



MUNICIPAL SERVICES, FACILITIES AND INFRASTRUCTURE COMMITTEE
Council Chambers, Keene City Hall
September 23, 2026
6:00 PM

A. AGENDA ITEMS

1. Duncan Hilchey and Amy Christian - Request for a Traffic Study and Safety Assessment of School Street
2. Michael Guitard/Guitard Homes LLC - Petition for City Ownership of Water and Sewer Infrastructure - Cottage Court Community on Court Street
Additional Submittals from Petitioner: Tax Card, Utility Easement, Property Plans, Construction Plans
3. Shawn Voisine/Elmside LLC - Request for Waiver of City Public Utilities Standards for Water Metering Infrastructure - Elmside Cottage Court
Staff Recommendation: Elmside Cottage Court Petition to Waive Public Utility Standards - City Engineer
4. Kenneth Kost - Request for a Comprehensive Planning Study of the Gilbo Avenue Site Prior to Solar Pavilion Development
Additional Submittal from Petitioner: Written Testimony
5. Gilbo Ave Solar Pavilion Conceptual Design - City Engineer
6. Partner City Committee - Annual Report
7. Continued Discussion: Erik Murphy - Petition - Safety Improvements at the South Lincoln Street and Roxbury Street Intersection
8. Relating to the Installation of Two Stop Signs on Roxbury Street
Ordinance O-2026-15
9. Relating to the Alteration of Thompson Road
Resolution R-2026-37
10. Roadway Safety Action Plan Update - Public Works Director
11. Downtown Infrastructure Project Update - Public Works Director

B. MORE TIME ITEMS

NON PUBLIC SESSION

ADJOURNMENT

Honorable Mayor and Members of the Keene City Council
3 Washington Street
Keene, NH 03431

Re: Request for Traffic Study and Safety Assessment — School Street

Dear Honorable Mayor and Members of the City Council:

We respectfully request that the City of Keene undertake a traffic study and safety assessment of **School Street**, with particular attention to vehicle speeds, traffic volumes, traffic patterns, and excessive vehicle noise.

Residents have become increasingly concerned that School Street is experiencing substantial through traffic, including drivers who appear to be using the street as an alternative route to avoid congestion and delays associated with the work being done in Central Square and the surrounding downtown area. While School Street is located near the central business district, it is also fundamentally a **residential neighborhood**, and its transportation function should be evaluated in light of the people who live here.

Of particular concern is the frequency of vehicles traveling at speeds that seem to exceed the posted speed limit. Vehicles are also accelerating after turning onto School Street, which is loud and disruptive. Speeding as a concern is especially significant because of the presence of a school and the number of older pedestrians, children, families, bike riders, and other vulnerable road users in the area.

Residents are also concerned about excessive traffic noise. Motorcycles in particular can be **especially loud and disruptive**, affecting the peaceful enjoyment of homes and the overall quality of life in the neighborhood. We recognize that some noise issues may involve vehicle equipment or enforcement questions rather than roadway design alone, but we believe these concerns should be considered as part of a broader assessment of traffic conditions on School Street.

We therefore respectfully request that the City, working through the appropriate departments and committees, consider conducting a study that examines:

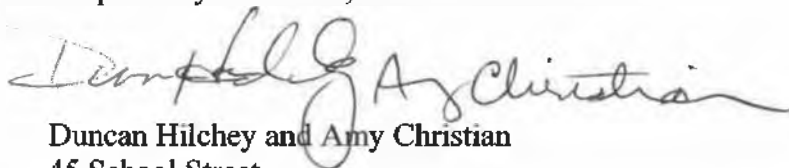
- Vehicle traffic volumes and patterns on School Street;
- Vehicle speeds and the prevalence of speeding;
- The extent to which School Street may be functioning as a cut-through route for drivers seeking to avoid Central Square or other downtown congestion;
- Pedestrian and school-related safety conditions;
- Traffic noise and particularly disruptive motorcycle and vehicle noise; and
- Potential opportunities for traffic calming, safety improvements, enforcement, or other appropriate measures.

We are not presupposing what the appropriate solution should be. Rather, we believe that a careful, data-informed assessment would provide the City and neighborhood residents with a clearer understanding of current conditions and help identify whether changes are warranted.

Keene's downtown and surrounding neighborhoods are closely interconnected. Maintaining a vibrant central business district should not come at the expense of the safety, livability, and the residential character of nearby streets. We respectfully ask the City to give this matter careful consideration and to work with School Street residents in evaluating possible next steps.

Thank you for your attention to this request and for your service to the City of Keene.

Respectfully submitted,

A handwritten signature in cursive script, appearing to read "Duncan Hilchey and Amy Christian".

Duncan Hilchey and Amy Christian
45 School Street
Keene, NH

PS: we want to acknowledge our appreciation of the Keene Public Works Department for following through with our request to add the radar speed sign for a month or so earlier this year. Although, we're not sure it made much of a difference. ☺

August 31, 2026

To Mayor Jay Kahn and the Keene City Council:

Please consider this letter a formal request that the water and sewer infrastructure be owned by the City of Keene for the approved Cottage Court Community on Court Street in Keene, New Hampshire.

The Cottage Court Community is an approved, phased 29-home development, consisting of:

- Phase 1: 12 homes
- Phase 2: 11 homes
- Phase 3: 6 homes

Thank you for your time and consideration. We appreciate your attention to this request and look forward to working with the City of Keene to move this project forward.

Sincerely,

Michael Guitard
Guitard Homes LLC



0 COURT ST.

Location	0 COURT ST.	Map/Lot #	228/ / 016/000 000/000
Acct#	2280160000000000	Owner	GUITARD HOMES LLC
Building Name		Total Market Va...	\$366,700
Appraisal	\$366,700	PID	5667
Building Count	1		

Current Value

Appraisal			
Valuation Year	Improvements	Land	Total
2026	\$0	\$366,700	\$366,700
Assessment			
Valuation Year	Improvements	Land	Total
2026	\$0	\$366,700	\$366,700

Parcel Addresses

Additional Addresses
No Additional Addresses available for this parcel

Owner of Record

Owner	GUITARD HOMES LLC	Sale Price	\$320,000
Co-Owner		Book	3290
Address	PO BOX 604	Page	1192
	JAFFREY, NH 03452	Sale Date	10/11/2024

Ownership History

Ownership History				
Owner	Sale Price	Sale Date	Book	Page
GUITARD HOMES LLC	\$320,000	10/11/2024	3290	1192
CHESHIRE HEALTH FOUNDATION	\$700,000	12/05/2006	2404	0063
WELDON LEONARD L.	\$250,000	07/09/2001	1818	0140
SBD TRUST	\$0	12/31/1973	0877	0241

Building Information

Building 1 : Section 1

Year Built:
Living Area: 0
Replacement Cost: \$0
Building Percent Good:
Replacement Cost
Less Depreciation: \$0

Building Attributes	
Field	Description
Style	Vacant Land
Model:	
Grade	
Stories	
Occupancy	
Exterior Wall 1	
Exterior Wall 2	
Roof Structure:	
Roof Cover	
Interior Wall 1	
Interior Wall 2	
Interior Floor 1	
Interior Floor 2	
Heat Fuel	
Heat System	
Central AC	
Bedrooms	
Full Baths	
Half Baths	
ADU	
Bsmt Garage	
MH Park	
FBLA	
Prefab Fireplace	
WBFP Openings	
Heat Pump	
Generator	

Building Photo



(/photos/KeeneNHPhotos//\0007\263.0.jpg)

Building Layout

(ParcelSketch.ashx?pid=5667&bid=5667)

Building Sub-Areas (sq ft)	Legend
No Data for Building Sub-Areas	

Extra Features

Extra Features	Legend
No Data for Extra Features	

Land

Land Use	Land Line Valuation
Use Code 200	Size (Acres) 9.7
Description Comm Dev Vacant	Depth
Zone LD	Total Market Land \$366,700
Category	Appraised Value \$366,700

Outbuildings

Outbuildings	Legend
No Data for Outbuildings	

Valuation History

Appraisal			
Valuation Year	Improvements	Land	Total
2025	\$0	\$288,700	\$288,700

Assessment			
Valuation Year	Improvements	Land	Total
2025	\$0	\$288,700	\$288,700

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Prepared by:
Shaheen & Gordon, P.A.
20 Depot Street, Suite 220
Peterborough, NH 03458

E Doc # 2606493 08/18/2026 08:25:07 AM
Book 3359 Page 1123 Page 1 of 3
Register of Deeds, Cheshire County

Anna Z Tift

Exempt from Transfer Tax Pursuant to RSA 78-B:2, I
Exempt from LChip Pursuant to RSA 478:17-g, II(a)

UTILITY EASEMENT

KNOW ALL PERSONS BY THESE PRESENTS:

That, **Guitard Homes, LLC**, a New Hampshire limited liability company, with an address of P.O. Box 604, Jaffrey, NH 03452 (the "**Grantor**"),

for consideration paid,

grants to the **City of Keene**, a New Hampshire municipal corporation, with an address of 3 Washington Street, Keene, NH 03431 (the "**Grantee**"),

with WARRANTY COVENANTS:

A permanent ten (10) foot wide easement over land of the Grantor identified as Parcel 228-016 on the easterly side of Court Street, City of Keene, County of Cheshire and State of New Hampshire, to construct, install, maintain, repair, remove and replace, underground sewer pipes and waterline pipes, and appurtenant gates, valves, pumps, manholes and other incidental equipment.

The permanent right and easement to use the area on, over, and under that portion of the Grantor's common area property located within the boundaries of the roadways to be laid out within the Woodland Trail Village Condominium.

MEANING AND INTENDING to describe and convey an interest in land of the Grantor as described the Warranty Deed from The Cheshire Health Foundation, dated October 11, 2024, recorded at the Cheshire County Registry of Deeds (the "**Registry**"), Book 3290, Page 1192.

In exercising its rights under this easement, the Grantee, its successors and assigns, shall be obligated to restore, at its expense, all disturbed areas as nearly as practical as their former

condition, and shall take measures to control and minimize soil erosion, including, without limitation, seeding with grasses and other erosion controlling vegetation.

In granting this easement, the Grantor covenants on behalf of itself, and its successors and assigns, not to erect or permit to be erected upon the described easement area any structure, nor to do any act which would interfere with the use of said easement by the Grantee, or its successors and assigns.

Homestead rights do not apply to this Grantor.

[SIGNATURE TO FOLLOW]

Dated this 17th day of August, 2026

Guitard Homes, LLC

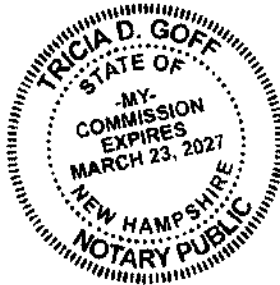
By: [Signature]
Michael D. Guitard, Manager
Duly Authorized

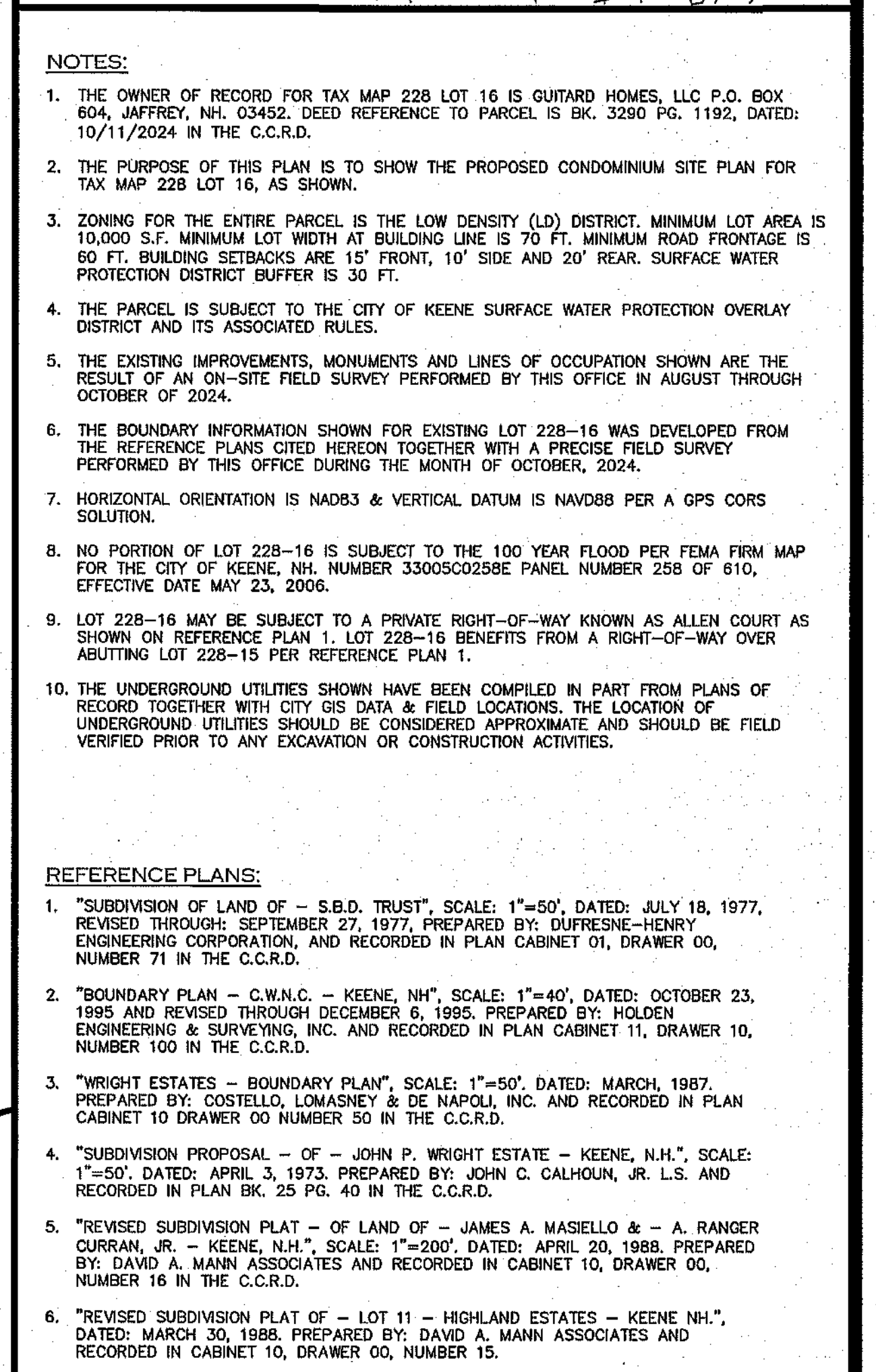
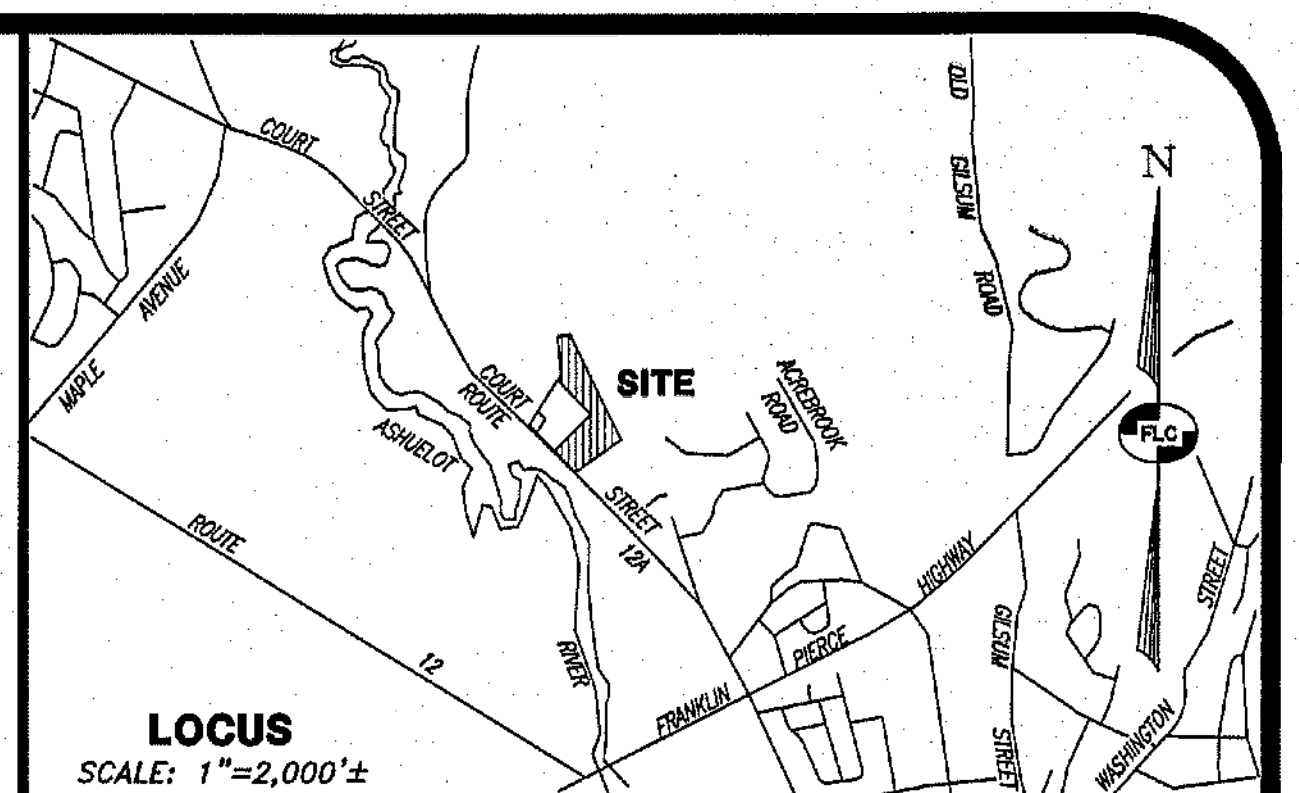
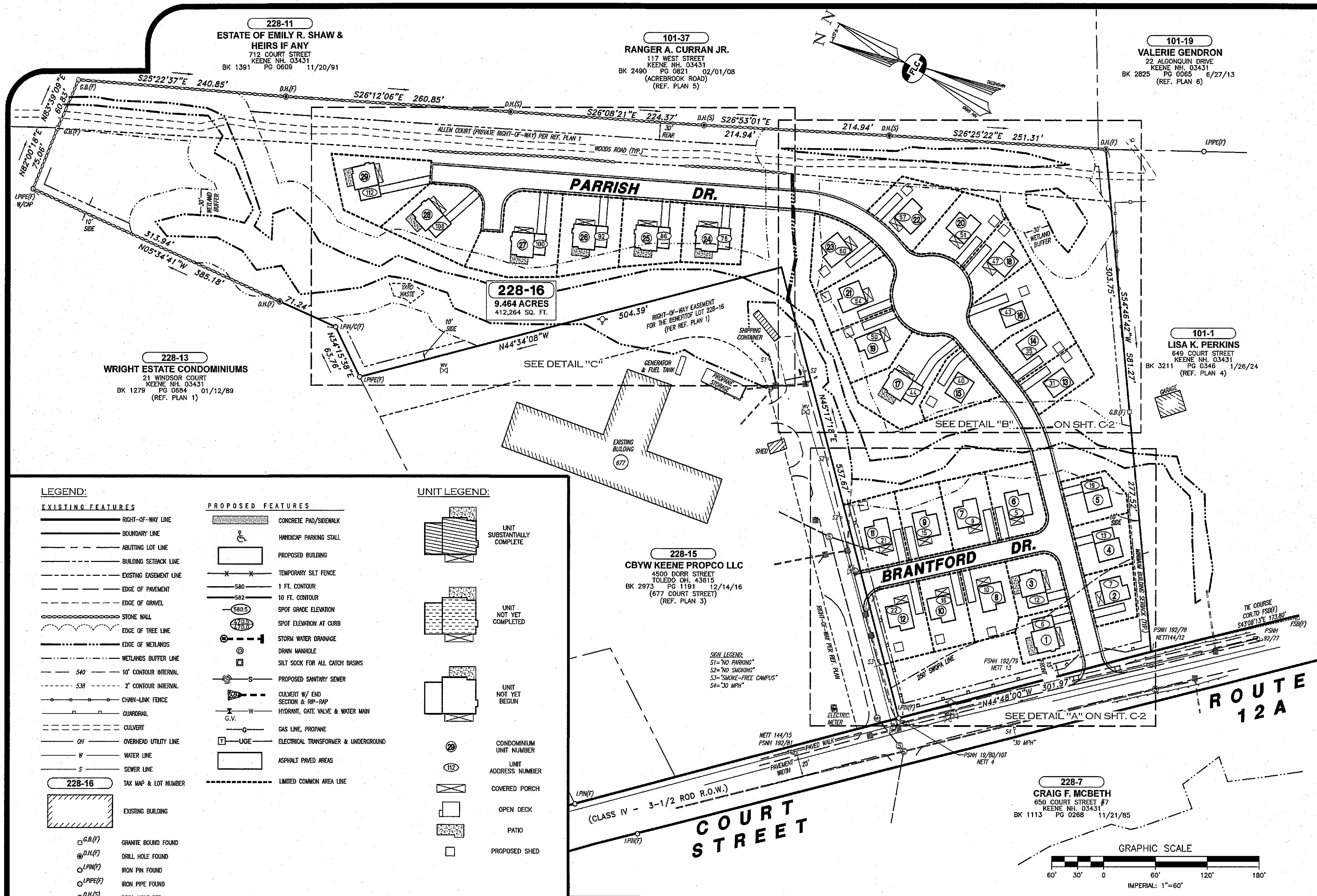
STATE OF NEW HAMPSHIRE
COUNTY OF Hillsborough, ss.

The foregoing instrument was acknowledged before me, the undersigned officer, this 17th day of August, 2026, by Michael D. Guitard, as the duly authorized Manager of Guitard Homes, LLC known to me or satisfactorily proven to be the same, and stated that they executed this instrument as their free and voluntary act for the purposes therein expressed on behalf of the said company.

[Signature]
Notary Public/Justice of the Peace

AS-FIX SEAL





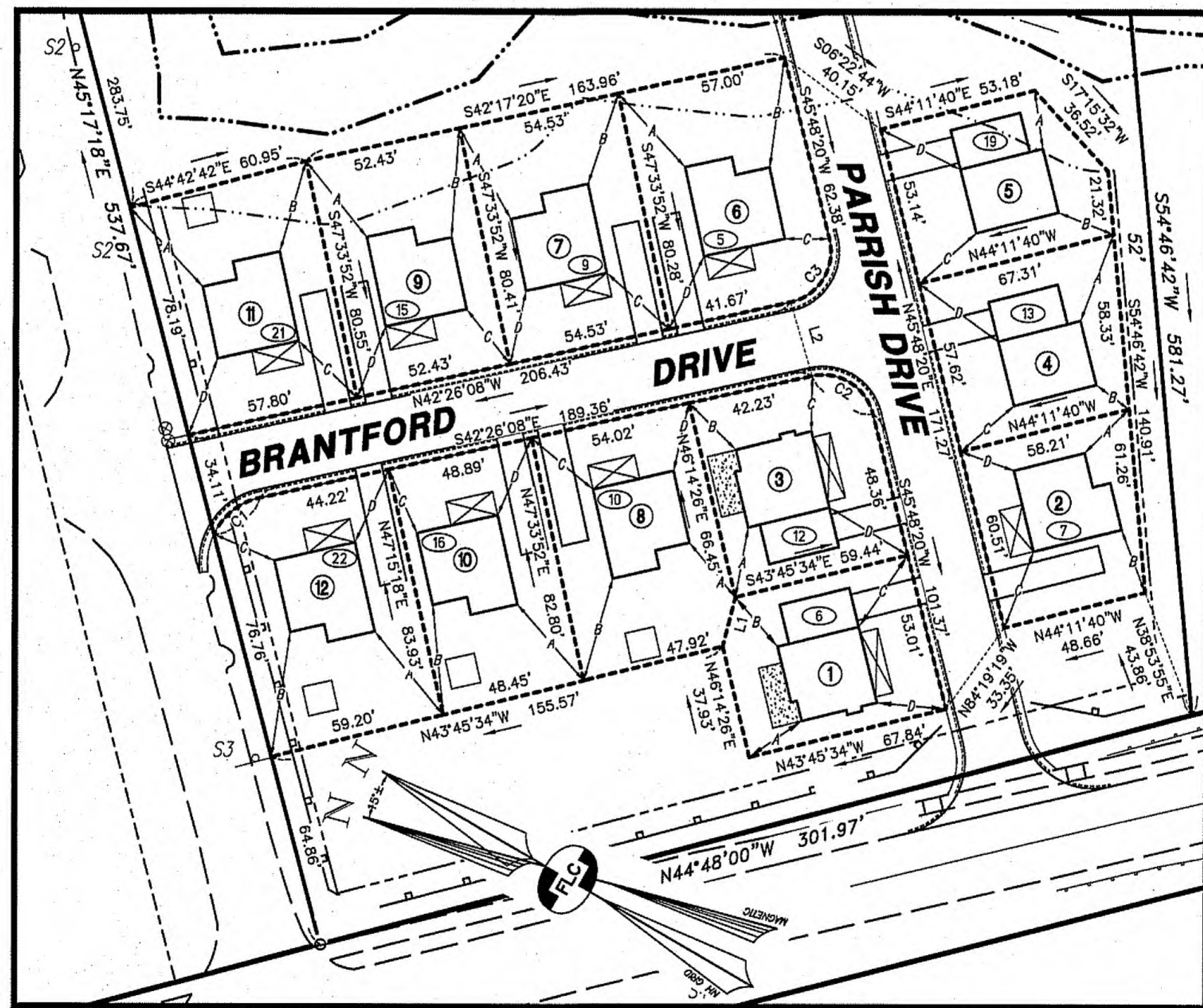
CERTIFICATION:

"I HEREBY CERTIFY AS TO THE ACCURACY OF THIS PLAN AND ITS COMPLIANCE WITH RSA 356-B:20, I & V, AND THAT CONSTRUCTION FOR UNIT 1 THRU UNIT 29 HAS NOT YET BEGUN."

"AND, I CERTIFY THAT THIS SURVEY PLAT IS NOT A SUBDIVISION PURSUANT TO THIS TITLE AND THAT THE LINES OF STREETS AND WAYS SHOWN ARE THOSE OF PUBLIC OR PRIVATE STREETS OR WAYS ALREADY ESTABLISHED AND THAT NO NEW WAYS ARE SHOWN. (RSA 676:18,III & 672:14)."

3/24/26

REV.	DATE	DESCRIPTION	C/O	DR	CK



LIMITED COMMON AREAS
DETAIL C-2A

BUILDING TIES - UNIT 1		
A	S64°29'57"E	21.25'
B	S19°19'47"W	21.67'
C	N85°52'28"W	29.08'
D	N24°53'56"W	23.22'

BUILDING TIES - UNIT 2		
A	N74°44'20"W	20.82'
B	N32°21'39"E	22.37'
C	N80°50'18"E	27.35'
D	S10°48'18"E	18.96'

BUILDING TIES - UNIT 3		
A	N70°24'24"E	23.67'
B	S14°32'18"W	22.19'
C	S60°32'50"W	18.46'
D	N00°59'31"E	30.67'

BUILDING TIES - UNIT 4		
A	S81°00'12"W	30.77'
B	N08°17'52"E	13.58'
C	S70°37'29"E	23.53'
D	S05°50'18"W	32.81'

BUILDING TIES - UNIT 5		
A	S51°36'42"W	19.60'
B	N08°22'02"W	19.89'
C	S71°22'01"E	25.49'
D	S03°30'25"E	29.91'

BUILDING TIES - UNIT 6		
A	S17°22'40"W	33.24'
B	S67°34'21"W	36.86'
C	N30°11'13"W	15.02'
D	N65°41'31"E	28.83'

BUILDING TIES - UNIT 7		
A	S30°00'44"W	34.89'
B	S63°56'19"W	31.75'
C	N11°29'08"E	27.95'
D	N72°09'08"E	24.89'

BUILDING TIES - UNIT 8		
A	N19°34'14"E	23.84'
B	N75°43'28"E	35.45'
C	S09°26'48"W	26.83'
D	S72°35'58"W	23.13'

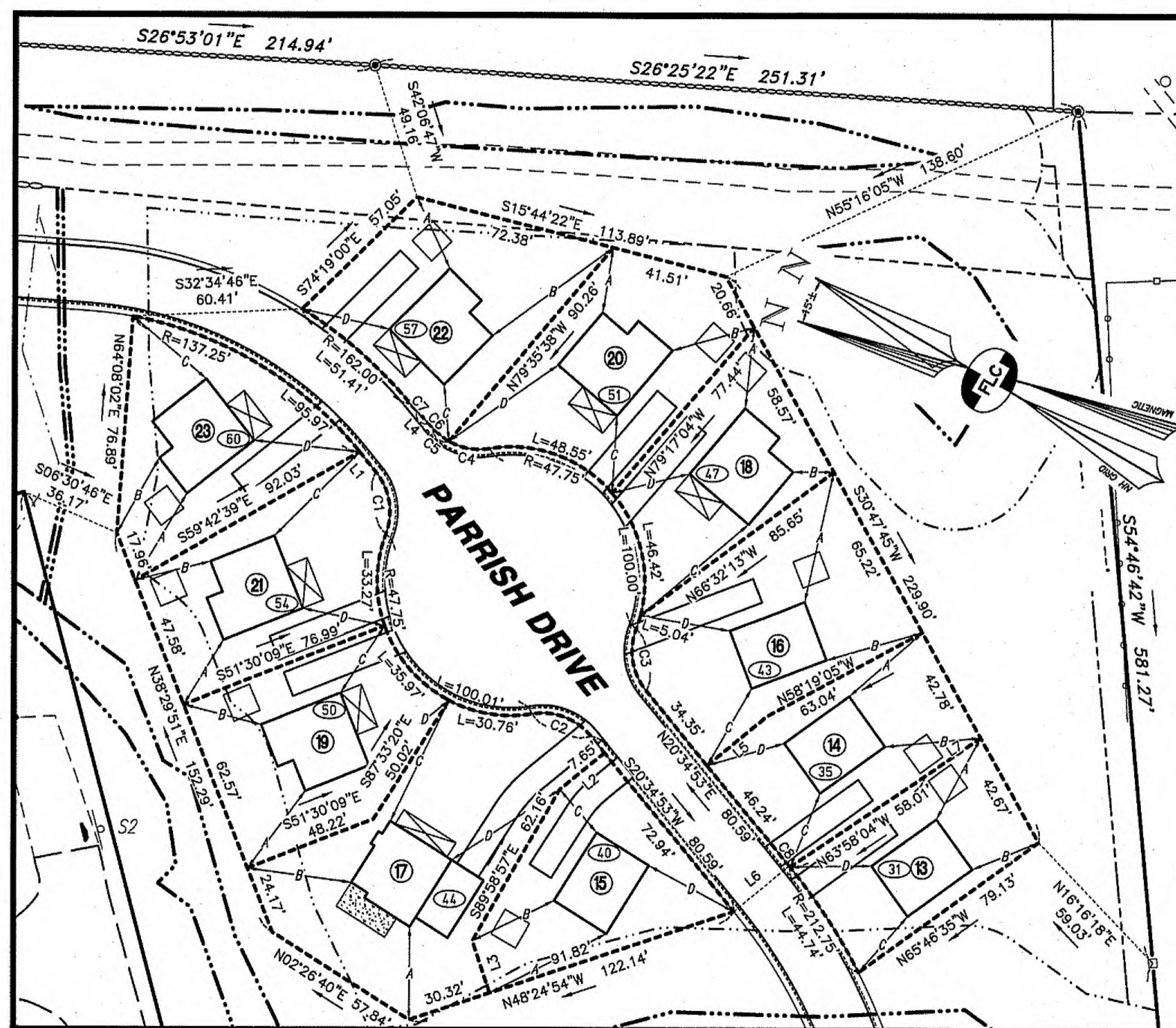
BUILDING TIES - UNIT 9		
A	S20°59'18"W	32.43'
B	S63°56'19"W	36.50'
C	N21°09'48"E	24.41'
D	N81°47'55"E	25.52'

BUILDING TIES - UNIT 10		
A	N18°52'15"E	33.93'
B	N54°27'31"E	37.02'
C	S35°44'11"W	23.24'
D	S83°38'12"W	27.95'

BUILDING TIES - UNIT 11		
A	S17°06'32"W	35.83'
B	S76°09'36"W	31.34'
C	N13°34'18"E	27.20'
D	N80°57'52"E	27.42'

BUILDING TIES - UNIT 12		
A	N20°27'04"E	36.14'
B	N68°29'41"E	42.16'
C	S06°45'35"E	22.08'
D	S84°45'38"W	26.77'

BOUNDARY LINE CHART CD-2A		
L1	N74°11'16"E	17.07'
C1	R=22.25'	L=20.24'
C2	R=17.75'	L=26.57'
L2	N43°26'55"E	25.57'
C3	R=17.25'	L=27.63'



LIMITED COMMON AREAS
DETAIL C-2B

BUILDING TIES - UNIT 24		
A	N01°22'00"W	30.33'
B	S72°11'54"E	33.12'
C	S41°48'52"W	30.00'
D	N68°38'40"E	37.47'

BUILDING TIES - UNIT 25		
A	N05°21'17"W	30.02'
B	S80°50'53"E	29.22'
C	S34°11'31"W	34.04'
D	N72°25'51"W	40.75'

BUILDING TIES - UNIT 26		
A	N05°13'25"E	33.94'
B	N87°20'12"E	39.62'
C	S39°40'22"W	37.05'
D	N75°17'08"W	44.53'

BUILDING TIES - UNIT 27		
A	N05°55'07"E	35.20'
B	S44°52'47"E	55.18'
C	S32°07'39"E	11.50'
D	N84°38'28"W	56.24'

BUILDING TIES - UNIT 28		
A	N12°04'47"E	60.98'
B	S53°50'36"E	39.86'
C	S63°47'53"W	16.89'
D	N07°30'03"W	47.05'

BUILDING TIES - UNIT 29		
A	S89°40'22"E	28.47'
B	S03°48'52"W	28.71'
C	N31°50'15"W	66.60'
D	N31°24'09"W	59.14'

BOUNDARY LINE CHART CD-2C		
L1	S64°07'39"W	26.15'
L2	S25°51'58"E	40.39'
C1	R=23.00'	L=36.13'
L3	S64°08'02"E	26.85'
L4	S25°51'58"E	24.75'
L5	N84°08'02"E	27.00'
C2	R=22.25'	L=34.95'

UNIT NO.	ADDRESS	DETAIL	LCA SIZE	STYLE
1	6 PARRISH DRIVE	C-2A	3,525 SF	3 W/GARAGE RT.
2	7 PARRISH DRIVE	C-2A	3,233 SF	2
3	12 PARRISH DRIVE	C-2A	3,833 SF	3 W/GARAGE LT.
4	13 PARRISH DRIVE	C-2A	3,616 SF	1 W/GARAGE LT.
5	19 PARRISH DRIVE	C-2A	3,439 SF	1 W/GARAGE LT.
6	5 BRANTFORD DRIVE	C-2A	4,601 SF	2
7	9 BRANTFORD DRIVE	C-2A	4,381 SF	2
8	10 BRANTFORD DRIVE	C-2A	4,456 SF	2
9	15 BRANTFORD DRIVE	C-2A	4,220 SF	2
10	16 BRANTFORD DRIVE	C-2A	4,087 SF	2
11	21 BRANTFORD DRIVE	C-2A	4,711 SF	2
12	22 BRANTFORD DRIVE	C-2A	5,086 SF	2
13	31 PARRISH DRIVE	C-2B	3,464 SF	1
14	35 PARRISH DRIVE	C-2B	4,016 SF	1
15	40 PARRISH DRIVE	C-2B	3,991 SF	1
16	43 PARRISH DRIVE	C-2B	5,548 SF	1
17	44 PARRISH DRIVE	C-2B	6,977 SF	3 W/GARAGE LT.
18	47 PARRISH DRIVE	C-2B	3,719 SF	2
19	50 PARRISH DRIVE	C-2B	4,680 SF	2
20	51 PARRISH DRIVE	C-2B	4,803 SF	2
21	54 PARRISH DRIVE	C-2B	4,698 SF	2
22	57 PARRISH DRIVE	C-2B	4,845 SF	2
23	60 PARRISH DRIVE	C-2B	4,579 SF	2
24	78 PARRISH DRIVE	C-2C	4,661 SF	3 W/GARAGE LT.
25	86 PARRISH DRIVE	C-2C	5,233 SF	3 W/GARAGE LT.
26	92 PARRISH DRIVE	C-2C	6,181 SF	3 W/GARAGE LT.
27	100 PARRISH DRIVE	C-2C	7,989 SF	3 W/GARAGE LT.
28	108 PARRISH DRIVE	C-2C	6,967 SF	3 W/GARAGE LT.
29	112 PARRISH DRIVE	C-2C	6,601 SF	3 W/GARAGE LT.

BUILDING TIES - UNIT 13		
A	S88°23'11"W	33.01'
B	N52°11'23"W	25.54'
C	S78°51'53"E	28.50'
D	S30°53'35"E	28.09'

BUILDING TIES - UNIT 14		
A	N71°01'55"W	34.58'
B	N35°22'08"W	34.28'
C	N82°02'47"E	28.36'
D	S46°11'06"E	28.64'

BUILDING TIES - UNIT 15		
A	S56°48'36"E	46.60'
B	S56°01'48"E	32.41'
C	S19°32'42"W	20.32'
D	N02°00'10"W	34.39'

BUILDING TIES - UNIT 16		
A	S72°30'18"W	47.30'
B	N45°59'01"W	35.61'
C	S89°56'57"E	31.64'
D	S20°30'40"E	32.39'

BUILDING TIES - UNIT 17		
A	N50°20'41"E	32.93'
B	S22°19'29"E	37.34'
C	S85°48'54"W	47.69'
D	N63°22'23"W	46.11'

BUILDING TIES - UNIT 18		
A	S66°12'18"W	28.76'
B	N29°54'56"W	14.51'
C	S70°08'43"E	56.30'
D	S44°03'08"E	28.53'

BUILDING TIES - UNIT 19		
A	S33°11'41"E	34.49'
B	S15°37'27"E	28.55'
C	N86°47'16"W	28.68'
D	N69°08'02"W	36.66'

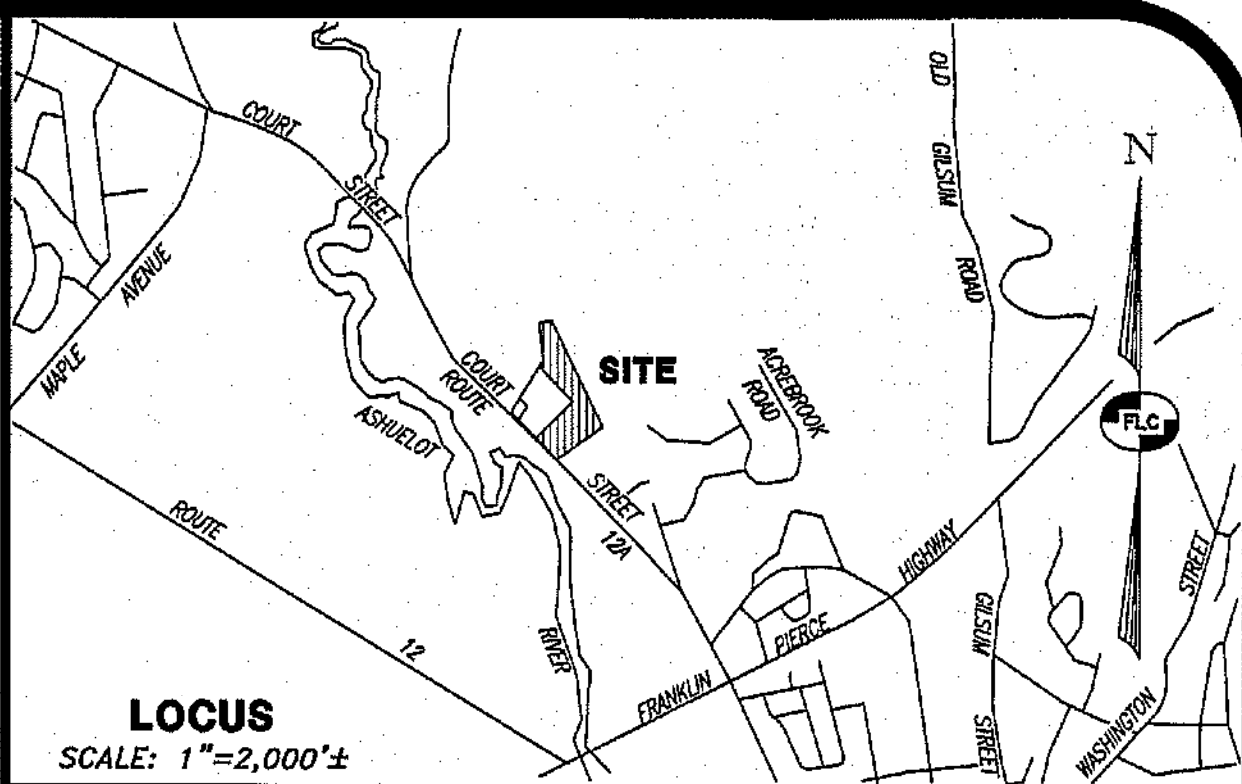
BUILDING TIES - UNIT 20		
A	S89°14'49"W	23.39'
B	N45°41'15"W	30.24'
C	N63°29'54"E	27.39'
D	S64°47'55"E	47.38'

BUILDING TIES - UNIT 21		
A	S85°28'25"E	27.09'
B	S44°03'47"E	27.74'
C	N74°28'41"W	42.53'
D	N18°02'25"W	29.85'

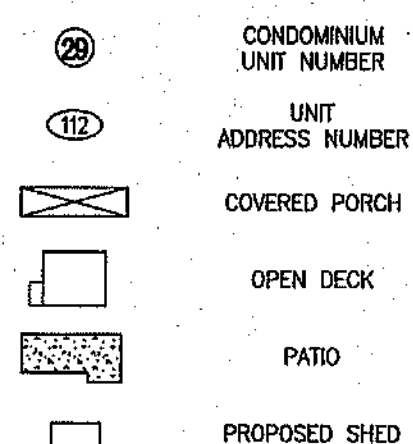
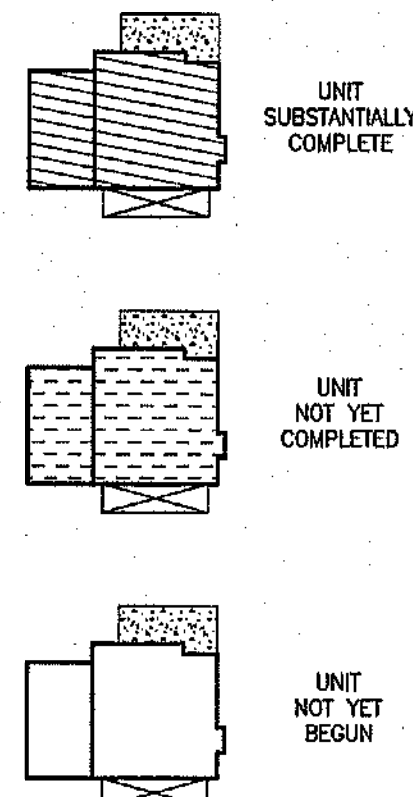
BUILDING TIES - UNIT 22		
A	S35°03'31"W	28.01'
B	N66°08'19"W	53.25'
C	N54°54'55"E	20.15'
D	S17°11'41"E	31.80'

BUILDING TIES - UNIT 23		
A	S89°09'51"E	35.85'
B	N69°28'49"E	30.60'
C	S11°04'50"W	34.25'
D	N23°55'26"W	37.85'

BOUNDARY LINE CHART CD-2B		
L1	S20°34'53"W	9.21'
C1	R=22.25'	L=23.30'
C2	R=22.25'	L=23.30'
L2	N69°25'07"W	20.54'
L3	S41°35'06"W	19.50'
C3	R=22.25'	L=23.30'
C4	R=22.25'	L=15.05'
C5	R=22.25'	L=2.44'
C6	R=50.00'	L=2.55'
L4	N15°55'29"E	5.89'
C7	R=50.00'	L=2.25'
L5	N69°25'07"W	27.46'
L6	N69°25'07"W	25.50'
L7	N64°49'32"W	24.07'
C8	R=212.75'	L=0.56'



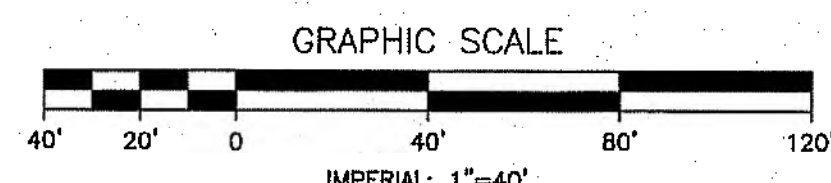
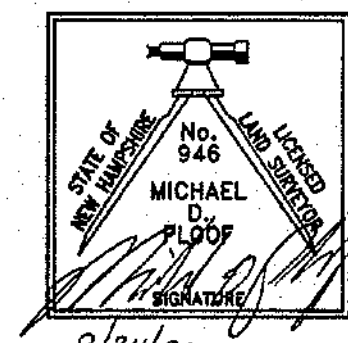
UNIT
LEGEND:



CERTIFICATION:

"I HEREBY CERTIFY AS TO THE ACCURACY OF THIS PLAN AND ITS COMPLIANCE WITH RSA 356-B:20, I & V, AND THAT CONSTRUCTION FOR UNIT 1 THRU UNIT 29 HAS NOT YET BEGUN."

"AND, I CERTIFY THAT THIS SURVEY PLAT IS NOT A SUBDIVISION PURSUANT TO THIS TITLE AND THAT THE LINES OF STREETS AND WAYS SHOWN ARE THOSE OF PUBLIC OR PRIVATE STREETS OR WAYS ALREADY ESTABLISHED AND THAT NO NEW WAYS ARE SHOWN. (RSA 678:18,III & 672:14)."



REV.	DATE	DESCRIPTION	C/O	DR	CK
------	------	-------------	-----	----	----

CONDOMINIUM SITE PLAN
LIMITED COMMON AREAS - UNITS 1 THRU 29
TAX MAP 228 LOT 16
(COURT STREET)
KEENE, NEW HAMPSHIRE
PREPARED FOR & LAND OF:
GUITARD HOMES, LLC
P.O. BOX 604, JAFFREY, NH 03452

SCALE: 1"=40' JANUARY 15, 2026

Surveying ♦ Engineering ♦ Land Planning ♦ Permitting ♦ Septic Designs

FIELDSTONE
LAND CONSULTANTS, PLLC
 206 Elm Street, Milford, NH 03055
 45 Roxbury Street, Keene, NH 03431
 Phone: (603) 672-5456 Fax: (603) 413-5456
 www.FieldstoneLandConsultants.com

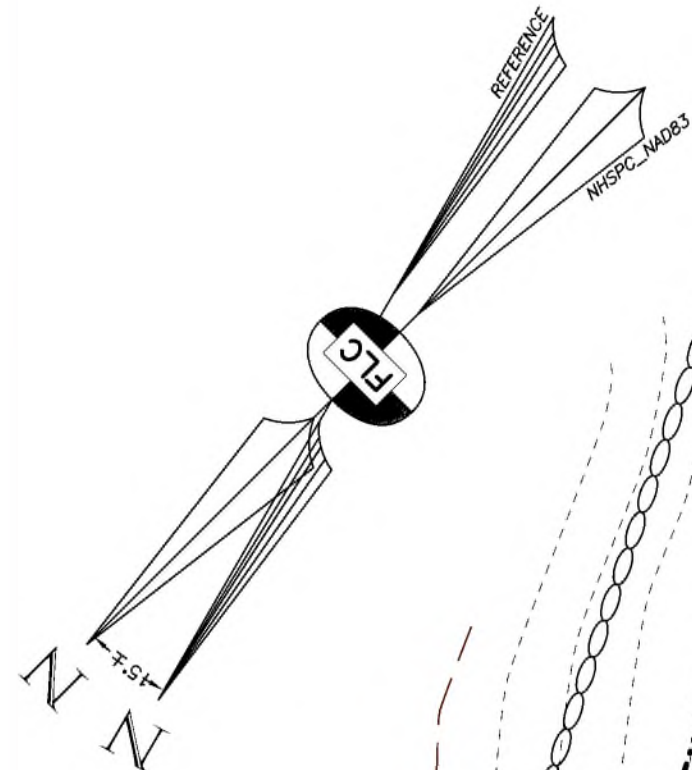
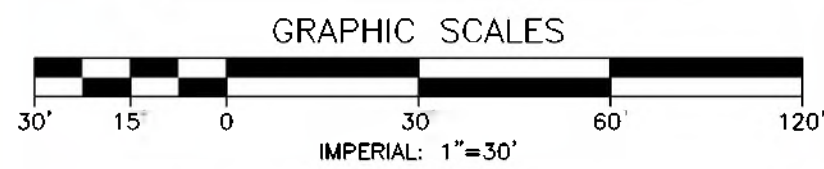
CONTACT DIG SAFE
72 HOURS PRIOR
TO CONSTRUCTION

DIGSAFE.COM
OR DIAL 8 1 1

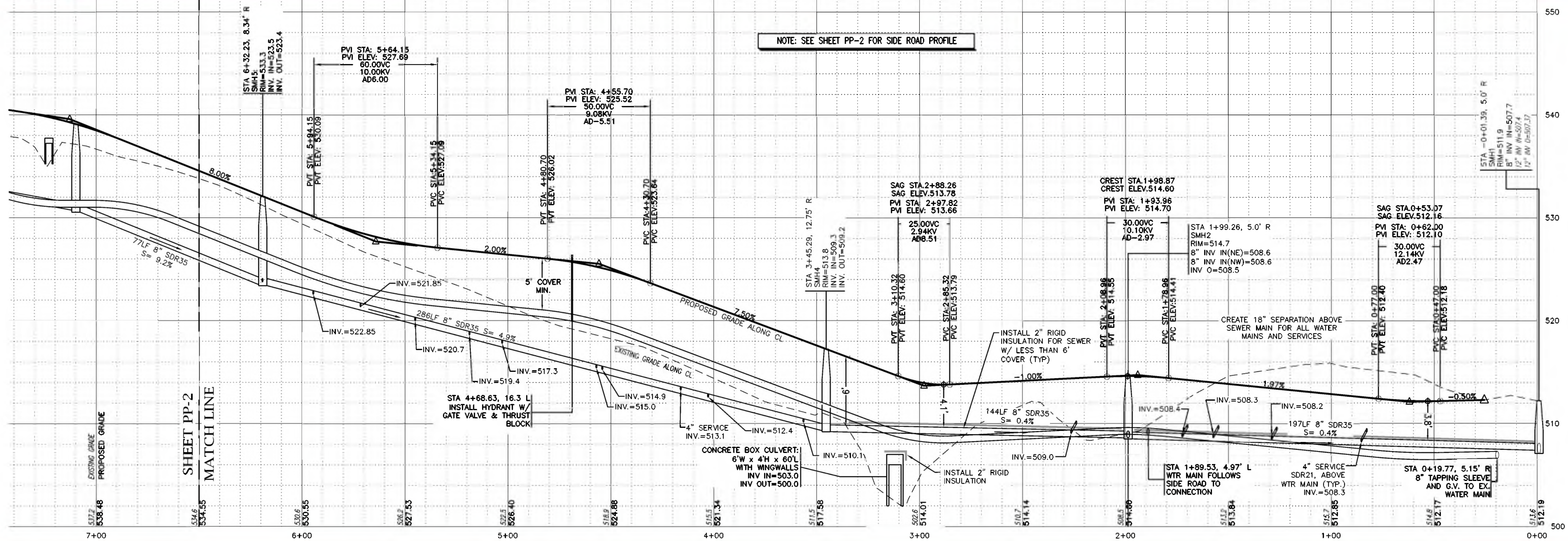
CALL 811 - KNOW WHAT'S BELOW



811
Dig Safe
MADE WITH NY



MATCH LINE
SHEET PP-2



NOTE: SEE SHEET PP-2 FOR SIDE ROAD PROFILE

Surveying ♦ **Engineering** ♦ **Land Planning** ♦ **Permitting** ♦ **Septic Designs**

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45 Roxbury Street, Keene, NH 03431
Phone: (603) 672-5456 **Fax:** (603) 413-5456
www.FieldstonelandConsultants.com

REV.	DATE	DESCRIPTION	DR	CK
A	4/14/25	REYS PER START COMMENTS	CJC	JEN
B	4/24/25	REYS PER START COMMENTS	CJC	JEN
C	9/11/25	REYS FOR INHES SEWER PERMIT	CJC	JEN
D	12/18/25	REYS PER DOT RMI	CJC	JEN
E	12/19/25	REYS PER INHES SEWER RMI	CJC	JEN
F	6/3/26	REYS PER INHES SEWER RMI	CJC	JEN

GUILLARD HOMES
P.O. BOX 604
JAFFREY, NH. 03452

ROAD PROFILE 1 (SOUTH)

GUITARD HOMES

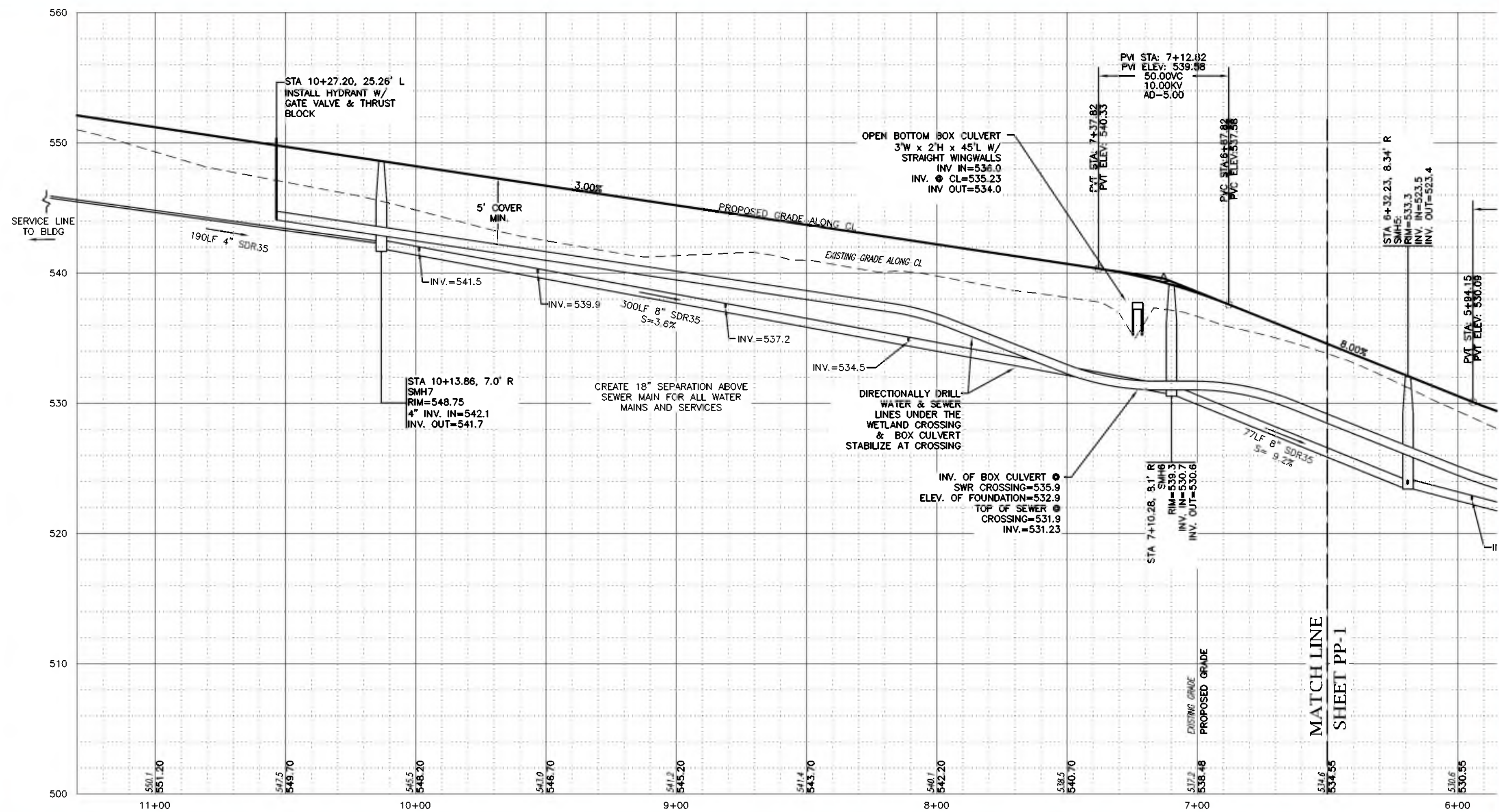
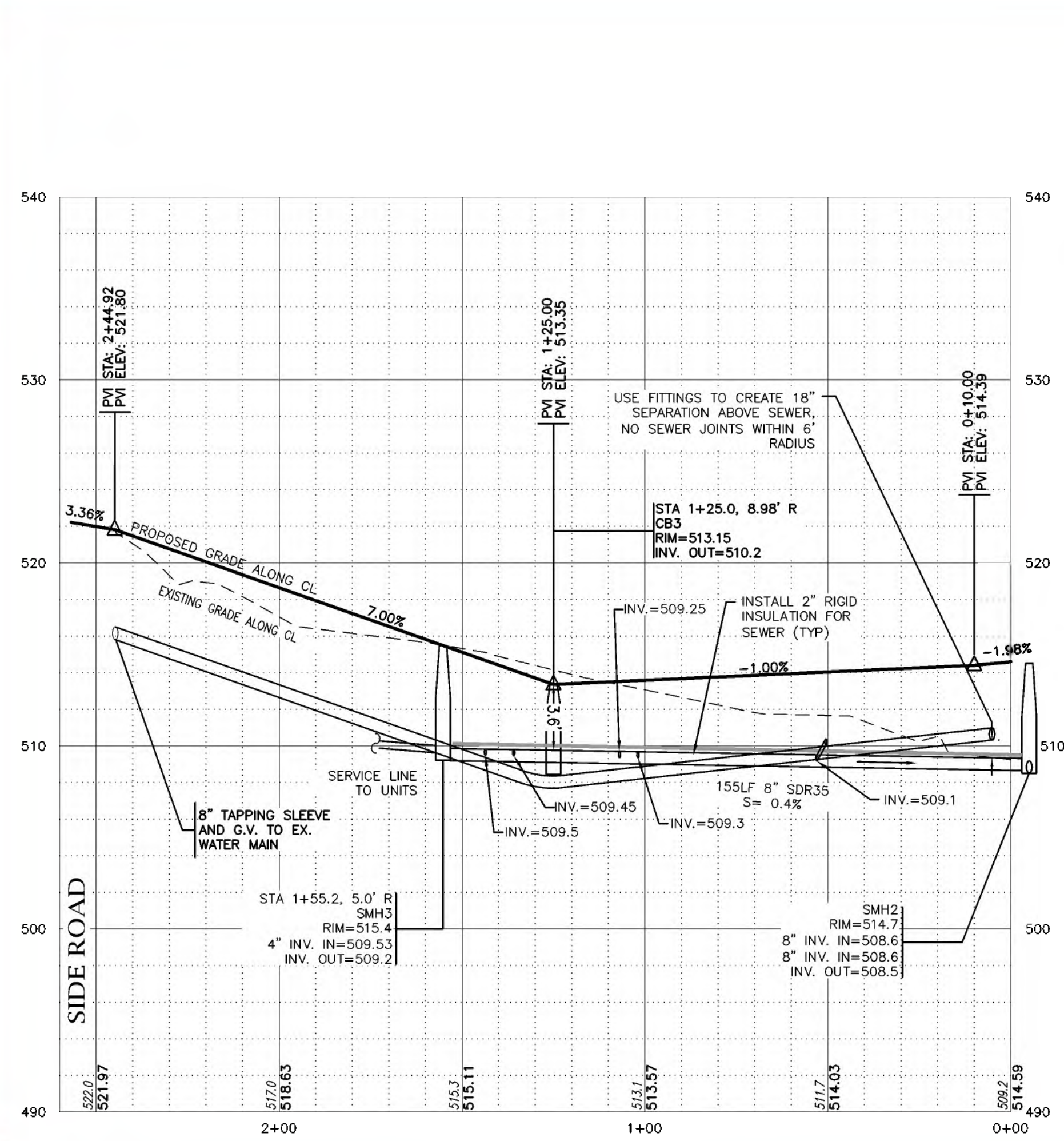
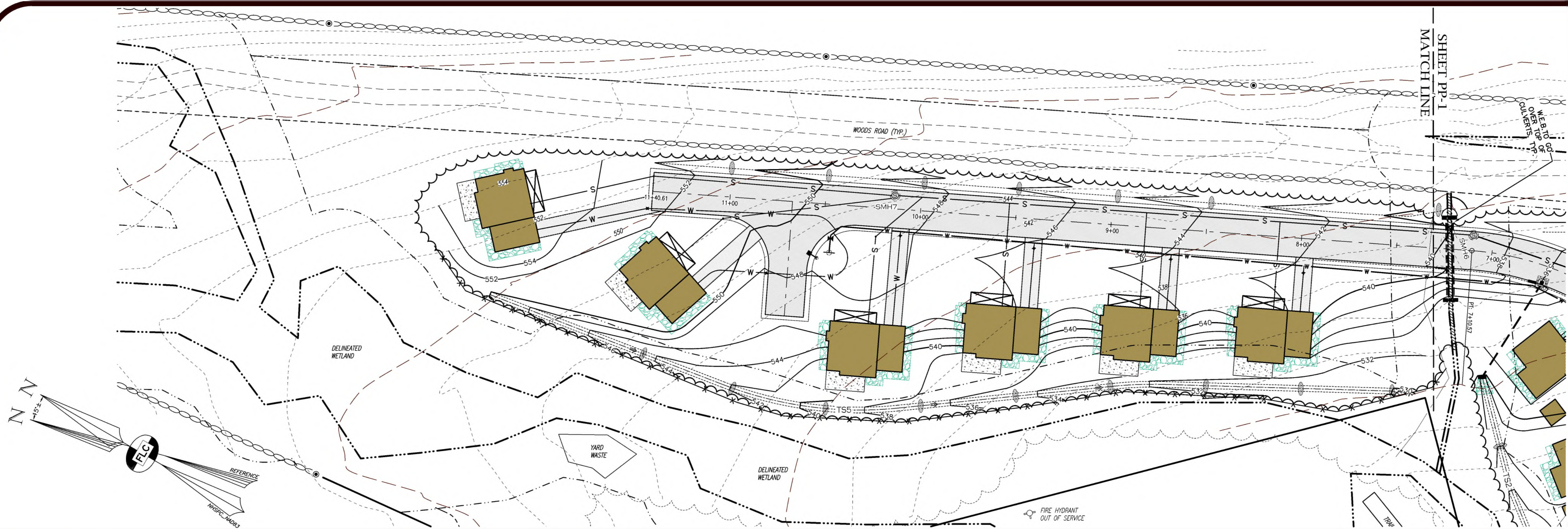
**TAX MAP 228 LOT 16
KEENE, NEW HAMPSHIRE**

MARCH 21, 2025

FILE: 3941SN

PP-1
SHEET

PROJECT NO. 3941.00
SHEET NO. 8 OF 17



Surveying + Engineering + Land Planning + Permitting + Sepsic Designs

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PP-2

SHEET

PROJECT NO. 3941.00
SHEET NO. 9 OF 17

ROAD PROFILE 2 (NORTH)

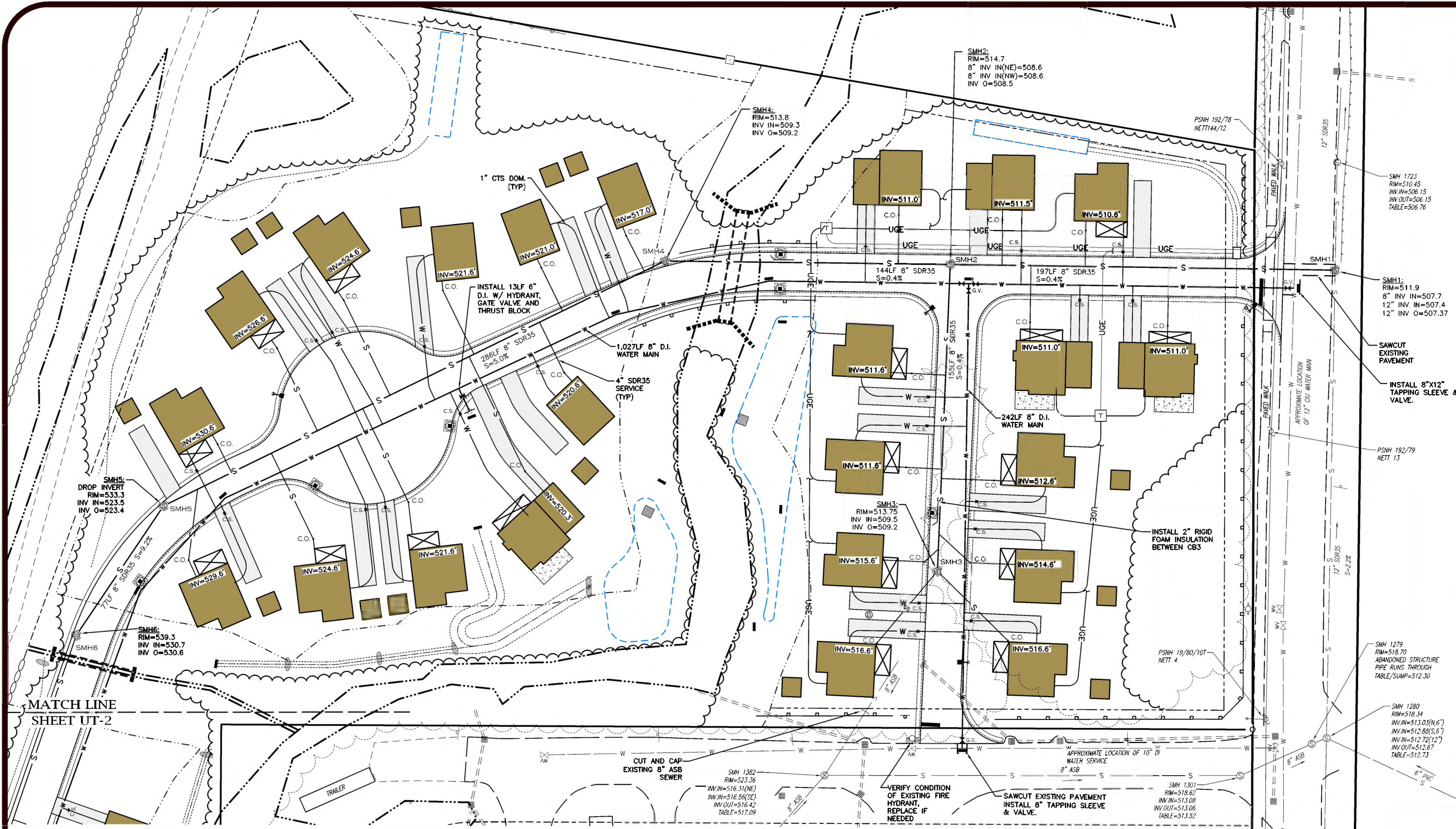
GUITARD HOMES
P.O. BOX 604
JAFFREY, NH. 03452

TAX MAP 228 LOT 16
KEENE, NEW HAMPSHIRE

PLANS ISSUED FOR:
REVIEW

FILE: 3941SP00F.dwg
MARCH 21, 2025
SCALE: 1" = 30'

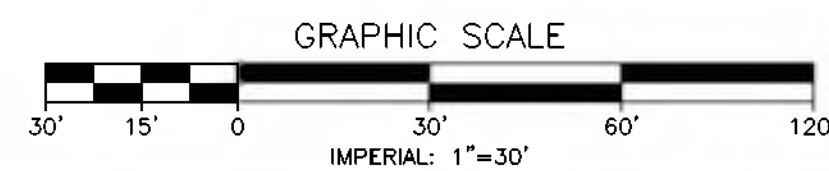
REV	DATE	DESCRIPTION	DR	OK
F	6/2/26	RECS PER NHDES SEWER RMI	CJC JEN	
E	12/19/25	RECS PER NHDES SEWER RMI	CJC JEN	
D	12/18/25	RECS PER NHDES SEWER PERMIT	CJC JEN	
C	9/17/25	RECS FOR NHDES SEWER PERMIT	CJC JEN	
B	4/24/25	RECS PER STAFF COMMENTS	CJC JEN	
A	4/14/25	RECS PER STAFF COMMENTS	CJC JEN	



- NOTES:**
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 - INSTALL PRECAST THRUST BLOCKS AT ALL FITTINGS ON WATER MAIN. MECHANICALLY RESTRAIN TEE FITTINGS.

SANITARY SEWER CALCULATIONS:

- DAILY FLOW RATE: 150 GPD/BED X 81 BEDS = 12,150 GPD
- INFILTRATION: 1,088 LF 8" SDR 35 (0.21 MILE)
[(8"x0.21 MI)] x 300 GPD/INCH-MILE = 504 GPD
- AVERAGE DAILY FLOW: 12,150 + 504 = 12,654 GPD
- PEAK DESIGN FLOW: PEAKING FACTOR = 6 (< 100,000 GPD)
6 x 12,654 GPD = 75,924 GPD



LEGEND:

EXISTING FEATURES

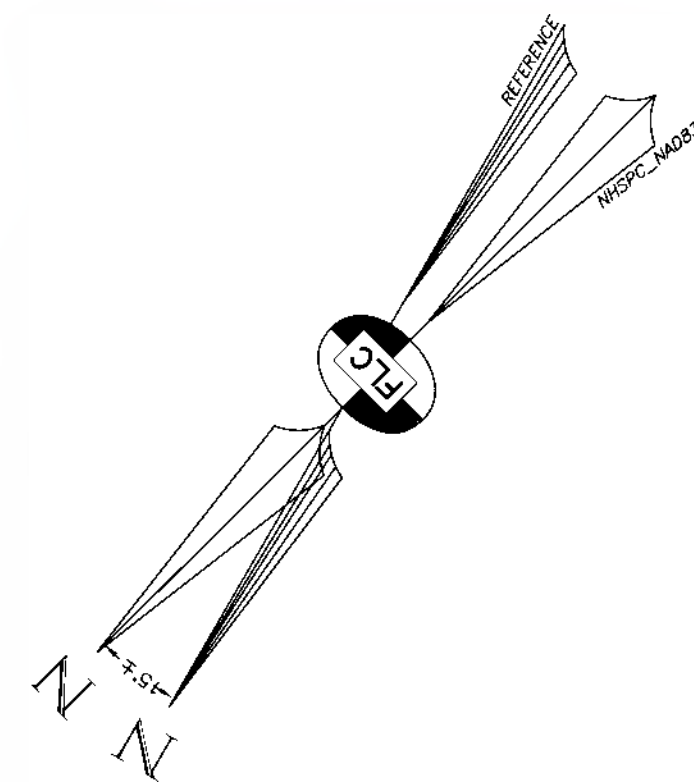
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- WETLANDS BUFFER LINE
- 10' CONTOUR INTERVAL
- 2' CONTOUR INTERVAL
- CHAIN-LINK FENCE
- GUARDRAIL
- CULVERT
- OVERHEAD UTILITY LINE
- WATER LINE

- SEWER LINE**
- TAX MAP & LOT NUMBER**
- EXISTING BUILDING**
- G.B.(F)
 - D.H.(F)
 - I.PIN(F)
 - I.PIPE(F)
 - D.H.(S)
 - F.S.B.(F)
 - GRANITE BOUND FOUND
 - DRILL HOLE FOUND
 - IRON PIN FOUND
 - IRON PIPE FOUND
 - DRILL HOLE SET
 - FIELD STONE BOUND FOUND
 - UTILITY POLE & GUY
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 - WATER HYDRANT
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- RETAINING WALL



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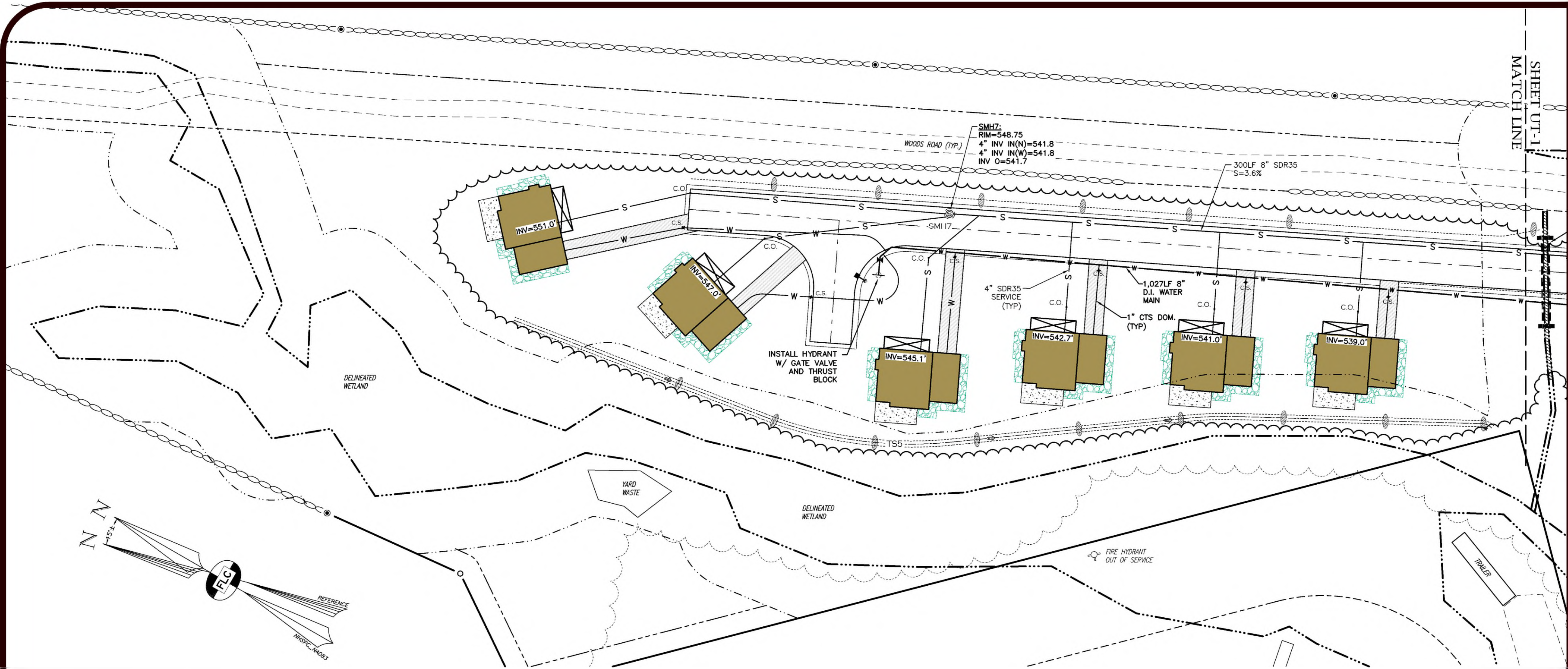
UTILITY PLAN (SOUTH)
TAX MAP 228 LOT 16
(COURT STREET)
KEENE, NEW HAMPSHIRE
PREPARED FOR AND LAND OF:
GUITARD HOMES
P.O. BOX 604 JAFFREY, NH. 03452

SCALE: 1" = 30' MARCH 21, 2025

Surveying ♦ Engineering ♦ Land Planning ♦ Permitting ♦ Septic Designs

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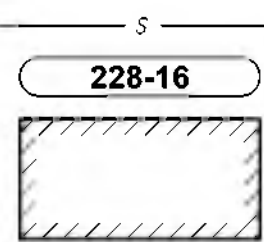
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LEGEND:

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- BOUNDARY LINE
- ABUTTING LOT LINE
- BUILDING SETBACK LINE
- EXISTING EASEMENT LINE
- EDGE OF PAVEMENT
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- CULVERT
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SEWER NOTES:

ENV-WQ 704.05: GRAVITY SEWER PIPE MATERIAL

- JOINTS SHALL BE MECHANICAL TYPE, PUSH-ON TYPE, OR BALL-AND-SOCKET TYPE AS APPROPRIATE FOR THE SPECIFIC APPLICATION.
- PLASTIC GRAVITY SEWER PIPE AND FITTINGS SHALL BE CERTIFIED BY THE MANUFACTURER AS COMPLYING WITH ASTM D3034 FOR 8 INCH THROUGH 15 INCH SDR 35 PVC PIPE.
- PLASTIC SEWER PIPE SHALL HAVE A PIPE STIFFNESS RATING OF AT LEAST 46 POUNDS PER SQUARE INCH AT 5 PERCENT PIPE DIAMETER DEFLECTION, AS MEASURED BY THE MANUFACTURER IN ACCORDANCE WITH THE ASTM D2412 STANDARD IN EFFECT WHEN PIPE WAS MANUFACTURED.

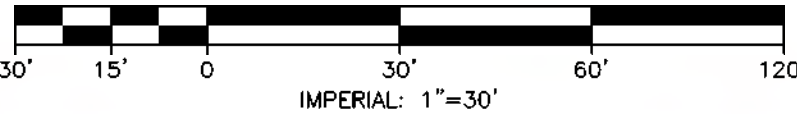
ENV-WQ 704.06: GRAVITY SEWER PIPE TESTING

- ALL NEW GRAVITY SEWERS SHALL BE TESTED FOR WATER TIGHTNESS BY THE USE OF LOW-PRESSURE AIR TESTS.
 - ASTM F1417 "STANDARD TEST METHOD FOR INSTALLATION ACCEPTANCE OF PLASTIC GRAVITY SEWER LINES USING LOW-PRESSURE AIR", AVAILABLE AS NOTED IN APPENDIX D; OR
 - UNI-BELL PVC PIPE ASSOCIATION UNI-B-6, "LOW-PRESSURE AIR TESTING OF INSTALLED SEWER PIPE", AVAILABLE AS NOTED IN APPENDIX D.
- ALL NEW GRAVITY SEWERS SHALL BE:
 - CLEANED AND VISUALLY INSPECTED USING A LAMP TEST AND BY INTRODUCING WATER TO DETERMINE THAT THERE IS NO STANDING WATER IN THE SEWER; AND
 - TRUE TO LINE AND GRADE FOLLOWING INSTALLATION AND PRIOR TO USE.
- ALL PLASTIC SEWER PIPE SHALL BE VISUALLY INSPECTED AND DEFLECTION TESTED NOT LESS THAN 30 DAYS NOR MORE THAN 90 DAYS FOLLOWING INSTALLATION.
- THE MAXIMUM ALLOWABLE DEFLECTION OF FLEXIBLE SEWER PIPE SHALL BE 5% PERCENT OF AVERAGE INSIDE DIAMETER. A RIGID BALL OR MANDREL WITH A DIAMETER OF AT LEAST 95% OF THE AVERAGE INSIDE PIPE DIAMETER SHALL BE USED FOR TESTING PIPE DEFLECTION. THE DEFLECTION TEST SHALL BE CONDUCTED WITHOUT MECHANICAL PULLING DEVICES.

ENV-WQ 704.17: MANHOLES TESTING

- MANHOLES SHALL BE TESTED FOR LEAKAGE USING A VACUUM TEST IN ACCORDANCE WITH THE ASTM C1244 STANDARD IN EFFECT WHEN THE TESTING IS PERFORMED, AVAILABLE AS NOTED IN APPENDIX D. A MANHOLE MAY BE BACKFILLED PRIOR TO PERFORMING A VACUUM TEST, BUT IF THE MANHOLE FAILS THE VACUUM TEST, BACKFILL SHALL BE REMOVED SO REPAIRS TO THE MANHOLE CAN BE MADE FROM THE OUTSIDE OF THE MANHOLE PRIOR TO RETESTING.
- THE MANHOLE VACUUM TEST SHALL CONFORM TO THE FOLLOWING:
 - THE INITIAL VACUUM GAUGE TEST PRESSURE SHALL BE 10 INCHES HG; AND
 - THE MINIMUM ACCEPTABLE TEST HOLD TIME FOR A 1-INCH HG PRESSURE DROP TO 9 INCHES HG SHALL BE:
 - NOT LESS THAN 2 MINUTES FOR MANHOLES LESS THAN 10 FEET DEEP IN DEPTH;
 - NOT LESS THAN 2.5 MINUTES FOR MANHOLES 10 TO 15 FEET DEEP; AND
 - NOT LESS THAN 3 MINUTES FOR MANHOLES MORE THAN 15 FEET DEEP;
- THE MANHOLE SHALL BE REPAIRED AND RETESTED IF THE TEST HOLD TIMES FAIL TO ACHIEVE THE ACCEPTANCE LIMITS SPECIFIED IN (b), ABOVE.
- INVERTS AND SHELVES SHALL NOT BE INSTALLED UNTIL AFTER SUCCESSFUL TESTING IS COMPLETED.
- IMMEDIATELY FOLLOWING COMPLETION OF THE LEAKAGE TEST, THE FRAME AND COVER SHALL BE PLACED ON THE TOP OF THE MANHOLE OR SOME OTHER MEANS USED TO PREVENT ACCIDENTAL ENTRY BY UNAUTHORIZED PERSONS, CHILDREN, OR ANIMALS, UNTIL THE CONTRACTOR IS READY TO MAKE FINAL ADJUSTMENT TO GRADE.

GRAPHIC SCALE



E	6/3/26	REVS PER NHDES SEWER RMI		CJC	JEN
D	12/19/25	REVS PER NHDES SEWER RMI		CJC	JEN
C	9/11/25	REVS FOR NHDES SEWER PERMIT		CJC	JEN
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REV.	DATE	DESCRIPTION	C/O	DR	CK

UTILITY PLAN (NORTH)

TAX MAP 228 LOT 16

(COURT STREET)

KEENE, NEW HAMPSHIRE

PREPARED FOR AND LAND OF:

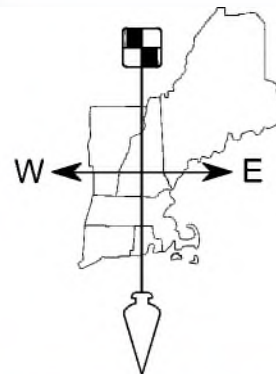
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SCALE: 1" = 30'

MARCH 21, 2025

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September 15, 2026

Dear Mayor Jay Kahn and Members of the Keene City Council,

I am writing to formally request that the private water and sewer infrastructure serving Elmside Cottage Court **not** require a main water meter to be located within a stand-alone heated building. I appreciate the time Public Works and the City Manager have already taken to discuss this matter and understand the circumstances surrounding the development.

Elmside Cottage Court is an approved 18-home development consisting of two phases:

- **Phase 1:** Fifteen homes along the outer perimeter
- **Phase 2:** Three centrally located homes

Our request is based on several concerns associated with maintaining the privately owned infrastructure:

1. **The requirement for a separate heated building** at the entrance of the property to house a main water meter.
 - In the event of a power outage, the building could freeze, disrupting water service to all homes.
 - A single master meter creates challenges in determining fair and accurate billing for individual homeowners.
 - This setup increases long-term costs for residents.

The current design already includes a curb stop at each home, with the intention that each residence would have its own meter, consistent with standard City practice. All necessary easements have been planned, approved, and incorporated into the development plans.

For these reasons, we respectfully request that the City remove the requirement of a main water meter to be located within a stand-alone heated building within Elmside Cottage Court and supply each home with its own meter. We believe this approach will provide the most reliable, equitable, and sustainable outcome for future homeowners.

Thank you for your consideration. I look forward to continuing to work collaboratively with the City of Keene on this development and future projects.

Sincerely,

Shawn Voisine
Elmside LLC

[Redacted]
[Redacted]

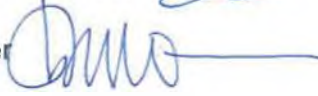




DATE: September 22, 2026

TO: MSFI Committee

THRU: Elizabeth A. Ferland, City Manager
Donald R. Lussier, P.E., Public Works Director 

FROM: Bryan M. Ruoff, P.E., City Engineer 

RE: Elmside Cottage Court, City of Keene PB Case#26-29
Petition to Waive Public Works Standard for Private Utilities

Recommendation:

Move to recommend that the City of Keene Municipal, Services, Facilities and Infrastructure Committee recommend denying the portioners request to waiver City of Keene Public Works standards for the referenced project.

Background:

On September 15, 2026, the developer for the Elmside Cottage Court development project petitioned the Mayor and City Council to request a waiver from the requirements of the City of Keene Public Works standards. The petition was received by the City Clerks office, was a City Council agenda item and referred to MSFI at the September 17th, 2026 City Council meeting. The waiver, as written specifically requests that the referenced, proposed cottage court development be permitted to install individual curb stop and water meters for each of the proposed 18-residential dwellings on the property being built as part of the project, in lieu of the City of Keene Public Works standard to provide a heated building to house a single water meter and RPZ backflow preventor for the entire development.

We have reviewed the petition and the associated waiver request as it specifically pertains to the Elmside Cottage Court development project and find no circumstances relative to the proposed development that would justify granting the requested waiver for this specific case and associated development.

City of Keene Public Works standards, in conformance with NHDES and EPA regulations, require that a back flow preventor be installed to separate a commercial development from the public water supply system. For the purposes of backflow prevention, "commercial" includes residential properties with three or more dwelling units. The Elmside development proposes a private on-site water distribution system. If approved, the requested waiver would require the City to install maintain, read meters for individual tenants, and to bill each tenant in the development. If we were required to shut down service to an individual tenant, due to non-payment or some other reason, we would be operating a privately owned valve and would become responsible for repair if it was damaged. In addition, NH RSA 38-22 defines the City's rights with respect to utility billing. If a waiver is granted, we would be in a position of placing a lien on the property due to an individual tenant's non-payment.

In discussing this matter with the developer, City Staff discussed the option of designing and installing the system as a public water main. This would make the City fully responsible for the maintenance and operation of the distribution system and would provide a system that met public utility standards.

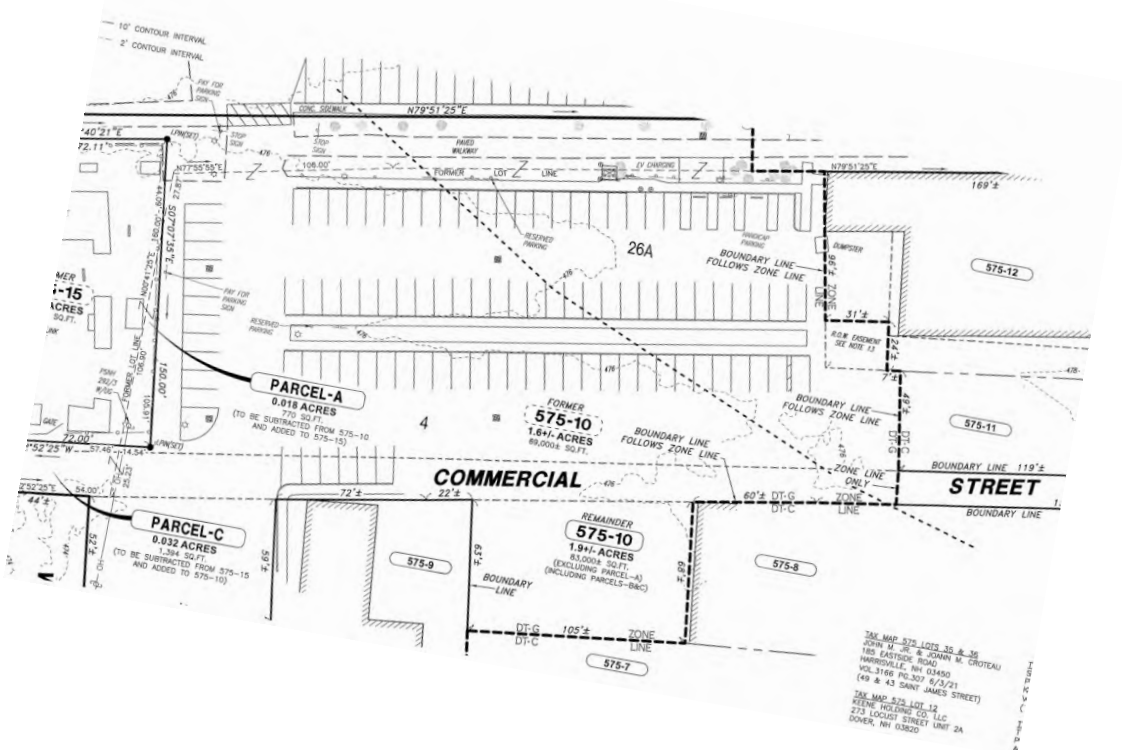
Public Infrastructure	Proposed Development
8" Minimum	4" & 2" water mains
Looped unless waived	Dead-end main
City standard fire hydrants	Flushing hydrants only

Based on these reasons, we recommend denying the portioners' request to waiver City of Keene Public Works standards for the referenced project.

Subject: Site Layout for Proposed Gilbo Avenue Solar Pavilion
August 6, 2026

Dear Mayor and Members of the City Council,

I am writing to ask the City to conduct a comprehensive planning study for the 1.9-acre Gilbo Avenue site before finalizing the layout for the solar pavilion.



Under the current Land Development Code for the Downtown Growth zone, this parcel supports a multi-story mixed-use project with roughly 300 housing units, ground-floor commercial space, structured parking, and public open space. Given our region's need for housing, we need to understand the potential of this entire parcel before locating the solar pavilion in a way that would preclude future development.

Taking time to study the site layout does not mean abandoning renewable energy or the Farmers Market. A master plan can show how housing, commercial space, the market, and a solar pavilion can function together on the same property. Strategic placement of the pavilion now will protect future building options.

From a financial standpoint, the long-term tax revenue from high-density housing and commercial space will far exceed the energy savings of a surface-lot solar pavilion.

A brief study to establish a full vision for this parcel will allow the City to move forward with the solar pavilion while preserving the downtown's long-term growth potential.

Plan NH -<https://plannh.org> will conduct a charrette with professional designers for about \$6k or the city can contract with a professional urban design firm for a more thorough study.

This parcel is one of our best downtown locations to build housing and commercial. Please do not squander its potential.

A handwritten signature in black ink, appearing to read 'Ken Kost' with a stylized flourish at the end.

Ken Kost

51 Railroad Street

Additional information added September 2, 2026: I planned to testify at the MSFI meeting, but now may not be available. In case I am not there, here is what I intended to speak to.

I moved to Keene six years ago. As I walked around downtown, I was impressed with how nice downtown is. When I turned the corner to Gilbo, off of Main Street, I thought— the city is not finished. It's all parking lot!

That observation led me to be a planning board member.

My first engagement concerning Gilbo was the KSC Architect School project I initiated to have students study how Gilbo could be developed based on Keene's land development code. The project demonstrated the great potential all this paved area had to support housing, commercial, parking, and civic spaces.

I've discussed the idea of doing a professional urban design study of the Gilbo parking lots with the City on several occasions.

Today, this is urgent as we are about to build a solar pavilion on 1.9 acres of city land, precluding more important development for housing.

By the numbers:

Building 300 housing units taxed at \$6,000 per unit per unit (my estimate) equals \$1,800,000 income for the city per year. Plus tax on the commercial ground floor space brings this to about \$2,500,000. This site can accommodate the 300 housing units, commercial space, and 414 parking spots in structured parking. This is one spot per unit and replacement of the about 114 surface spots there now.

The solar pavilion, more of a glorified solar car port, will generate 36,000 kWh of power per year. If electricity costs .20 per kWh, the total generation is \$7,200 per year. In terms of return on investment, the \$2.2M project will take 305 years for economic payback. Even if we just look at the city's \$440,000 investment, payback is 62 years.

This is not a good use of this property and does not make economic sense.

My ask

I urge that the city develop a small area plan that shows full development on this 1.9- acre site, including siting the solar "car port" in a place that does not destroy the real potential of this site.

To underscore the importance of this 1.9 acres, the developer Jack Franks recently said in a Keene radio interview:

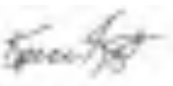
*"I believe the area (Gilbo Avenue) is one of the most underutilized portions of downtown Keene and that a combination of privately owned and **city-owned property** could potentially accommodate hundreds of new housing units.*

By my calculations with the parking that we have and the land across the street, we could probably put four or five hundred units down there, and that's going to be such a huge win for downtown Keene."

On top of all this, this lot is in Opportunity Zone 1, making the site attractive to investors.

So please accept this discussion as an actionable item, and not just for information.

Please conduct an urban design study to appropriately site the pavilion so it does not preclude future needed development.


Kent Koel
51 Railroad Street



CITY OF KEENE NEW HAMPSHIRE

ITEM #A.5.

Meeting Date: September 23, 2026

To: Municipal Services, Facilities and Infrastructure Committee

From: Bryan Ruoff, City Engineer

Through: Donald Lussier, Public Works Director
Elizabeth Ferland, City Manager

Subject: **Gilbo Ave Solar Pavilion Conceptual Design - City Engineer**

Recommendation:

Move that the Municipal Services Facilities & Infrastructure Committee recommend that Option _____ be selected as the preferred alternative and that the City Manager be authorized to do all things necessary to complete the design for the Gilbo Avenue Solar Pavilion project.

Or

Move that the Municipal Services Facilities & Infrastructure Committee recommend that the Gilbo Avenue Solar Pavilion Project be delayed, pending the outcome of a comprehensive Planning Study of the Gilbo Avenue Site.

Attachments:

None

Background:

As part of the City's Downtown Infrastructure Improvements Project, the City was awarded a federal **Timber for Transit** grant in 2024 to support the design and construction of the Gilbo Avenue Solar Pavilion. In accordance with federal funding requirements, the City publicly solicited Requests for Qualifications (RFQs) for engineering design services in September 2025 and subsequently executed an agreement with NXTGen to provide design services for the project.

Since that time, NXTGen has developed preliminary design concepts for the solar pavilion and presented them at two community engagement meetings to gather input from residents, stakeholders, and City staff. At this MSFI Committee meeting, NXTGen will present the design concepts, summarize the public and City feedback received to date, and seek additional Committee input before advancing the project into the next phase of design.



CITY OF KEENE

In the Year of Our Lord Two Thousand and Twenty Six

AN ORDINANCE Relating to the Installation of Two Stop Signs on Roxbury Street

Be it ordained by the City Council of the City of Keene, as follows:

That the City Code of the City of Keene, New Hampshire, as amended, is hereby further amended by adding the bolded underlined text to the provisions of Section 94-321, "Stop Signs" in Division 5, "Specific Street Regulations", in Article IV of Chapter 94, entitled "TRAFFIC, PARKING AND PUBLIC WAYS" as follows:

Roxbury Street for eastbound traffic at South Lincoln Street

Roxbury Street for westbound traffic at North Lincoln Street

Jay V. Kahn, Mayor

In City Council August 6, 2026.
Referred to the Municipal Services,
Facilities and Infrastructure Committee.


Assistant Clerk



CITY OF KEENE

In the Year of Our Lord Two Thousand and Twenty Six

A RESOLUTION Relating to the Alteration of Thompson Road

Resolved by the City Council of the City of Keene, as follows:

WHEREAS: Thompson Road is a public highway in the City of Keene; and

WHEREAS: There is now occasion to alter the location of Thompson Road by widening the existing Right-of-Way to accommodate the historically traveled roadway plus improvements constructed as part of the City's Thompson Road Stabilization Project;

NOW THEREFORE BE IT RESOLVED:

That the Keene City Council hereby accepts the following tracts of land, identified as "Proposed ROW Taking #1, #2 & #3" on a plan entitled "Thompson Road, Keene N.H., Right of Way Boundary Plan, Land Located along the Branch River", prepared for the City of Keene by Bailey Associates, dated 3/21/2025, to be recorded at the Cheshire County Registry of Deeds.

Tract No. 1

A certain tract of land situated in Keene, Cheshire County, New Hampshire, bounded and described as follows:

Beginning at a point on the westerly line of Thompson Road, said point being the southernmost point on Tax Map Parcel No. 241-094 and the easternmost point on Tax Map Parcel No. 241-093;

Thence N 26°12'26" E along the westerly line of said Thompson Road for a distance of 252.16 feet to a point, said point being a 5/8" diameter iron rod, set flush with the surrounding pavement;

Thence S 28°38'17" W for a distance of 34.87 feet to a point said point being a 5/8" diameter iron rod, set flush with the surrounding earth;

Thence along a curve to the left whose chord bearing is S 26°21'35"W and whose radius is 694.63 feet for a distance of 135.86 feet to a point;

Thence S 20°45'24" W for a distance of 82.83 feet to the place of beginning.

Containing one thousand seven hundred ninety-five (1,795) square feet, more or less.

Said tract is depicted as "Proposed ROW Taking #1" on the aforementioned plan.

Tract No. 2

A certain tract of land situated in Keene, Cheshire County, New Hampshire, bounded and described as follows:

Beginning at a point on the westerly line of Thompson Road, said point being a 1" diameter iron pipe set 0.6 feet above the surrounding earth, marking the southernmost point on Tax Map Parcel No. 241-095 and the easternmost point on Tax Map Parcel No. 241-094;

Thence N 44°12'34"E along the westerly line of said Thompson Road for a distance of 115.38 feet to a point;

Thence N 47°28'44" W for a distance of 11.01 feet to a point;

Thence S 39°17'58" W for a distance of 115.48 feet to the place of beginning.

Containing six hundred ninety-nine (699) square feet, more or less.

Said tract is depicted as "Proposed ROW Taking #2" on the aforementioned plan.

Tract No. 3

A certain tract of land situated in Keene, Cheshire County, New Hampshire, bounded and described as follows:

Beginning at a point on the westerly line of Thompson Road, said point being the northeastern most point on Tax Map Parcel No. 241-096;

Thence S 44°12'34" W along the westerly line of said Thompson Road for a distance of 226.92 feet to a point;

Thence N 47°28'44" W for a distance of 11.01 feet to a point;

Thence N 39°17'58" E for a distance of 29.87 feet to a point;

Thence along a curve to the right whose chord bearing is N 46°18'27" E and whose radius is 600 feet for a distance of 146.78 feet to a point;

Thence N 53°18'56" E for a distance of 51.82 feet to the place of beginning.

Containing two thousand six hundred four (2,604) square feet, more or less.

Said tract is depicted as "Proposed ROW Taking #3" on the aforementioned plan.

BE IT FURTHER RESOLVED:

That the Petitioner's request for a waiver to the requirements of Article 23.2.1 of the Land Development Code to allow conveyance of an easement for highway purposes rather than fee title is hereby granted.

BE IT FURTHER RESOLVED:

That the City Manager be, and hereby is, authorized to accept and record deeds for highway easements across the above-described tracts of land from the current property owners, with the final form and content of said easements to be subject to the review and approval of the City Attorney.

BE IT FURTHER RESOLVED:

That the City Manager be, and hereby is, authorized to pay damages to the owners of the aforementioned tracts as follows;

- a. Tract #1 – Seven Thousand Eight Hundred Forty-One Dollars and Thirty Cents (\$7,841.30)
- b. Tract #2 – One Thousand Ninety Dollars and Forty-Four Cents (\$1,090.44)
- c. Tract #3 – Four Thousand Sixty-Two Dollars and Twenty-Four Cents (\$4,062.24)

Jay V. Kahn, Mayor

In City Council August 6, 2026.
Referred to the Municipal Services,
Facilities and Infrastructure Committee and
the Planning Board. Site visit set
September 17, 2026, at 5:30 PM.
Public Hearing set September 17, 2026, at 7:00 PM.



Assistant Clerk



RETURN OF ALTERATION OF THE THOMPSON ROAD LAYOUT

A petition requesting the alteration of the Thompson Road Layout having come before the Keene City Council, and their having considered the same, all parties having been heard and the owners of land over which such highway shall pass having agreed to accept compensation and waive their right to any additional damages, it is hereby ordered and adjudged that:

- I. Public exigency and convenience require that a highway be and the same hereby is laid out, taken and appropriated over three tracts of land situated in the City of Keene, Cheshire County, New Hampshire. Said tracts are more particularly bounded and described as follows:

Tract No. 1

A certain tract of land situated in Keene, Cheshire County, New Hampshire, bounded and described as follows:

Beginning at a point on the westerly line of Thompson Road, said point being the southernmost point on Tax Map Parcel No. 241-094 and the easternmost point on Tax Map Parcel No. 241-093;

Thence N 26°12'26" E along the westerly line of said Thompson Road for a distance of 252.16 feet to a point, said point being a 5/8" diameter iron rod, set flush with the surrounding pavement;

Thence S 28°38'17" W for a distance of 34.87 feet to a point said point being a 5/8" diameter iron rod, set flush with the surrounding earth;

Thence along a curve to the left whose chord bearing is S 26°21'35"W and whose radius is 694.63 feet for a distance of 135.86 feet to a point;

Thence S 20°45'24" W for a distance of 82.83 feet to the place of beginning.

Containing one thousand seven hundred ninety five (1,795) square feet, more or less.

Said tract is depicted as "Proposed ROW Taking #1" on a plan entitled "Thompson Road, Keene N.H., Right of Way Boundary Plan, Land Located along the Branch River", prepared for the City of Keene by Bailey Associates, dated 3/21/2025, to be recorded at the Cheshire County Registry of Deeds.

Tract No. 2

A certain tract of land situated in Keene, Cheshire County, New Hampshire, bounded and described as follows:

Beginning at a point on the westerly line of Thompson Road, said point being a 1" diameter iron pipe set 0.6 feet above the surrounding earth, marking the southernmost point on Tax Map Parcel No. 241-095 and the easternmost point on Tax Map Parcel No. 241-094;

Thence N 44°12'34"E along the westerly line of said Thompson Road for a distance of 115.38 feet to a point;

Thence N 47°28'44" W for a distance of 11.01 feet to a point;

Thence S 39°17'58" W for a distance of 115.48 feet to the place of beginning.

Containing six hundred ninety-nine (699) square feet, more or less.

Said tract is depicted as "Proposed ROW Taking #2" on the aforementioned plan.

Tract No. 3

A certain tract of land situated in Keene, Cheshire County, New Hampshire, bounded and described as follows:

Beginning at a point on the westerly line of Thompson Road, said point being the northeastern most point on Tax Map Parcel No. 241-096;

Thence S 44°12'34" W along the westerly line of said Thompson Road for a distance of 226.92 feet to a point;

Thence N 47°28'44" W for a distance of 11.01 feet to a point;

Thence N 39°17'58" E for a distance of 29.87 feet to a point;

Thence along a curve to the right whose chord bearing is N 46°18'27" E and whose radius is 600 feet for a distance of 146.78 feet to a point;

Thence N 53°18'56" E for a distance of 51.82 feet to the place of beginning.

Containing two thousand six hundred four (2,604) square feet, more or less.

Said tract is depicted as "Proposed ROW Taking #3" on the aforementioned plan.

- II. The City Council finds that there is occasion and necessity for such layout.
- III. The tracts of land hereby laid out shall be added to and become part of the Thompson Road Right-of-Way.
- IV. These findings and orders shall be recorded in the office of the Keene City Clerk.

Jay V. Kahn, Mayor

Date

Approved as to Description:

Approved as to Form:

Bryan Ruoff, P.E.
City Engineer

Amanda Palmiera
City Attorney

SURVEY DATUM

THIS SURVEY IS ORIENTED TO N.H. STATE PLANE GRID NORTH AS DETERMINED BY UTILIZING BAD ELF "FLEX 5600" DUAL FREQUENCY GNSS (GPS, GLONASS, GALELIO & BEIDOU) RECEIVERS, RTK, VIRTUAL NETWORK METHODS. OBSERVATIONS WERE TAKEN ON 11/21/2022
VERTICAL DATUM: NAVD 88 UTILIZING (GEOID 18).
HORIZONTAL DATUM: NAD83(2011) MULTI-YEAR CORS SOLUTION 2 (MYCS2) EPOCH = 2010.000
BEGIN DATE = 01/04/2020
REFERENCE FRAME: ITRF2014

TAX MAP 241 LOT 93
642 MARLBORO RD.
KAREN B. CURRAN
BK.969 PG.152

TAX MAP 241 LOT 94
17 THOMPSON RD.
GREGORY C. WALKER
BK.840 PG.522

TAX MAP 241 LOT 95
27 THOMPSON RD.
ROBERT W. BALL
BK.1155 PG.126

TAX MAP 241 LOT 96
OTHER LAND OF BALL
BK.1810 PG.805

TAX MAP 241 LOT 99
NORMAN C. KRISTOFF
BK.2646 PG.20

TAX MAP 241 LOT 100
HEIRS OF GEORGE A. LAROCHE
BK.1775 PG.159

TAX MAP 241 LOT 101
59 THOMPSON ROAD
MICHAEL R. LAROCHE ET AL
BK.1775 PG.159

TAX MAP 241 LOT 105
ANTIOCH UNIVERSITY
BK.2190 PG.411

THOMPSON ROAD RIGHT OF WAY TAKINGS

PROPOSED RIGHT-OF-WAY
TAKING #1
FOR TAX MAP 241 LOT 94
(17 THOMPSON ROAD)

ROW PARCEL SEGMENTS TABLE			
ID	Length	Bearing	Curve Radius
C1	135.86	Chrd Brg:S26°21'35"W	694.63
L1	82.83	S20°45'24"W	
L2	252.16	N26°12'26"E	
L3	34.87	S38°38'17"W	

ROW TAKING #1: 1,795 S.F.

PROPOSED RIGHT-OF-WAY
TAKING #2
FOR TAX MAP 241 LOT 95
(27 THOMPSON ROAD)

ROW PARCEL SEGMENTS TABLE	
ID	Bearing
L4	S39°17'58"W
L5	N44°12'34"E
L6	N47°28'44"W

ROW TAKING #2: 699 S.F.

PROPOSED RIGHT-OF-WAY
TAKING #3
FOR TAX MAP 241 LOT 96
(27 THOMPSON ROAD)

ROW PARCEL SEGMENTS TABLE			
ID	Length	Bearing	Curve Radius
C10	146.78	Chrd Brg:N46°18'27"E	600.00
L6	11.01	N47°28'44"W	
L7	29.87	N39°17'58"E	
L8	51.82	N53°18'56"E	
L9	226.92	S44°12'34"W	

ROW TAKING #3: 2,604 S.F.

MULTIPLE SHEET RECORDABLE ROW PLAN

THIS PLANSET CONTAINS A TOTAL OF 8 SHEETS. SHEET NUMBER 8 IS RECORDED AT THE CHESHIRE COUNTY REGISTRY OF DEEDS. THE REMAINING SHEETS ARE ON FILE AT THE CITY OF KEENE DEPARTMENT OF PUBLIC WORKS, WHICH IN ITS ENTIRETY, CONSTITUTE THE SITE PLAN AS APPROVED BY THE PLANNING BOARD.

PT: 85+33.80

GENERAL NOTES

- THE PURPOSE OF THIS PLAN IS TO SHOW THE RIGHT OF WAY OF THOMPSON ROAD, ACCORDING TO CITY OF KEENE HIGHWAY BOOK #1, RETURN OF LAYOUT AS DESCRIBED AS BEING 2 RODS IN WIDTH, 100 RODS IN LENGTH, AND DESCRIBED BY NON-CURVILINEAR POINTS OF INTERSECTION AND DISTANCES ONLY. THE ORIGINAL RIGHT OF WAY IS SHOWN AS A CENTERLINE USING RODS AS THE UNIT LENGTH.
- IN ADDITION TO THE EXISTING RIGHT OF WAY, A NUMBER OF NEW RIGHT-OF-WAY TAKINGS ARE NEEDED TO FACILITATE ON GOING ROAD MAINTENANCE. THIS PLAN SHOWS THE PROPOSED PARCELS TO BE ANNEXED TO THE EXISTING RIGHT OF WAY.
- THERE ARE TWO APPARENT LOCATIONS WHERE THE HIGHWAY AS TRAVELED IS LOCATED OUTSIDE OF THE ORIGINAL RIGHT OF WAY, AND PRESCRIPTIVE EASEMENTS MAY HAVE RIPPENED OVER TIME.
- AN ASBUILT SURVEY AND RIGHT-OF-WAY BOUNDARY SURVEY WAS PERFORMED BY THIS OFFICE ON 12/12/2024. THE AS-BUILT LOCATIONS OF THE RECENTLY CONSTRUCTED RETAINING WALLS AND ASSOCIATED SLOPES WITH DRAINAGE WAS LOCATED, AND THE PROPOSED TAKINGS SHOWN ON THIS PLAT WERE MONUMENTED.

PLAN REFERENCES

- BOUNDARY ADJUSTMENT PREPARED FOR KAREN B. CURRAN, KEENE NH AS PREPARED BY DAVID A. MANN ASSOCIATES, DATED 01/03/1987 AND RECORDED AS CCRD PLAN 521.
- BOUNDARY LINE ADJUSTMENT LOTS 241-102-000, 101, 103 & 104, THOMPSON ROAD KEENE, NH 03431 AS PREPARED BY CARDINAL SURVEYING & LAND PLANNING, DATED 05/22/2023 AND RECORDED AS CCRD PLAN #23049.
- EXISTING CONDITIONS PLAN, BULK STORAGE FACILITY, TAX MAP PARCEL 68-1-1, 639 MARLBORO ROAD, KEENE, NH. AS PREPARED BY FIELDSTONE LAND CONSULTANTS, DATED MARCH 16, 2017 AND ON FILE AT THE CITY OF KEENE.
- BOUNDARY ADJUSTMENT PLAN PREPARED FOR JEANNETTE L. GARDNER & GREGORY H. GARDNER 67 THOMPSON ROAD, AS PREPARED BY DAVID A. MANN, LAND SURVEYOR DATED MARCH 27, 2007 AND RECORDED AS CCRD PLAN #C13-D06-0085
- SUBDIVISION PLAT, MAP 199 LOT 01-1, MAP 901 LOTS 21-6 & 21-28, CHAPMAN AND THOMPSON ROADS KEENE, N.H. AS PREPARED BY RICHARD P. DREW LLC DATED 08/13/2003 AND ON FILE AT THE CITY OF KEENE.
- STATE OF NEW HAMPSHIRE, STATE HIGHWAY DEPARTMENT - PLAN AND PROFILE OF PROPOSED T.L.R. PROJECT NO. 14117, SOUTH SIDE ROAD, DATED 05/31/1993. ON FILE AT THE NH DOT ARCHIVES.

PLAN CERTIFICATIONS

I HEREBY CERTIFY THAT THIS SURVEY AND SURVEY PLAT WAS PREPARED BY ME OR THOSE UNDER MY DIRECT SUPERVISION.

I CERTIFY THAT THIS SURVEY PLAT IS NOT A SUBDIVISION PURSUANT TO THIS TITLE AND THAT THE LINES OF STREETS AND WAYS SHOWN ARE THOSE OF PUBLIC OR PRIVATE STREETS OR WAYS ALREADY ESTABLISHED AND THAT NO NEW WAYS ARE SHOWN.

I HEREBY CERTIFY THAT I HAVE FILED A COPY OF THIS PLAN WITH THE PLANNING BOARD OF KEENE, IN ACCORDANCE WITH RSA 676:18 (IV)

I HEREBY CERTIFY THAT THIS PLAN IS A TRUE AND ACCURATE REPRESENTATION OF A THEODOLITE / EDM SURVEY PERFORMED ON THE GROUND AND UNDER MY DIRECT SUPERVISION DURING 2022 & 2024, AND THAT THE ADJUSTED ERROR OF CLOSURE IS NOT GREATER THAN 1 PART IN 10,000. EVERY REASONABLE EFFORT HAS BEEN MADE TO PROPERLY ESTABLISH THE BOUNDARIES OF THE SUBJECT TRACT. THIS CERTIFICATION DOES NOT INCLUDE ANY GUARANTEE OF TITLE.

03/21/2025

DATE:

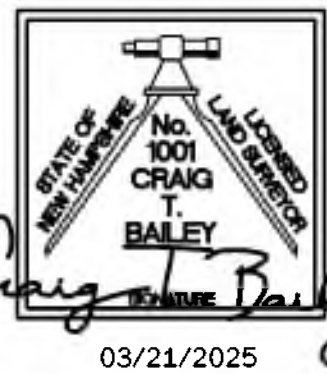
CRAIG T. BAILEY L.L.S. #1104

INSET "A"
SCALE: 1"=50'

TAX MAP 241 LOT 102
67 THOMPSON ROAD
JESSICA & FRANCIS PIERANNUNZI
BK.2624 PG.361

TAX MAP 241 LOT 103
60 THOMPSON ROAD
JEFFREY & ALLISON GARDNER
BK.1962 PG.474

DATE	REVISION	INITIALS
12/30/2024	ASBUILT & PROPOSED EASEMENT ADDED	CTB
03/07/2025	ROW TAKINGS PARCELS REVISED	CTB
03/21/2025	PARCEL TABLES RE-ALIGNED	CTB
DATE 3/21/2025 JOB No. 3673		
DRAWN BY :CTB		
SCALE : 1"=50'		



LEGEND

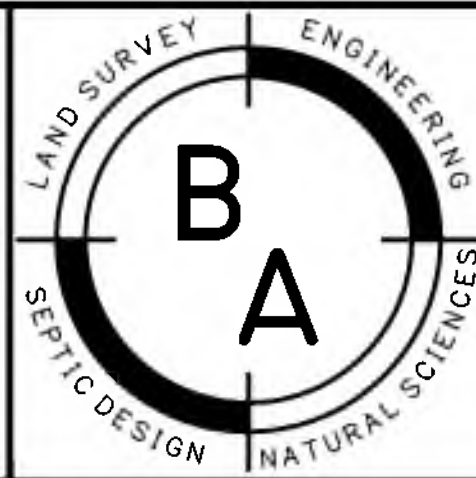
- REBAR FND/SET
- REBAR TO BE SET
- DRILL HOLE FND/SET
- CONCRETE BND.FND/SET
- CONC. BND. TO BE SET
- GRANITE BND. FND/SET
- STONE ERECT FOUND
- STAKE & STONES FND.
- IRON ROD FOUND
- IRON PIPE FOUND
- SPIKE FND/SET
- MAGNAIL (PK) FND/SET
- HUB & TACK FND/SET

- TEMPORARY BENCHMARK
- PERMANENT BENCHMARK
- WETLAND FLAG
- WETLAND
- TREE STUMP FOUND
- MAIL BOX
- PERCOLATION TEST
- TEST PIT
- DECIDUOUS TREE
- CONIFEROUS TREE
- SOIL TYPE
- EDGE OF GRAVEL DRIVE

- SEWER MAN HOLE
- DRAIN MAN HOLE
- WELL
- HYDRANT
- WATER SHUTOFF
- GATE VALVE
- CATCH BASIN
- UTILITY POLE
- LIGHT POLE
- GUY ANCHOR

STONEWALL

- TREELINE
- WETLAND
- EXISTING WATER
- EXISTING SEWER LINE
- EXISTING STORM DRAIN
- BUILDING SETBACK LINE



BAILEY ASSOCIATES

LAND SURVEYING & LAND PLANNING
ENGINEERING & NATURAL RESOURCE SCIENCES
WWW.BAILEY-ASSOCIATES.COM
(603) 528-3734
217 COTTON HILL RD. GILFORD, NH 03249
SERVING NEW HAMPSHIRE SINCE 1978

THOMPSON ROAD, KEENE N.H. RIGHT OF WAY BOUNDARY PLAN

LAND LOCATED ALONG THE BRANCH RIVER
CITY OF KEENE, CHESHIRE COUNTY N.H.
PROJECT OWNER:
CITY OF KEENE, N.H.
DEPARTMENT OF PUBLIC WORKS
3 WASHINGTON ST.
KEENE, NH 03431
DATE OF PLAN: March 2025

SHEET 4 OF 4



Roadway Safety Action Plan Update

2025 Annual Progress Update

September 23, 2026 | MSFI Committee

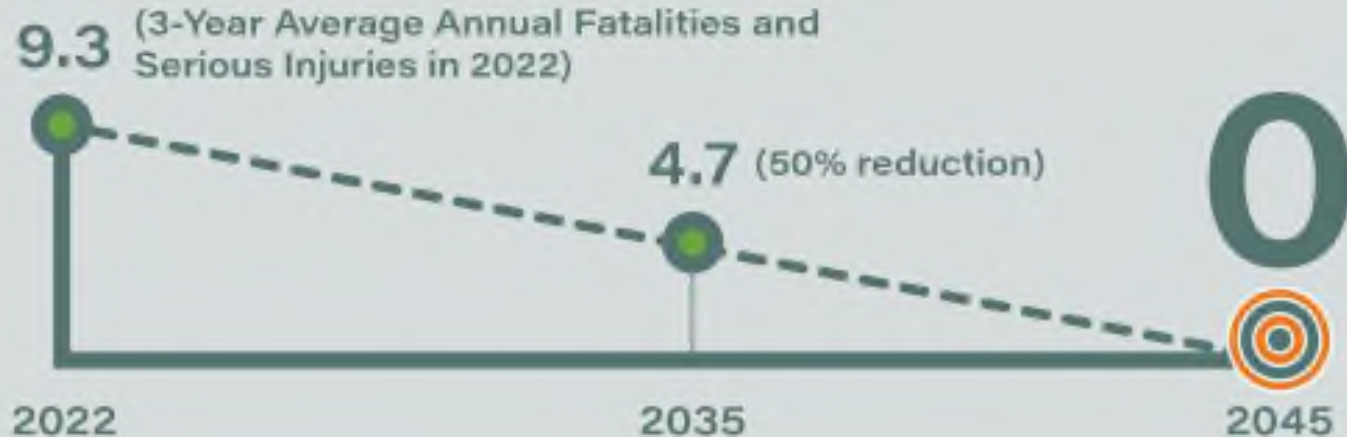
Agenda

- **Outcomes**: Annual fatalities and serious injuries
- **Actions**: What have we done?
- **Looking Ahead**: Planned actions, funding opportunities, MSFI & Public feedback



Safety Action Plan Goal

The goal of the Keene Roadway Safety Action Plan is to make transportation safety a top priority for City officials and staff. The Mayor and City Council have set a goal for this Plan to reduce the number of fatalities and serious injuries by 50% by 2035, working toward zero by 2045.





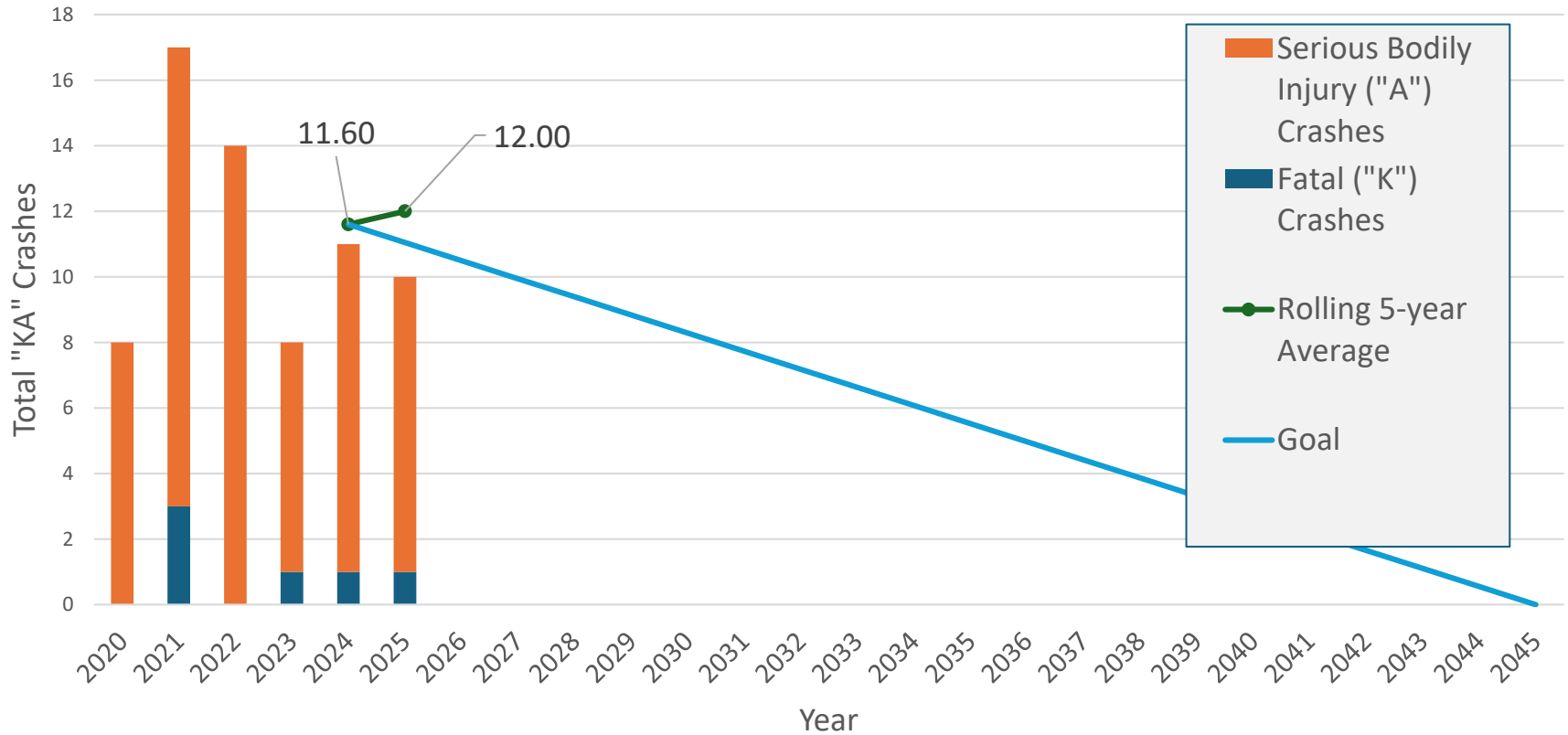
1. Outcomes

What did we see in 2025

Crash Data

<u>Year</u>	<u>Fatal ("K") Crashes</u>	<u>Serious Bodily Injury ("A") Crashes</u>	<u>Total "KA" Crashes</u>	<u>Rolling 5-year Average</u>	<u>Goal</u>
2020	0	8	8		
2021	3	14	17		
2022	0	14	14		
2023	1	7	8		
2024	1	10	11	11.60	
2025	1	9	10	12.00	11.05

Crash Data



Emphasis Area Observations

VRU-Motorized

- Electric micromobility devices
 - E-bikes
 - E-scooters
 - Electric personal assistive mobility devices

VRU-Non-motorized

- Keene's only fatality in 2026 was a pedestrian struck on Rt. 101





2. Actions

What the City accomplished

“Highest Priority” Work Completed

Priority No.	Location	Action Taken
3	Island St.	Narrowed travel lanes
8	Roxbury St.	Asphalt Sidewalk Overlay
15	Washington St. @ Spring	Resolved pick-up traffic
17	Water St. @ Eastern Ave.	Advanced Warning for Westbound traffic
19	Water St. @ Grove	Added lighting
24	Winchester St. @ Island	Correct Confusing Crosswalk Signs
25	Winchester St. @ Foundry	Installed RRFB's



Implementation: Completed Work

	Type	Location	Description	Score
	Intersection	Grove / Water	Convert intersection to All-Way Stop	66
★	Signage	Winchester / Island	Correct confusing Crosswalk Signs	59
★	Pedestrian	Roxbury St	Asphalt Sidewalk Overlay	57
★	Pedestrian	West / Pearl	Install RRFB and Crosswalk	55
	Signage	Water / Eastern	Advanced warnings for Westbound traffic	54
★	Intersection	Water / Grove	Improve intersection lighting	53
★	Pedestrian	Washington / Spring	Modified School pick-up / drop-off to eliminate conflicts	51
★	Traffic Calming	Island St	Narrowed lanes and widened shoulders to slow traffic	51

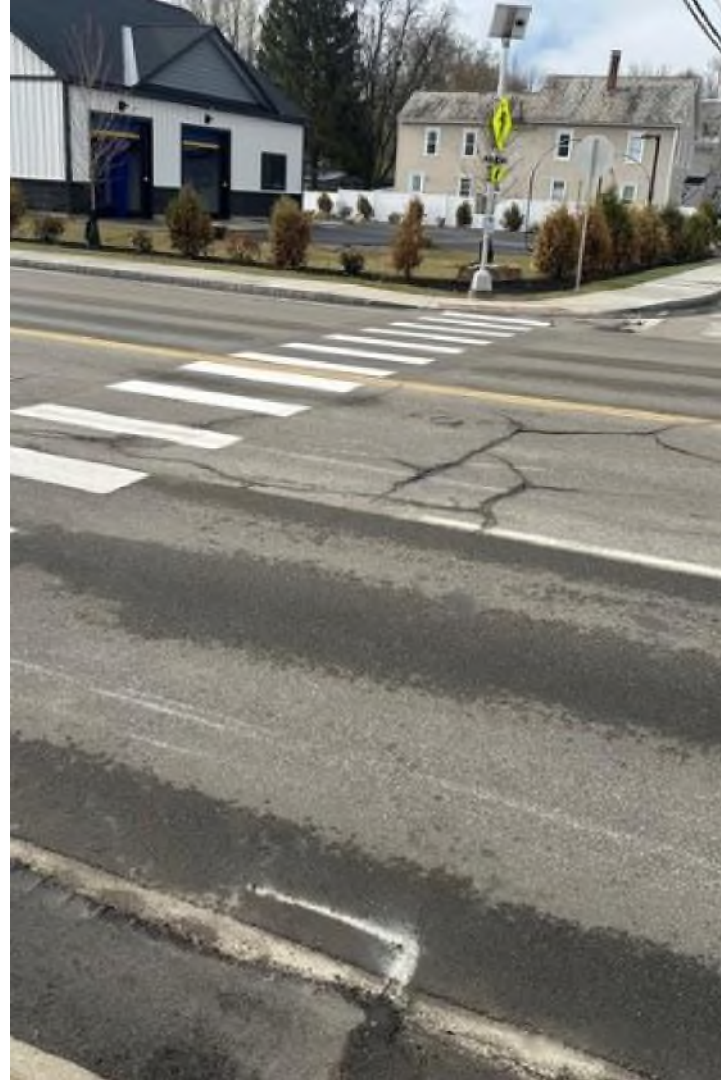
Implementation: Completed Work



Type	Location	Description	Score
Markings	Rt. 101 / Winchester	Requested NHDOT repaint roundabout	44
Crosswalk	Water St. / CRT	Install RRFB at trail crossing	38
Intersections	School / Winter Forest / Chestnut	Installed new stop control	34
Intersection	Rt. 9 / Whitcomb's Mill	Completed Road Safety Audit	34
Bike	Maple Ave	Restriped to widen bike shoulder	30
Signage	Arch St. Chesterfield Rd.	Advanced warning signs approaching the tunnel	28
Vegetation	Various	Clearing sight lines and signage	N/A

Implementation: Underway

- Downtown Infrastructure Project
 - Crosswalk enhancements
 - Lighting
 - Bike lanes
- Marlboro Street
 - New / Enhanced Crosswalks
 - Traffic Calming
 - Bike Lanes



Implementation: Underway

- THT Trailhead
 - New Crosswalk
 - Parking Improvements
- Royal Ave
 - RRFB Installed





3. Looking Ahead

2026 and beyond

Planned Projects



6 Roxbury / Lincoln Intersection

Reconstruction of Rte. 101

Reconstruction of Lower Winchester Street

Reconstruction of Rte. 9/10/12 “T” Intersection

Prowse Bridge over Rt. 101

Reconstruction of West Street



Funding Opportunities

Transportation Alternatives Program

- Project Solicitation in 2027

Highway Safety Improvement Program

- Applications in Fall of 2026
- Possible funding for Rt. 9 / Whitcombs Mill

Congestion Mitigation & Air Quality

- Project Solicitation in 2026
- Possible Funding for Rt. 12 / Rt. 32





Discussion and Feedback