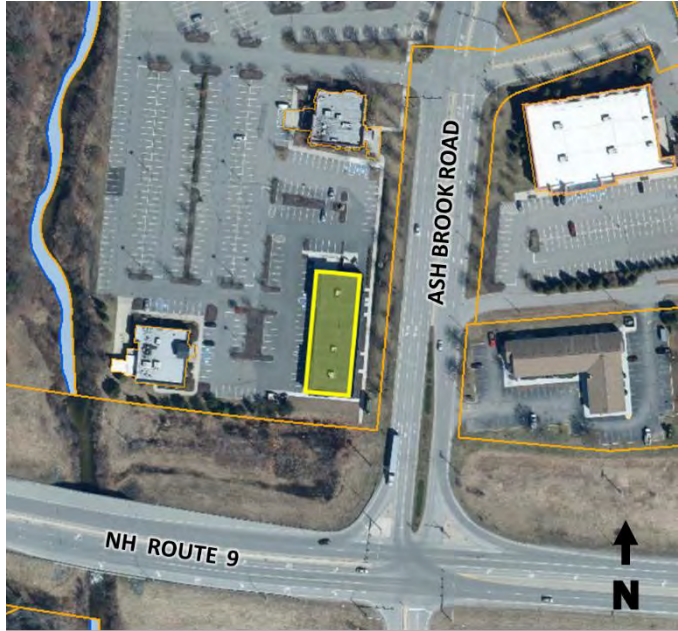


Figure 1. Aerial image of the 12 Ash Brook Road #12 property (“Small Shops Building”), outlined in yellow.

Current tenants include Kay Jewelers, Yankee Candle, and Game Stop. The proposal is to renovate the 3,634-sf tenant space at the north end of the building for use as a Panera Bread Restaurant with 70 indoor seats, 24 outdoor seats, and a drive-through window. A drive-through lane is proposed within the parking lot area immediately to the north of the existing building.

Completeness:

The Applicant requests an exemption from submitting a Lighting Plan, a Drainage Report, a Soil Analysis, an Historic Evaluation, and a Screening Analysis. Staff recommend that the Board grant the requested exemptions and accept the application as “complete.”

Departmental Comments:

- Code Enforcement: No issues related to floodplain. Please be aware that a building permit and sign permit will be required.
- Engineering: Please provide a vehicle trip analysis for the proposed drive-through lane.
- Fire & Police: No comments

STAFF REPORT

Application Analysis: The following is a review of the Planning Board development standards relevant to this application.

- 20.2 **Drainage and Stormwater Management:** The Applicant notes that no changes are proposed to the existing drainage system that was designed for this site. Overall, there will be a reduction of 200 sf in impervious surfaces as a result of the proposed changes. Due to the fact that impervious surfaces are decreasing, the Applicant has requested an exemption from providing a Drainage Report. In lieu of a full report, the Applicant submitted a letter prepared by an engineer stating that the existing drainage pattern will be maintained. No new LID measures are proposed. This standard appears to be met.
- 20.3 **Sediment & Erosion Control:** During construction, the Applicant proposes to install silt fencing along the south end of the disturbed area and silt socks at all existing catch basins. In addition, the perimeter of any stockpiles will be protected with silt fencing, and they will be seeded and mulched if left in place for more than 21 days. Post-construction, Sheet N-1 of the plan set specifies that all finished graded areas shall be seeded, fertilized, and mulched within 72 hours of finish grading. This standard appears to be met.
- 20.4 **Snow Storage & Removal:** No changes are proposed to the existing snow storage and removal practices, which include using remote parking spaces for storage and removing excess snow when necessary. This standard appears to be met.
- 20.5 **Landscaping:** The Applicant proposes to remove several small sections of existing planting beds (areas shown in orange in Figure 2) and install new landscaping areas (areas shown in green in Figure 2). The Applicant proposes to install a mix of 31 shrubs and 36 perennials in the new landscaping areas. No existing trees are proposed to be removed. During construction, the Applicant proposes to protect the root system of existing trees by installing temporary fencing, as shown on Sheet LA-1. This standard appears to be met.

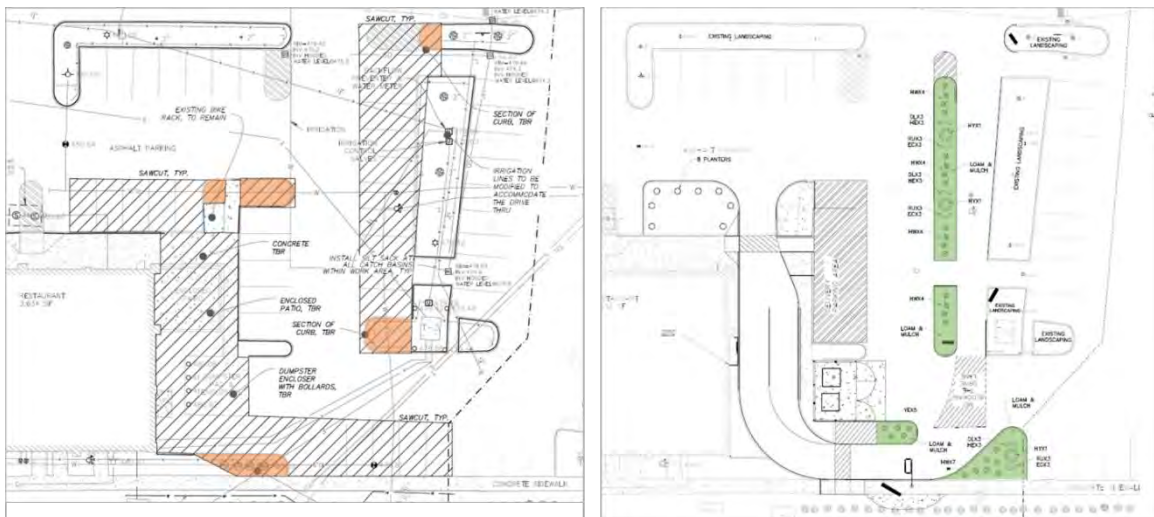


Figure 2. Excerpts from the proposed Demolition Plan (left) and proposed Landscaping Plan (right) that show the landscaping areas that are proposed to be removed in orange, and the landscaping areas that are proposed to be added in green.

STAFF REPORT

- 20.6 Screening: The Applicant proposes to relocate the existing dumpster and dumpster enclosure from the north side of the building to the north side of the drive-through lane. The dumpster will continue to be screened by a six foot tall fence. The Applicant notes in the project narrative that the rooftop HVAC equipment will be screened from view by the existing roof parapet, which is 4' high on the east and north walls and 7'-8" high on the northwest and west wall over the Panera Space. The proposed roof top unit will be a total of five feet tall (including the base) and will be set back 20 feet from the edge of the roof. This standard appears to be met.
- 20.7 Lighting: The Applicant proposes to install six wall-mounted lights, including four on the north façade (along the drive-through) and two on the west façade. The proposed light fixture is an LED wall sconce with an "Antique Bronze" finish. The color temperature of the proposed light fixtures is 3000K, and the color rendering index (CRI) is 90. The Applicant has requested an exemption from providing a photometric plan as no changes to the existing parking lot lights are proposed at this time. This standard appears to be met.
- 20.8 Sewer & Water: No changes to the existing water and sewer service for this site are proposed. This standard does not apply.
- 20.9 Traffic & Access Management: The Applicant submitted a Trip Generation Analysis prepared by VHB, which concludes that the proposed Panera restaurant with a drive-through window would not change the trip generation estimates for the previously approved Small Shops building as there would be no change in the overall square footage of the building. This analysis also notes that the 95th percentile drive-through queue length would reach a maximum of 6 vehicles, assuming a 2-minute processing time at the pick-up window (the franchisee estimates a processing time of 1 minute). This calculation also assumes that during the peak hour, about 30% of total customer transactions will occur at the drive-through window. As proposed, the drive through area would have enough space to accommodate 10-11 vehicles.
- No changes are proposed to the existing access to the site. With respect to internal traffic flow, the Applicant proposes to stripe a "No Blocking the Drive Lane" area to prevent queuing vehicles for the drive-through from blocking the flow of vehicular traffic in that area of the parking lot. The Applicant has identified the six parking spaces on the north side of the drive-through lane as a "delivery parking area." There is an accessible path of travel from these spaces to the front door of the building as well as the rear (east) side of the building.
- 20.10 Filling & Excavation: This project does not involve the commercial taking of earth. No fill will be placed in the floodplain, and the project will not impact any wetland areas or the Shoreland Protection Area. This standard does not apply.
- 20.11 Surface Waters & Wetlands: There are no surface waters present on the site; however, Ash Swamp Brook is located about 300 feet to the west of the proposed development. No development is proposed within the Surface Water Protection buffer or the Shoreland Protection Area. This standard does not apply.
- 20.12 Hazardous or Toxic Materials: There are no known hazardous or toxic materials on the site. This standard does not apply.

STAFF REPORT

20.13 Noise: The Applicant notes that the proposed restaurant would not generate excessive noise. There are no adjacent residential properties. This standard appears to be met.

20.14 Architecture & Visual Appearance: No changes to the architecture of the building are proposed; however, the Applicant does propose to change the colors on this section of the building to match the corporate Panera aesthetic. These colors include charcoal gray for the building cornice and brick columns, “Berber white” (a tan/cream color) for the EIFS panels and remaining brick areas, and dark green for the prefabricated aluminum canopy over the drive-through window. Figure 3 provides a comparison between photos of the existing building facades (top) and the proposed color elevations (bottom) for the west and east sides of the building.



Figure 3. Comparison between the existing (top) and proposed (bottom) colors for the northern tenant space of the Small Shops building.

Recommended Motion:

If the Board is inclined to approve this request, the following language is recommended for a motion:

Approve SPR-927, Modification #11B for renovations to the northern tenant space in the Small Shops building located at 2 Ash Brook Road #12, as presented in the plan set identified as “Panera Drive Thru-Lot 16B” prepared by Brickstone Land Use Consultants and SVE Associates at a scale of 1 inch = 20 feet on February 18, 2022 and last revised on March 4, 2022 with the following conditions prior to signature by Planning Board Chair:

1. Submittal of security for erosion control, landscaping, and as-built plans in a form and amount acceptable to the City Engineer.
2. Owner’s signature appears on the plan.

City of Keene, NH

Planning Board Major/Minor Project Application



If you have questions about how to complete this form, please call: (603) 352-5440 or email: communitydevelopment@keene-nh.gov

SECTION 1: PROJECT INFORMATION		
PROJECT NAME: SPR 927 MOD # PANERA WITH DRIVE THRU	TYPE OF APPLICATION BEING SUBMITTED: <input checked="" type="checkbox"/> MAJOR PROJECT APPLICATION <input type="checkbox"/> MINOR PROJECT APPLICATION	
PROJECT ADDRESS(ES): 2 ASH BROOK ROAD #12		
SECTION 2: CONTACT INFORMATION		
PROPERTY OWNER	APPLICANT	
NAME/COMPANY: Keene MZL, LLC	NAME/COMPANY: KPR Centers LLC	
MAILING ADDRESS: 254 West 31st St 4th Floor New York, NY 10001	MAILING ADDRESS: 338 Steele Road West Hartford CT 06117-2231	
PHONE: 212-710-9360	PHONE: 212-710-9366	
EMAIL: N/A	EMAIL: sglantz@kprcenters.com	
SIGNATURE: 	SIGNATURE: 	
PRINTED NAME: Stan Glantz, authorized agent for Keene MZL, LLC	PRINTED NAME: Stan Glantz, Authorized agent for KPR Centers LLC	
PROPERTY OWNER (if different from Applicant)	PROPERTY DESCRIPTION	
NAME/COMPANY: James Phippard / Brickstone Land Use Consultants	TAX MAP PARCEL #(s): 109 027-000 010-012 109-027 000 000 000	
MAILING ADDRESS: 185 Winchester Street Keene NH 03431	PARCEL SIZE: 70.5 acres	<div style="border: 2px solid blue; padding: 10px; width: fit-content; margin: auto;"> <p style="font-size: 2em; margin: 0;">RECEIVED</p> <p style="font-size: 1.2em; margin: 0;">FEB 18 2022</p> <p style="margin: 0;">By _____</p> </div>
PHONE: 603-357-0116	ZONING DISTRICT: Commerce	
EMAIL: jphippard@ne.rr.com	PROJECT #: SPR-927, Mod #11B	
SIGNATURE: 		
PRINTED NAME: James Phippard		



Keene MZL LLC
c/o Katz Properties Retail

2/25/2022

Megan Fortson
Planning Technician
City of Keene, Community Development Department
3 Washington Street
Keene, NH 03421

Re: Major Site Plan Application (SPR-927, Mod. 11B)

Please accept this letter as authorization for Stanley Glantz to sign the Major Site Plan Application (SPR-927, Mod. 11B) for the relocation of Panera to the Monadnock Marketplace Plaza as authorized agent for Keene MZL LLC.

Best Regards,

A handwritten signature in black ink, appearing to read 'Dan Kaufthal', written over a horizontal line.

Daniel Kaufthal | Managing Partner
dnk@kprcenters.com | 212.710.9363

Site Plan Application Narrative

Modification to SPR-927, Monadnock Marketplace, Phase II Panera Bread with Drive-Thru at Small Shops, Lot 16B

**2 Ash Brook Road
Keene, NH**

February 18, 2022

Keene MZL, LLC is the owner of Monadnock Marketplace in Keene. This is a 70.5 acre site occupied by approximately 500,000 sf of mixed retail and restaurant uses and is located in the Commerce District. This proposal is a modification to SPR-927 which will add a Panera Bread restaurant with a drive-thru in a vacant space in the north end of the Small Shops building. This is the former Olive Garden restaurant location at Monadnock Marketplace.

The Panera Bread restaurant will occupy 3634 sf at the north end of the existing building. The drive-thru lane will be added in the existing parking lot by eliminating and reconfiguring some of the existing parking spaces and adding new islands to control traffic circulation. Queueing for up to ten cars will be provided.

The restaurant will have approximately 70 indoor seats and approximately 24 outdoor seats on a newly constructed patio area at the front of the building. Zoning requires 47 parking spaces for the Small Shops building. 82 parking spaces will be provided.

The existing parking lot lights remain unchanged. We will be adding wall mounted lights at the Panera storefront and north side wall. All wall mounted lights will be full cutoff LED fixtures permitting downlighting only.

Because this is an existing developed shopping center, no new LID measures are proposed. The existing drainage system was previously designed for a 50 year design storm and contains LID measures which include: hooded outlet pipes, grass lined drainage swales, and a stormwater wetland for detention and stormwater treatment.

No changes are proposed to the remaining restaurants and parking areas outside the Small Shops limited common area.

Exemptions are requested from providing a formal drainage report, a formal traffic report, a lighting plan and from checklist items which do not exist on the property or are not applicable. A letter from SVE Associates regarding drainage is provided indicating there is no increase in runoff from the proposal since we are eliminating approximately 200 sf of impervious surface. The changes in traffic with the proposed change in use are not expected to be significant and fall well below the threshold requiring a new traffic study.

Site Plan Review Modification Narrative

Modification to SPR-927, Monadnock Marketplace, Phase II Small Shops, Lot 16B (Former Olive Garden Site) 2 Ash Brook Road Keene, NH

February 18, 2022

Site Development Standards

- 20.2 Drainage & Stormwater Management-** The existing drainage patterns at the site will be maintained. There will be no increase in runoff as a result of the modifications. A waiver from providing a drainage report is requested since we are eliminating approximately 200 sf of impervious surface. See attached letter from SVE Associates.
- 20.3 Sediment/Erosion Control –** Sediment and erosion control will be provided using silt fencing during construction at the disturbed areas and inlet protection (silt socks) at existing catch basins.
- 20.4 Snow Storage & Removal –** No changes proposed. Snow is stored in remote parking spaces and removed from the site after each snowstorm as needed.
- 20.5 Landscaping –** No existing trees are affected by this modification. New shrubs, perennials and grasses will be planted along the queue line in a new median and at the new parking lot islands. Planters will be added around the perimeter of the new patio area at the front of the restaurant. New plantings will match the existing plantings within that lease area. New plantings will include Hydrangea trees, juniper, yews, perennials, rudbeckia, echinacea, and heuchera. Zoning requires 1329 sf of landscaped area within the parking lot. 6372 sf of landscaped area is provided.
- 20.6 Screening –** The dumpster enclosure will be relocated as shown and screened with 6' high solid fencing. An existing building parapet screens rooftop HVAC units.
- 20.7 Lighting –** No existing parking lot lights will be changed and no new parking lot lights will be added. Wall mounted lights will be added to the front (west side) of the restaurant and along the north side of the building. Each fixture is a full cutoff LED fixture, downlight only. A lighting cut sheet is attached.
An exemption from providing a photometrics plan is requested since the only change in lighting is the additional wall mounted lights.
- 20.8 Sewer & Water –** No changes to the existing water and sewer services are proposed.

20.9 Traffic & Access Management – The original site plan approval for the Small Shops building included a 2600 sf restaurant within the building. The Panera restaurant will be 3634 sf with up to 70 indoor seats and 24 outdoor seats, and will also add a drive-thru lane and pickup window. Panera reports that this size restaurant will average approximately 500 tickets per day which equates to approximately 500 vehicle trips per day.

The previous use at this site was an Olive Garden restaurant which generated approximately 800 vehicle trips per day. The Small Shops are part of the Monadnock Marketplace, a large shopping center. The ITE Trip Generation Manual estimates this shopping center space will generate approximately 600 vehicle trips per day.

The addition of the drive-thru lane will not result in a significant change to traffic at the shopping center.

20.10 Filling & Excavation – Minor filling and excavation will be necessary to add the drive-thru lane and construct the new patio area. Trucks will haul materials from NH Rt. 9 to Ash Brook Road to the building site. No filling is proposed within the 100 year floodplain or within the Shoreland Protection area for Ash Swamp Brook.

20.11 Surface Waters & Wetlands – Ash Swamp Brook is located approximately 300 feet west of the development site. There will be no impacts to Ash Swamp Brook resulting from this proposal.

20.12 Hazardous or Toxic Materials – The applicant has no knowledge of hazardous or toxic materials at this site.

20.13 Noise – No excessive noise will result from this proposal.

20.14 Architecture & Visual Appearance - The applicant is proposing to maintain the existing architectural design features in the Small Shops building. These features include brick columns with split face block bases, brick and block areas of façade, raised parapet sections with a continuous cornice at the roof line, color EIFS panels, and tile accent areas. The existing block, brick and EIFS wall sections will be painted as shown on the attached color building elevations.

The dark bronze aluminum storefront will be maintained with new door locations for access to the revised floor plan. The dark fabric awnings along the existing storefront will be replaced with a new fabric awning, slightly different in style but also dark in color. The new awnings will be installed on the west and north elevations over the glassed areas and on the east elevation where the wall sections are capped with existing parapets.

The wide concrete walkway along the front of the building will be maintained and an outdoor patio/seating area will be added at the storefront. The patio will be surrounded with shrubs in large planters during the season to provide an urban pedestrian scale appropriate for the small shops. Overall, this design provides visual interest and a pedestrian scale creating a clean urban setting which is compatible with Monadnock Marketplace.

SVE Associates

Engineering * Surveying * Landscape Architecture * Planning

February 18, 2022

Keene Planning Board
City of Keene
3 Washington Street
Keene, NH 03437

Re: SPR-927 - Site Plan Modification Application – 2 Ash Brook Road #12

Dear Keene Planning Board:

On behalf of the applicant, Keene MZL, LLC, we respectfully request a waiver of full compliance with Development Standard 20.2, Drainage & Stormwater Management, specifically the submission of the drainage report. Our application is seeking approval to construct drive thru at the “Small Shoppes” building in the Monadnock Plaza. There is a net decrease in impervious surface by 200 sf. The existing stormwater runoff from this area sheet flows across the existing parking lot to the existing catch basins. The proposed drive thru maintains existing drainage patterns.

We think the sheet flow of this stormwater runoff to the existing catch basins meets the spirit and intent of the regulations. There is no net increase in impervious surfaces, therefore we see no adverse impacts to the abutters, community or environment, and no diminution of abutting property values. Creating a professionally prepared drainage report with calculations seems an unnecessary expense and exercise given there is no net increase in impervious surfaces, and the existing drainage patterns will be maintained.

Thank you for your consideration of our waiver request.

Respectfully submitted,

SVE Associates

Liza Sargent

Liza Sargent, P.E.

Panera Drive Thru-Lot 16B

ASH BROOK ROAD & ROUTE 9, KEENE, NEW HAMPSHIRE

OWNER:

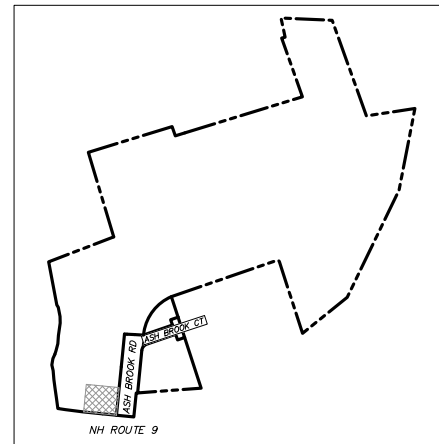
Keene MZL LLC

254 WEST 31st St., 4th FLOOR
 NEW YORK, NY 10001
 (212) 710-9360

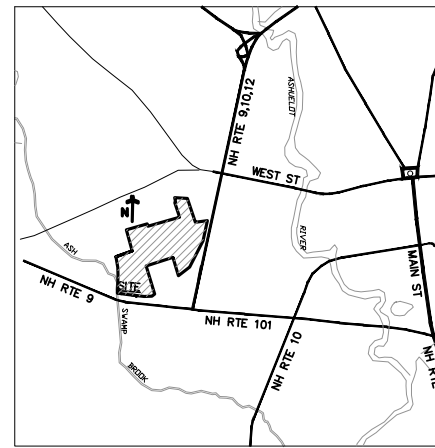
APPLICANT:

KPR Centers, LLC

338 STEELE ROAD
 WEST HARTFORD, CT 06117-2231
 (212) 710-9366



SITE



KEENE, NH

Project K2462C
PREPARED BY

Civil Engineer:

SVE Associates

439 WEST RIVER ROAD
 BRATTLEBORO, VT 05302

PHONE (802) 257-0561
 FAX (802) 257-0721

Development Consultant:

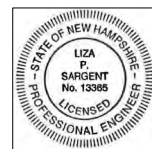
Brickstone Land Use Consultants

185 WINCHESTER ST.
 KEENE, NH 03431

PHONE (603) 357-0116

INDEX OF PLANS

- N-1 NOTES & LEGEND
- S-1 EXISTING CONDITIONS
- D-1 DEMOLITION PLAN
- C-1 SITE PLAN
- C-2 GRADING PLAN
- C-3 CONSTRUCTION DETAILS
- LA-1 LANDSCAPING PLAN



Liza Sargent 2/18/22
 LIZA P. SARGENT DATE
 R.C.E. NUMBER: 13365

APPROVED BY THE APPLICANT:

ON _____

INSPECTION PERMISSION: UPON APPROVAL OF THIS SITE PLAN, THE OWNER GRANTS PERMISSION FOR THE MEMBERS OR AGENTS OF THE KEENE PLANNING BOARD TO INSPECT THIS SITE AS NECESSARY.

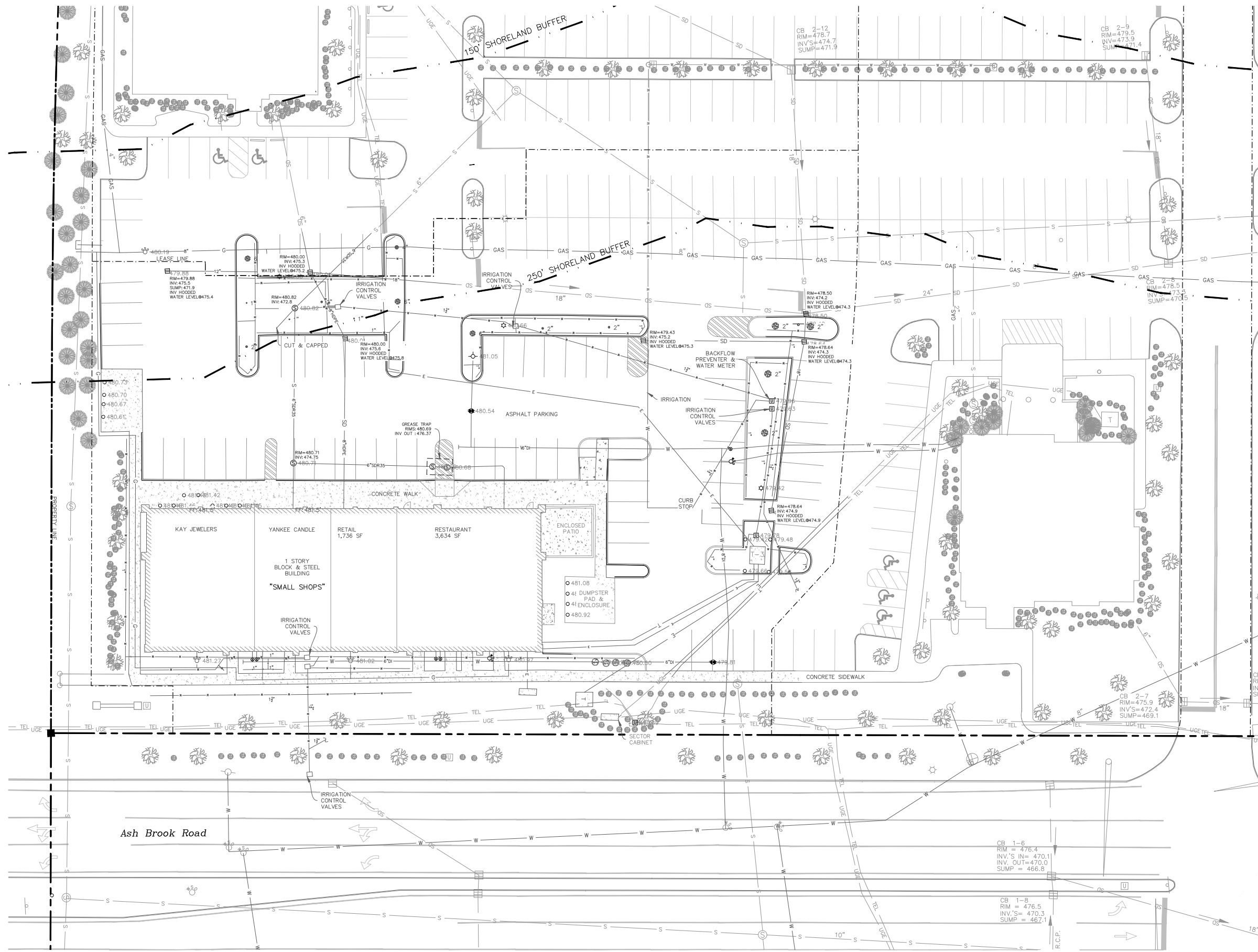
APPROVED BY THE KEENE PLANNING BOARD

ON _____

CERTIFIED BY CHAIRMAN _____

February 18, 2022

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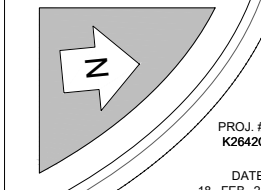
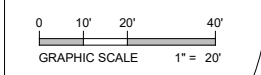
Liza Sargent 2/18/22
 LIZA P. SARGENT DATE
 R.C.E. NUMBER: 13365

NO.	REVISION	DATE	DWN	CHK

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 Engineering
 Planning
 Landscape Architecture
 Surveying

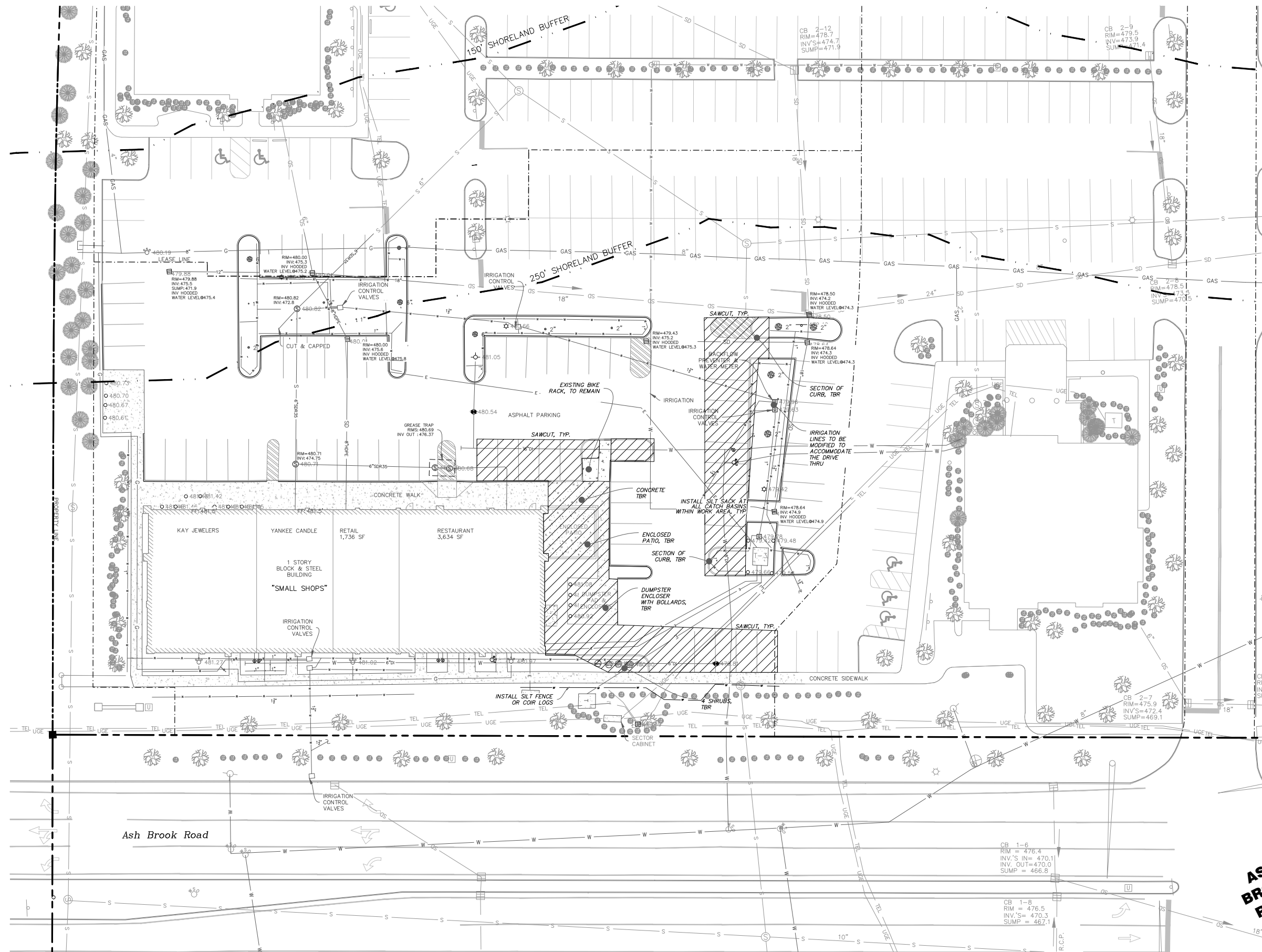
 SVE Associates
 P.O. Box 1818
 439 West River Road
 Brattleboro, VT 05302
 T 802.257.0561
 F 802.257.0721
 www.sveassoc.com

EXISTING CONDITIONS PLAN
 PANERA DRIVE THRU
 KEENE MZL, LLC
 254 WEST 31st St., 4th FLOOR
 NEW YORK, NY 10001



PROJ. #
 K2642C
 DATE:
 18 - FEB - 22
 DESIGN:
 DRAWN: LPS
 SHEET
 Page 38 of 70 **S-1**

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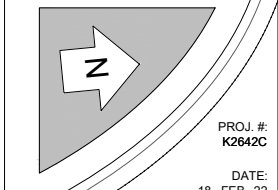
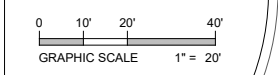
Liza Sargent 2/18/22
 LIZA P. SARGENT DATE
 R.C.E. NUMBER: 13365

NO.	REVISION	DATE	DWN	CHK

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 Engineering
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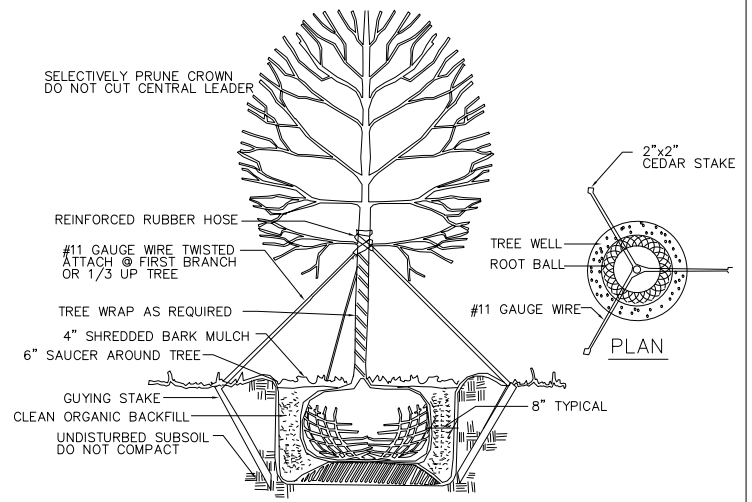
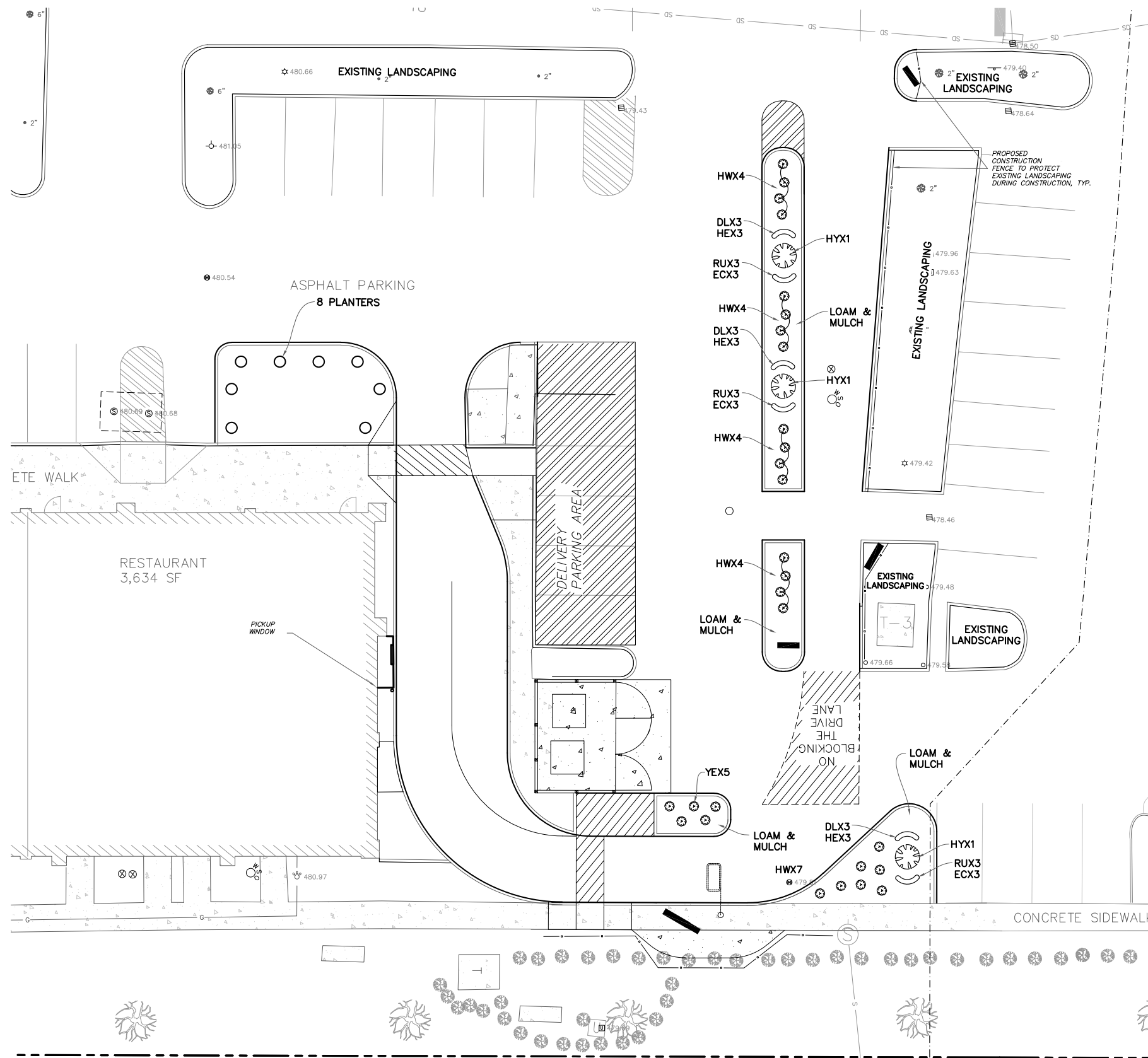
DEMOLITION PLAN
 PANERA DRIVE THRU
 KEENE MZL, LLC
 254 WEST 31st St., 4th FLOOR
 NEW YORK, NY 10001



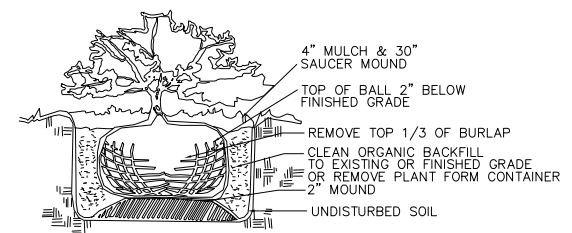
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 Page 39 of 20

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 DATE:
 18-FEB-22

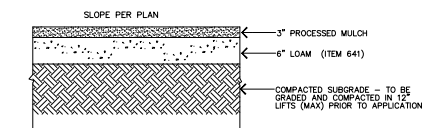
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TREE PLANTING & GUYING DETAIL
NO SCALE



SHRUB PLANTING DETAIL
NO SCALE



1. SPREAD LOAM TO SPECIFIED DEPTH AND RAKE TO REMOVE ALL UNSUITABLE MATERIALS AND STONES LARGER THAN 1 INCH. ROLL LOAM TO COMPACT.
2. SPREAD LIMESTONE AT A RATE OF 80 LBS/1000 SF OF SURFACE AREA
3. FERTILIZE WITH 10-10-10 FERTILIZER AT A RATE OF 20 LBS/1000 SF
4. SPREAD MULCH IN AREAS SHOWN ON PLAN.

LOAM AND MULCH DETAIL
NOT TO SCALE

LANDSCAPE LEGEND

TYPE	QUANTITY	PLANTING SIZE	MATURE HEIGHT
HW WILTON HORIZ. JUNIPER (BLUE RUG)	23 EA	3 GAL	4 FEET
HY PINKY WINKY HYDRANGEA TREE	3 EA	5 GAL	12 FEET
YE UPRIGHT YEW	5 EA	3 GAL	6 FEET
RU RUDBECKIA	9 EA	1 GAL	PERENNIAL
EC ECHINACEA	9 EA	1 GAL	PERENNIAL
DL DAYLILY (HAPPY & ROSY)	9 EA	1 GAL	PERENNIAL
HE HEUCHERA	9 EA	1 GAL	PERENNIAL

PARKING LOT LANDSCAPING
LANDSCAPING REQUIRED - 82X9X18X0.1=1,329 SF
LANDSCAPING PROVIDED - 6,372 SF

NO.	REVISION	DATE	DWN	CHK
1	REVISED PER PB STAFF COMMENTS	04-MAR-22	LPS	LPS

SVE

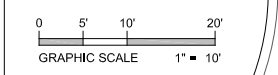
Engineering
Planning
Landscape Architecture
Surveying

SVE Associates
P.O. Box 1818
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Brattleboro, VT 05302
T 802.257.0561
F 802.257.0721
www.sveassoc.com

LANDSCAPING PLAN

PANERA DRIVE THRU

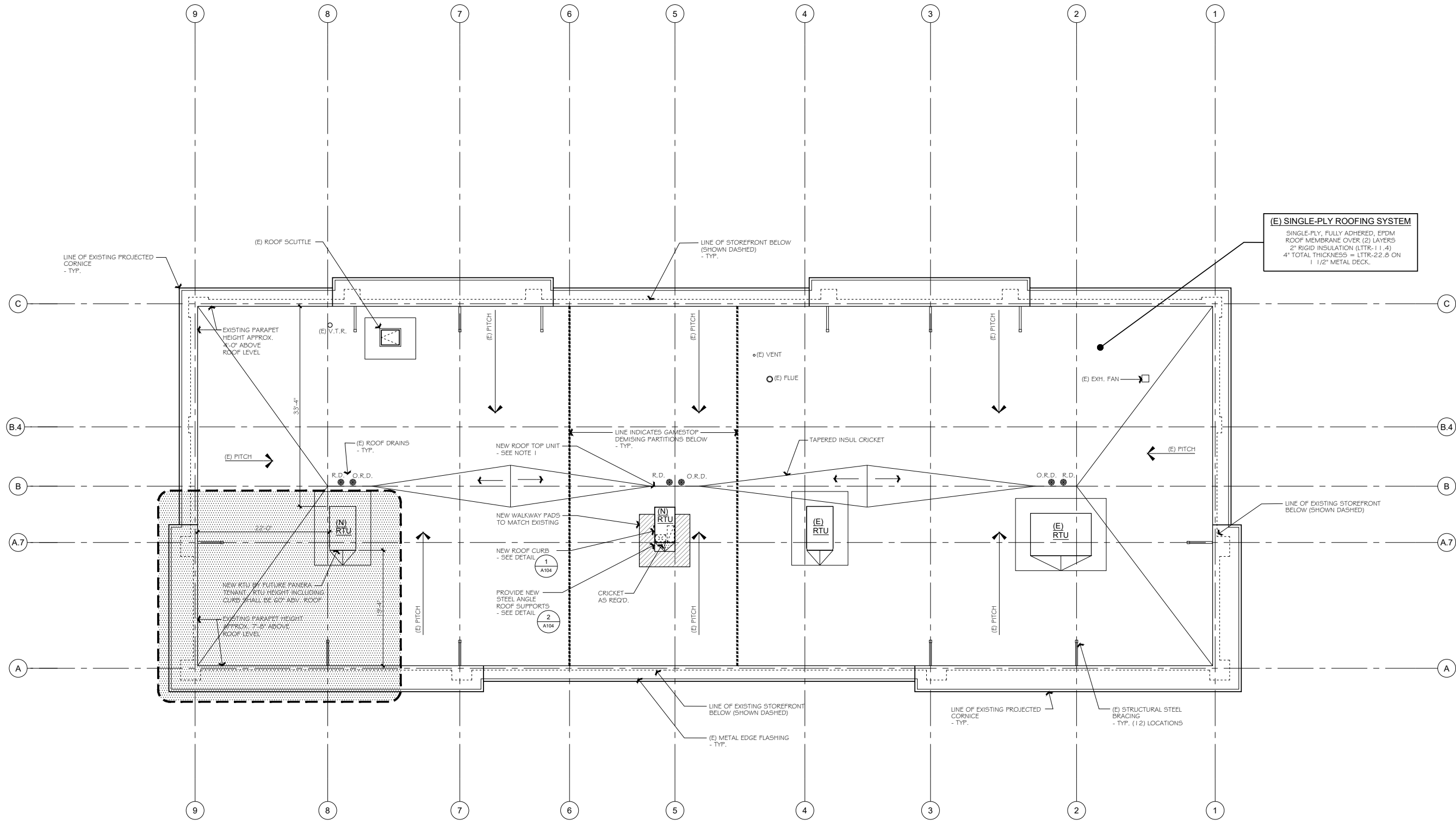
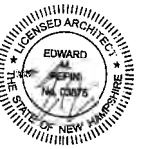
KEENE MZL, LLC
254 WEST 31st St, 4th FLOOR
NEW YORK, NY 10001



PROJ. #:
K2642C

DATE:
18-FEB-22

Certification



(E) SINGLE-PLY ROOFING SYSTEM
SINGLE-PLY, FULLY ADHERED, EPDM ROOF MEMBRANE OVER (2) LAYERS
2" RIGID INSULATION (LTR-1 1/4)
4" TOTAL THICKNESS = LTR-22.0 ON 1 1/2" METAL DECK.

ROOF PLAN
SC: 1/8" = 1'-0"

Rev. #	Date	Reason For Issue

Project - Location
MONADNOCK MARKETPLACE

TENANT IMPROVEMENTS AT FUTURE PANERA TENANT

Ash Brook Road
Keene, NH

Drawing Set Issue

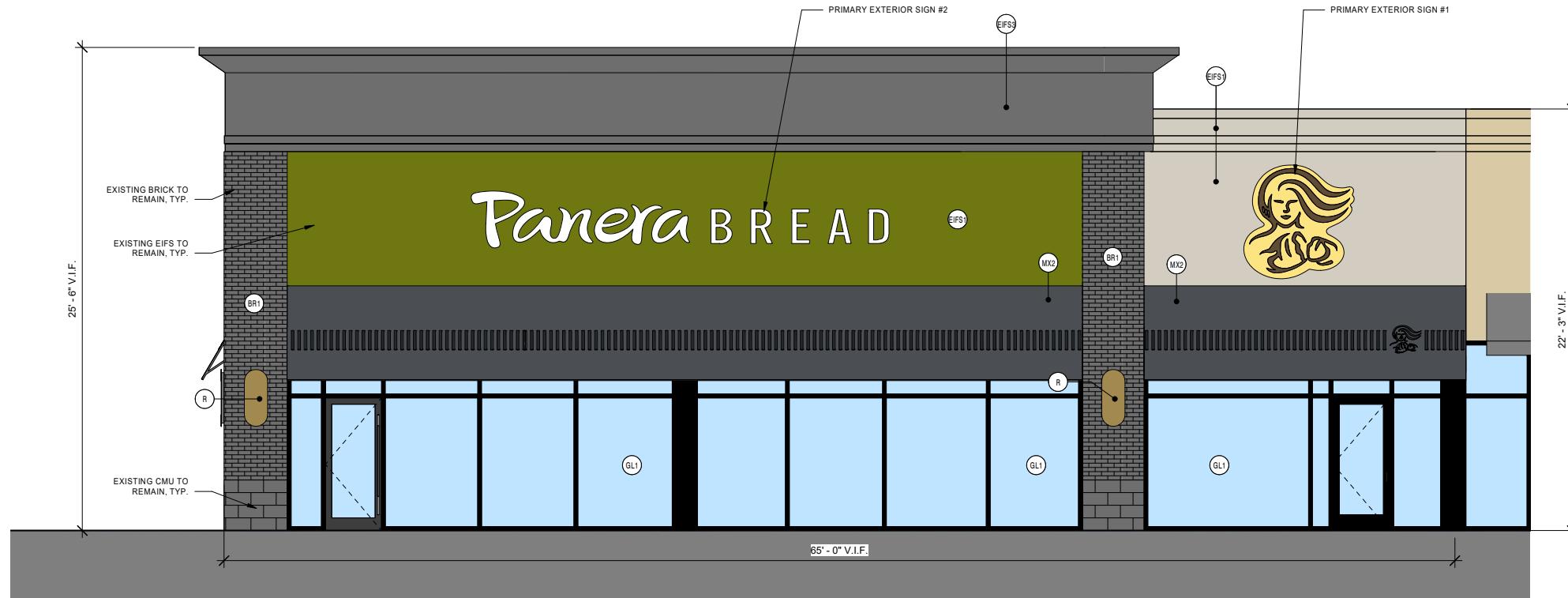
Sheet Title
ROOF PLAN AT FUTURE PANERA TENANT

Date	Revision
3/11/2022	

Project Number	Sheet Number
22-03	

Page 44 of 79 RF-1

ELEVATIONS



EXTERIOR MATERIALS:

- G1 ALUM. STOREFRONT SYSTEM, MATCH EXISTING (DARK BRONZE)
- MX1 PREFABRICATED ALUMINUM CANOPY COLOR: DARK GREEN
- EIFS1 EXISTING EIFS TO REMAIN, PAINT COLOR: MATCH BENJAMIN MOORE 955 BERBER WHITE
- EIFS2 EXISTING EIFS TO REMAIN, PAINT COLOR: MATCH PMS 2307
- EIFS3 EXISTING EIFS TO REMAIN, PAINT COLOR: MATCH BENJAMIN MOORE KENDALL CHARCOAL HC-166
- CMU1 CMU, MATCH EXISTING EXTERIOR SPLIT FACE BLOCK - PAINT KENDALL CHARCOAL HC-166
- BR1 EXISTING BRICK - PAINT KENDALL CHARCOAL HC-166
- BR2 EXISTING BRICK - PAINT BERBER WHITE 955
- R WALL SCNCE, CREO INDUSTRIAL ARTS
- MX2 PANERA BRANDED FABRIC CANOPY

West Elevation



North Elevation

ELEVATIONS



East Elevation



March 11, 2022

Ref: 52884.00

Stan Glantz
Keene MZL LLC
254 West 31st Street, 4th Floor
New York, NY 10001

Re: Trip-Generation and Drive-Through Queue Letter
Panera Restaurant within Monadnock Marketplace
Keene, New Hampshire

Dear Mr. Glantz:

As proposed, a Panera restaurant with a drive-through window will be located within the Small Shops commercial building at Monadnock Marketplace off Franklin Pierce Highway (NH Route 9) in Keene, New Hampshire. The existing 10,644 square foot Small Shops building was previously approved to replace the former 200 seat, 7,800 +/- square foot Olive Garden restaurant. At this time, a Panera restaurant with a drive-through window is proposed to occupy 3,634 square feet of the Small Shops building with the remaining space to be occupied by the existing Kay Jewelers and Yankee Candle stores, as well as 1,736 square feet of vacant commercial space. This letter has been prepared to summarize the trip-generation methodologies associated with the proposed change of commercial space to accommodate the proposed Panera restaurant.

Trip Generation Methodology

To determine the vehicular trips that would be generated by the proposed Panera restaurant with a drive-through window, trip-generation rates published in the Institute of Transportation Engineers (ITE) Trip Generation Manual¹ were researched. For the Monadnock Marketplace shopping plaza, ITE Land Use Code 820 (Shopping Center) was selected based on the associated description, "A shopping center is an integrated group of commercial establishments that is planned, developed, owned, and managed as a unit . . . Many shopping centers – in addition to the integrated unit of shops in one building or enclosed around a mall – include outparcels (peripheral buildings or pads located on the perimeter of the center adjacent to the streets and major access points). These buildings are typically drive-in banks, retail stores, restaurants, or small offices." Based on the ITE Trip Generation Handbook, the rationale is that some patrons of mixed-use or multi-use developments could visit more than one of the uses on the

¹ Trip Generation Manual. 11th ed. Washington, DC: Institute of Transportation Engineers, 2021.



site (internal trips). The existing Monadnock Marketplace development consists of various commercial uses, such as restaurants, a home improvement store, a department store, and a sporting goods store. Since the proposed Panera restaurant with a drive-through window would not change the overall square footage of the Small Shops building or the Monadnock Marketplace shopping plaza, the reallocation of approved space would not change the trip-generation estimates previously reviewed and approved for the Small Shops project (i.e., based on the overall shopping plaza square footage and not on individual uses or tenants).

Table 1 has been prepared to provide a conservative (worse-case) trip-generation methodology in which the proposed Panera restaurant is a stand-alone use without any patrons visiting any of the other use within Monadnock Marketplace.

Table 1 Trip-Generation Summary

Peak Hour/Direction	Restaurant Trips ^a
Weekday AM Peak Hour:	
<i>Enter</i>	3
<i>Exit</i>	3
<i>Total</i>	6
Weekday PM Peak Hour:	
<i>Enter</i>	25
<i>Exit</i>	21
<i>Total</i>	46
Saturday Peak Hour:	
<i>Enter</i>	65
<i>Exit</i>	53
<i>Total</i>	119

^a ITE Land Use Code 930 (Fast Casual Restaurant) for 3,634 square feet.

In accordance with ITE methodologies² and New Hampshire Department of Transportation (NHDOT) guidance,³ a development may have a noticeable impact if the addition of site trips would increase peak hour traffic volumes on an intersection approach by 100 vehicles or more. As shown in Table 1, a proposed stand-alone Panera restaurant (i.e., no internal trip credit) is not anticipated to exceed this threshold (i.e., entering trips <100 vehicles per hour, and exiting trips <100 vehicle per hour). Therefore, standard traffic engineering practice suggests that the proposed Panera restaurant would be expected to result in negligible impacts to the adjacent roadway system.

² Transportation Impact Analyses for Site Development: An ITE Proposed Recommended Practice. Washington, DC: Institute of Transportation Engineers, 2010.

³ Bollinger, Robert E. Inter-Department Communication. New Hampshire Department of Transportation, Bureau of Traffic. 17 Feb. 2010.



Drive-Through Queuing

As proposed, there would be a designated drive-through area for the proposed Panera restaurant that would provide approximately 75 feet of storage between the pick-up window and the menu ordering board with approximately 45 feet provided beyond the menu ordering board. The site plans show that there is an additional 90 feet of area to accommodate motorists destined for the drive-through lane should an occasional need for such added stacking arise. As shown on the site plans, there are proposed "No Blocking the Drive Lane" pavement markings to alert motorists not to stop or park in the area between the end of the drive-through lane and the menu preview board. Using a 20-foot long standard vehicle spacing within drive-through lanes, the proposed 210 feet of paved area associated with the Panera restaurant drive-through area would accommodate 10 to 11 vehicles without impacting on-site circulation.

Based on standard traffic engineering practice for the mathematical theory of probability, a queue algorithm was used to estimate the vehicular queues for the proposed Panera restaurant drive-through area. Vehicle queue lengths were calculated by applying franchisee provided estimates of 1 minute for processing time from the pick-up window and 30 percent of total customer transactions during the peak hours would occur at the drive-through window. This methodology resulted in the 95th percentile calculated drive-through queue length at a maximum of 2 vehicles. Further, this queueing methodology suggests that the 95th percentile calculated drive-through queue length would reach a maximum of 6 vehicles should the processing time be extended to 2 minutes. Based on this methodology, the proposed Panera restaurant drive-through lane would provide adequate storage to exceed the calculated vehicular queues. The queue length calculations are attached to this letter.

Conclusion

In accordance with ITE methodologies, the proposed Panera restaurant with a drive-through window would not change the trip-generation estimates for the previously approved Small Shops building and Monadnock Marketplace shopping plaza because there would be no change in overall square footage. In addition, the proposed Panera restaurant drive-through area is anticipated to be able to accommodate between 10 and 11 vehicles within a dedicated area. Based on a standard traffic engineering methodology, the drive-through storage area would be able to accommodate the calculated 95th percentile vehicle queues.

Sincerely,

VHB

A handwritten signature in blue ink that reads "Jason R. Plourde". The signature is written in a cursive style with a large, looping initial "J".

Jason R. Plourde, PE, PTP
Transportation Systems Team Leader



PANERA BREAD

Light Fixture Specification

ITEM#: EXT19

EXTERIOR WALL SCONCE

Revision Date: 04.18.19

APPROVED MANUFACTURER:	Progress Lighting
VENDOR MODEL#:	P5641-20/30K
DIMENSIONS:	6"W x 8.875"D x 12"H
ELECTRICAL:	29 Max. Fixture Wattage; 120 Voltage Input
BULB TYPE:	3000K; 1 LED Integrated Bulb (included);
BEAM SPREAD:	N/A
FINISHES & FIXTURE NOTES:	Antique bronze finish
ACCESSORIES:	Lense cover: P8798 (required for wet listing)
LISTING:	Wet Location
ENERGY STAR:	No
TITLE 24:	Yes





City of Keene

New Hampshire

February 16, 2022

Pamela Russell Slack, Chair
Keene Planning Board
3 Washington Street
Keene, NH 03431

**RE: Notification of Governmental Land Use
Parcel No. 113 / 007**

Dear Pamela,

On behalf of the City of Keene, I'm pleased to provide the enclosed written notification to the Keene Planning Board pursuant to New Hampshire RSA 674:54.

The Governmental Land Use in question is the proposed relocation of the City's existing salt storage activity approximately 1,000 feet west of its current location. In 2019, the City subdivided and sold a portion of the Public Works material handling yard to Frohling Energy LLC. Under the terms of that sale, the City is required to relocate its salt storage and demolish the existing shed by December 31, 2022. The City Council has appropriated funding in FY 2023. We expect site preparation to begin this spring and construction of the new facility to begin in July.

After careful evaluation of a number of potential storage sites, City staff has determined that the remaining City-owned portion of the DPW site is the preferred location. This site has a number of advantages:

- The surrounding land uses are consistent with DPW operations
- The site is unlikely to be redeveloped due to the presence of the historic City landfill
- The proposed storage area is outside of the floodplain and the Shoreland Protection buffer
- The site is conveniently located for both material deliveries and winter maintenance operations
- The proposed buildings will be well screened from public view from both Main Street and NH Route 101

In accordance with New Hampshire RSA 674:54, this notification is accompanied by plans and other details related to the proposed construction. In order to provide this information in a format that is familiar to the Board, we have used your standard Site Plan application as a framework. The RSA provides you the opportunity to hold a public hearing relative to the proposed governmental use, though it is not required. We anticipate that the Board will choose to hold such a public hearing and have included the appropriate list of abutters with the supporting materials. We welcome the opportunity to discuss the project and answer your questions at your

K:\ENGRNG\2020 projects\20-07 ROSE LANE SALT SHED\3-Design\PLANNING SUBMISSION\Cover Letter.docx

City of Keene, Public Works Department • 350 Marlboro Street • Keene, NH • 03431 • www.ci.keene.nh.us



City of Keene

New Hampshire

regularly scheduled meeting on March 28. You also have the option, but not the obligation, to issue non-binding written comments relative to the project. We welcome your input.

In keeping with longstanding City practice, the project has been designed to comply with applicable land use regulations to the maximum extent feasible. However, we are proposing some project features that deviate from the Boards regulations. Specifically:

- The proposal will result in a modest increase in peak stormwater discharge of 0.88 cfs. Due to the presence of contaminated soil and the historic landfill, we believe it is undesirable to encourage stormwater infiltration on this site.
- The project does not include defined on-site parking, general site lighting or landscaping. The site is not open to the public and is used only for public works operations. When employees are on site, it is only in City vehicles and their presence is transitory.
- The proposed storage shed will have a maximum height of approximately 36 feet. The industrial zone allows 35 feet with 50 feet permitted by special exception.

If you have any questions, please contact me at 352-6550.

Sincerely,

Donald R. Lussier, P.E.

City Engineer



City of Keene, NH Planning Board Major / Minor Project Application

If you have questions about how to complete this form, please call: (603) 352-5440 or email: communitydevelopment@keenenh.gov

SECTION 1: PROJECT INFORMATION	
PROJECT NAME: CITY OF KEENE SALT SHED RELOCATION	TYPE OF APPLICATION BEING SUBMITTED: <input type="checkbox"/> MAJOR PROJECT APPLICATION <input type="checkbox"/> MINOR PROJECT APPLICATION <input checked="" type="checkbox"/> WRITTEN NOTIFICATION PER RSA 674:54
PROJECT ADDRESS(ES): 560 MAIN STREET	
SECTION 2: CONTACT INFORMATION	
OWNER	APPLICANT
NAME/COMPANY: CITY OF KEENE	NAME/COMPANY: DON LUSSIER, PE CITY ENGINEER
MAILING ADDRESS: 3 WASHINGTON ST. KEENE, NH 03431	MAILING ADDRESS: 350 MARLBORO ST. KEENE, NH 03431
PHONE:	PHONE: 603-352-6550
EMAIL:	EMAIL: dlussier@keenenh.gov
SIGNATURE: 	SIGNATURE:
PRINTED NAME: ELIZABETH DRAGON, CITY MANAGER	PRINTED NAME: DON LUSSIER
AUTHORIZED AGENT (if different than Owner/Applicant)	FOR OFFICE USE ONLY:
NAME/COMPANY:	TAX MAP PARCEL #(s): 114-012-000-000-000
MAILING ADDRESS:	-----
PHONE:	PARCEL SIZE: 23.1 ac
EMAIL:	ZONING DISTRICT: Industrial
SIGNATURE:	DATE STAMP:
PRINTED NAME:	PROJECT #: N/A



City of Keene

New Hampshire

City of Keene Public Works Department **Proposed Salt Shed Relocation Project Narrative**

Keene’s winter road maintenance requires stockpiling a significant amount of deicing material, or “road salt”. The storage facility is currently located on a portion of the former Public Works yard at 560 Main Street, property recently sold for the development of a wood chip processing plant. Relocation of the salt storage function and demolition of the existing shed by December 31, 2022 was a condition of the sale.

The proposed site is on Public Works land between NH Route 101, the Branch River, and lower Main Street. It contains a total of 23.1 acres (20.2 ac north and 2.9 ac south of the Branch River) part of which previously served as the City landfill. There is a Public Service utility easement through the lot. The property is zoned Industrial which permits the use Outdoor Storage Yard (Section 8.3.5F).

Access to the property is through a driveway off Manchester Street, where the City maintains an easement. The site is currently home to several Public Works operations including road and utility construction & maintenance, tree, leaf, and snow removal, material storage and others. This proposal includes a paved access road 24 feet wide, and a total impermeable area of 49,210 S.F.

The proposed site design follows NHDES Best Management Practices for the Storage and Management of deicing materials:

- The site is located outside of environmentally sensitive areas, away from wells and aquifers.
- The site is relatively flat, away from surface waters and wetlands.
- Site drainage directs clean storm water away from operations and storage areas.
- The enclosed storage facility keeps material dry and impermeable working areas help prevent groundwater contamination.

The proposed salt storage structure is a Clear Span truss with fabric enclosure, 66’ wide by 104’ long. It has one open end for an entrance, and its support wall consists of a concrete block on grade foundation. Wall buttresses extend 6 feet beyond the above dimensions on three sides.

To store seasonal equipment and building materials used by Public Works, the proposal includes a 30’ deep by 80’ wide pole barn at the rear end wall of the salt storage structure. This building includes four garage bays each with 12’ overhead doors.

The existing site is completely disturbed, with a graveled driveway surface. Public Works is in the process of relocating numerous soil, material and debris stockpiles in preparation for the

development. Concrete and asphalt construction debris will be pulverized, then recycled as base gravel for this project.

The proposed site plan complies with the Site Development Standards in Article 20 of the LDC:

20.2 Drainage& Stormwater Management:

This proposal will produce a slight increase in peak runoff rate of 0.88 cfs. However, with the concurrence of the NHDOT, approximately 43% of the site runoff will be re-directed toward the existing grassed areas beside Rt. 101. This flow will benefit from nutrient uptake and contaminant removal that will occur naturally in the vegetated buffer before it reaches Beaver Brook near Main Street. The remaining flow toward the southern portion of the site and the Branch River will be reduced from its current rate.

The presence of the former landfill and contaminated soil makes groundwater recharge or underground stormwater utilities undesirable. Therefore, the intent of the stormwater management features is to direct runoff toward the southeast and away from the impacted soil areas. Impermeable surfaces “sheet flow” into grassed treatment swales leading to areas currently receiving runoff. Gradual grades will permit settlement of contaminants and runoff attenuation.

20.3 Sedimentation and Erosion Control:

The site is relatively flat which helps limit the ability of runoff to become erosive. Undisturbed receiving areas will be protected by silt fence. All disturbed areas will be stabilized with loam, seed, and mulch, with the addition of erosion control matting on slopes and in grassed swales that may receive concentrated runoff.

20.4 Snow Storage & Removal:

The snow storage area currently supporting the City’s winter operations is located at the east end of this site. Proposed snow storage areas for the site are shown on the plan, with surplus material proposed to be relocated to the larger storage area as necessary.

20.5 Landscaping:

The site is not accessible to the public and is not directly adjacent to any City Streets. On-site parking is not required for the proposed use, therefore no parking facilities are proposed that would require plantings. **A waiver is requested for the landscaping plan.**

20.6 Screening:

A substantial evergreen buffer exists along the State of NH Right-of-Way. Evergreen tree plantings will supplement the existing screening along Route 101, particularly in the northwest corner of the site

20.7 Lighting:

The site is not accessible to the public and general site lighting is not proposed. Solar LED fixtures will light the entrances of the buildings, and the gate area. Each fixture will be equipped with a photo sensor and motion detector, programmed for reduced light level at night and only activating when PWD crews are actively working in the area. **A waiver is requested for the lighting plan.**

20.8 Sewer & Water:

No water or sewer facilities are proposed. Fire and Building Code do not require fire protection for this type of building with a footprint less than 12,000 s.f.

20.9 Traffic & Access Management:

This proposal is to relocate an existing use on the site without any traffic increase. The existing access drive will be improved with a 24 foot wide bituminous asphalt surface.

20.10 Filling & Excavation:

The site will be raised with gravel to minimize disturbance of the underlying landfill. It is not practical to construct frost footings for that reason along with the poor structural quality of the native soil. Backfill for the foundation will partly include recycled construction material already on site.

20.11 Wetlands & Surface Waters:

Delineated wetlands appear on the included parcel subdivision plan from 2019. Planned improvements are +/- 380 feet from the nearest delineated wetland. The site and building are located entirely outside the Surface Water Overlay District and the Protected Shoreland. A portion of the existing access road is located 243 feet from the "Reference Line". However, an exemption was previously granted for this site in accordance with RSA 483-B:12 and the Shoreland Protection Act does not apply.

20.12 Hazardous & Toxic Materials:

The site is a former landfill and there are documented areas of contaminated soil. The proposed building is located outside the areas of contamination delineated on the plan. Portions of the proposed asphalt pavement are above delineated extents of impacted soil. For this reason, the site will be elevated using both recycled on-site material and imported gravel to minimize disturbance of potentially impacted soil.

20.13 Noise:

No change from the current use of the site is proposed. Noise generated from trucks and equipment moving is typical of the adjacent highway corridor and Industrial Use. The building will be located further away from residential properties (+/- 1,000 feet). That said, the activities performed on this site are specifically exempted from the City's noise ordinance per Sec. 66-125(3) of the Keene Code of Ordinances.

20.14 Architecture & Visual Appearance:

The site is not accessible to the public and cannot easily be seen from City streets. The building is designed for the purpose of salt storage and does not include any windows or other accents that may be of architectural interest. The green color of the architectural vinyl fabric roof was selected to make the structure blend in with the adjacent evergreen screening. There will be a small strip of white in the center of the canopy to provide natural day lighting. The proposed building height is 36

feet. Maximum building height permitted in the Industrial Zone is 35 feet, with up to 50 feet allowed with Special Exception from the Zoning Board of Adjustment.

Public Works respectfully requests the following plan waivers:

Landscaping Plan:

No access is permitted beyond the driveway easement at Manchester Street and there are no parking spaces proposed for Staff. The full time operations at the facility are much like a construction site. Given the variability of current operations and the proposed use, it is unlikely landscaping could be protected to ensure that it survives.

This proposal complies with the spirit and intent of Section 20.5. Disturbed areas will be seeded in accordance with NHDOT Standard Specifications. Screening trees have been selected from native species compatible with this climate, with a rapid growth rate, drought and salt tolerance.

Lighting Plan:

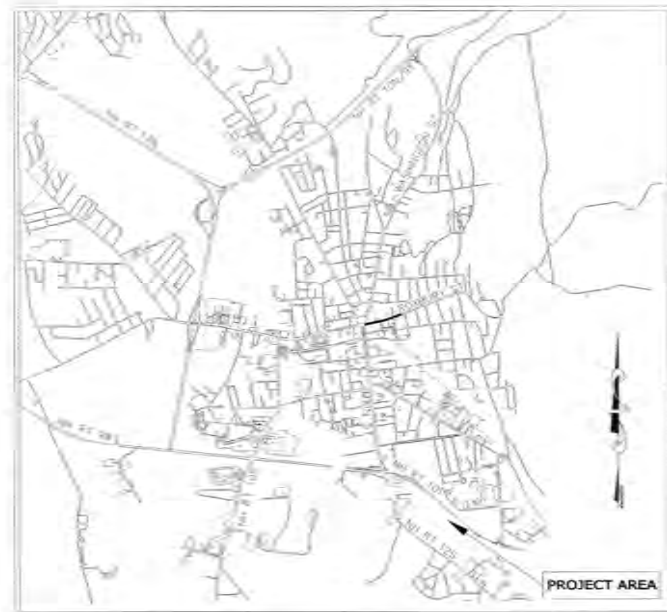
The site is not accessible to the public; we do not intend to provide general site lighting. There is no electric utility available on site. The nearest electricity source is approximately 1,100 feet away and extending service to the facility would add unnecessary expense.

During night hours, solar LED fixtures equipped with photo-sensors will provide a reduced light level at building entrances and the access gate. For the safety of Highway personnel, motion sensors will activate the lights when crews are working directly in the area. Light fixture cut sheets are included with this application.

This proposal complies with the spirit and intent of Section 20.7. Lighting will be provided as required for safety, but at very low levels where it will not impact neighboring properties.

CITY OF KEENE, PUBLIC WORKS SALT SHED RELOCATION PROJECT

FEBRUARY 18, 2022



CITY OF KEENE

ENGINEERING DIVISION
350 MARLBORO STREET
KEENE, NH 03431
(603) 352 - 6550

LEGEND

SEWER MANHOLE	—————	⊙
SEWER STAND PIPE (CLEAN OUT)	—————	◇
WATER GATE	—————	⊙
FIRE HYDRANT	—————	⊙
WATER SERVICE SHUT OFF (CURB STOP)	—————	⊙
DRAIN CATCH BASIN	—————	⊙
DRAIN MANHOLE	—————	⊙
LIGHT POLE	—————	⊙
UTILITY POLE	—————	⊙

CONTACTS:

CITY ENGINEER, CITY OF KEENE: DONALD R. LUSSIER, P.E. (603) 352-6550 X6334

KEENE WATER & SEWER DIVISION: TODD CALDERWOOD (603) 352-6550 X6325

ELECTRIC UTILITY: EVERSOURCE, VAUGHN TAYLOR (603) 357-7309 (800) 662-7764
MARC GAGNON (603) 533-0477

TELEPHONE UTILITY: CONSOLIDATED, BRIAN BASSINGTHWAITE (603) 903-7377

LIBERTY UTILITIES (GAS), STEVE ROKES (603) 209-2582

KEENE FIRE DEPARTMENT (K.F.D.) (603) 357-9861

KEENE POLICE DEPARTMENT (K.P.D.) (603) 357-9815

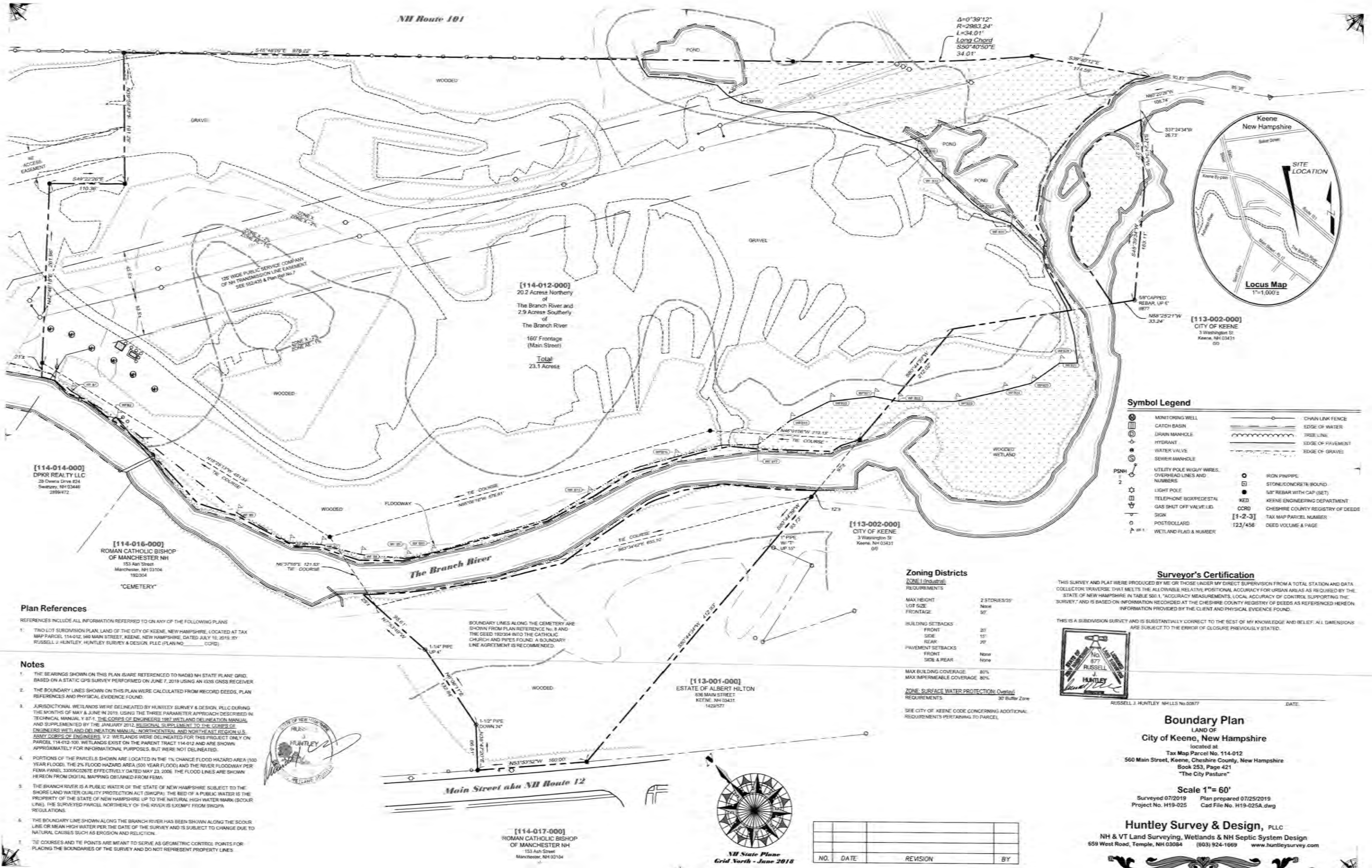
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DESCRIPTION	SHEET
BOUNDARY PLAN	1
EXISTING CONDITIONS/ DEMOLITION PLAN	2
PROPOSED SITE PLAN	3
GRADING & DRAINAGE PLAN	4
CONSTRUCTION NOTES & DETAILS	5



DONALD R. LUSSIER, P.E.
LICENSE NO. 14144

NOT FOR CONSTRUCTION



Plan References
 REFERENCES INCLUDE ALL INFORMATION REFERRED TO ON ANY OF THE FOLLOWING PLANS

- TWO LOT SUBDIVISION PLAN, LAND OF THE CITY OF KEENE, NEW HAMPSHIRE, LOCATED AT TAX MAP PARCEL 114-012, 160 MAIN STREET, KEENE, NEW HAMPSHIRE, DATED JULY 10, 2015 BY RUSSELL J. HUNTLEY, HUNTLEY SURVEY & DESIGN, PLLC (PLAN NO. _____ CCRD)

- Notes**
- THE BEARINGS SHOWN ON THIS PLAN IS/ARE REFERENCED TO NAD83 NH STATE PLANE GRID, BASED ON A STATIC GPS SURVEY PERFORMED ON JUNE 7, 2019 USING AN IGSS GNSS RECEIVER
 - THE BOUNDARY LINES SHOWN ON THIS PLAN WERE CALCULATED FROM RECORD DEEDS, PLAN REFERENCES AND PHYSICAL EVIDENCE FOUND.
 - JURISDICTIONAL WETLANDS WERE DELINEATED BY HUNTLEY SURVEY & DESIGN, PLLC DURING THE MONTHS OF MAY & JUNE IN 2019, USING THE THREE PARAMETER APPROACH DESCRIBED IN TECHNICAL MANUAL V.07-1, THE CORPS OF ENGINEERS 1987 WETLAND DELINEATION MANUAL AND SUPPLEMENTED BY THE JANUARY 2012 REGIONAL SUPPLEMENT TO THE CORPS OF ENGINEERS WETLAND DELINEATION MANUAL - NORTHCENTRAL AND NORTHEAST REGION U.S. ARMY CORPS OF ENGINEERS V.2. WETLANDS WERE DELINEATED FOR THIS PROJECT ONLY ON PARCELS 114-012-000. WETLANDS EXIST ON THE PARENT TRACT 114-012 AND ARE SHOWN APPROXIMATELY FOR INFORMATIONAL PURPOSES, BUT WERE NOT DELINEATED.
 - PORTIONS OF THE PARCELS SHOWN ARE LOCATED IN THE 1% CHANCE FLOOD HAZARD AREA (100 YEAR FLOOD), THE 2% FLOOD HAZARD AREA (500 YEAR FLOOD) AND THE RIVER FLOODWAY PER FEMA PANEL 330650207E EFFECTIVELY DATED MAY 23, 2006. THE FLOOD LINES ARE SHOWN HEREON FROM DIGITAL MAPPING OBTAINED FROM FEMA.
 - THE BRANCH RIVER IS A PUBLIC WATER OF THE STATE OF NEW HAMPSHIRE SUBJECT TO THE SHORELAND WATER QUALITY PROTECTION ACT (SWQPA). THE BED OF A PUBLIC WATER IS THE PROPERTY OF THE STATE OF NEW HAMPSHIRE UP TO THE NATURAL HIGH WATER MARK (SOUND LINE). THE SURVEYED PARCEL NORTHERLY OF THE RIVER IS EXEMPT FROM SWQPA REGULATIONS.
 - THE BOUNDARY LINE SHOWN ALONG THE BRANCH RIVER HAS BEEN SHOWN ALONG THE SCOUR LINE OR MEAN HIGH WATER PER THE DATE OF THE SURVEY AND IS SUBJECT TO CHANGE DUE TO NATURAL CAUSES SUCH AS EROSION AND REFLUCTION.
 - THE COURSES AND THE POINTS ARE MEANT TO SERVE AS GEOMETRIC CONTROL POINTS FOR PLACING THE BOUNDARIES OF THE SURVEY AND DO NOT REPRESENT PROPERTY LINES.

Zoning Districts

ZONE I (Industrial)
 REQUIREMENTS

MAX HEIGHT	2 STORIES/20'
LOT SIZE	None
FRONTAGE	50'
BUILDING SETBACKS	
FRONT	20'
SIDE	15'
REAR	20'
PAVEMENT SETBACKS	
FRONT	None
SIDE & REAR	None
MAX BUILDING COVERAGE	80%
MAX IMPERMEABLE COVERAGE	80%

ZONE SURFACE WATER PROTECTION (Overlay)
 REQUIREMENTS

SEE CITY OF KEENE CODE CONCERNING ADDITIONAL REQUIREMENTS PERTAINING TO PARCEL

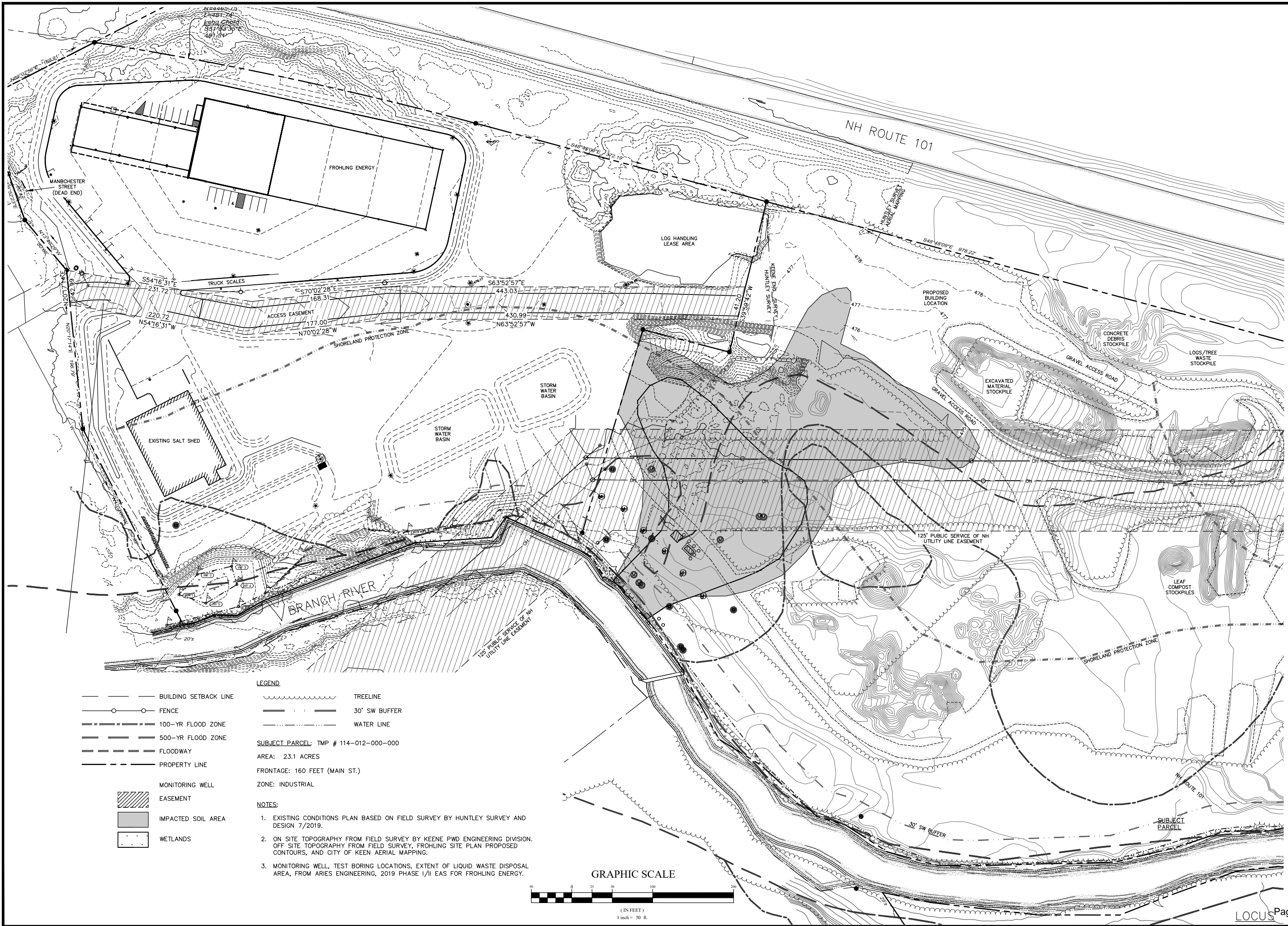


Boundary Plan
 LAND OF
City of Keene, New Hampshire
 located at
 Tax Map Parcel No. 114-012
 560 Main Street, Keene, Cheshire County, New Hampshire
 Book 253, Page 421
 "The City Pasture"

Scale 1" = 60'
 Surveyed 07/2019 Plan prepared 07/25/2019
 Project No. H19-025 Cad File No. H19-025A.dwg

Huntley Survey & Design, PLLC
 NH & VT Land Surveying, Wetlands & NH Septic System Design
 659 West Road, Temple, NH 03084 (603) 924-1669 www.huntleysurvey.com

NO.	DATE	REVISION	BY

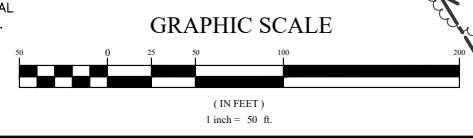


- BUILDING SETBACK LINE
- FENCE
- 100-YR FLOOD ZONE
- 500-YR FLOOD ZONE
- FLOODWAY
- PROPERTY LINE
- MONITORING WELL
- EASEMENT
- IMPACTED SOIL AREA
- WETLANDS

- LEGEND**
- TREELINE
 - 30' SW BUFFER
 - WATER LINE

SUBJECT PARCEL: TMP # 114-012-000-000
 AREA: 23.1 ACRES
 FRONTAGE: 160 FEET (MAIN ST.)
 ZONE: INDUSTRIAL

- NOTES:**
- EXISTING CONDITIONS PLAN BASED ON FIELD SURVEY BY HUNTLEY SURVEY AND DESIGN 7/2019.
 - ON SITE TOPOGRAPHY FROM FIELD SURVEY BY KEENE PWD ENGINEERING DIVISION. OFF SITE TOPOGRAPHY FROM FIELD SURVEY, FROHLING SITE PLAN PROPOSED CONTOURS, AND CITY OF KEEN AERIAL MAPPING.
 - MONITORING WELL, TEST BORING LOCATIONS, EXTENT OF LIQUID WASTE DISPOSAL AREA, FROM ARIES ENGINEERING, 2019 PHASE 1/II EAS FOR FROHLING ENERGY.



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EXISTING CONDITIONS PLAN
 PWD SALT SHED RELOCATION
 560 MAIN STREET
 CITY OF KEENE, NEW HAMPSHIRE

CITY OF KEENE
 NEW HAMPSHIRE
 ENGINEERING DEPARTMENT
 Drawn by: LJD
 Designed by: DRL, City Engineer
 Checked by: DRL, City Engineer
 MARCH, 2021
 SCALE: 1" = 50'

EXISTING CONDITIONS

REVISIONS						
NO.	DATE	DESCRIPTION	BY	CHKD.	APP.	SCALE
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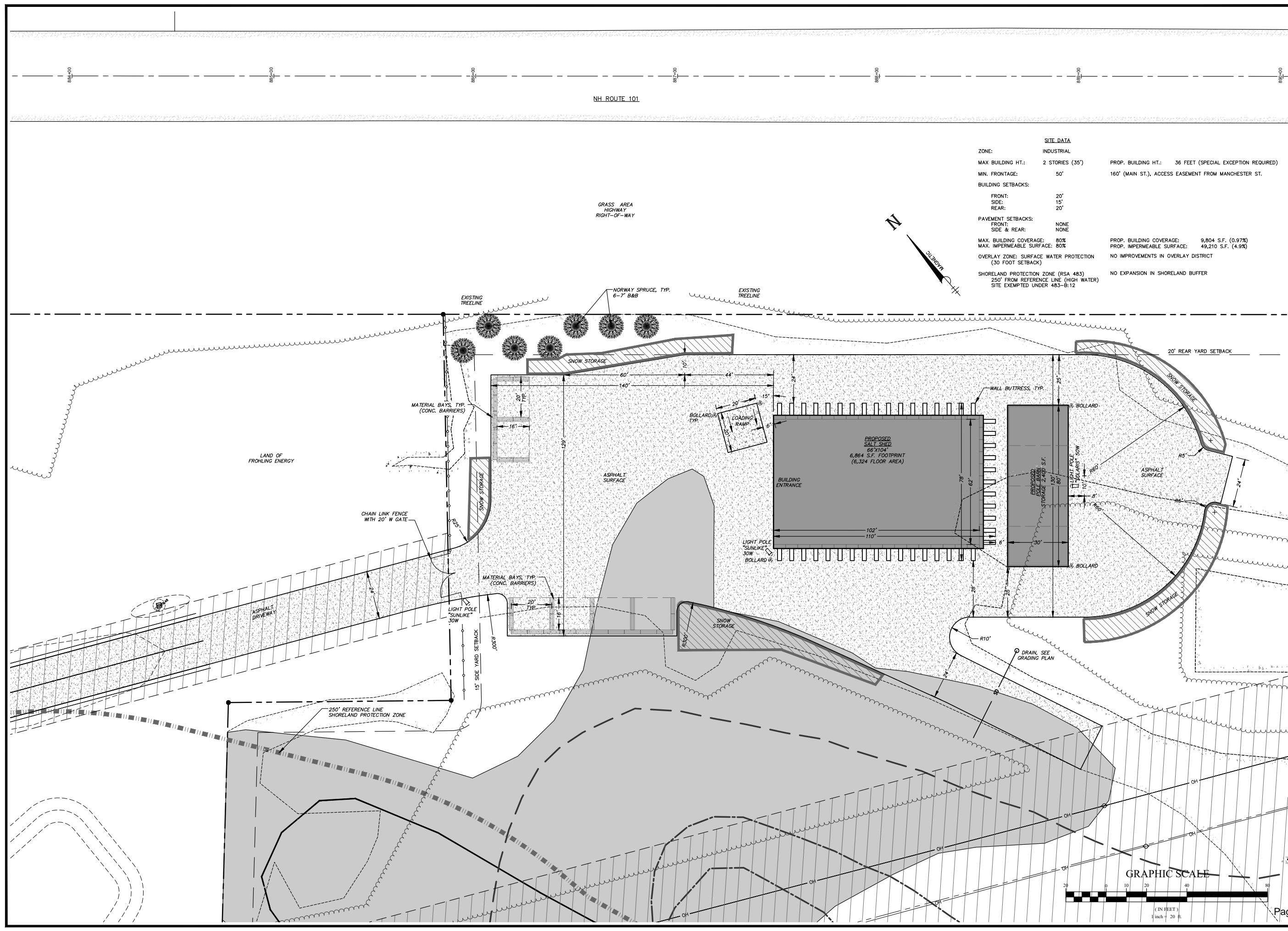


PROPOSED SITE PLAN
PWD SALT SHED RELOCATION
560 MAIN STREET
CITY OF KEENE, NEW HAMPSHIRE

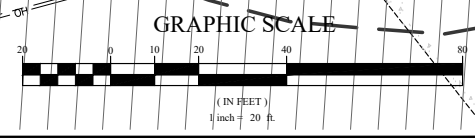
CITY OF KEENE
 NEW HAMPSHIRE
 ENGINEERING DEPARTMENT
 Drawn by: LJD
 Designed by: DRL, City Engineer
 Checked by: DRL, City Engineer
 MARCH, 2021
 SCALE: 1" = 50'

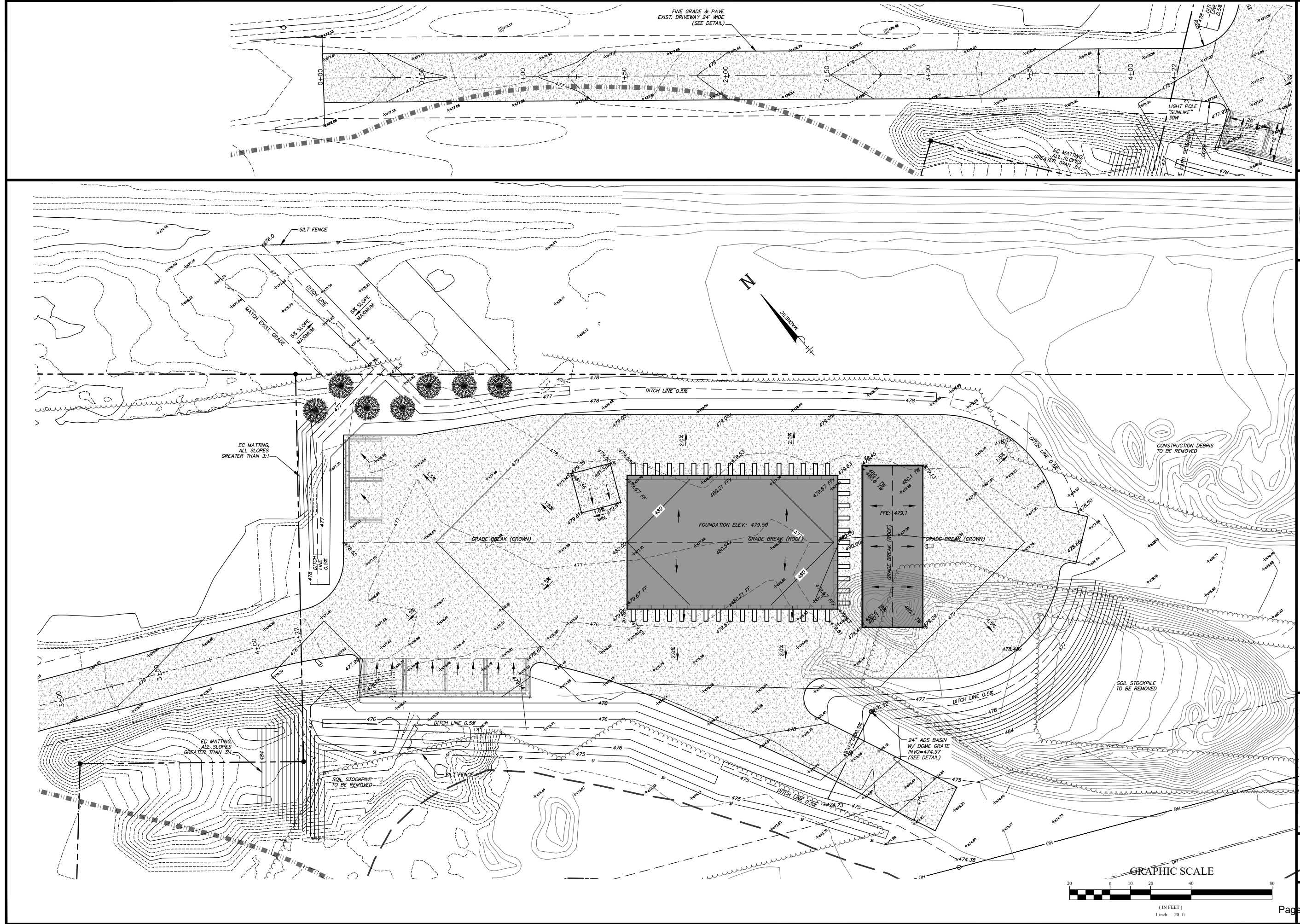
PROPOSED SALT SHED

SHEET
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 OF 5



SITE DATA			
ZONE:	INDUSTRIAL	PROP. BUILDING HT.:	36 FEET (SPECIAL EXCEPTION REQUIRED)
MAX. BUILDING HT.:	2 STORIES (35')	MIN. FRONTAGE:	160' (MAIN ST.), ACCESS EASEMENT FROM MANCHESTER ST.
MIN. FRONTAGE:	50'	BUILDING SETBACKS:	
BUILDING SETBACKS:		FRONT:	20'
		SIDE:	15'
		REAR:	20'
PAVEMENT SETBACKS:		FRONT:	NONE
		SIDE & REAR:	NONE
MAX. BUILDING COVERAGE:	80%	PROP. BUILDING COVERAGE:	9,804 S.F. (0.97%)
MAX. IMPERMEABLE SURFACE:	80%	PROP. IMPERMEABLE SURFACE:	49,210 S.F. (4.9%)
OVERLAY ZONE:	SURFACE WATER PROTECTION (30 FOOT SETBACK)	OVERLAY ZONE:	NO IMPROVEMENTS IN OVERLAY DISTRICT
SHORELAND PROTECTION ZONE (RSA 483)	250' FROM REFERENCE LINE (HIGH WATER)	SHORELAND PROTECTION ZONE (RSA 483)	NO EXPANSION IN SHORELAND BUFFER
	SITE EXEMPTED UNDER 483-B:12		





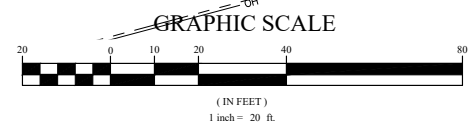
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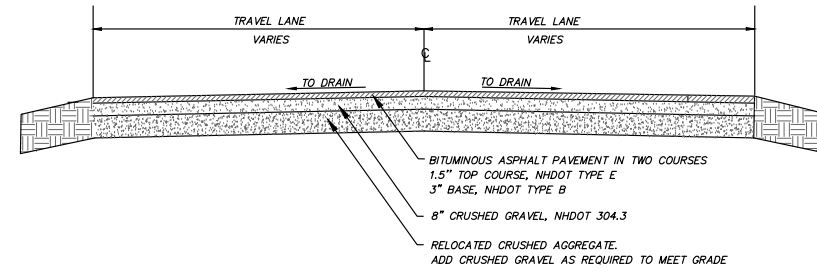


GRADING & DRAINAGE PLAN
 PWD SALT SHED RELOCATION
 560 MAIN STREET
 CITY OF KEENE, NEW HAMPSHIRE

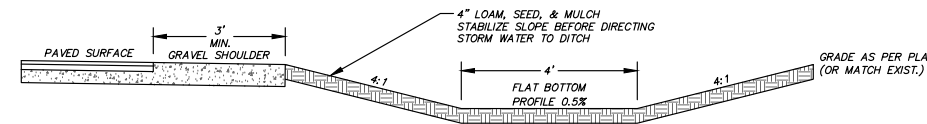
CITY OF KEENE
 NEW HAMPSHIRE
 ENGINEERING DEPARTMENT
 Drawn by: LJD
 Designed by:
 Checked by: DRL, City Engineer
 DECEMBER, 2021
 SCALE: 1" = 30'

PROPOSED SALT SHED
SHEET
 Page 62 of 74
 OF 5

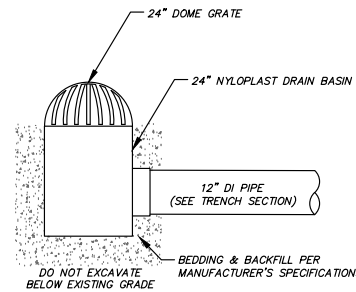




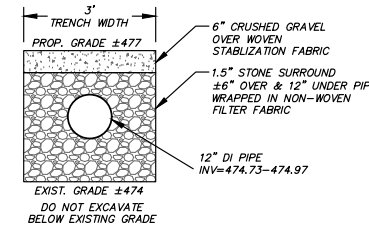
DRIVEWAY SECTION



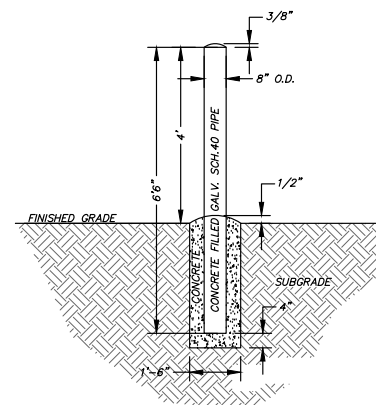
DRAINAGE DITCH SECTION



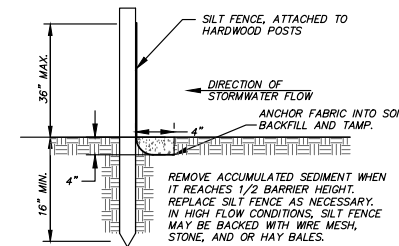
DRAIN BASIN DETAIL



PIPE TRENCH SECTION



CONCRETE BOLLARD DETAIL



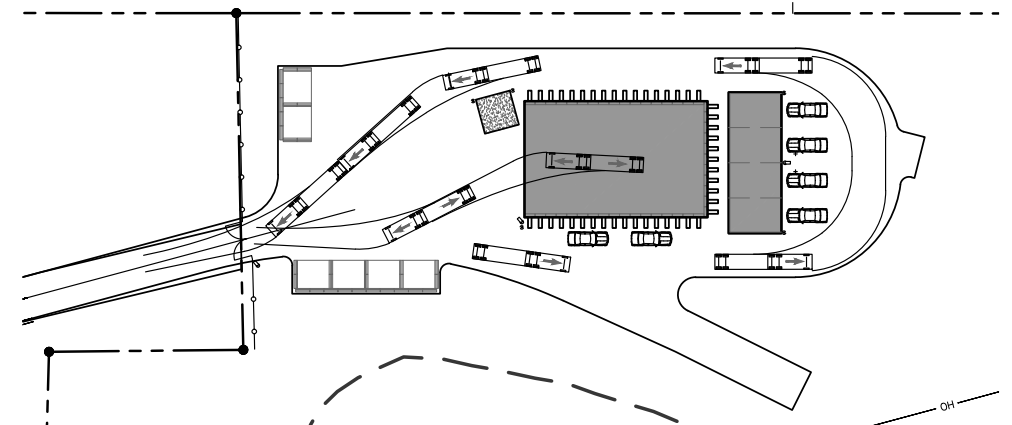
SILT FENCE DETAIL

SEDIMENT, EROSION, & DUST CONTROL

1. INSTALL ALL SEDIMENT & EROSION CONTROL MEASURES IN ACCORDANCE WITH MANUFACTURER'S DIRECTION OR DETAILS PROVIDED.
2. THE CONTRACTOR IS RESPONSIBLE FOR ALL EROSION CONTROL. HE SHALL TAKE ALL MEASURES NEEDED TO MINIMIZE EROSION TO THE GREATEST EXTENT POSSIBLE, AT NO ADDITIONAL COST TO THE OWNER, REGARDLESS OF DETAIL SHOWN ON THESE PLANS.
3. CONTRACTOR SHALL INSPECT AND REPAIR ALL SEDIMENT AND EROSION CONTROL MEASURES AFTER EACH RAINFALL AND NOT LESS THAN ONCE A WEEK.
4. SEED, FERTILIZE & MULCH ALL FINISH GRADED AREAS WITHIN 72 HOURS OF FINISH GRADING.
5. STONE CHECK DAMS AND/OR SILT FENCES SHALL BE REPLACED WHEN CLOGGED AND NO LONGER FUNCTIONAL.
6. STONE CHECK DAMS AND/OR SILT FENCES SHALL REMAIN IN PLACE UNTIL ALL UPHILL VEGETATED AREAS ARE STABILIZED.
9. ALL SLOPES GREATER THAN 3:1 SLOPE SHALL BE PROTECTED WITH EROSION CONTROL BLANKETS.
10. AN AREA SHALL BE CONSIDERED STABLE IF ONE OF THE FOLLOWING HAS OCCURRED:
 - A. BASE COURSE GRAVEL HAS BEEN INSTALLED IN AREAS TO BE PAVED.
 - B. A MINIMUM OF 85% VEGETATIVE COVER HAS BEEN ESTABLISHED.
 - C. A MINIMUM OF 3 INCHES OF NON-ERROSIVE MATERIAL SUCH AS STONE OR RIP-RAP HAS BEEN INSTALLED.
 - D. EROSION CONTROL BLANKETS HAVE BEEN PROPERLY INSTALLED.

GENERAL CONSTRUCTION NOTES

1. THE CONTRACTOR SHALL CALL DIG-SAFE, AT 1-888-344-7233 AT LEAST 72 HOURS BEFORE THE START OF EXCAVATION. THE CONTRACTOR SHALL MAINTAIN UTILITY MARK-OUTS FOR THE DURATION OF CONSTRUCTION.
2. THE CONTRACTOR IS EXPECTED TO BE AWARE OF AND COMPLY WITH ALL PERMITS AND PERMIT CONDITIONS.
3. ALL TRENCHING, EXCAVATION, SHEETING, SHORING, ETC. SHALL COMPLY WITH THE MOST CURRENT OSHA REGULATIONS.
4. THE CONTRACTOR SHALL NOTIFY THE ENGINEER IF FIELD CONDITIONS VARY FROM THAT SHOWN ON THE PLAN(S). THE CONTRACTOR'S WORK SHALL NOT VARY FROM THE PLAN(S) UNLESS SO AUTHORIZED BY THE ENGINEER.
5. ALL WORK SHALL BE COMPLETED IN ACCORDANCE WITH PLANS AND SPECIFICATIONS PROVIDED, OR IN ACCORDANCE WITH NH DEPT. OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, LATEST EDITION.
6. IN CASE OF CONFLICTS, THE MOST STRINGENT INTERPRETATION OF THE PLANS, SPECIFICATIONS, LOCAL OR STATE REGULATIONS, OR PERMIT CONDITIONS SHALL APPLY. THE ENGINEER SHALL BE THE DETERMINANT AS TO WHAT APPLIES.
7. ALL KNOWN SUBSURFACE UTILITIES AND STRUCTURES HAVE BEEN INDICATED ON THE PLAN(S) AS ACCURATELY AS POSSIBLE. THE EXACT LOCATION MAY VARY AND THE CONTRACTOR IS CAUTIONED TO PROCEED WITH CARE.
8. CONTRACTOR SHALL VERIFY INVERT ELEVATIONS OF ALL PIPES AND STRUCTURES PRIOR TO START OF WORK. NOTIFY THE ENGINEER IF THE FIELD INFORMATION DOES NOT MATCH PLAN INFORMATION.
9. THE CONTRACTOR IS RESPONSIBLE FOR THE HOLDING OF UTILITY POLES DURING CONSTRUCTION.
10. THE ENGINEER WILL PROVIDE BENCH MARKS AT EACH END OF THE PROJECT. THE CONTRACTOR WILL BE RESPONSIBLE FOR ALL OTHER LAYOUT AND FOR REPLACEMENT OF LAYOUT DISTURBED DURING CONSTRUCTION.
11. THE CONTRACTOR SHALL HAVE ADEQUATE MEANS TO RESPOND TO DUST CONTROL WHEN NEEDED. THE CITY WILL ENFORCE A NO DUST POLICY.
12. MONUMENTATION THAT HAS BEEN DISTURBED SHALL BE RESET BY A NEW HAMPSHIRE LICENSED LAND SURVEYOR AT NO COST TO THE OWNER OR ENGINEER.
13. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL DEWATERING AT NO ADDITIONAL COST.
14. ALL EXCESS MATERIAL WILL BE PROPERLY DISPOSED OF BY THE CONTRACTOR.
15. ALL SURFACES SHALL BE GRADED TO DRAIN.
16. THE CONTRACTOR SHALL RESTORE ALL DISTURBED SURFACES TO THEIR ORIGINAL CONDITION OR BETTER. ALL NEW AND EXISTING PIPES AND STRUCTURES SHALL BE CLEANED. ALL SIGNS SHALL BE REPLACED. ALL DAMAGED VEGETATION SHALL BE REPLACED.
17. THIS PROJECT WILL BE CONSTRUCTED IN SEVERAL PHASES. THE CONTRACTOR WILL SUBMIT A DETAILED PLAN SHOWING PROPOSED CONSTRUCTION SEQUENCE.
18. THE CONTRACTOR SHALL SUBMIT PROJECT RECORD DRAWINGS AND OTHER REQUIRED DOCUMENTATION IN ACCORDANCE WITH THE SPECIFICATIONS PRIOR TO SUBSTANTIAL COMPLETION.



TRUCK TURNING DIAGRAM (WB-50 DESIGN VEHICLE)

REVISIONS	
NO.	DESCRIPTION
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CONSTRUCTION NOTES & DETAILS

PWD SALT SHED RELOCATION
560 MAIN STREET
CITY OF KEENE, NEW HAMPSHIRE

CITY OF KEENE
NEW HAMPSHIRE
ENGINEERING DEPARTMENT
Drawn by: LJD
Designed by: DRL, City Engineer
Checked by: DRL, City Engineer
MARCH, 2021
SCALE: 1" = 50'

EXISTING CONDITIONS

SHEET

NOTES:

- ALL DIMENSIONS ARE FROM CENTER TO CENTER UNLESS OTHERWISE NOTED.
- FRAMING FOR WALK DOORS, VENTS, AND ADDITIONAL FRAMING, IF REQUIRED, WILL BE CUT IN FIELD USING 2" X 3" RECTANGULAR TUBING & 107923HDG OR 110356ALTHDG/110356BALTHDG INSERTS.
- DIMENSIONS FOR TELESCOPING PURLIN LOCATIONS ARE APPROXIMATE. LOCATIONS CAN BE FIELD ADJUSTED +/- 12" FROM WHAT IS SHOWN.
- FOUNDATION ELEMENTS ARE NOT SHOWN.

LINE STYLES:

- COLUMN: _____
- HEADER: - - - - -
- TELESCOPING PURLIN: - · - · - · -
- 2" X 3" TUBING: ·····

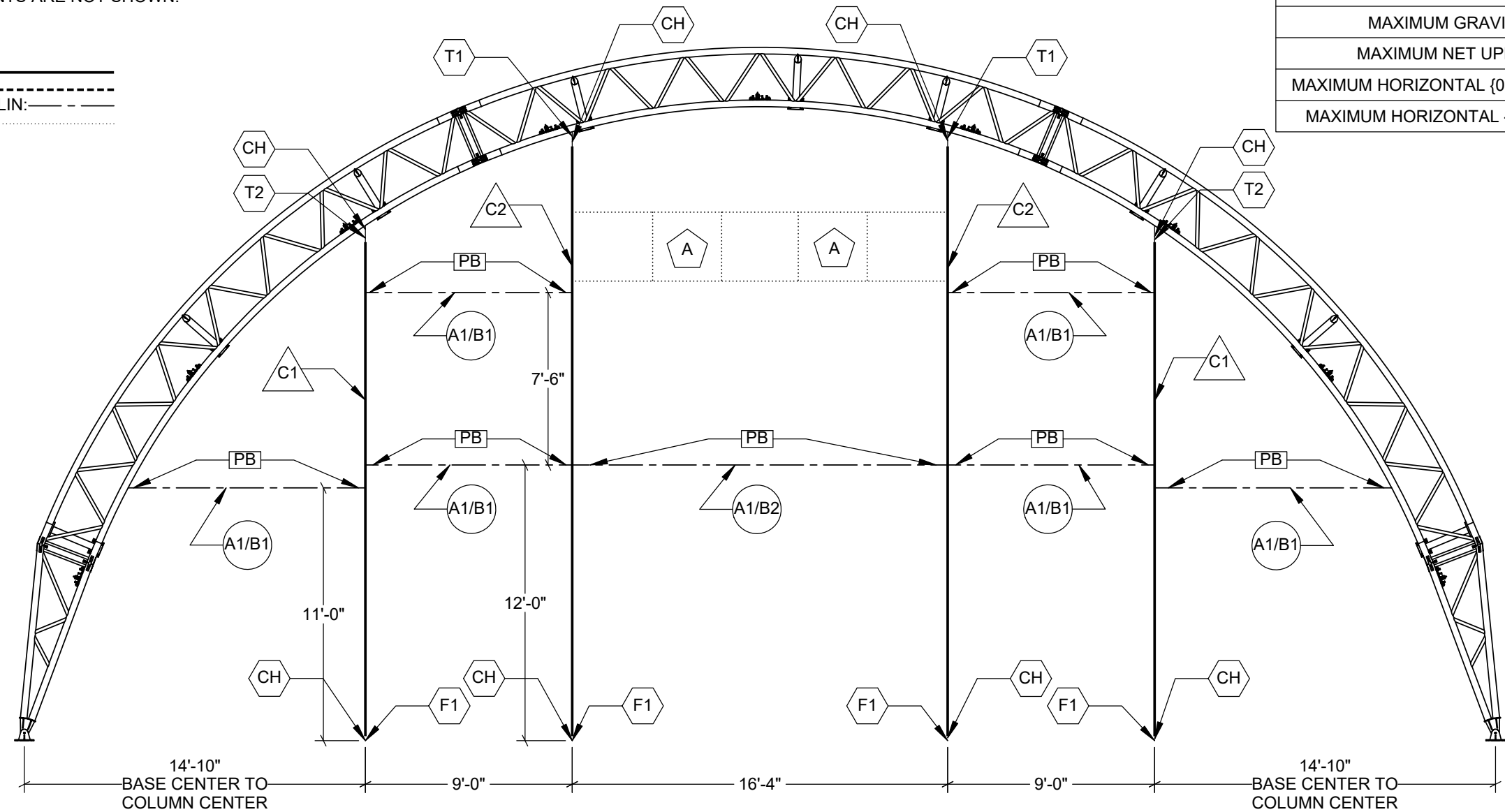


FRAME-OUT SCHEDULE		
CALL-OUT	DESCRIPTION	ROUGH OPENING
A	36" X 36" LOUVER	VERIFY

ENDWALL CONTENT GUIDE:

- [M1]COVER SHEET/PART LOCATIONS/LAYOUT
- [M2]CONNECTION DETAILS

ASD ENDWALL COLUMN REACTIONS	
MAXIMUM GRAVITY	380 LBS
MAXIMUM NET UPLIFT	410 LBS
MAXIMUM HORIZONTAL {0.6 * MWFRS}	1960 LBS
MAXIMUM HORIZONTAL {0.6 * C&C}	2850 LBS



△ COLUMNS
HSS 8" X 4" X 3/16" A500 Gr. B

C1: EWC8X4X3X258HG
C2: EWC8X4X3X308HG

○ TELESCOPING PURLINS
2.0" OD, 11 GA ROUND PIPE

A1: 200TP060HG

2.375" OD, 14 GA ROUND PIPE

B1: 2375TP085HG
B2: 2375TP157HG

⬡ INSERTS
TOP COLUMN INSERTS
(3.5" SQ. TUBE / 3/16" STEEL PLATE)

T1: 107456HG
T2: 108151SAHDG/108151SBHDG

BOTTOM COLUMN INSERTS
(3.5" SQ. TUBE / 3/8" STEEL PLATE)

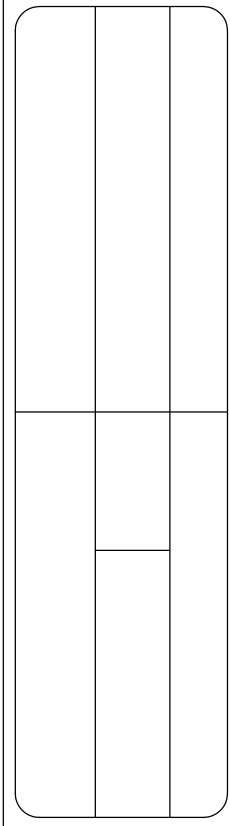
F1: 111700HG

"C" CHANNEL INSERTS
(1/8" STEEL PLATE)

CH: 111260HDG

□ BRACKETS
PURLIN BRACKET (1/8" STEEL PLATE)

PB: 106755HDG



DRAWING DETAILS	
DRAWN BY: CE	CREATION DATE: 10/19/2017
REVISIONS:	
NO.	BY:
1	
2	
3	
4	
NOT TO SCALE	
SHEET SIZE: 11X17	

REVISIONS	
DATE	DESCRIPTION



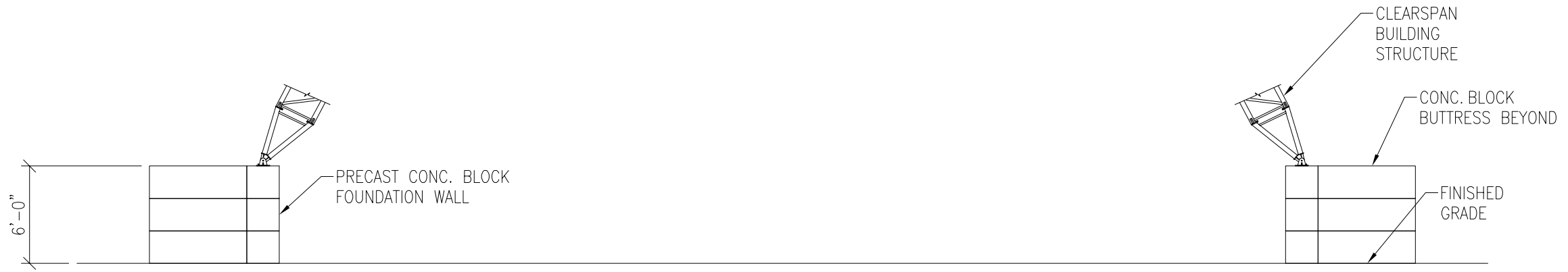
A Division of Engineering Services & Products Co.
1440 18th Ave. SW
Dyersville, IA 52040
www.esapco.com

ELEVATION VIEWS
FOUNDATION SYSTEM FOR
CLEARSPAN STRUCTURE
CONCRETE BLOCK FOUNDATION

U1382-573-171

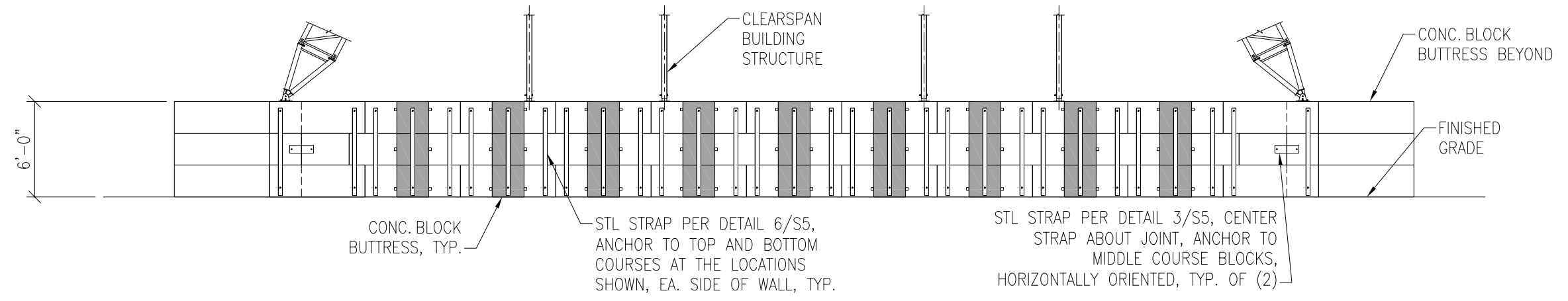
S4
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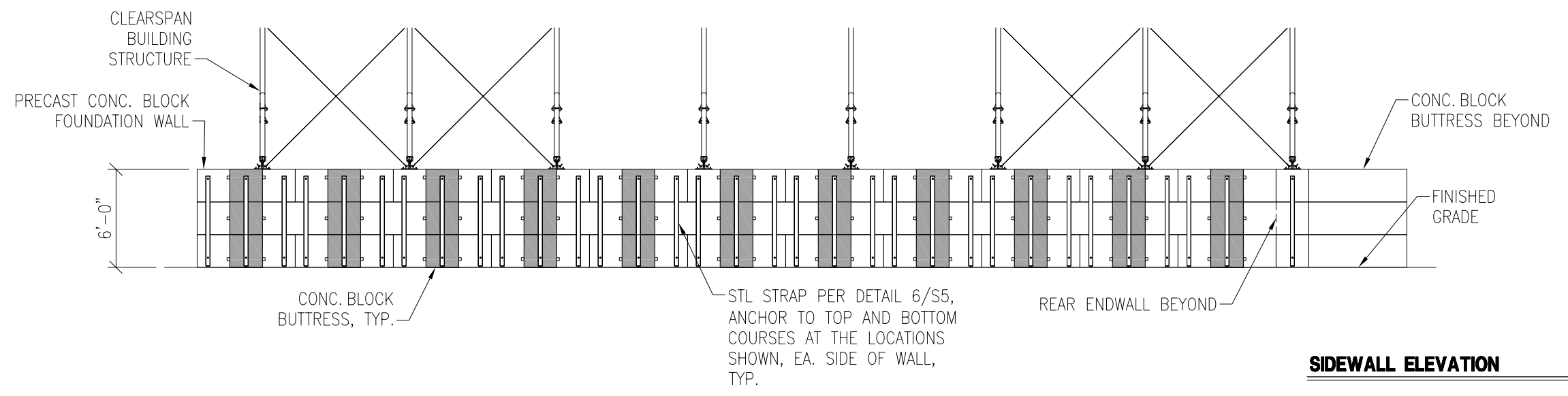
FRONT ENDWALL ELEVATION

N.T.S. **1**



REAR ENDWALL ELEVATION

N.T.S. **2**



SIDEWALL ELEVATION

N.T.S. **3**

SALT SHED

Type III 24-hr 25-YR Rainfall=4.96"

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Printed 2/8/2022

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Summary for Subcatchment 1S: POST-DEV (NORTH)

Runoff = 3.43 cfs @ 12.05 hrs, Volume= 0.215 af, Depth> 3.33"

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 5.00-20.00 hrs, dt= 0.05 hrs
Type III 24-hr 25-YR Rainfall=4.96"

Area (sf)	CN	Description
16,470	98	Paved parking, HSG B
4,570	98	Roofs, HSG B
12,700	69	50-75% Grass cover, Fair, HSG B
33,740	87	Weighted Average
12,700		37.64% Pervious Area
21,040		62.36% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
1.2	76	0.0150	1.08		Sheet Flow, Smooth surfaces n= 0.011 P2= 2.77"
1.6	100	0.0050	1.06		Shallow Concentrated Flow, Grassed Waterway Kv= 15.0 fps
2.8	176	Total			

Summary for Subcatchment 2S: POST-DEV(SOUTH)

Runoff = 4.64 cfs @ 12.05 hrs, Volume= 0.288 af, Depth> 3.13"

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 5.00-20.00 hrs, dt= 0.05 hrs
Type III 24-hr 25-YR Rainfall=4.96"

Area (sf)	CN	Description
21,670	98	Paved parking, HSG B
4,690	98	Roofs, HSG B
21,670	69	50-75% Grass cover, Fair, HSG B
48,030	85	Weighted Average
21,670		45.12% Pervious Area
26,360		54.88% Impervious Area

SALT SHED

Type III 24-hr 25-YR Rainfall=4.96"

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Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
1.2	82	0.0150	1.10		Sheet Flow, Smooth surfaces n= 0.011 P2= 2.77"
1.6	100	0.0050	1.06		Shallow Concentrated Flow, Grassed Waterway Kv= 15.0 fps
2.8	182	Total			

Summary for Subcatchment 3S: PRE-DEVELOPMENT

Runoff = 7.19 cfs @ 12.03 hrs, Volume= 0.441 af, Depth > 2.95"

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 5.00-20.00 hrs, dt= 0.05 hrs
Type III 24-hr 25-YR Rainfall=4.96"

Area (sf)	CN	Description
50,720	85	Gravel roads, HSG B
27,620	79	<50% Grass cover, Poor, HSG B
78,340	83	Weighted Average
78,340		100.00% Pervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
1.3	100	0.0200	1.28		Sheet Flow, Smooth surfaces n= 0.011 P2= 2.77"
0.4	50	0.0200	2.28		Shallow Concentrated Flow, Unpaved Kv= 16.1 fps
1.7	150	Total			

Summary for Reach 4R: NORTH SWALE

Inflow Area = 0.775 ac, 62.36% Impervious, Inflow Depth > 3.33" for 25-YR event

Inflow = 3.43 cfs @ 12.05 hrs, Volume= 0.215 af

Outflow = 2.74 cfs @ 12.18 hrs, Volume= 0.213 af, Atten= 20%, Lag= 8.0 min

Routing by Stor-Ind+Trans method, Time Span= 5.00-20.00 hrs, dt= 0.05 hrs

Max. Velocity= 1.33 fps, Min. Travel Time= 5.0 min

Avg. Velocity= 0.42 fps, Avg. Travel Time= 15.9 min

Peak Storage= 849 cf @ 12.09 hrs

Average Depth at Peak Storage= 0.38', Surface Width= 7.07'

Bank-Full Depth= 1.00' Flow Area= 8.0 sf, Capacity= 18.08 cfs

SALT SHED

Type III 24-hr 25-YR Rainfall=4.96"

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4.00' x 1.00' deep channel, n= 0.035 Earth, dense weeds
Side Slope Z-value= 4.0 '/' Top Width= 12.00'
Length= 400.0' Slope= 0.0050 '/'
Inlet Invert= 479.00', Outlet Invert= 477.00'



Summary for Reach 5R: SOUTH SWALE

Inflow Area = 1.103 ac, 54.88% Impervious, Inflow Depth > 3.13" for 25-YR event
Inflow = 4.64 cfs @ 12.05 hrs, Volume= 0.288 af
Outflow = 3.93 cfs @ 12.15 hrs, Volume= 0.286 af, Atten= 15%, Lag= 6.1 min

Routing by Stor-Ind+Trans method, Time Span= 5.00-20.00 hrs, dt= 0.05 hrs
Max. Velocity= 1.48 fps, Min. Travel Time= 3.7 min
Avg. Velocity= 0.48 fps, Avg. Travel Time= 11.6 min

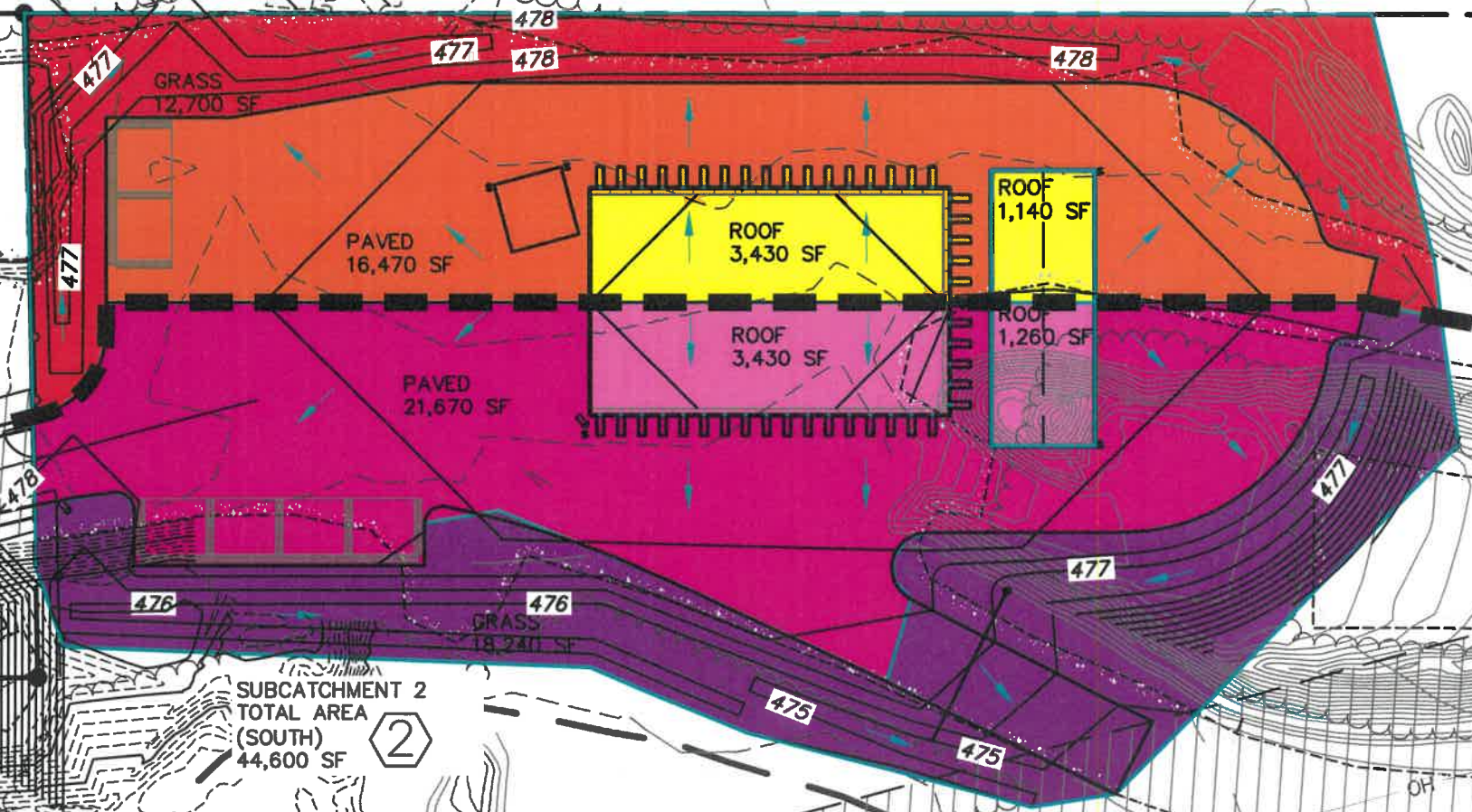
Peak Storage= 896 cf @ 12.09 hrs
Average Depth at Peak Storage= 0.46' , Surface Width= 7.71'
Bank-Full Depth= 1.00' Flow Area= 8.0 sf, Capacity= 18.08 cfs

4.00' x 1.00' deep channel, n= 0.035 Earth, dense weeds
Side Slope Z-value= 4.0 '/' Top Width= 12.00'
Length= 330.0' Slope= 0.0050 '/'
Inlet Invert= 476.00', Outlet Invert= 474.35'



NH ROUTE 101

SUBCATCHMENT 1
TOTAL AREA
(NORTH)
33,740 SF



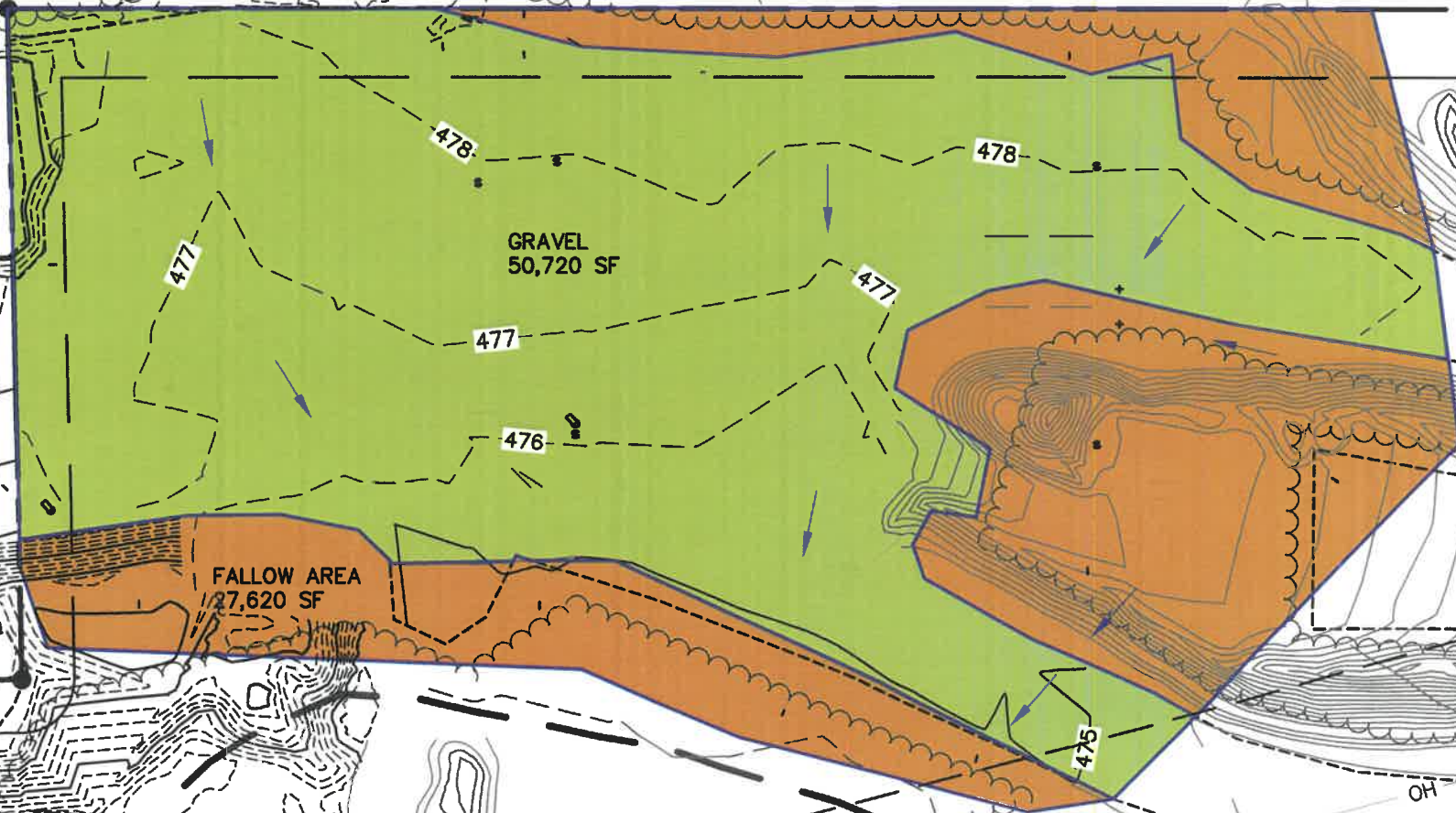
SUBCATCHMENT 2
TOTAL AREA
(SOUTH)
44,600 SF



POST-DEVELOPMENT DRAINAGE

NH ROUTE 101

SUBCATCHMENT 3
TOTAL AREA
78,340 SF



PRE-DEVELOPMENT DRAINAGE



POLE BARN TYPICAL, BIEGE SIDING AND GREEN ROOF



POLE BARN TYPICAL (EXCLUDE COVERED ENTRY)



SALT SHED STRUCTURE, BACK WALL DETAIL



SALT SHED STRUCTURE, WHITE STRIP FOR NATURAL LIGHTING



SALT SHED TYPICAL, GREEN CANOPY



SALT SHED STRUCTURE, CONCRETE BLOCK FOUNDATION

SUNLIKE Solar Area Lights 30W

Even in the most frigid weather, solar panels can turn sunlight into electricity. Solar panels create energy from sunlight—not the sun's heat. When photons from sunlight hit the solar panel's photovoltaic cells, electrons in the silicon are put into motion. In addition, the coldest winter days after snow typically have the best sunshine in the winter.



For more information, please visit our website:
www.soltechlighting.com



APPLICATIONS

30W SUNLIKE PRO Area Lights

Does Off-grid Solar Lighting Work In Cold Weather?

THE SUNLIKE PRO 30W CAN.

Even in winter months, if sunlight is hitting a solar panel, it will generate electricity. Cold climates are actually optimal for solar panel efficiency. Contrary to common belief, heat diminishes the solar panel's electricity production. Soltech adopted unique, innovative battery technology to overcome the shortcomings of solar lighting system's cold weather performance. The super cold-tolerant battery technology in the SUNLIKE PRO 30W provides excellent low-temperature charge and discharge performance.



MPPT Controller

- Maximum Power Point Tracking (MPPT) is a technique for tracking and regulating the output energy from the solar panel to the battery.
- Measures the solar panel output voltage and current in real-time and continuously tracks the maximum power.
- Regulates the output voltage so that the system can always charge the battery with the maximum power.
- Significantly improves the solar system energy utilization rate, with a conversion efficiency up to 97%.
- Increases the solar system's charging efficiency by at least 20% compared to Pulse Width Modulation (PWM).

(IAP) Intelligent Adaptive Program Battery Control Technology

In order to extend the off-grid autonomy of the SUNLIKE PRO 30W under shady trees, heavy rain, and thick clouds, our controllers now integrate an adaptive smart control feature to actively track battery capacity and adjust light output accordingly. This feature out-performs utilizing a constant percentage of max LED brightness. With (IAP), the controller actively monitors the battery and optimizes the electrical current to the LEDs. The IAP controller applies the selected percentage output from the remote, to the battery capacity, rather than the max LED output. This smart-control feature can increase SUNLIKE PRO's off-grid performance by up to 40%.

CITY OF KEENE
NEW HAMPSHIRE

MEMORANDUM

Date: March 18, 2022
To: Planning Board
From: Mari Brunner, Senior Planner
Re: Minor Project Review Committee Member Designation

The following individuals are proposed to serve as regular and alternate members on the Minor Project Review Committee.

Regular Members:

1. John Rogers, Acting Community Development Director
2. Mari Brunner, Senior Planner
3. Don Lussier, City Engineer
4. Deputy Chief Jeff Chickering, Fire Department
5. Med Kopczynski, Economic Development Director

Alternates:

1. Michael Hagan, Plans Examiner
2. Kurt Blomquist, Public Works Director
3. Evan Clements, Planner
4. Deputy Chief Donald Farquhar, Fire Department